



DATE: July 14, 2022

File: UP 19-25  
AR 19-22

TO: PLANNING COMMISSION

FROM: Kelly Murphy, Senior Planner (879-6535; [kelly.murphy@chicoca.gov](mailto:kelly.murphy@chicoca.gov))  
Bruce Ambo, Principal Planner (879-6801; [bruce.ambo@chicoca.gov](mailto:bruce.ambo@chicoca.gov))

RE: Use Permit 19-25 and Architectural Review 19-22 (TownePlace Suites Hotel);  
Proposed at Bruce Road, Deer Creek Highway (State Route 32) and Sierra Sunrise  
Terrace, APN 018-230-001.

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## SUMMARY

The project proposes to construct a four-story, 112 room hotel building on a vacant 4.09 acre site located south of Sierra Sunrise Terrace and northeast of the intersection of Deer Creek Highway (SR 32) and Bruce Road. The site is designated Commercial Mixed Use on the General Plan Land Use Diagram and zoned CC (Community Commercial). Pursuant to Chico Municipal Code (CMC) Section 19.44.020, Table 4-6, a hotel use may be established in the CC zoning district subject to use permit approval.

An associated application for architectural review (AR 19-22) has been submitted for the project and will be reviewed at an upcoming meeting of the Architectural Review and Historic Preservation Board (ARHPB). The project has been determined to be categorically exempt from further environmental review pursuant to California Environmental Quality Act (CEQA) Guidelines Section 15332 (Infill Development Projects).

In accordance with CMC Section 19.24.040, following a public hearing the Planning Commission may approve a use permit application, with or without conditions, only if all the following findings can be made:

- A. The proposed use is allowed within the subject zoning district and complies with all of the applicable provisions of Chapter 19.24 (Use Permits);
- B. The proposed use would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed use;
- C. The proposed use would not be detrimental and/or injurious to property and improvements in the neighborhood of the proposed use, as well as the general welfare of the City;
- D. The proposed entitlement is consistent with the General Plan, any applicable specific plan, and any applicable neighborhood or area plan;
- E. The design, location, size, and operating characteristics of the proposed use are compatible with the existing and future land uses in the vicinity.

Should the Planning Commission concur with the recommendation provided by staff and find the project to be consistent with the use permit findings, a motion to approve the project has been prepared and included below.

Recommendation:

Planning staff recommends adoption of Resolution No. 22-04 (**Attachment A**), approving Use Permit application 19-25 and Architectural Review application 19-22 for the property identified as APN 018-230-001, subject to the recommended conditions herein.

Proposed Motion:

I move that the Planning Commission adopt Resolution No. 22-04 (**Attachment A**), approving Use Permit application 19-25 and Architectural Review application 19-22 for the property identified as APN 018-230-001, subject to the recommended conditions herein.

Alternatively, if the Planning Commission does not align with the staff recommendation and determines that the project does not meet one or more of the required findings A through E, the Planning Commission may impose modifications to the project or deny the project.

Pursuant to CMC Chapter 19.12 (Appeals), decisions on use permits are appealable directly to the City Council.

## **BACKGROUND**

The proposed project is comprised of a new four-story, 112-room hotel building on a 4.09 acre site located south of Sierra Sunrise Terrace and northeast of the intersection of Deer Creek Highway (SR 32) and Bruce Road (see **Attachment B**, Location Map and **Attachment C**, Site Plan). The project site is designated Commercial Mixed Use on the General Plan Land Use Diagram and zoned CC (Community Commercial). Pursuant to CMC Section 19.44.020, Table 4-6, a hotel may be allowed in the CC zoning district subject to use permit approval.

The project parcel is Lot 59 of the Sierra Sunrise Village Unit 2 Subdivision Map for California Park recorded in 1986 (see **Attachment D**, Recorded Map). The subject parcel has historically been designated and zoned for commercial development. The City's General Plan land use diagram and zoning maps reflect a designation of Neighborhood Commercial (NC) as far back as 1976; maps post-1986 reflect the current zoning of Community Commercial (CC).

The parcel directly north of the site is designated Primary Open Space and zoned OS1 (Primary Open Space). Adjacent parcels to the south, east and west are designated Commercial Mixed Use and zoned CC (Community Commercial); beyond these commercial properties, Medium and Medium-High Density Residential zoning is present. Low Density Residential properties of the California Park Subdivision extend further north of the site.

In the context of the greater area, substantial new growth and development is projected along the south side of Deer Creek Highway (SR 32) east of Bruce Road, primarily characterized by single-family residential, multi-family residential and affordable housing developments (see **Attachment E**, Development Activity Vicinity Map). For example, 1,324 residential units have been approved within the Oak Valley Master Plan area, which at this time consists of 630 low density residential units, 302 medium density residential units, and 392 high density residential units. Projects currently under development within this master plan area include Lava Ridge



Apartments, a three-story multi-family housing project containing 98 units; Deer Creek Apartments, a three-story affordable housing project containing 204 units; and Senator Conness Apartments, a three-story affordable housing project containing 162 units.

Additionally, building permits have been issued for the development of a gas station, convenience store and drive through car wash facility on the commercial parcel at the southeast corner of the intersection at Bruce Road and Deer Creek Highway (SR 32), immediately south of the project site. Further, Meriam Park continues to build out and development has begun on the north side of Little Chico Creek.

## **DISCUSSION AND ANALYSIS**

The proposal consists of a new commercial hotel building with a footprint of approximately 16,655 square feet. Other site features would include a covered passenger loading/unloading zone near the building entrance, outdoor swimming pool area, commercial trash enclosure, vehicle and bicycle parking, and landscaping (see **Attachment F**, Patio and Pool Plans and **Attachment G**, Building Elevations). The proposed building would be situated at the center of the parcel oriented towards Sierra Sunrise Terrace, with parking offered around the building. Large, shaded seating areas would be provided for the use of visitors and employees at the building's main entrance (see **Attachment H**, Renderings). All new utility connections would be undergrounded. A covered trash enclosure structure would be located at the rear of the building and utilize the same materials and complementary design (see **Attachment I**, Site Details).

The CC zoning district allows for building coverage up to 95 percent of the total site area, requiring at least 5 percent be landscaped. The project's total building coverage would represent approximately 10 percent of the site (16,655 square feet), while 57 percent (102,241 square feet) of the site would be landscaped. Pursuant to CMC Section 19.44.030, no setbacks are required for the proposed hotel structure. The hotel building would be setback 157 feet from Bruce Road, 89 feet from Sierra Sunrise Terrace, 80 feet from Deer Creek Highway (SR 32), and 344 feet from the eastern property line.

### Creekside Development Standards

Dead Horse Slough forms the western and southern property boundary lines and serves as an overflow to California Park Lake. Pursuant to CMC Section 19.60.030, property adjoining Little Chico Creek and its tributaries, including Dead Horse Slough, are subject to the City's creekside development standards.

In compliance with these standards, the project has been designed to accommodate a minimum 30 foot setback from the top of bank (greater than the minimum 25 feet); no structure, parking access, parking space, paved area, or swimming pool is proposed within a creek or creekside setback; no grading, filling, planting of non-native or non-riparian plant species, or removal of native vegetation is proposed within a creekside setback; and, where constructed drainage devices and improvements are required, they would be placed in the least visible locations and naturalized through the use of river rock, earth-tone concrete, and native landscaping. Stormwater from the site will be screened by the proposed landscaping to the south and a proposed bioswale to the west before being directed to an existing concrete storm drain system along Deer Creek Highway (SR 32).

### Site Access

Consistent with CMC Section 19.70.070(A), vehicle access is proposed from the lesser-traveled street on the subject corner parcel. Access to the project site would be via Sierra Sunrise Terrace, a two-lane collector road which extends westerly to Bruce Road (a four-lane arterial roadway). Two 30-foot driveway entrances would be provided from Sierra Sunrise Terrace, narrowing to 24-foot drive aisles around the building.

Sierra Sunrise Terrace is a private road dedicated for use by all lots of the Sierra Sunrise Village Subdivision, which includes the project site.

### Architectural Review

In addition to deciding on the proposed land use, staff is requesting that the Planning Commission consider the associated architectural review application, specifically as it relates to the four-story building volume and total building height. This approach is intended to help inform one of the more controversial issues of the project of whether a four-story hotel should exist at this site prior to the Architectural Review and Historic Preservation Board's review of architectural details and design elements.

The maximum height for main buildings in the CC zoning district is 57 feet. Per CMC Section 19.60.070.E, parapet walls are permitted height projections that may extend up to 6 feet above the height limit of the structure. The four-story building would have a height of predominately 49 feet, 3 inches, with tower and parapet elements extending to 59 feet, 3 inches (or approximately 2-feet, 3 inches above the base height allowance of 57 feet into the 6-foot permitted height projection), consistent with building height requirements.

Should the Planning Commission vote to approve the proposed use permit for a hotel use in the CC zoning district with a four-story building volume, a condition is recommended to have the Architectural Review and Historic Preservation Board review the site design and architecture for the proposed building, including surrounding site landscaping, lighting, and parking improvements, and take action on the project's architectural review application (AR 19-22) at their meeting on July 27, 2022 (see Condition #4 on **Attachment A**, Exhibit I).

Alternatively, the Planning Commission may impose modifications to the project to reduce the building volume, such as requiring a lesser building height, in the event the Planning Commission determines that such modifications are necessary to make the required findings for Use Permit approval.

### Parking and Landscaping

Pursuant to CMC Section 19.70.040, hotels shall provide a minimum of one off-street vehicle parking space per guest room. The applicant proposes to provide 117 off-street spaces and 20 bicycle spaces, exceeding parking requirements for the 112-bedroom hotel. Bicycle parking would be located near the building entrance and separated from vehicle parking areas, consistent with CMC Section 19.70.080.B.2(c) and (d).

Per the requirements for the CC zoning district, a minimum of 5 percent of the site shall be landscaped. The project proposes to landscape approximately 57 percent of the site. Landscaping features are proposed around the new building and site perimeter. Flowering trees and plantings would accentuate the site and building entrance. Screening of the trash

enclosure would be achieved with evergreen shrubs and vines. All utilities would be screened from view by evergreen plantings.

A minimum landscape buffer of 10 feet would be provided between the parking areas and the adjacent streets, in compliance with CMC Sections 19.70.060.B(3) and 19.70.060.E.(3)(a). Planting of three-foot-high evergreen flowering shrubs in this buffer area would screen vehicle parking from public views along the street frontages, addressing CMC Section 19.70.060.B(1). To demonstrate the effectiveness of this screening, the applicant has prepared visual simulations at the pedestrian scale from five different viewpoints around the project site (see **Attachment J**, Pedestrian Scale Viewpoints).

Plant species would be of low to moderate water demands. A total of 69 trees are proposed throughout the site; the 59 trees intended to shade the parking areas would consist of Chinese Pistache, Chinese Elm, Holly Oak and Japanese Zelkova species, while a variety of oak, crape myrtle and arbutus trees would be planted to further ornament the site (see **Attachment K**, Landscape Plan). Parking lot shading is projected to reach 78 percent at maturity, exceeding the City’s requirements.

Traffic and Signal Warrant Analysis

Potential impacts to traffic and circulation as a result of project development were analyzed in a traffic study performed for the project, which concluded that the project would create a less-than-significant impact to Vehicle Miles Travelled (VMT). As shown in Table 1 below (excerpted from the study), traffic data confirms that a hotel use generates significantly less traffic as compared to other commercial uses, such as retail, office, and restaurant uses, which are permitted by right in the CC zoning district.

Table 1: Trip Generation Comparison to Permitted Land Uses

| Land Use   | ITE Code   | Quantity   | Daily | AM    |     |     | PM    |     |     |
|--|--|--|-------|-------|-----|-----|-------|-----|-----|
|  |  |  |       | Total | In  | Out | Total | In  | Out |
| Hotel  | All-Suites<br>Hotel (311)  | 112 Rooms  | 500   | 38    | 20  | 18  | 40    | 19  | 21  |
| Medical Office   | Medical- Dental<br>Office<br>(720)                               | 60,000 sq ft   | 2,088 | 167   | 130 | 37  | 208   | 58  | 150 |
| Restaurant/<br>Retail<br>Mix   | Fast Food w/<br>Drive-thru (934)<br>and Shopping<br>Center (820) | 5,000 sq ft<br>Restaurant<br>and<br>15,000 sq ft<br>Retail | 2,921 | 215   | 111 | 104 | 220   | 112 | 108 |
| Note: Trip rates obtained from the ITE Trip Generation Manual, 10th Edition. |  |  |       |       |     |     |       |     |     |

The signal warrant analysis suggests there will be a future need for a traffic signal at the intersection of Bruce Road and Sierra Sunrise Terrace if application is made for development on the west side of Bruce Road, which is not within the scope of this project. However,

community population and development growth are known to increase traffic over time. After reviewing the traffic and signal warrant analyses, recent collision data, and the Development Impact Fee Program, Traffic and Development Engineering staff recommend installing a traffic signal at this intersection to improve intersection safety. The developer is supportive of the safety improvement and has agreed to install a traffic signal as part of the project. The cost of the traffic signal is estimated to be approximately \$375,000. The City will reimburse up to \$280,000 for the traffic signal from the Development Impact Fee Program budget, and the remaining cost shall be covered at the developer's expense.

#### Consistency with General Plan Designation and Zoning

The General Plan land use designation for the proposed project site is Commercial Mixed Use. This designation encourages the integration of retail and service commercial uses with office and/or residential uses. This designation may also allow hospitals and other public/quasi-public uses. Pursuant to Chico Municipal Code (CMC) Section 19.44.020, Table 4-6, a "hotel or motel" is a service use that may be established in the CC zoning district subject to use permit approval, which can be found consistent with the Commercial Mixed Use designation.

The CC zoning district is applied to areas appropriate for a wide range of retail businesses. This district also accommodates mixed-use developments with residential uses above the ground floor. For example, the following land uses are principally permitted in the CC zoning district:

|                                    |   |
|------------------------------------|---|
| Alcoholic beverage establishment   | Multi-family housing                      |
| Banks and credit unions (any size) | Offices, business and professional        |
| Churches                           | Personal services                         |
| Clinics and labs                   | Pet shops                                 |
| Drug stores/pharmacies             | Restaurants of any size                   |
| Grocery stores of any size         | Retail stores of any size                 |
| Health/fitness clubs               | Shopping centers (200,000 sf. Or larger)  |
| Indoor Amusement/Entertainment     | Storage                                   |
| Liquor stores                      | Temporary Emergency shelters              |
| Medical offices                    | Transitional and supportive housing       |
| Mortuary/funeral home              | Vet clinics and animal hospitals (indoor) |

"The Chico 2030 General Plan is a statement of community priorities to guide public decision-making. Given the broad scope of the General Plan, not all goals and policies are obviously complementary, and yet they all support the overarching vision for the City. When making decisions, goals and policies should be examined comprehensively, not individually. It is not the intent of the General Plan to predetermine decisions, but rather to help guide the decision-making process" (Page 1-1, Introduction, Chico General Plan).

The proposal is consistent with various policies and actions under Land Use (LU) Goals LU-2, LU-3 and LU-4; Community Design (CD) Goals CD-1 and CD-5; Economic Development (ED) Goals ED-2; Open Space (OS) Goal OS-2; and Circulation (CIRC) Goal CIRC-1.

Specifically, the project would be consistent with General Plan goals and policies to enhance regional tourism opportunities and resources to attract visitors who support the local businesses and encourage the development of additional recreation and tourism businesses

and industries, such as hotels (Goal ED-2; Policy ED-2.1 and ED-2.2). The proposed hotel would be subject to a TOT (Transient Occupancy Tax) of 10 percent of the fees charged by the operator, generating revenue for the City's general fund.

The project would develop a commercially zoned property consistent with a land use plan that provides a mix and distribution of uses that meet the identified needs of the community (Goal LU-2). In recent years, hotel occupancy rates have been high, and the development of a new hotel would meet the anticipated need for more short-term stay uses. A majority of hotel uses are located adjacent to Highway 99, accommodating traffic and visitors traveling to or from regions to the north or south. The location chosen for the proposed project currently represents one of the only hotel sites in east Chico north of Deer Creek Highway (SR 32). The hotel site would be conveniently located adjacent to another key transportation corridor and accommodate traffic and visitors traveling east to popular recreation destinations such as Lake Almanor. The project site is also proximal to medical facilities off Bruce Road which provide outpatient services for Enloe Hospital including an urgent care and surgery center, making this an appropriate and desirable location for travelling healthcare and medical professionals. The proposed hotel would also be located near The Terraces, a senior housing development offering both independent and assisted living facilities, making it a sensible option for relatives of patients and residents in the senior living community.

Consistent with Policy LU-2.4 to "promote land use compatibility through use restrictions, development standards, environmental review and special design considerations", the project is consistent with the purpose of the CMU designation and CC zoning district and meets all applicable development standards. Special design considerations were made to minimize visual impacts, such as providing large building setbacks and extensive landscaping around the site. Additionally, the project has been designed in compliance with creekside development standards and would provide a minimum 30-foot setback from the top of bank to any development or site improvement (Policy OS-2.5, Action OS-2.5.1). Most of the site would consist of landscaped area, and the landscape plan would incorporate new plantings as well as enhance the existing natural environment (Goal CD-1, Action CD-1.1.2).

Development of the project would result in traffic and circulation improvements, including the future installation of a traffic signal at the intersection of Bruce Road and Sierra Sunrise Terrace (Goal CIRC-1, Policy CIRC-1.1, Action CIRC-1.1.1, Policy CIRC-1.2). A Vehicle Miles Travelled (VMT) Analysis was prepared as part of the environmental review process (Policy CIRC-1.5, Action CIRC-1.5.1) that concluded impacts would be less than most of the principally permitted uses that are allowed and would result in net reduction in vehicle miles traveled.

In accordance with Land Use Action 4.2.2 (Pre-Application Meetings), community engagement included two noticed neighborhood meetings in December of 2019 and March of 2020 in which there was substantial public attendance. The applicant circulated a third invitation to the neighborhood for an open house meeting on June 23, 2022; however, attendance was minimal with only a handful of individuals stopping through.

Although not required, an initial 30-day courtesy public review of the environmental documents and project plans was provided from January 28, 2022, through February 28, 2022, due to the interest surrounding this project. However, these environmental documents and plans have been posted on the City's website since January 2022 and the public review has remained open and staff continues to receive public comments. As of the date of this report, the City has

received numerous written comments expressing opposition to the project, and both the applicant and staff have extended great efforts to ensure those communications have been included in the public record and addressed in the Response to Comments letter, **Attachment L** (Policy LU-3.3, Action LU-3.3.1).

The project site is in an area of Chico that is experiencing significant growth and development. The project would activate a second corner parcel at this intersection, and new landscaping and site design would improve aesthetics along the north Deer Creek Highway (SR 32) corridor, consistent with CD Policy-2.3.

***Planning Commission to consider/discuss the project's neighborhood compatibility:***

The General Plan provides the following definitions for compatibility:

**Compatible (Design).** A project design that does not conflict with the site, architecture, and landscape design of surrounding projects.

**Compatible (Land Use).** Capable of existing together without significant conflict.

Staff's recommendation to the Planning Commission and assessment of compatibility is based upon the project's compliance with the land use, design, and development standards of the City's General Plan, Design Guidelines Manual and Municipal Code. Though staff's analysis did not result in the identification of any significant adverse impacts as a result of project implementation, a substantial amount of public correspondence was received in opposition to the proposed hotel. While staff has responded to comments relating to traffic, safety and environmental concerns in the form of Response to Comments (**Attachment L**), the Planning Commission may wish to consider how the project addresses the General Plan goals and policies listed below.

1. Discuss whether the project would enhance the character, identity, and livability of the existing neighborhood, consistent with General Plan Land Use Goal LU-3 and Policy LU-3.3.
2. Discuss whether the project would be considered "compatible" infill development providing a missing neighborhood element, such as neighborhood retail, enhanced architectural quality, and circulation improvements, or that otherwise contributes positively to the existing neighborhood, consistent with General Plan Land Use Goal LU-4, Policy LU-4.2 and Policy LU-4.4.
3. Discuss whether the project would reinforce the desirable elements of the neighborhood including architectural scale, style, and setback patterns, and if the project reflects a context sensitive design that maintains compatibility and raises the quality of the area's architectural character, consistent with General Plan Community Development Goal CD-5, Policy CD-5.1 and Policy CD-5.3.

The proposal represents an infill development project and complies with all the development standards set forth in Title 19, including lot coverage, landscaping, parking, setbacks and building height. The project is located within a widely permissive commercial zoning district and represents a land use that would have lesser impacts to traffic, vehicle miles traveled, parking, and density of development than other principally permitted land uses such as grocery stores, restaurants, or multi-family residential housing. The location chosen for the proposed



project currently represents one of the only hotel sites in east Chico north of Deer Creek Highway (SR 32) and would be conveniently located adjacent to this key transportation corridor. The project site is also proximal to medical facilities off Bruce Road which provide outpatient services for Enloe Hospital and The Terraces senior living community, making this an appropriate and desirable location for travelling healthcare professionals and visitors of residents in the senior living community. For these reasons, staff believes that the hotel use would be an acceptable addition to the existing neighborhood.

### Surrounding Development Context

Several multi-story residential buildings are located in the project vicinity, including Sierra Sunrise Apartments (3 stories) off of Sierra Sunrise Terrace, and Sterling Oaks Apartments (3 stories) off Bruce Road. As previously mentioned in this report, new residential projects along the south side of Deer Creek Highway (SR 32) (directly south of the project site) are approved for several three-story apartment buildings.

While the four-story hotel building would represent one of the taller structures in the area at predominately 49 feet, 3 inches tall, the building height would be comparable to the four-story assisted living facility “The Terraces” located approximately 1,000 feet east of the project site on Sierra Sunrise Terrace, which has a height of roughly 45 feet. Visual impacts of the hotel building would be further reduced by the natural topography of the area. For instance, the proposed location of the hotel sits at an elevation approximately 20 feet lower than “The Terraces” facility and 20-40 feet lower than the building sites for the three-story apartment projects being developed just south of the project site.

The project would comply with the maximum allowed building height of 57 feet in the CC zoning district. The project site is adjacent to other CC zoned parcels which would be entitled to the same building height, so future development resulting in a similar building height to the proposed hotel could be expected. Special design considerations were made to minimize visual impacts, such as providing large building setbacks and extensive landscaping around the site.

### Consistency with the Design Guidelines Manual

As it relates to the four-story building volume, the project is consistent with Design Guidelines that call for commercial buildings to use appropriate massing, fenestration, and materials to provide a pedestrian-level scale (DG 2.2.11). As shown on **Attachment J**, the visual prominence of the proposed four-story building would be lessened by the building placement at the center of the site and setback from the surrounding street frontages. The positioning of the building in this location would take advantage of topography of the site, siting the building at a lower elevation point than surrounding development to the east and south.

A detailed analysis of the project’s consistency with the City’s Design Guidelines Manual will be included in the report to the ARHPB for its review and consideration.

## **ENVIRONMENTAL REVIEW**

The site has been graded and a previous stockpile of soil and construction rubble and debris removed in 2020 for which a mitigated negative declaration (MND) was prepared in 2018.

The proposed development project has been determined to be categorically exempt pursuant

to the California Environmental Quality Act (CEQA) Guidelines Section 15332 (In-Fill Development Projects). Consistent with this exemption, the project is: consistent with the applicable General Plan designation, zoning regulations, and General Plan policies; is less than five acres in size, substantially surrounded by urban uses; has no significant habitat value for special status species; will not result in any significant impacts regarding traffic, noise, air quality, or water quality; and can be adequately served by all required utilities and public services (see **Attachment M**, Notice of Exemption – Environmental Analysis). Several supporting special studies were prepared for the project, including an updated Biological Assessment, Vehicle Miles Travelled (VMT) Analysis and Signal Warrant Evaluation, Noise Assessment, Air Quality and Greenhouse Gas Assessment, and Energy Consumption Analysis that concluded there is no potential for any environmental impact. Please refer to the Response to Comments in **Attachment L** for further discussion on the lack of environmental impacts associate with the proposed hotel.

### **ALTERNATIVE ACTIONS**

There has been significant public input received highlighting issues related to the proposed project's incompatibility with the neighborhood, and other potential impacts associated with hotel development and operation (see **Attachment N**, Public Correspondence). It is, therefore, important to provide a balanced perspective of alternative actions for the Planning Commission's consideration. Staff understands and respects the arguments against the project but based on the General Plan and Municipal Code consistency analysis, as well as the environmental review, cannot find a basis to recommend denial of the project. As a Planning Commissioner, though, it is appropriate to feel differently about this proposal and the only requirement for denial is a determination that the site is not suitable for a hotel, and to find that the hotel would negatively impact the neighborhood. Therefore, staff has provided alternative actions in the event that a majority of the Planning Commission feels the hotel is inappropriate at this location, or that a redesign would make the project more compatible with the adjacent neighborhood. They include:

1. Approve the project per staff's recommendation;
2. Deny the project based on incompatibility with the neighborhood; or
3. Modify the project to be more compatible with a lower building height.

If denial or modification of the project is considered, staff will need to get a clear reading of the particular issues of concern from the Planning Commission in order to develop the findings for the denial or modification of the project.

### **REQUIRED FINDINGS FOR APPROVAL**

Following a public hearing, the Planning Commission may approve a use permit application, with or without conditions, only if all the required findings set forth in CMC Section 19.24.040 can be made.

#### Use Permit Findings

- A. *The proposed use is allowed within the subject zoning district and complies with all of the applicable provisions of Chapter 19.24 (Use Permits).*

The proposed development is allowed within the subject zoning district and generally complies with all the applicable provisions of these Regulations and applicable design guidelines. Chico Municipal Code 19.44.020, Table 4-6, allows for the development of a hotel in the CC (Community Commercial) zoning district, subject to use permit approval. This use permit has been processed in accordance with the requirements of Chapter 19.24.

B. *The proposed use would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed use.*

No impacts to the health, safety, or welfare of persons residing or working in the neighborhood have been identified in connection with the proposed hotel project. The project complies with all applicable development standards for the CC zoning district and has been reviewed by the City's Fire and Police Departments to ensure any concerns with evacuation are vetted and resolved prior to project approval. Neither department identified any concerns with wildland fires or with inhibiting evacuation capabilities with implementation of the proposed hotel. In addition, the installation of a new traffic signal at the intersection of Bruce Road and Sierra Sunrise Terrace as a condition of the project's development would improve general traffic and circulation patterns of the greater area.

C. *The proposed use will not be detrimental and/or injurious to property and improvements in the neighborhood of the proposed use, as well as the general welfare of the City.*

The proposed hotel use would not be detrimental to property and improvements in the neighborhood of the proposed use, as well as the general welfare of the City. The surrounding area currently contains a mix of residential and commercial uses, along with improved streets and public transportation facilities including three bus stops on Bruce Road between Deer Creek Highway (SR 32) and Lakewest Drive. Existing regulations require that any public improvements damaged during construction be repaired or reconstructed by the applicant. The site can be adequately served by all required utilities and public services. Stormwater from the site will be screened by the proposed landscaping to the south and a proposed bioswale to the west before being directed to an existing concrete storm drain system along Deer Creek Highway (SR 32). No impacts to property or improvements have been identified, including impacts related to public parking.

D. *The proposed use will be consistent with the policies, standards, and land use designations established by the General Plan.*

The proposed project is consistent with several General Plan goals, policies and actions under Land Use (LU) Goals LU-2, LU-3 and LU-4; Community Design (CD) Goals CD-1, CD-2 and CD-5; Economic Development (ED) Goals ED-2; Open Space (OS) Goal OS-2; and Circulation (CIRC) Goal CIRC-1. Specifically, the project would be consistent with General Plan goals and policies to enhance regional tourism opportunities and resources to attract visitors who support the local businesses and encourage the development of additional recreation and tourism businesses and industries, such as hotels (Goal ED-2; Policy ED-2.1 and ED-2.2). The project would develop a commercially zoned property consistent with a land use plan that provides a mix and distribution of uses that meet the identified needs of the community (Goal LU-2). Consistent with Policy LU-2.4 to "promote land use compatibility through use restrictions, development standards, environmental

review and special design considerations”, the project is consistent with the purpose of the CMU designation and CC zoning district and meets all applicable development standards. Special design considerations were made to minimize visual impacts, such as providing large building setbacks and extensive landscaping around the site.

Additionally, the project has been designed in compliance with creekside development standards and would provide a minimum 30-foot setback from the top of bank to any development or site improvement (Policy OS-2.5, Action OS-2.5.1). Most of the site would consist of landscaped area, and the landscape plan would incorporate new plantings as well as enhance the existing natural environment (Goal CD-1, Action CD-1.1.2). New landscaping and site design would improve aesthetics along the north Deer Creek Highway (SR 32) corridor, consistent with CD Policy-2.3.

E. *The design, location, size, and operating characteristics of the proposed use are compatible with the existing and future land uses in the vicinity.*

The project would be consistent with all applicable development standards including lot coverage, landscaping, parking, setbacks and building height as set forth in CMC Section 19.44.030. The project is located within a widely permissive commercial zoning district and represents a land use that would have lesser impacts to traffic, vehicle miles traveled, parking, and density of development than other principally permitted land uses in the CC zoning district. The location chosen for the proposed project would be conveniently located adjacent to a key transportation corridor.

The CC zoning district allows for building coverage up to 95 percent of the total site area, requiring at least 5 percent be landscaped. The project’s total building coverage would represent only 10 percent of the site, with the remaining area dedicated to parking and landscaping. While the four-story hotel building would represent one of the taller structures in the area at predominately 49 feet, 3 inches tall, the building height would be comparable to the four-story assisted living facility “The Terraces” located approximately 1,000 feet east of the project site on Sierra Sunrise Terrace, which has a height of roughly 45 feet. Furthermore, the site design is context sensitive as it provides for enhanced setbacks and landscaping to lessen visual impacts of the proposed hotel building.

### Architectural Review Findings

A. *The proposed development is consistent with the General Plan, any applicable specific plan, and any applicable neighborhood or area plans.*

The proposal is consistent with various General Plan policies and actions under Land Use (LU) Goals LU-2, LU-3 and LU-4; Community Design (CD) Goals CD-1 and CD-5; Economic Development (ED) Goals ED-2; Open Space (OS) Goal OS-2; and Circulation (CIRC) Goal CIRC-1. Specifically, the project would be consistent with General Plan goals and policies to enhance regional tourism opportunities and resources to attract visitors who support the local businesses and encourage the development of additional recreation and tourism businesses and industries, such as hotels (Goal ED-2; Policy ED-2.1 and ED-2.2).

Consistent with Policy LU-2.4 to “promote land use compatibility through use restrictions, development standards, environmental review and special design considerations”, the

project is consistent with the purpose of the CMU designation and CC zoning district and meets all applicable development standards. The project has been designed in compliance with creekside development standards and would provide a 30-foot setback from the top of bank to any development or site improvement (Policy OS-2.5, Action OS-2.5.1). Most of the site would consist of landscaped area, and the landscape plan would incorporate new plantings as well as enhance the existing natural environment (Goal CD-1, Action CD-1.1.2).

Development of the project would result in traffic and circulation improvements, including the future installation of a traffic signal at the intersection of Bruce Road and Sierra Sunrise Terrace (Goal CIRC-1, Policy CIRC-1.1, Action CIRC-1.1.1, Policy CIRC-1.2). A Vehicle Miles Travelled (VMT) Analysis was prepared as part of the environmental review process (Policy CIRC-1.5, Action CIRC-1.5.1) that concluded impacts would be less than most of the principally permitted uses that are allowed and would result in net reduction in vehicle miles traveled. The project would activate a second corner parcel at this intersection, and new landscaping and site design would improve aesthetics along the north Deer Creek Highway (SR 32) corridor, consistent with CD Policy-2.3.

*B. The proposed development, including the character, scale, and quality of design are consistent with the purpose/intent of this chapter and any adopted design guidelines.*

As it relates to the four-story building volume, the Planning Commission finds that the project is consistent with Design Guidelines that call for commercial buildings to use appropriate massing, fenestration, and materials to provide a pedestrian-level scale (DG 2.2.11). The scale and visual prominence of the proposed four-story building would be lessened by the building placement at the center of the site and setback from the surrounding street frontages. The positioning of the building in this location would take advantage of topography of the site, siting the building at a lower elevation point than surrounding development to the east and south.

The ARHPB shall review the project for consistency with this finding as it relates to the character and quality of design.

*C. The architectural design of structures, including all elevations, materials and colors are visually compatible with surrounding development. Design elements, including screening of equipment, exterior lighting, signs, and awnings, have been incorporated into the project to further ensure its compatibility with the character and uses of adjacent development.*

As it relates to the four-story building volume, the Planning Commission finds that enhanced setbacks and landscaping buffers lessen visual impacts of the proposed hotel and reduce the visual prominence of the building volume to a reasonable scale. The building height would be comparable to the four-story assisted living facility “The Terraces” located approximately 1,000 feet east of the project site on Sierra Sunrise Terrace, which has a height of roughly 45 feet. Furthermore, the Planning Commission finds that the proposed hotel building would be visually compatible in terms of size and height with future surrounding development of the Oak Valley Master Plan area, which includes multiple three-story apartment buildings sited on higher elevation points than the project site.

The ARHPB shall review the project for consistency with this finding as it relates to architectural design elements such as color and material selection, equipment screening, exterior lighting, signs, and awnings.

*D. The location and configuration of structures are compatible with their sites and with surrounding sites and structures, and do not unnecessarily block views from other structures or dominate their surroundings.*

As it relates to the four-story building volume, the Planning Commission finds that the proposed hotel would not unnecessarily block views from other structures. The CC zoning district allows for building coverage up to 95 percent of the total site area, requiring at least 5 percent be landscaped. The project's total building coverage would represent approximately 10 percent of the site (16,655 square feet), while 57 percent (102,241 square feet) of the site would be landscaped. While the four-story hotel building would represent one of the taller structures in the area at predominately 49 feet, 3 inches tall, the building height would be comparable to the four-story assisted living facility "The Terraces" located approximately 1,000 feet east of the project site on Sierra Sunrise Terrace, which has a height of roughly 45 feet. Visual impacts of the hotel building would be further reduced by the natural topography of the area.

The project would comply with the maximum height for main buildings in the CC zoning district (57 feet). Per CMC Section 19.60.070.E, parapet walls are permitted height projections that may extend up to 6 feet above the height limit of the structure. The four-story building would have a height of predominately 49 feet, 3 inches, with tower and parapet elements extending to 59 feet, 3 inches (or approximately 2-feet, 3 inches (above the base height allowance of 57 feet) into the 6-foot permitted height projection), consistent with building height requirements. The project site is adjacent to other CC zoned parcels which would be entitled to the same building height, so future development resulting in a similar building height to the proposed hotel could be expected. Special design considerations were made to minimize visual impacts, such as providing large building setbacks and extensive landscaping around the site. The building's dominance will diminish over time as the onsite landscaping and trees grow to maturity and with additional surrounding development, as more buildings are added less impact will be felt from each individual one.

*E. The general landscape design, including the color, location, size, texture, type, and coverage of plant materials, and provisions for irrigation and maintenance, and protection of landscape elements, have been considered to ensure visual relief, to complement structures, and to provide an attractive environment.*

As it relates to the four-story building volume, the Planning Commission finds that the landscape design would provide visual relief with extensive plantings, including a total of 69 new trees and a variety of shrubs and groundcover. The proposed landscaping will provide a variety of seasonal color, while minimizing irrigation demands. The project proposes to landscape approximately 57 percent of the site, far exceeding the minimum landscaping requirement of 5 percent. The ARHPB shall review the project for consistency with this finding as it relates to the overall quality of the landscape design including variety of plantings, irrigation demands, attractiveness.



## **PUBLIC CONTACT**

Community engagement included two noticed neighborhood meetings in December of 2019 and March of 2020 in which there was substantial public attendance. The applicant circulated a third invitation to the neighborhood for an open house meeting on June 23, 2022; however, attendance was minimal with only a handful of individuals stopping through.

Due to the public interest surrounding this project, an initial 30-day courtesy public review of the environmental documents and project plans was provided from January 28, 2022, through February 28, 2022. However, these environmental documents and plans have been posted on the City's website since January 2022 and the public review has remained open and staff continues to receive public comments. Staff received a significant amount of public input, with the majority of concerns relating to safety, traffic and visual impacts, and general neighborhood compatibility. All written comments received prior to the distribution of this report have been included for the Planning Commission's review as **Attachment N**, Public Correspondence.

A 10-day public hearing notice was mailed to all landowners and residents within 500 feet of the site and a legal notice was displayed in the Chico Enterprise Record, consistent with standard noticing procedure for use permit applications.

## **DISTRIBUTION**

### Internal

PC Distribution

CDD Vieg

### External

Continuum Hospitality, LLC., 5080 California Avenue, Suite 415, Bakersfield, CA 93309

MWT Architect, Attn: Melissa Stevens [melissa@mwtusa.com](mailto:melissa@mwtusa.com)

Attn: Sirvan Pour [sirvan@mwtusa.com](mailto:sirvan@mwtusa.com)

Northstar, Attn: Mark Wolfe [mwolfe@northstareng.com](mailto:mwolfe@northstareng.com);

Attn: Jim Stevens [jstevens@northstareng.com](mailto:jstevens@northstareng.com)

Thomas H. Phelps Landscape Architecture, Attn: Thomas Phelps; [thphelps@sbcglobal.net](mailto:thphelps@sbcglobal.net)

Michael Baker International, LLC., Attn: John Hope; [john.hope@mbakerintl.com](mailto:john.hope@mbakerintl.com)

Centric Health, Attn: Ajay Anand; [aanand@centrichcare.com](mailto:aanand@centrichcare.com)

Clement, Fitzpatrick & Kenworthy, Attn: Erin B. Carlstrom; [ecarlstrom@cfk.com](mailto:ecarlstrom@cfk.com)

Carter Law Office, Attn: Jeff Carter; [jeff@jjcarterlaw.com](mailto:jeff@jjcarterlaw.com)

Jeffrey Lambkin, Attorney at Law; [jplambkin@gmail.com](mailto:jplambkin@gmail.com)

## **ATTACHMENTS**

- A. Planning Commission Resolution No. 22-04  
Exhibit I (Conditions of Approval)
- B. Location Map
- C. Site Plan
- D. Recorded Map
- E. Development Activity Vicinity Map
- F. Patio and Pool Plans
- G. Building Elevations
- H. Renderings
- I. Site Details

- J. Pedestrian Scale Viewpoints
- K. Landscape Plan
- L. Response to Comments
- M. Notice of Exemption – Environmental Analysis (without Appendices)
- N. Public Correspondence

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**RESOLUTION NO. 22-04**

**RESOLUTION OF THE CITY OF CHICO PLANNING COMMISSION  
APPROVING USE PERMIT 19-25 AND ARCHITECTURAL REVIEW 19-22  
FOR A NEW HOTEL SOUTH OF SIERRA SUNRISE TERRACE AND NORTHEAST  
OF THE INTERSECTION OF DEER CREEK HIGHWAY (SR 32) AND BRUCE  
ROAD; APN 018-230-001  
(TOWNEPLACE SUITES HOTEL)**

WHEREAS, applications have been submitted for a Use Permit and Architectural Review to establish a hotel land use south of Sierra Sunrise Terrace and northeast of the intersection of Deer Creek Highway (SR 32) and Bruce Road (APN 018-230-001); and

WHEREAS, the Planning Commission considered the staff report and comments submitted at a duly noticed public hearing held on July 21, 2022; and

WHEREAS, the Project has been determined to be categorically exempt under Section 1.40.220 of the Chico Municipal Code (CMC) and pursuant to the California Environmental Quality Act (CEQA) Guidelines Section 15332 (In-Fill Development Projects).

NOW, THEREFORE, BE IT RESOLVED BY THE PLANNING COMMISSION OF THE CITY OF CHICO AS FOLLOWS:

1. With regard to the Use Permit, the Planning Commission finds that:

A. The proposed development is allowed within the subject zoning district and generally complies with all the applicable provisions of these Regulations and applicable design guidelines. Chico Municipal Code 19.44.020, Table 4-6, allows for the development of a hotel in the CC (Community Commercial) zoning district, subject to use permit approval. This use permit has been processed in accordance with the requirements of Chapter 19.24.

B. The proposed development would not result in adverse impacts to the health, safety, or welfare of persons residing or working in the neighborhood. The project complies with all applicable development standards for the CC zoning district and has been reviewed by the City's Fire and Police Departments to ensure any concerns with evacuation are vetted and resolved prior to project approval. Neither department identified any concerns with wildland fires or with inhibiting evacuation capabilities with implementation of the proposed hotel. In

1 addition, the installation of a new traffic signal at the intersection of Bruce Road and Sierra  
2 Sunrise Terrace as a condition of the project's development would improve general traffic  
3 and circulation patterns of the greater area.

4 C. The proposed hotel use would not be detrimental to property and improvements in the  
5 neighborhood of the proposed use, as well as the general welfare of the City. The surrounding  
6 area currently contains a mix of residential and commercial uses, along with improved streets  
7 and public transportation facilities including three bus stops on Bruce Road between Deer  
8 Creek Highway (SR 32) and Lakewest Drive. Existing regulations require that any public  
9 improvements damaged during construction be repaired or reconstructed by the applicant.  
10 The site can be adequately served by all required utilities and public services. Stormwater  
11 from the site will be screened by the proposed landscaping to the south and a proposed  
12 bioswale to the west before being directed to an existing concrete storm drain system along  
13 Deer Creek Highway (SR 32). No impacts to property or improvements have been identified,  
14 including impacts related to public parking.

15 D. The proposed project is consistent with several General Plan goals, policies and actions  
16 under Land Use (LU) Goals LU-2, LU-3 and LU-4; Community Design (CD) Goals CD-1,  
17 CD-2 and CD-5; Economic Development (ED) Goals ED-2; Open Space (OS) Goal OS-2;  
18 and Circulation (CIRC) Goal CIRC-1. Specifically, the project would be consistent with  
19 General Plan goals and policies to enhance regional tourism opportunities and resources to  
20 attract visitors who support the local businesses, and encourage the development of additional  
21 recreation and tourism businesses and industries, such as hotels (Goal ED-2; Policy ED-2.1  
22 and ED-2.2). The project would develop a commercially zoned property consistent with a  
23 land use plan that provides a mix and distribution of uses that meet the identified needs of the  
24 community (Goal LU-2). Consistent with Policy LU-2.4 to "promote land use compatibility  
25 through use restrictions, development standards, environmental review and special design  
26 considerations", the project is consistent with the purpose of the CMU designation and CC  
27 zoning district and meets all applicable development standards. Special design considerations

1 were made to minimize visual impacts, such as providing large building setbacks and  
2 extensive landscaping around the site. Additionally, the project has been designed in  
3 compliance with creekside development standards and would provide a 30-foot setback from  
4 the top of bank to any development or site improvement (Policy OS-2.5, Action OS-2.5.1).  
5 Most of the site would consist of landscaped area, and the landscape plan would incorporate  
6 new plantings as well as enhance the existing natural environment (Goal CD-1, Action CD-  
7 1.1.2). New landscaping and site design would improve aesthetics along the north Deer Creek  
8 Highway (SR 32) corridor, consistent with CD Policy-2.3.

9 E. The design, location, size, and operating characteristics of the proposed hotel use are  
10 compatible with the existing and future land uses in the vicinity. The project would be  
11 consistent with all applicable development standards including lot coverage, landscaping,  
12 parking, setbacks and building height as set forth in CMC Section 19.44.030. The project is  
13 located within a widely permissive commercial zoning district and represents a land use that  
14 would have lesser impacts to traffic, vehicle miles traveled, parking, and density of  
15 development than other principally permitted land uses in the CC zoning district. The location  
16 chosen for the proposed project would be conveniently located adjacent to a key  
17 transportation corridor. The CC zoning district allows for building coverage up to 95 percent  
18 of the total site area, requiring at least 5 percent be landscaped. The project's total building  
19 coverage would represent only 10 percent of the site, with the remaining area dedicated to  
20 parking and landscaping. While the four-story hotel building would represent one of the taller  
21 structures in the area at predominately 49 feet, 3 inches tall, the building height would be  
22 comparable to the four-story assisted living facility "The Terraces" located approximately  
23 1,000 feet east of the project site on Sierra Sunrise Terrace, which has a height of roughly 45  
24 feet. Furthermore, the site design is context sensitive as it provides for enhanced setbacks and  
25 landscaping to lessen visual impacts of the proposed hotel building.

26 2. With regard to Architectural Review, the Planning Commission finds that:

27 A. The proposal is consistent with various General Plan policies and actions under Land Use

1 (LU) Goals LU-2, LU-3 and LU-4; Community Design (CD) Goals CD-1, CD-2 and CD-5;  
2 Economic Development (ED) Goals ED-2; Open Space (OS) Goal OS-2; and Circulation  
3 (CIRC) Goal CIRC-1. Specifically, the project would be consistent with General Plan goals  
4 and policies to enhance regional tourism opportunities and resources to attract visitors who  
5 support the local businesses and encourage the development of additional recreation and  
6 tourism businesses and industries, such as hotels (Goal ED-2; Policy ED-2.1 and ED-2.2).  
7 Consistent with Policy LU-2.4 to “promote land use compatibility through use restrictions,  
8 development standards, environmental review and special design considerations”, the project  
9 is consistent with the purpose of the CMU designation and CC zoning district and meets all  
10 applicable development standards. The project has been designed in compliance with  
11 creekside development standards and would provide a 30-foot setback from the top of bank  
12 to any development or site improvement (Policy OS-2.5, Action OS-2.5.1). Most of the site  
13 would consist of landscaped area, and the landscape plan would incorporate new plantings as  
14 well as enhance the existing natural environment (Goal CD-1, Action CD-1.1.2).  
15 Development of the project would result in traffic and circulation improvements, including  
16 the future installation of a traffic signal at the intersection of Bruce Road and Sierra Sunrise  
17 Terrace (Goal CIRC-1, Policy CIRC-1.1, Action CIRC-1.1.1, Policy CIRC-1.2). A Vehicle  
18 Miles Travelled (VMT) Analysis was prepared as part of the environmental review process  
19 (Policy CIRC-1.5, Action CIRC-1.5.1) that concluded impacts would be less than most of the  
20 principally permitted uses that are allowed and would result in net reduction in vehicle miles  
21 traveled. The project would activate a second corner parcel at this intersection, and new  
22 landscaping and site design would improve aesthetics along the north Deer Creek Highway  
23 (SR 32) corridor, consistent with CD Policy-2.3.

24 B. As it relates to the four-story building volume, the scale of the proposed development is  
25 consistent with Design Guidelines that call for commercial buildings to use appropriate  
26 massing, fenestration, and materials to provide a pedestrian-level scale (DG 2.2.11). The scale  
27 and visual prominence of the proposed four-story building would be lessened by the building



1 placement at the center of the site and setback from the surrounding street frontages. The  
2 positioning of the building in this location would take advantage of topography of the site,  
3 siting the building at a lower elevation point than surrounding development to the east and  
4 south. The ARHPB shall review the project for consistency with this finding as it relates to  
5 the character and quality of design.

6 C. As it relates to the four-story building volume, enhanced setbacks and landscaping buffers  
7 lessen visual impacts of the proposed hotel and reduce the visual prominence of the building  
8 volume to a reasonable scale. The building height would be comparable to the four-story  
9 assisted living facility “The Terraces” located approximately 1,000 feet east of the project site  
10 on Sierra Sunrise Terrace, which has a height of roughly 45 feet. Furthermore, the Planning  
11 Commission finds that the proposed hotel building would be visually compatible in terms of  
12 size and height with future surrounding development of the Oak Valley Master Plan area,  
13 which includes multiple three-story apartment buildings sited on higher elevation points than  
14 the project site. The ARHPB shall review the project for consistency with this finding as it  
15 relates to architectural design elements such as color and material selection, equipment  
16 screening, exterior lighting, signs, and awnings.

17 D. As it relates to the four-story building volume, the location and configuration of structures  
18 are compatible with their sites and with surrounding sites and structures, and do not  
19 unnecessarily block views from other structures or dominate their surroundings. The CC  
20 zoning district allows for building coverage up to 95 percent of the total site area, requiring  
21 at least 5 percent be landscaped. The project’s total building coverage would represent  
22 approximately 10 percent of the site (16,655 square feet), while 57 percent (102,241 square  
23 feet) of the site would be landscaped. While the four-story hotel building would represent one  
24 of the taller structures in the area at predominately 49 feet, 3 inches tall, the building height  
25 would be comparable to the four-story assisted living facility “The Terraces” located  
26 approximately 1,000 feet east of the project site on Sierra Sunrise Terrace, which has a height  
27 of roughly 45 feet. Visual impacts of the hotel building would be further reduced by the

1 natural topography of the area. The project would comply with the maximum height for main  
2 buildings in the CC zoning district (57 feet). Per CMC Section 19.60.070.E, parapet walls are  
3 permitted height projections that may extend up to 6 feet above the height limit of the  
4 structure. The four-story building would have a height of predominately 49 feet, 3 inches,  
5 with tower and parapet elements extending to 59 feet, 3 inches (or approximately 2-feet, 3  
6 inches (above the base height allowance of 57 feet) into the 6-foot permitted height  
7 projection), consistent with building height requirements. The project site is adjacent to other  
8 CC zoned parcels which would be entitled to the same building height, so future development  
9 resulting in a similar building height to the proposed hotel could be expected. Special design  
10 considerations were made to minimize visual impacts, such as providing large building  
11 setbacks and extensive landscaping around the site. The building's dominance will diminish  
12 over time as the onsite landscaping and trees grow to maturity and with additional surrounding  
13 development.

14 E. As it relates to the four-story building volume, the landscape design would provide visual  
15 relief with extensive plantings, including a total of 69 new trees and a variety of shrubs and  
16 groundcover. The project proposes to landscape approximately 57 percent of the site, far  
17 exceeding the minimum landscaping requirement of 5 percent. The ARHPB shall review the  
18 project for consistency with this finding as it relates to the overall quality of the landscape  
19 design including variety of plantings, irrigation demands, and attractiveness.

20 3. Based on all of the above, the Planning Commission approves the project subject to the  
21 conditions set forth in Exhibit I attached hereto.

22 4. The Planning Commission hereby specifies that the materials and documents which constitute  
23 the record of proceedings upon which its decision is based are located at and under the custody of  
24 the City of Chico Community Development Department.

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THE FOREGOING RESOLUTION WAS ADOPTED at a meeting of the Planning Commission of the City of Chico held on July 21, 2022, by the following vote:

- AYES:
- NOES:
- ABSENT:
- ABSTAIN:
- DISQUALIFIED:
- ATTEST:

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APPROVED AS TO FORM



Vincent C. Ewing, City Attorney\*

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Bruce Ambo  
Planning Commission Secretary

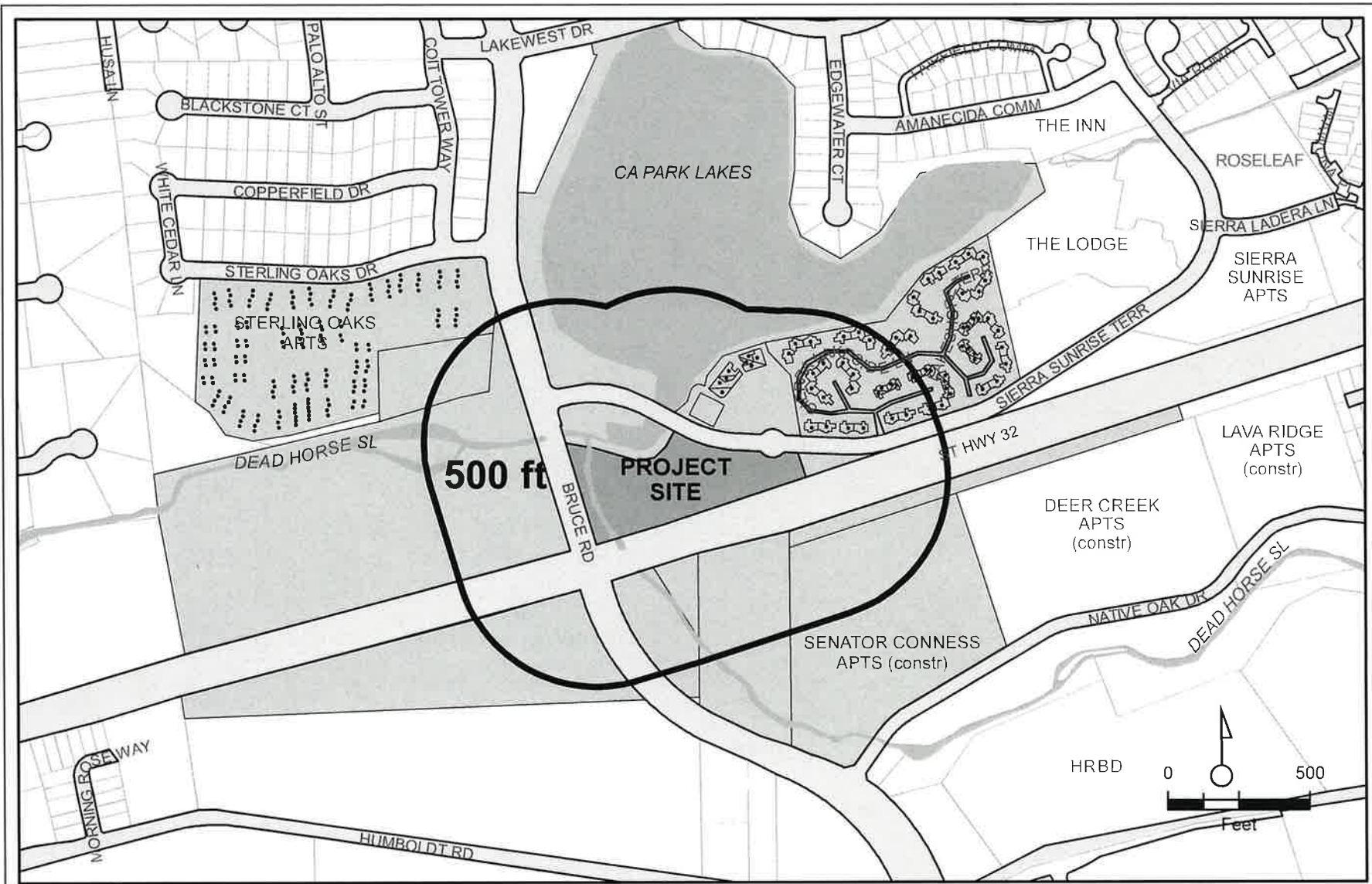
\*Pursuant to The Charter of  
the City of Chico, Section 906(E)

**EXHIBIT I**  
**CONDITIONS OF APPROVAL**  
**Use Permit 19-25 and Architectural Review 19-22**  
**(TownePlace Suites Hotel)**

1. The front page of all approved building plans shall note in bold type face that the project shall comply with Use Permit 19-25 and Architectural Review 19-22 (TownePlace Suites Hotel). No building permits related to this approval shall receive final approval without prior authorization of Community Development Department Planning staff.
2. All development shall comply with all other Federal, State and local Code provisions, including those of the City of Chico Community Development and Public Works Departments. The permittee is responsible for contacting these offices to verify the need for compliance.
3. All approved building plans and permits shall note that wall-mounted utilities and roof or wall penetrations, including vent stacks, utility boxes, exhaust vents, gas meters and similar equipment, shall be screened by appropriate materials and colors. All parapet caps and other metal flashing shall be painted, consistent with the approved building colors. Exterior louvers of the PTAC units shall be painted to match the exterior building façade. Adequate screening shall be verified by Planning staff prior to issuance of a certificate of occupancy.
4. Site and architectural design shall be subject to review and approval by the Architectural Review and Historic Preservation Board (ARHPB).
5. Installation of a traffic signal at the intersection of Bruce Road and Sierra Sunrise Terrace shall be the at the expense of the developer.
6. All exterior lighting shall be shielded and directed downward to avoid light spillage onto adjacent properties.
7. Applicant shall provide detailed landscaping plans compliant with AB 1881 water efficiency requirements to be reviewed and approved by planning staff prior to building permit issuance.
8. All signage proposed for the project shall be reviewed administratively and approved under a separate permit.
9. All new electric, telephone, and other wiring conduits for utilities shall be placed underground in compliance with CMC 19.60.120.
10. The applicant shall defend, indemnify, and hold harmless the City of Chico, its boards and commissions, officers and employees against and from any and all liabilities, demands, claims, actions or proceedings and costs and expenses incidental thereto (including costs of defense, settlement and reasonable attorney's fees), which any or all of them may suffer, incur, be responsible for or pay out as a result of or in connection with any challenge to or claim regarding the legality, validity, processing or adequacy associated with: (i) this requested entitlement; (ii) the proceedings undertaken in connection with the adoption or approval of this entitlement; (iii) any subsequent approvals or permits relating to this entitlement; (iv) the processing of occupancy

permits and (v) any amendments to the approvals for this entitlement. The City of Chico shall promptly notify the applicant of any claim, action or proceeding which may be filed and shall cooperate fully in the defense, as provided for in Government code section 66474.9.

11. The following conditions shall be noted on the front page of all building plans prior to commencement of construction activities:
  - a. If during ground disturbing activities, any bones, pottery fragments or other potential cultural resources are encountered, the applicant or their supervising contractor shall cease all work within the area of the find and notify the Community Development Department at 530-879-6800. A professional archaeologist who meets the Secretary of the Interior's Professional Qualification Standards for prehistoric and historic archaeology and who is familiar with the archaeological record of Butte County, shall be retained by the applicant to evaluate the significance of the find. Community Development Department staff shall notify all local tribes on the consultation list maintained by the State of California Native American Heritage Commission, to provide local tribes the opportunity to monitor evaluation of the site. Site work shall not resume until the archaeologist conducts sufficient research, testing and analysis of the archaeological evidence to make a determination that the resource is either not cultural in origin or not potentially significant. If a potentially significant resource is encountered, the archaeologist shall prepare a mitigation plan for review and approval by the Community Development Department, including recommendations for total data recovery, Tribal monitoring, disposition protocol, or avoidance, if applicable. All measures determined by the Community Development Director to be appropriate shall be implemented pursuant to the terms of the archaeologist's report. The preceding requirement shall be incorporated into construction contracts and documents to ensure contractor knowledge and responsibility for the proper implementation.
  - b. A qualified biologist will conduct a pre-construction bird nest survey prior to issuance of any grading permit for the Project, unless the work will commence during the non-breeding season (September 1 through February 28).
  - c. Up to 48 hours prior to any ground disturbance, pre-construction surveys will be conducted by a qualified biologist within the Project limits for western pond turtle and western spadefoot. If a pond turtle or western spadefoot is observed in the Project limits during construction, all work will be stopped and the turtle or western spadefoot will: 1) be allowed to leave on its own volition, or 2) be moved by the Project biologist in the direction it was heading, at a safe distance from the grading activities, and at a safe location. The biologist will report observations and relocations to the City Community Development Department.
12. No authorization is included in this approval for any grading or ground disturbance outside the previously graded pad area.

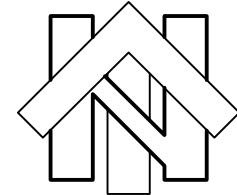


UP 19-25, AR 19-22  
 (Town Place Suites Hotel)  
 APN 018-230-001-000  
 Neighborhood Meeting

- Noticed Parcels
- Noticed Addresses







SCALE 1"=40' (24x36)  
SCALE 1"=20' (11x17)

### AREAS

TOTAL AREA = 4.09 ACRES  
NUMBER OF PROPOSED UNITS = 1 COMMERCIAL UNIT (16,655 SF)  
DENSITY = 0.24 UNITS/ACRE

**REQUIRED PARKING**  
112 ROOMS = 1 SPACE PER ROOM 112 SPACES  
TOTAL 112 SPACES

BICYCLE PARKING 0.05 SPACE/UNIT 6 SPACES

**PROVIDED PARKING**  
SURFACE PARKING 110 SPACES  
ACCESSIBLE PARKING 7 SPACES  
TOTAL 117 SPACES

PROVIDED BICYCLE PARKING 20 SPACES

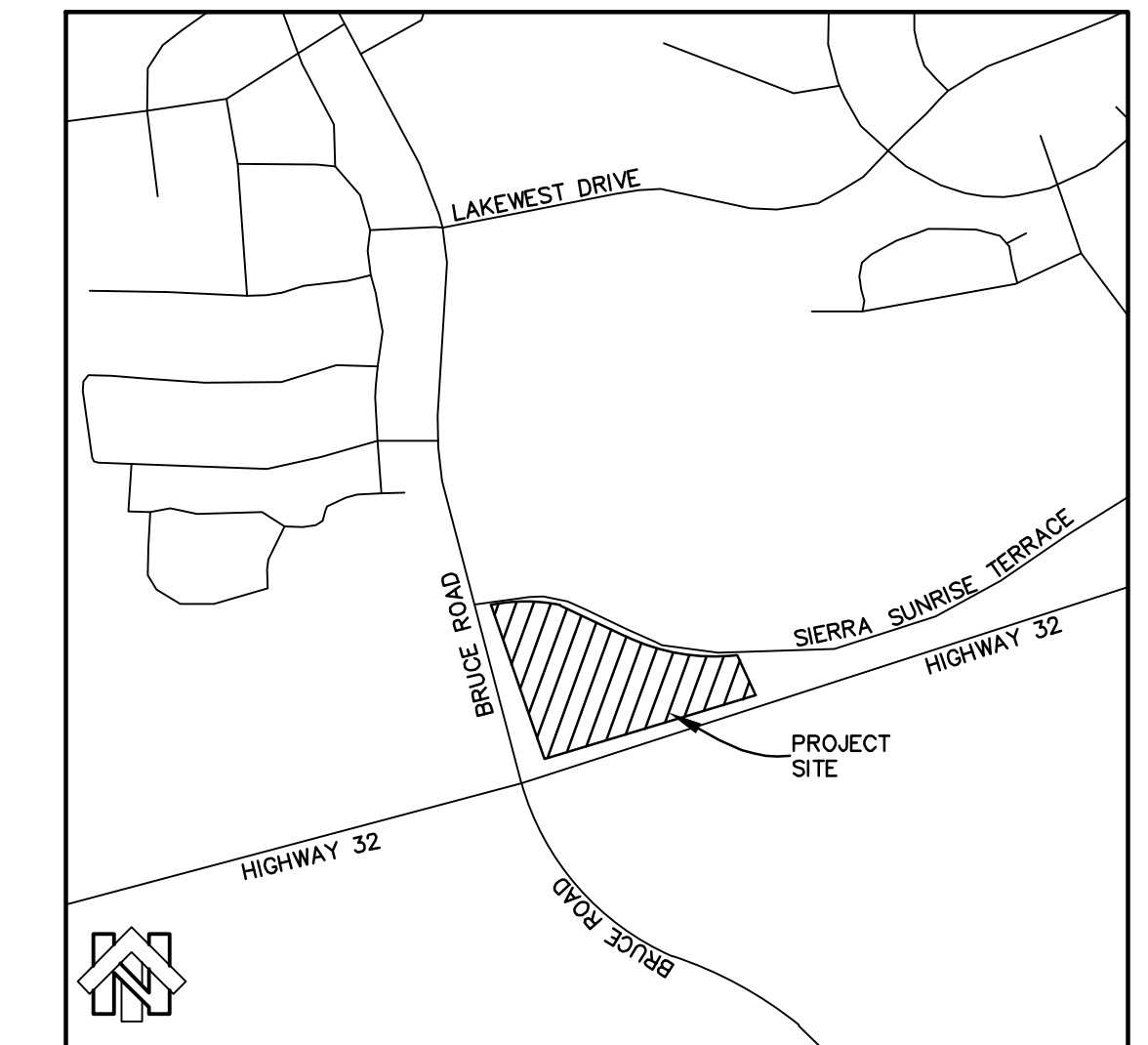
**SITE COVERAGE SUMMARY**

|                |            |     |
|----------------|------------|-----|
| PARKING AREA   | 59,265 SF  | 33% |
| BUILDING AREA  | 16,655 SF  | 10% |
| LANDSCAPE AREA | 102,241 SF | 57% |

| Curve Table |        |            |        |
|-------------|--------|------------|--------|
| Curve #     | Radius | Delta      | Length |
| C1          | 480.00 | 025°45'00" | 215.72 |
| C2          | 20.00  | 044°24'55" | 15.50  |
| C3          | 50.00  | 088°49'50" | 77.52  |
| C4          | 20.00  | 044°24'55" | 15.50  |

### NOTES

- ZONING: CC
- GENERAL PLAN: CMU
- LAND USE: EXISTING - VACANT  
PROPOSED - HOTEL
- SEWAGE: CITY OF CHICO
- STORM DRAINAGE: ON-SITE QUALITY MITIGATION
- WATER: CALIFORNIA WATER SERVICE
- POWER: PG&E
- TELEPHONE: AT&T
- CABLE TV: COMCAST
- EXISTING WELLS AND SEPTIC SYSTEMS TO BE ABANDONED VIA PERMIT FROM BUTTE COUNTY ENVIRONMENTAL HEALTH
- STANDARD EROSION CONTROL MEASURES (BMP's) WILL BE USED IN COMPLIANCE WITH THE CITY OF CHICO AND THE WATER QUALITY CONTROL BOARD
- OWNER: FIFTH SUN SIERRA SUNRISE TERRACE, CHICO, CA 95926
- DEVELOPER: H2H ASSET GROUP, LLC 5080 CALIFORNIA AVE, SUITE 415 BAKERSFIELD, CA 93311
- PROJECT SURVEYOR: MICHAEL MAYS PLS6967
- THIS USE PERMIT WAS PREPARED UNDER THE DIRECTION OF A REGISTERED CIVIL ENGINEER OR LICENSED LAND SURVEYOR.
- PORTIONS OF THE PROPERTY ARE LOCATED IN FLOOD ZONE "X" AND FLOOD ZONE "AE" PER THE FLOOD INSURANCE RATE MAP COMMUNITY PANEL NO. 06007C0506E, DATED JANUARY 6, 2011.



LOCATION MAP NTS

### LEGEND

- FOUND MONUMENT AS DESCRIBED
- WM EXISTING WATER METER
- ⊕ EXISTING POWER POLE WITH ANCHOR
- EXISTING STORM DRAIN INLET
- ⊙ EXISTING STORM DRAIN MANHOLE
- ⊕ EXISTING WATER VALVE
- ⊕ EXISTING SIGN
- ⊕ EXISTING FIRE HYDRANT
- ⊕ EXISTING GAS VALVE
- ⊕ EXISTING SANITARY SEWER MANHOLE
- EXISTING FENCE LINE
- EXISTING FLOW LINE
- EXISTING EDGE OF PAVEMENT
- EXISTING EASEMENT
- EXISTING TOP OF BANK
- EXISTING 25' FROM TOP OF BANK
- BUILDING SETBACK LINE
- EXISTING PROPERTY LINE FOR SURROUNDING PARCELS
- ⊕ XX" EXISTING TREE (XX" IN SIZE)
- ⊕ XX" EXISTING TREE TO BE REMOVED
- ⊕ PROPOSED FIRE HYDRANT
- PROPERTY BOUNDARY
- ⊕ PROPOSED ACCESSIBLE PATH OF TRAVEL
- ⊕ PARKING LOT CIRCULATION

*Mary N. Ledford*  
NOV 22, 2021  
NORTHSTAR  
MARY N. LEDFORD  
CE 65939



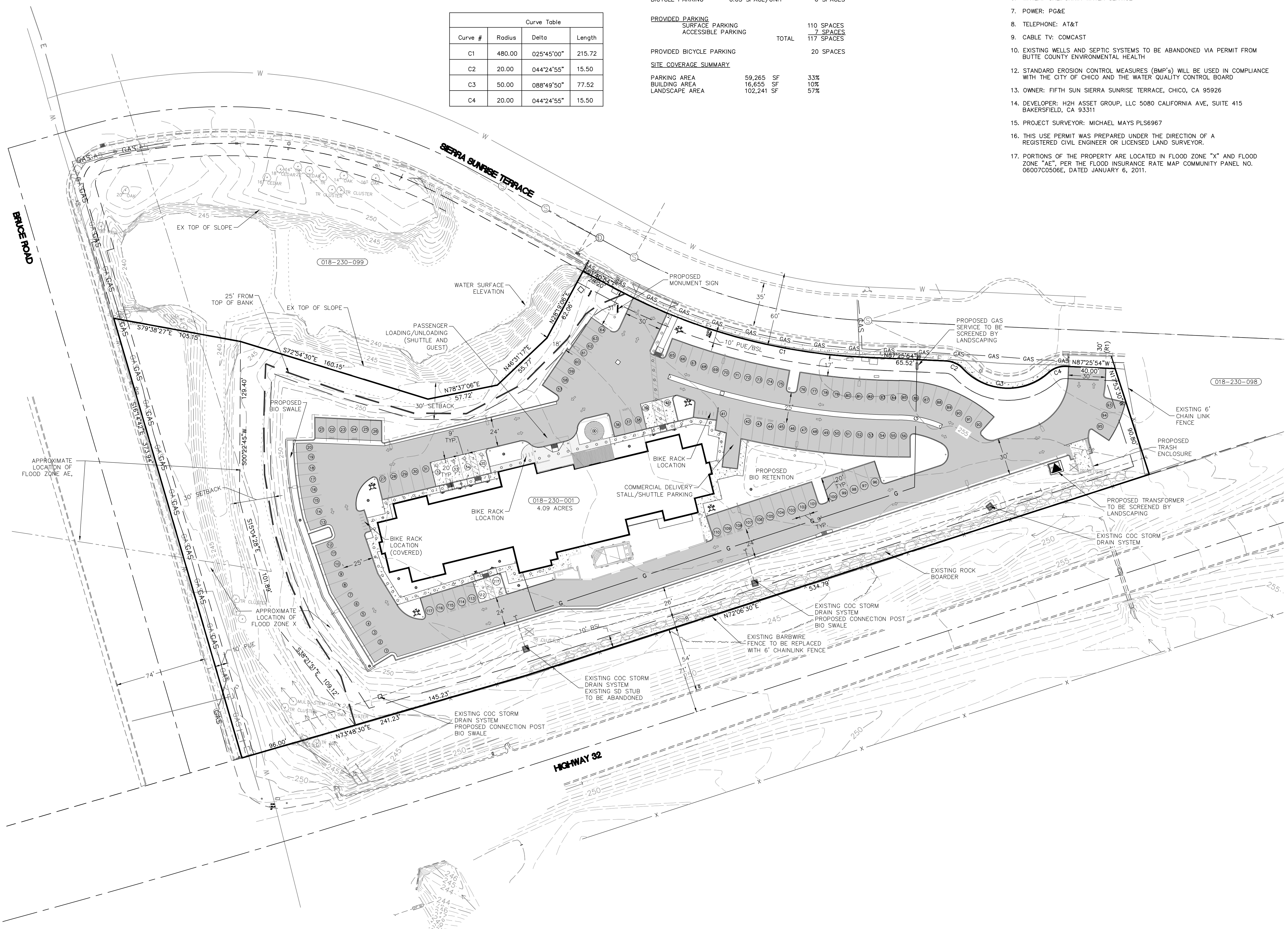
CONDITIONAL USE PERMIT MAP AND SITE PLAN  
**TOWNE PLACE SUITES**  
FOR  
H2H ASSET GROUP, LLC

BEING A PORTION OF SECTIONS 19 AND 20,  
TOWNSHIP 22 NORTH, RANGE 2 EAST, MOUNT  
DIABLO BASE AND MERIDIAN

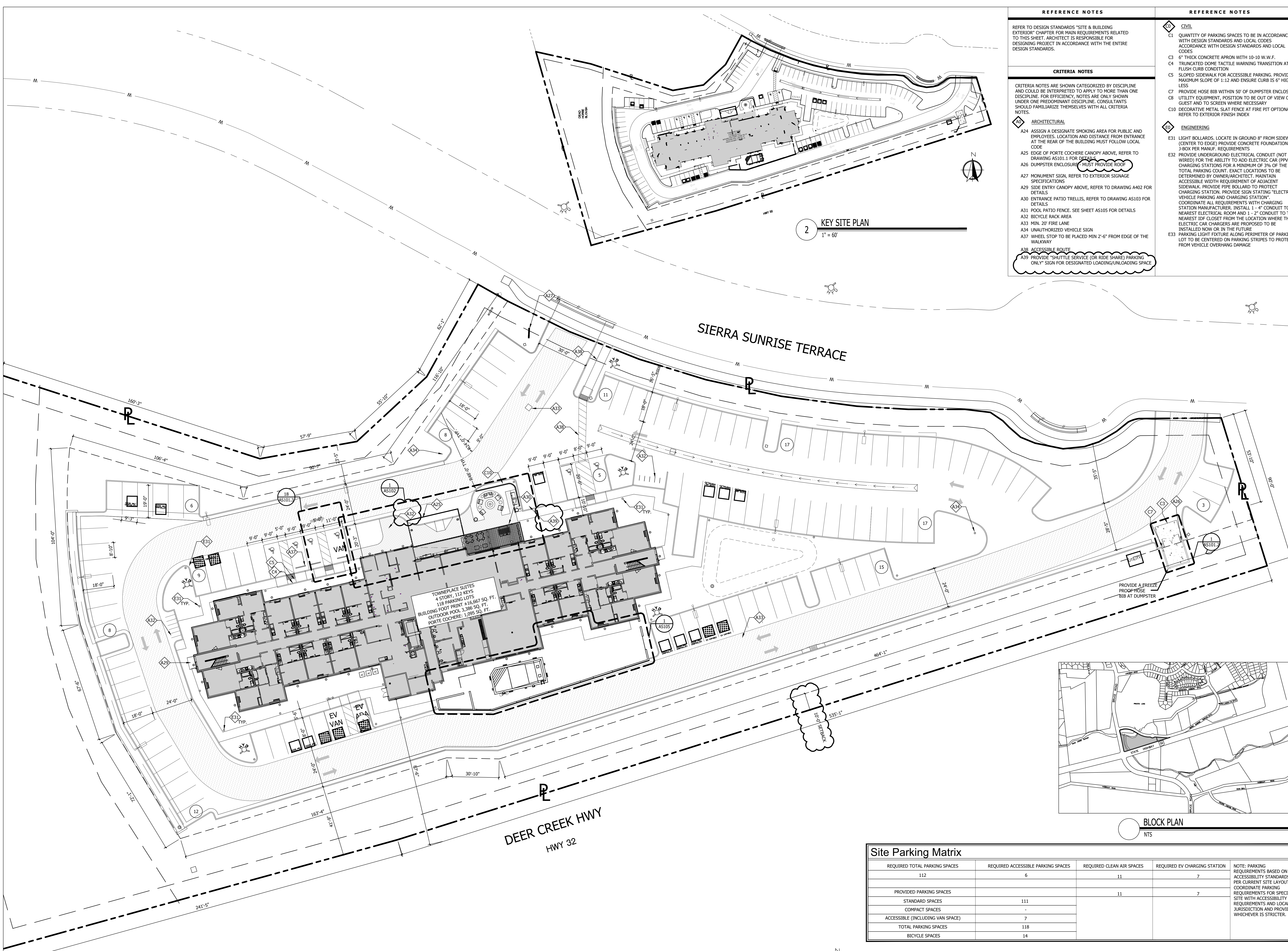
CITY OF CHICO  
COUNTY OF BUTTE  
STATE OF CALIFORNIA



111 MISSION RANCH BLVD, SUITE 100, CHICO, CA 95926  
PHONE: (530) 893-1600 www.northstareng.com







**1 SITE PLAN**  
1" = 20'

**2 KEY SITE PLAN**  
1" = 60'

- REFERENCE NOTES**
- REFER TO DESIGN STANDARDS "SITE & BUILDING EXTERIOR" CHAPTER FOR MAIN REQUIREMENTS RELATED TO THIS SHEET. ARCHITECT IS RESPONSIBLE FOR DESIGNING PROJECT IN ACCORDANCE WITH THE ENTIRE DESIGN STANDARDS.
- CRITERIA NOTES**
- CRITERIA NOTES ARE SHOWN CATEGORIZED BY DISCIPLINE AND COULD BE INTERPRETED TO APPLY TO MORE THAN ONE DISCIPLINE. FOR EFFICIENCY, NOTES ARE ONLY SHOWN UNDER ONE PREDOMINANT DISCIPLINE. CONSULTANTS SHOULD FAMILIARIZE THEMSELVES WITH ALL CRITERIA NOTES.
- ARCHITECTURAL**
- A24 ASSIGN A DESIGNATED SMOKING AREA FOR PUBLIC AND EMPLOYEES. LOCATION AND DISTANCE FROM ENTRANCE AT THE REAR OF THE BUILDING MUST FOLLOW LOCAL CODE
  - A25 EDGE OF PORTE COCHERE CANOPY ABOVE, REFER TO DRAWING AS101.1 FOR DETAILS
  - A26 DUMPSTER ENCLOSURE MUST PROVIDE ROOF
  - A27 MONUMENT SIGN, REFER TO EXTERIOR SIGNAGE SPECIFICATIONS
  - A29 SIDE ENTRY CANOPY ABOVE, REFER TO DRAWING A402 FOR DETAILS
  - A30 ENTRANCE PATIO TRELIS, REFER TO DRAWING AS103 FOR DETAILS
  - A31 POOL PATIO FENCE, SEE SHEET AS105 FOR DETAILS
  - A32 BICYCLE RACK AREA
  - A33 MIN. 20' FIRE LANE
  - A34 UNAUTHORIZED VEHICLE SIGN
  - A37 WHEEL STOP TO BE PLACED MIN 2'-6" FROM EDGE OF THE WALKWAY
  - A38 ACCESSIBLE ROUTE
  - A39 PROVIDE "SHUTTLE SERVICE (OR RIDE SHARE) PARKING ONLY" SIGN FOR DESIGNATED LOADING/UNLOADING SPACE

- REFERENCE NOTES**
- CIVIL**
- C1 QUANTITY OF PARKING SPACES TO BE IN ACCORDANCE WITH DESIGN STANDARDS AND LOCAL CODES
  - C3 6" THICK CONCRETE APRON WITH 10-10 W.W.F.
  - C4 TRUNCATED DOME TACTILE WARNING TRANSITION AT FLUSH CURB CONDITION
  - C5 SLOPED SIDEWALK FOR ACCESSIBLE PARKING. PROVIDE MAXIMUM SLOPE OF 1:12 AND ENSURE CURB IS 6" HIGH OR LESS
  - C7 PROVIDE HOSE BIB WITHIN 50' OF DUMPSTER ENCLOSURE
  - C8 UTILITY EQUIPMENT, POSITION TO BE OUT OF VIEW OF GUEST AND TO SCREEN WHERE NECESSARY
  - C10 DECORATIVE METAL SLAT FENCE AT FIRE PIT OPTIONAL; REFER TO EXTERIOR FINISH INDEX
- ENGINEERING**
- E31 LIGHT BOLLARDS, LOCATE IN GROUND 6" FROM SIDEWALK (CENTER TO EDGE) PROVIDE CONCRETE FOUNDATION W/ J-BOX PER MANUF. REQUIREMENTS
  - E32 PROVIDE UNDERGROUND ELECTRICAL CONDUIT (NOT WIRED) FOR THE ABILITY TO ADD ELECTRIC CAR (PEV) CHARGING STATIONS FOR A MINIMUM OF 3% OF THE TOTAL PARKING COUNT. EXACT LOCATIONS TO BE DETERMINED BY OWNER/ARCHITECT. MAINTAIN ACCESSIBLE WIDTH REQUIREMENT OF ADJACENT SIDEWALK. PROVIDE PIPE BOLLARD TO PROTECT CHARGING STATION. PROVIDE SIGN STATING "ELECTRIC VEHICLE PARKING AND CHARGING STATION". COORDINATE ALL REQUIREMENTS WITH CHARGING STATION MANUFACTURER. INSTALL 1 - 4" CONDUIT TO THE NEAREST ELECTRICAL ROOM AND 1 - 2" CONDUIT TO THE NEAREST IDF CLOSET FROM THE LOCATION WHERE THE ELECTRIC CAR CHARGERS ARE PROPOSED TO BE INSTALLED NOW OR IN THE FUTURE
  - E33 PARKING LIGHT FIXTURE ALONG PERIMETER OF PARKING LOT TO BE CENTERED ON PARKING STRIPES TO PROTECT FROM VEHICLE OVERHANG DAMAGE



| REQUIRED TOTAL PARKING SPACES    | REQUIRED ACCESSIBLE PARKING SPACES | REQUIRED CLEAN AIR SPACES | REQUIRED EV CHARGING STATION |
|----------------------------------|------------------------------------|---------------------------|------------------------------|
| 112                              | 6                                  | 11                        | 7                            |
| PROVIDED PARKING SPACES          |                                    |                           |                              |
| STANDARD SPACES                  | 111                                | 11                        | 7                            |
| COMPACT SPACES                   | -                                  |                           |                              |
| ACCESSIBLE (INCLUDING VAN SPACE) | 7                                  |                           |                              |
| TOTAL PARKING SPACES             | 118                                |                           |                              |
| BICYCLE SPACES                   | 14                                 |                           |                              |

NOTE: PARKING REQUIREMENTS BASED ON ACCESSIBILITY STANDARDS PER CURRENT SITE LAYOUT. COORDINATE PARKING REQUIREMENTS FOR SPECIFIC SITE WITH ACCESSIBILITY REQUIREMENTS AND LOCAL JURISDICTION AND PROVIDE WHICHEVER IS STRICTER.

**IMPORTANT NOTICE TO ALL CONTRACTORS AND/OR SUBCONTRACTORS (COMPLETE BUILDING SYSTEMS AND/OR INSTALLATIONS)**

IF AWARDED A CONTRACT FOR ANY PORTION OF WORK NEEDED TO COMPLETE THIS PROJECT YOU ARE AGREEING TO THE FOLLOWING TERMS:

1. YOU ARE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES FOR THE TYPE OF WORK AND SCOPE FOR THIS PROJECT.

2. YOU AGREE TO PROVIDE A COMPLETE DESIGN-BUILD SOLUTION BASED ON THE ARCHITECT'S INTENDED DESIGN FOR THIS PROJECT.

3. YOU WILL BE RESPONSIBLE FOR ALL THE STEPS, PROCEDURES, MATERIALS, PRODUCTS, LABOR, EQUIPMENT, PERMITS AND/OR APPROVALS NECESSARY TO PROVIDE A COMPLETE SYSTEM INSTALLATION WHETHER SUGGESTED BY THE ARCHITECT OR NOT.

4. YOU WILL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES FOR THE TYPE OF WORK AND SCOPE FOR THIS PROJECT.

5. YOU WILL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES FOR THE TYPE OF WORK AND SCOPE FOR THIS PROJECT.

6. YOU WILL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES FOR THE TYPE OF WORK AND SCOPE FOR THIS PROJECT.

| NO. | DESCRIPTION | DATE | BY |
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**CONSTRUCTION  
DRAWING  
REVIEW (90%)**



HOTELS, HOUSING & MIXED USE

DRAWING QUESTIONS  
CA@MWTUSA.COM  
1-800-717-5816



WARNING  
IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS ACTING UNDER THE DIRECTION OF THE ARCHITECT, TO ALTER OR REPRODUCE THESE DRAWINGS IN ANY WAY.

**TOWNEPLACE SUITES**  
BY HARRIOTT

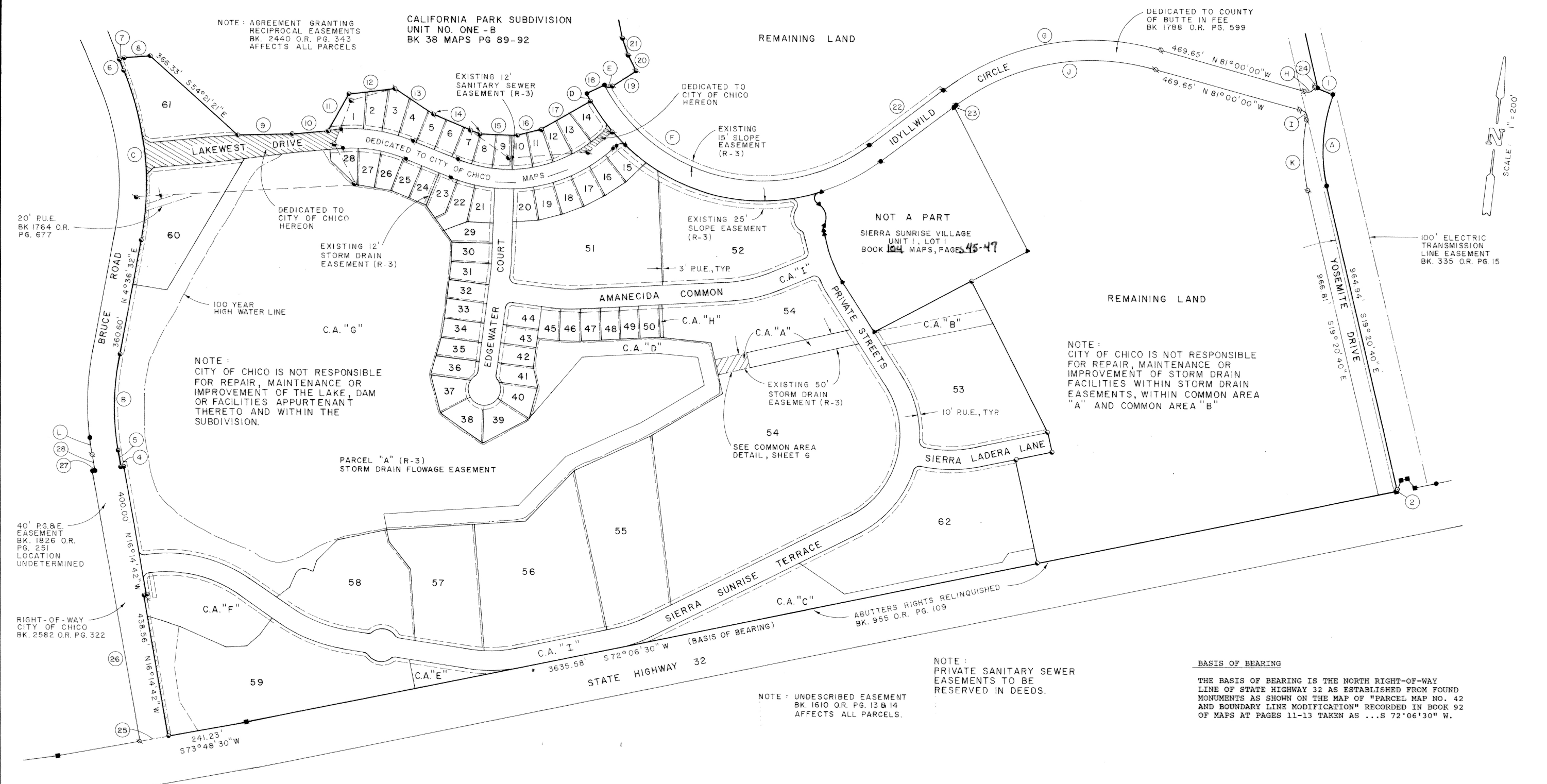
SIERRA SUNRISE TERRACE  
CHICO, CA

PROJECT NO.: 19-005  
DATE: 08-26-19  
DRAWN BY: WM/EH  
CHKD BY: AB

ARCHITECTURAL  
SITE PLAN

SHEET **AS101**  
19-005-AS101.DWG  
Attachment C

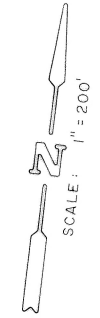




NOTE: AGREEMENT GRANTING RECIPROCAL EASEMENTS BK. 2440 O.R. PG. 343 AFFECTS ALL PARCELS

CALIFORNIA PARK SUBDIVISION UNIT NO. ONE - B BK 38 MAPS PG 89-92

DEDICATED TO COUNTY OF BUTTE IN FEE BK 1788 O.R. PG. 599



NOTE: CITY OF CHICO IS NOT RESPONSIBLE FOR REPAIR, MAINTENANCE OR IMPROVEMENT OF THE LAKE, DAM OR FACILITIES APPURTENANT THERETO AND WITHIN THE SUBDIVISION.

NOTE: CITY OF CHICO IS NOT RESPONSIBLE FOR REPAIR, MAINTENANCE OR IMPROVEMENT OF STORM DRAIN FACILITIES WITHIN STORM DRAIN EASEMENTS, WITHIN COMMON AREA "A" AND COMMON AREA "B"

NOTE: PRIVATE SANITARY SEWER EASEMENTS TO BE RESERVED IN DEEDS.

NOTE: UNDESCRIBED EASEMENT BK. 1610 O.R. PG. 13 & 14 AFFECTS ALL PARCELS.

BASIS OF BEARING  
THE BASIS OF BEARING IS THE NORTH RIGHT-OF-WAY LINE OF STATE HIGHWAY 32 AS ESTABLISHED FROM FOUND MONUMENTS AS SHOWN ON THE MAP OF "PARCEL MAP NO. 42 AND BOUNDARY LINE MODIFICATION" RECORDED IN BOOK 92 OF MAPS AT PAGES 11-13 TAKEN AS ... S 72°06'30" W.

**LINE DATA**

|      |         |   |           |   |
|------|---------|---|-----------|---|
| * 1  | 48.88'  | S | 75°58'03" | E |
| * 2  | 3.92'   | S | 12°06'30" | W |
| * 3  | 6.00'   | N | 73°45'18" | E |
| * 4  | 6.00'   | S | 73°45'18" | W |
| * 5  | 54.81'  | N | 16°14'42" | W |
| * 6  | 35.33'  | N | 29°44'28" | W |
| * 7  | 16.00'  | N | 60°15'32" | E |
| * 8  | 84.55'  | N | 77°26'42" | E |
| * 9  | 167.25' | N | 81°51'39" | E |
| * 10 | 109.92' | N | 78°54'23" | E |
| * 11 | 131.83' | N | 23°19'08" | E |
| * 12 | 141.50' | N | 77°05'53" | E |
| * 13 | 140.00' | S | 62°44'20" | E |
| * 14 | 158.42' | S | 74°27'02" | E |
| * 15 | 117.77' | N | 86°06'18" | E |
| * 16 | 75.96'  | N | 69°10'45" | E |
| * 17 | 169.63' | N | 52°54'00" | E |
| * 18 | 60.00'  | N | 55°38'51" | E |
| * 19 | 89.64'  | N | 50°30'00" | E |
| * 20 | 58.19'  | N | 33°01'34" | W |
| * 21 | 56.08'  | N | 24°46'03" | W |
| * 22 | 285.03' | N | 46°45'00" | E |
| * 23 | 8.00'   | N | 46°45'00" | E |
| * 24 | 11.12'  | N | 75°58'03" | W |
| * 25 | 93.99'  | S | 73°48'30" | W |
| * 26 | 838.46' | N | 16°14'42" | W |
| * 27 | 10.00'  | N | 73°45'18" | E |
| * 28 | 54.81'  | N | 16°14'42" | W |

**CURVE DATA**

|     |               |
|-----|---------------|
| * A | R = 490.00'   |
|     | D = 33°22'37" |
|     | L = 285.44'   |
| * B | R = 806.00'   |
|     | D = 20°51'14" |
|     | L = 293.36'   |
| * C | R = 894.00'   |
|     | D = 34°21'00" |
|     | L = 535.97'   |
| * D | R = 615.00'   |
|     | D = 02°02'59" |
|     | L = 22.00'    |
| * E | R = 20.00'    |
|     | D = 95°08'51" |
|     | L = 39.21'    |
| * F | R = 555.00'   |
|     | D = 98°53'51" |
|     | L = 957.98'   |
| * G | R = 780.00'   |
|     | D = 52°15'00" |
|     | L = 711.31'   |
| * H | R = 20.00'    |
|     | D = 84°58'03" |
|     | L = 29.66'    |
| * I | R = 20.00'    |
|     | D = 84°58'04" |
|     | L = 29.66'    |
| * J | R = 720.00'   |
|     | D = 52°15'00" |
|     | L = 656.59'   |
| * K | R = 550.00'   |
|     | D = 23°18'44" |
|     | L = 223.78'   |
| * L | R = 890.00'   |
|     | D = 03°36'36" |
|     | L = 56.07'    |

- LEGEND**
- BOUNDARY OF LAND BEING SUBDIVIDED
  - SET 5/8" REBAR WITH PLASTIC CAP TAGGED RCE 29113
  - SET 5/8" REBAR WITH PLASTIC CAP IN CONCRETE TAGGED RCE 29113
  - ⊗ FRONT PROPERTY CORNER, WITNESS CORNER SET ON PROLONGATION OF SIDE LOT LINE ON TOP OF CURB, SEE DETAIL
  - ⊘ CALCULATED POINT ONLY, NOTHING SET
  - FOUND CONCRETE HIGHWAY MONUMENT
  - FOUND 3/4" I.P. WITH PLASTIC CAP TAGGED LS 4035
  - ▲ 5/8" REBAR WITH PLASTIC CAP TAGGED RCE 29113 PER R-3
  - 5/8" REBAR WITH PLASTIC CAP IN CONCRETE TAGGED RCE 29113 PER R-3
  - ⊘ CALCULATED POINT PER R-1 & R-2
  - △ CENTERLINE MONUMENT
  - ▨ ABUTTERS RIGHTS RELINQUISHED
  - C.A. COMMON AREA
  - P.U.E. PUBLIC UTILITY EASEMENT
  - PROPOSED EASEMENT
  - - - EXISTING EASEMENT
  - R-1 \* RECORD DATA PER "PARCEL MAP NO. 42 AND BOUNDARY LINE MODIFICATION" RECORDED IN BOOK 92 OF MAPS AT PAGES 11-13
  - R-2 RECORD DATA PER "CALIFORNIA PARK SUBDIVISION UNIT NO. ONE-B" RECORDED IN BOOK 38 OF MAPS AT PAGE 89-92
  - R-3 RECORD DATA PER "SIERRA SUNRISE VILLAGE UNIT 1" RECORDED IN BOOK 104 OF MAPS AT PAGES 45-47
  - ① LINE DATA
  - ② CURVE DATA
  - CENTERLINE DATA

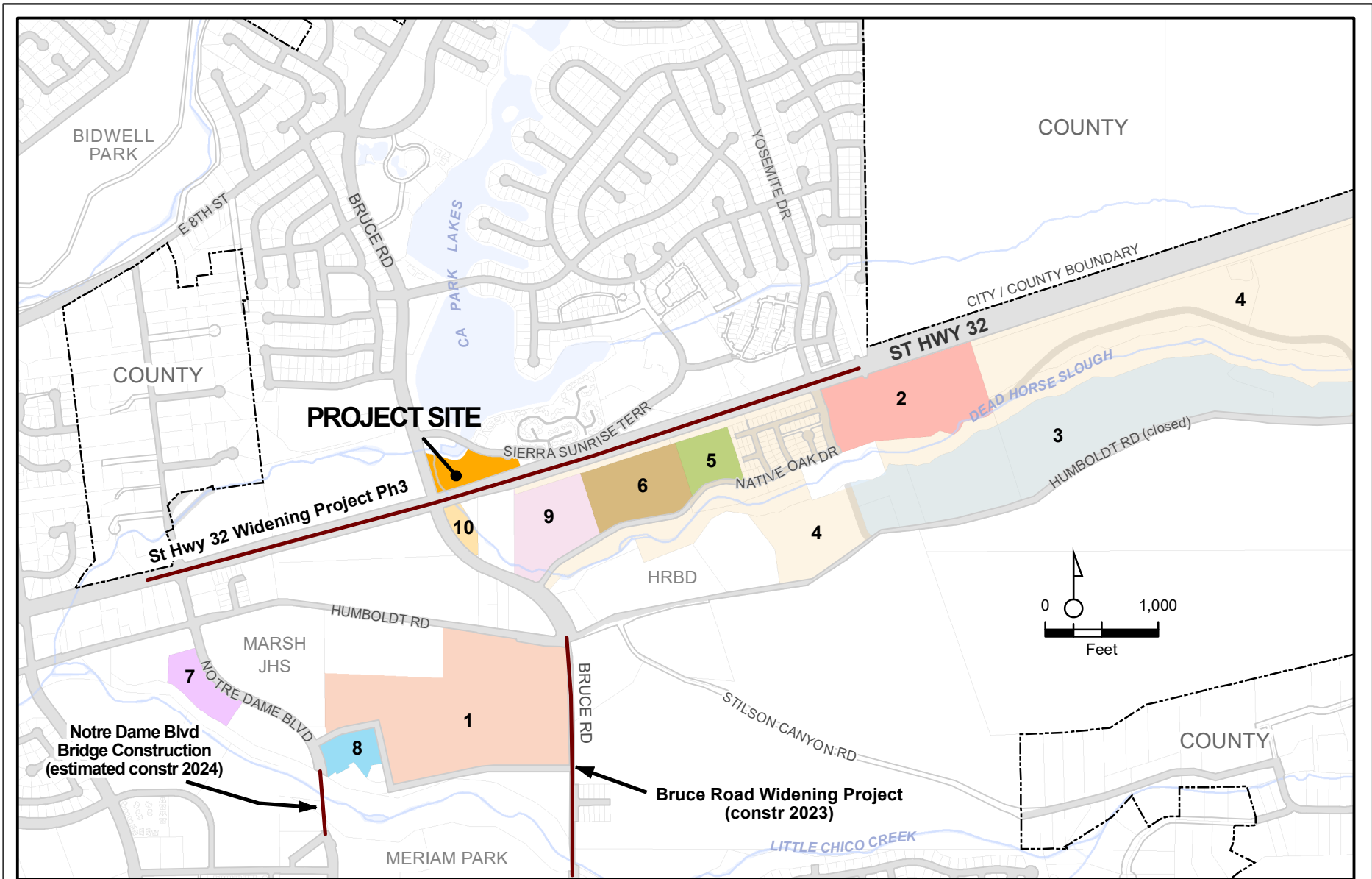
**SIERRA SUNRISE VILLAGE UNIT 2**

A SUBDIVISION OF A PORTION OF REMAINING LANDS AS SHOWN ON THAT CERTAIN MAP ENTITLED "SIERRA SUNRISE VILLAGE UNIT 1" RECORDED IN BOOK 104 OF MAPS, AT PAGES 45, 46, & 47 AND A PORTION OF PARCEL E AS SHOWN ON THAT CERTAIN MAP ENTITLED "CALIFORNIA PARK SUBDIVISION UNIT NO. ONE-B" RECORDED IN BOOK 38 OF MAPS AT PAGES 89 - 92 CITY OF CHICO, BUTTE COUNTY, CALIFORNIA

FOR  
**CALIFORNIA PARK A GENERAL PARTNERSHIP**  
ROLLS, ANDERSON & ROLLS  
CIVIL ENGINEERS  
CHICO, CALIFORNIA

SEPTEMBER, 1986

Attachment D

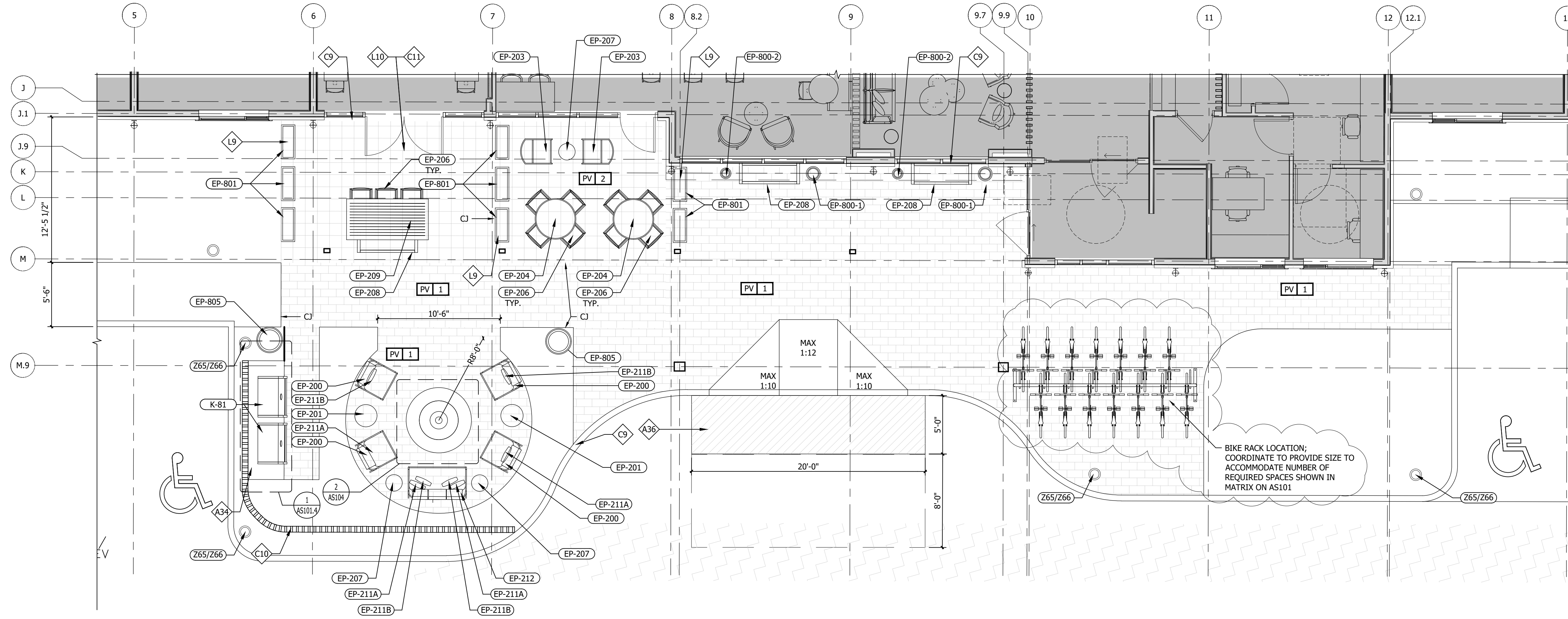


UP 19-25, AR 19-22  
 (Town Place Suites Hotel)  
 APN 018-230-001-000

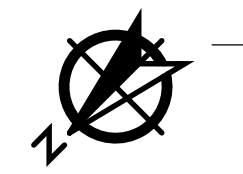
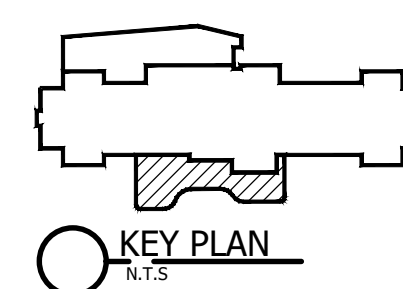
**Current Construction and Approved Projects**

- |   |                                 |   |
|---|---------------------------------|---|
| 1 Meriam Park North* - 239 lots                 | 5 Lava Ridge Apts* - 98 units   | 8 North Creek Crossings Apts* - 160 units |
| 2 Oak Valley Phs 1A&B* - 86 lots                | 6 Deer Creek Apts* - 204 units  | 9 Sen Conness Apts - 162 units            |
| 3 Oak Valley Ph2 - 231 lots                     | 7 North Creek Apts* - 101 units | 10 ARCO AMPM                              |
| 4 Oak Valley Planned Development Area           |                                 |   |
| Remaining Land - (approximately 585 units/lots) |                                 |   |
- \* Currently Under Construction





**1** ENLARGED PATIO PLAN  
3/16" = 1'-0"



**IMPORTANT NOTICE TO ALL CONTRACTORS AND SUBCONTRACTORS (COMPLETE BUILDING SYSTEMS AND OR INSTALLATIONS)**  
IF AWARDED A CONTRACT FOR ANY PORTION OF WORK NEEDED TO COMPLETE THIS PROJECT YOU ARE AGREEING TO THE FOLLOWING TERMS:  
1. YOU ARE A DELEGATED CONTRACTOR AND OR SUBCONTRACTOR SPECIFIED BY PROVIDING A COMPLETE, TIMELY SERVICE, IN YOUR TRADE OR PROFESSION, FOR THE TYPE OF WORK AND SCOPE OF THIS PROJECT.  
2. YOU AGREE TO PROVIDE A COMPLETE DESIGN SOLUTION BASED ON THE ARCHITECT'S INTENDED DESIGN FOR THIS PROJECT.  
3. YOUR RESPONSIBILITIES INCLUDE THE STUDY, PROCEDURE, MATERIALS, PRODUCTS, EQUIPMENT, FINISHES AND APPROVALS NECESSARY TO PROVIDE A COMPLETE AND GUARANTEED SYSTEM OF INSTALLATION THAT DOES NOT REQUIRE ANOTHER CONTRACTOR OR PRODUCT TO COMPLETE YOUR SCOPE OF WORK.  
4. YOUR DESIGN SOLUTIONS MUST INCLUDE ANY DESIGN INFORMATION REFERENCED WITHIN ANY CIVIL, ARCHITECTURAL, STRUCTURAL, MECHANICAL, ELECTRICAL AND/OR PLUMBING DRAWINGS, ALSO NOTE THESE DRAWINGS ARE SCHEMATIC AND ARE INTENDED TO SHOW ONLY BASIC CONCEPTS AND GENERAL INFORMATION. THE COMPLETION OF YOUR DESIGN SOLUTIONS INCLUDES THE STUDY, PROCEDURE, MATERIALS, PRODUCTS, LABOR, EQUIPMENT, FINISHES AND/OR APPROVALS NECESSARY TO PROVIDE A COMPLETE SYSTEM INSTALLATION WHETHER SUGGESTED ON THE DRAWINGS OR NOT.  
5. YOUR DESIGN SOLUTIONS SHALL BE SUBJECT TO THE ARCHITECT'S REVIEW AND APPROVAL.  
6. ANY CONTRACT ITEMS AND/OR TASKS LIKE: FIRE CAULK, GRAFT STOPS, FASTENERS, ANCHORS, EMBLEMMENTS, DUMPSTERS, DEBRIS CLEAN UP, ETC. ARE YOUR RESPONSIBILITY. ANY WORK NOT COMPLETED IN A TIMELY MANNER, OR INCOMPLETE WORK WILL BE LEFT BACK TO THE CONTRACTOR AND OR SUBCONTRACTOR, AT THE OWNER'S DISCRETION.

| REFERENCE NOTES   |  |
|---|--|
| A.  | REFER TO DESIGN STANDARDS "SITE & BUILDING EXTERIOR" CHAPTER FOR MAIN REQUIREMENTS RELATED TO THIS SHEET. ARCHITECT IS RESPONSIBLE FOR DESIGNING PROJECT IN ACCORDANCE WITH THE ENTIRE DESIGN STANDARDS. |
| CRITERIA NOTES  |  |
| CRITERIA NOTES ARE SHOWN CATEGORIZED BY DISCIPLINE AND COULD BE INTERPRETED TO APPLY TO MORE THAN ONE DISCIPLINE. FOR EFFICIENCY, NOTES ARE ONLY SHOWN UNDER ONE PREDOMINANT DISCIPLINE. CONSULTANTS SHOULD FAMILIARIZE THEMSELVES WITH ALL CRITERIA NOTES.   |  |
| <b>ARCHITECTURAL</b><br>A34 REFER TO THE SPECIFICATIONS FOR ASSISTANCE WITH ORDERING THE WEBER GRILL.<br>A36 ACCESSIBLE DROP-OFF SHALL BE MARKED WITH PAINTED BORDERLINE AROUND THEIR PERIMETER. THE AREA WITHIN THE BORDERLINES SHALL BE MARKED WITH HATCHED LINES A MAXIMUM OF 36 INCHES ON CENTER IN A COLOR CONTRASTING WITH THAT OF THE AISLE SURFACE. |  |
| <b>CIVIL</b><br>C9 PROVIDE EDGE/CURB RESTRAINT AT EDGE OF PAVERS.<br>C10 DECORATIVE METAL SLAT FENCE AT FIRE PIT OPTIONAL; REFER TO EXTERIOR FINISH INDEX.<br>C11 DEPRESS SLAB AT PATIO FOR TILE PAVES PV-2; COORDINATE WITH TILE MANUFACTURER.   |  |
| <b>LANDSCAPING</b><br>L9 PROVIDE EVERGREEN SHRUBS OR SEASONAL PLANTS AT TALL PLANTERS FOR PRIVACY.<br>L10 EXTERIOR TILE (PV-2) IS TO BE QUARTER TURNED TO MAKE A DIAMOND SHAPE OUT OF FOUR (4) TILES. EXTERIOR TILE (PV-2) AT PATIO (GRILLE AREA) IS A BRAND REQUIRED ITEM; THIS TILE MAY NOT BE VALUE-ENGINEERED.  |  |

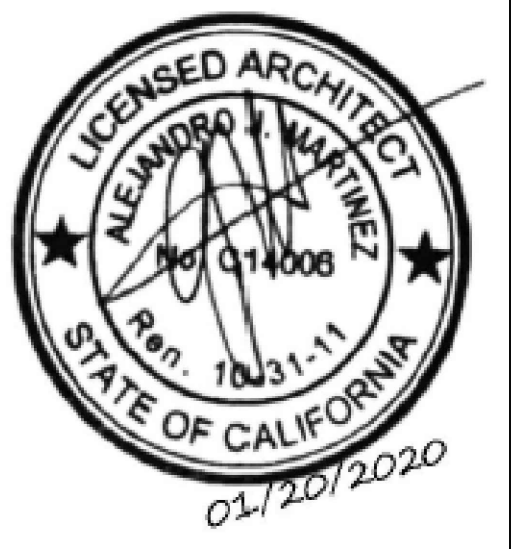
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**CONSTRUCTION DRAWING REVIEW (90%)**



HOTELS, HOUSING & MIXED USE

DRAWING QUESTIONS  
CA@MWTUSA.COM  
1-800-717-5816



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**TOWNEPLACE SUITES®**  
BY HARRIOTT

SIERRA SUNRISE TERRACE  
CHICO, CA

PROJECT NO.: 19-005  
DATE: 08-26-19  
DRAWN BY: AL  
CHKD BY: MWT

**SITE PLAN:**  
**ENTRANCE PATIO**

SHEET **AS102**  
19-005 AS\_102.DWG



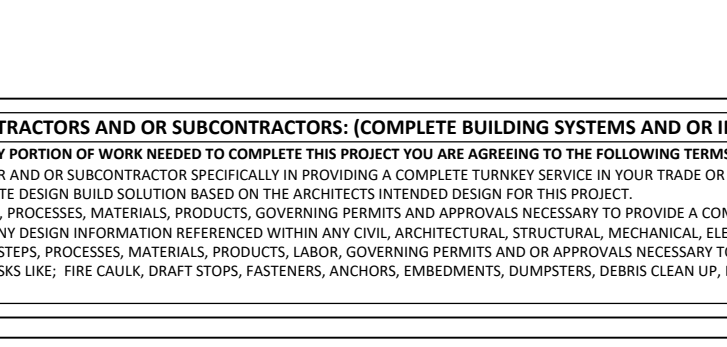
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**CONSTRUCTION DRAWING REVIEW (90%)**

**MWT**

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01/20/2020

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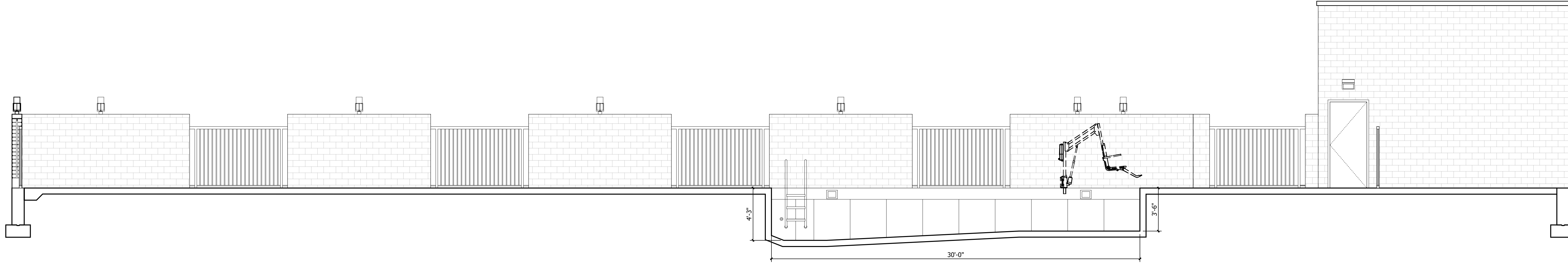
**TOWNEPLACE SUITES<sup>®</sup> BY MARRIOTT**

SIERRA SUNRISE TERRACE  
CHICO, CA

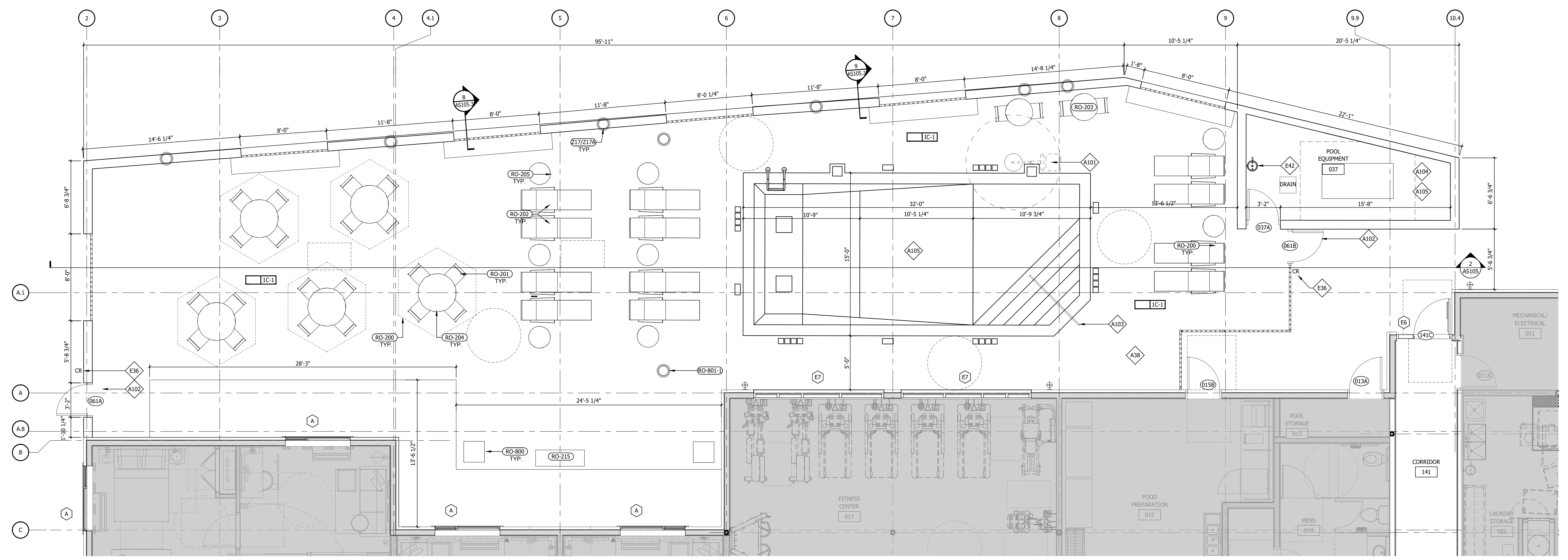
**PROJECT NO.:** 19-005  
**DATE:** 08-26-19  
**DRAWN BY:** AS  
**CHKD. BY:** MWT

**OUTDOOR POOL**

IMPORTANT NOTICE TO ALL CONTRACTORS AND/OR SUBCONTRACTORS: COMPLETE BUILDING SYSTEMS AND/OR INSTALLATIONS.



2 ENLARGED PUBLIC AREAS - OUTDOOR POOL SECTION  
1/4" = 1'-0"



1 ENLARGED PUBLIC AREAS - OUTDOOR POOL PLAN  
1/4" = 1'-0"

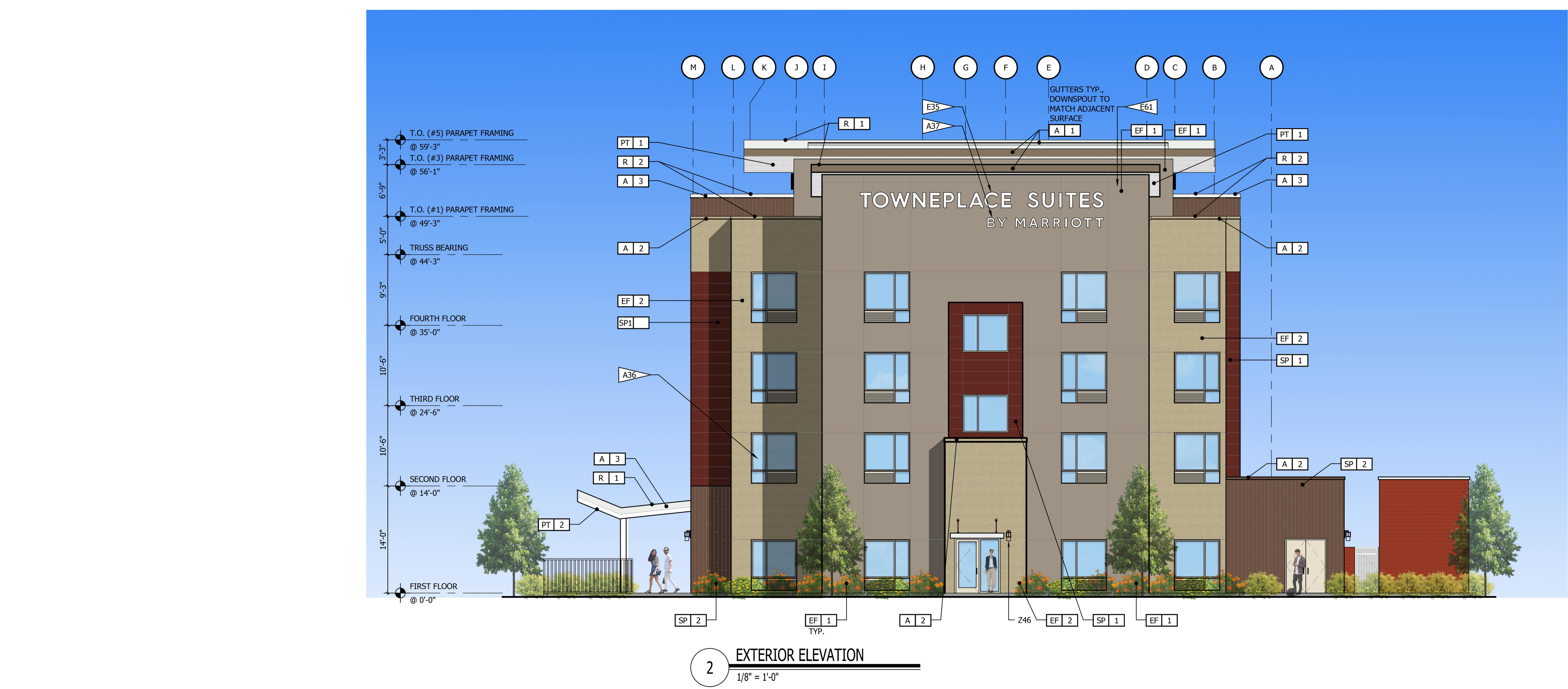
**KEY PLAN**

**IMPORTANT NOTICE TO ALL CONTRACTORS AND/OR SUBCONTRACTORS: COMPLETE BUILDING SYSTEMS AND/OR INSTALLATIONS.**

IF AWARDED A CONTRACT FOR ANY PORTION OF WORK NEEDED TO COMPLETE THIS PROJECT YOU ARE AGREEING TO THE FOLLOWING TERMS:

- YOU ARE ASSUMED RESPONSIBLE FOR ALL DESIGN RELATED SPECIFICATIONS AND REQUIREMENTS FOR THE TYPE OF WORK AND SCOPE OF THIS PROJECT.
- YOU AGREE TO PROVIDE A COMPLETE DESIGN RELATED SOLUTION BASED ON THE ARCHITECT'S INTENDED DESIGN FOR THIS PROJECT.
- YOUR REVIEW AND ALL THE STEPS, PROCESSES, MATERIALS, PRODUCTS, EQUIPMENT, FINISHES AND APPROVALS YOU PROVIDE A COMPLETE AND WARRANTY SYSTEM OR INSTALLATION THAT DOES NOT REQUIRE ANOTHER CONTRACTOR OR PRODUCT TO COMPLETE YOUR SCOPE OF WORK.
- YOUR REVIEW MUST INCLUDE ANY DESIGN INFORMATION REFERENCED WITHIN ANY CIVIL, ARCHITECTURAL, STRUCTURAL, MECHANICAL, ELECTRICAL AND/OR PLUMBING DRAWINGS, ALSO NOTE THESE DRAWINGS AND SCHEMATICS ARE INTENDED TO SHOW ONLY BASIC CONCEPTS AND GENERAL INFORMATION. THE COMPLETION OF YOUR SCOPE OF WORK SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ALL APPROVALS MUST BE OBTAINED FROM THE ARCHITECT AND/OR OWNER.
- YOU WILL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS NECESSARY TO PROVIDE A COMPLETE SYSTEM INSTALLATION WHETHER SUGGESTED OR NOT.
- ANY WORK COMPLETED OUTSIDE OF THE TYPE, TIME, CAUTION, DRAFT STOPS, FASTENERS, ANCHORS, EMBLEMENTS, DUMPSTERS, DODDS CLEAN UP, ETC. ARE YOUR RESPONSIBILITY. ANY WORK NOT COMPLETED IN A TIMELY MANNER, OR INCOMPLETE WORK WILL BE BILLED BACK TO THE CONTRACTOR AND/OR SUBCONTRACTOR, AT THE OWNER'S DISCRETION.





2  
1/8" = 1'-0"



1  
1/8" = 1'-0"

| EXTERIOR FINISH INDEX |  | REFERENCE NOTES       |   |
|-----------------------|--|-----------------------|---|
| EF1                   | DRYVIT SYSTEMS - FINE FINISH (BM : WEIMARANER AF-155)    | A.                    | REFER TO DESIGN STANDARDS "SITE & BUILDING EXTERIOR" CHAPTER FOR MAIN REQUIREMENTS RELATED TO THIS SHEET.   |
| EF2                   | DRYVIT SYSTEM - FINE FINISH (BM : SHAKESPEARE TAN - 228) | B.                    | REFER TO EXTERIOR FINISH INDEX FOR MATERIALS AND COLORS, AND ALSO FOR TYPICAL FINISHES FOR EXTERIOR ELEMENTS SUCH AS TYPICAL LOUVER COLOR, DOOR FRAMES, PORTE COCHERE AND CANOPY STRUCTURE FINISHES, ETC. |
| EF3                   | DRYVIT SYSTEM - LIMESTONE HDP (WHITE DIAMOND)            | C.                    | REFER TO THE EXTERIOR SIGNAGE SPECIFICATIONS - EXTERIOR GRAPHICS PACKAGE FOR REQUIREMENTS RELATED TO SIGNAGE SHOWN ON THIS SHEET.   |
| SP1                   | NICHHA - FIBER CEMENT SIDING PANEL (CARRIAGE RED)        | <b>GENERAL NOTES</b>  |   |
| SP2                   | NICHHA - FIBER CEMENT SIDING PANEL (TOBACCO)             |                       |   |
| A1                    | ALUMINUM COPING (BM: BUCHHORN 987)                       | A.                    | BUILDING ELEVATIONS ARE APPROXIMATE AND WILL VARY BASED ON STRUCTURAL SYSTEM.   |
| A2                    | ALUMINUM COPING - COLOR TO MATCH ADJACENT SURFACE        | B.                    | PROVIDE CONTRACTOR WITH COLOR PRINTS OF ELEVATIONS FOR ADDITIONAL CLARITY.  |
| A3                    | ALUMINUM COPING (WHITE DIAMOND)                          | C.                    | CONTINUE FINISH TO INSIDE CORNER VERSUS FINISH TRANSITIONS AT OUTSIDE CORNER.   |
| R1                    | MEMBRANE ROOFING - SLOPED (WHITE)                        | D.                    | ALL DOWNSPOUTS MUST HAVE CLEANOUTS.   |
| R2                    | MEMBRANE ROOFING (WHITE)                                 | E.                    | REVIEW FIBER CEMENT BOARD PANELS WHICH ARE MANUFACTURED IN METRIC SIZES TO COORDINATE WITH IMPERIAL (FEET AND INCHES) DIMENSIONS OF STRUCTURE. PAY PARTICULAR ATTENTION TO WINDOW SIZES.                  |
| PT1                   | PAINTED SURFACE (TO MATCH: WHITE DIAMOND)                | F.                    | PROVIDE INTERNAL DOWNSPOUTS, GUTTERS, ROOF DRAINS AND OVERFLOWS AS REQUIRED FOR LOCAL RAINFALL. PROVIDE SECONDARY OVERFLOWS TO DAYLIGHT IN AREAS THAT WILL NOT DRAIN ACROSS WALKING SURFACES.             |
| PT2                   | PAINTED SURFACE  | <b>CRITERIA NOTES</b> |   |

CRITERIA NOTES ARE SHOWN CATEGORIZED BY DISCIPLINE AND COULD BE INTERPRETED TO APPLY TO MORE THAN ONE DISCIPLINE. FOR EFFICIENCY, NOTES ARE ONLY SHOWN UNDER ONE PREDOMINANT DISCIPLINE. CONSULTANTS SHOULD FAMILIARIZE THEMSELVES WITH ALL CRITERIA NOTES.

**A0 ARCHITECTURAL**

A22 REFER TO GUESTROOM PLANS FOR HOLD-TO DIMENSIONS BETWEEN PTAC UNIT AND INTERIOR WALL NOTED

A36 TYPICAL GUESTROOM WINDOW

A37 CHANNEL LETTER SIGNAGE AT FACE OF BUILDING. SEE EXTERIOR SIGNAGE SPECIFICATIONS - EXTERIOR SIGNAGE PACKAGE FOR REQUIREMENTS. PROVIDE ADEQUATE BLOCKING BEHIND WALL FOR SIGNAGE

**E0 ENGINEERING**

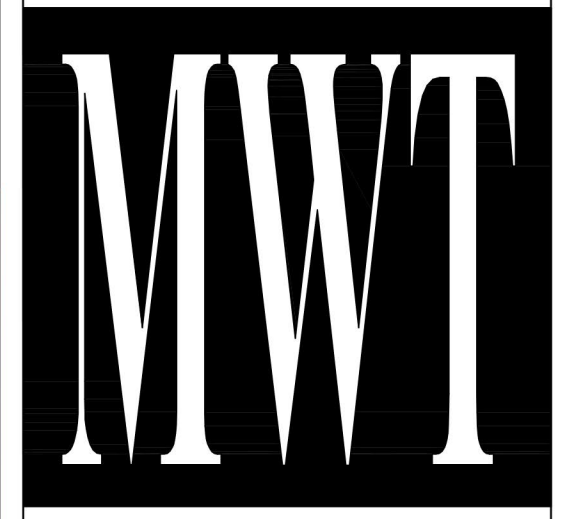
E34 PTAC LOUVER INTEGRAL WITH WINDOW FRAME. FINISH TO MATCH WINDOW FRAME.

E35 AT EXTERIOR SIGNAGE PROVIDE ACCESS FOR MOUNTING ELECTRICAL COMPONENTS AND MAKING FINAL ELECTRICAL CONNECTIONS. PROVIDE ADEQUATE DEDICATED CIRCUITRY BROUGHT TO SIGN LOCATION FROM ELECTRICAL PANEL.

E61 FOR STANDARD LINEAR LIGHT FIXTURE (Z94). PROVIDE CONTINUOUS BAND OF LIGHT TO ILLUMINATE BOTTOM OF SOFFIT.

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**CONSTRUCTION DRAWING REVIEW (90%)**



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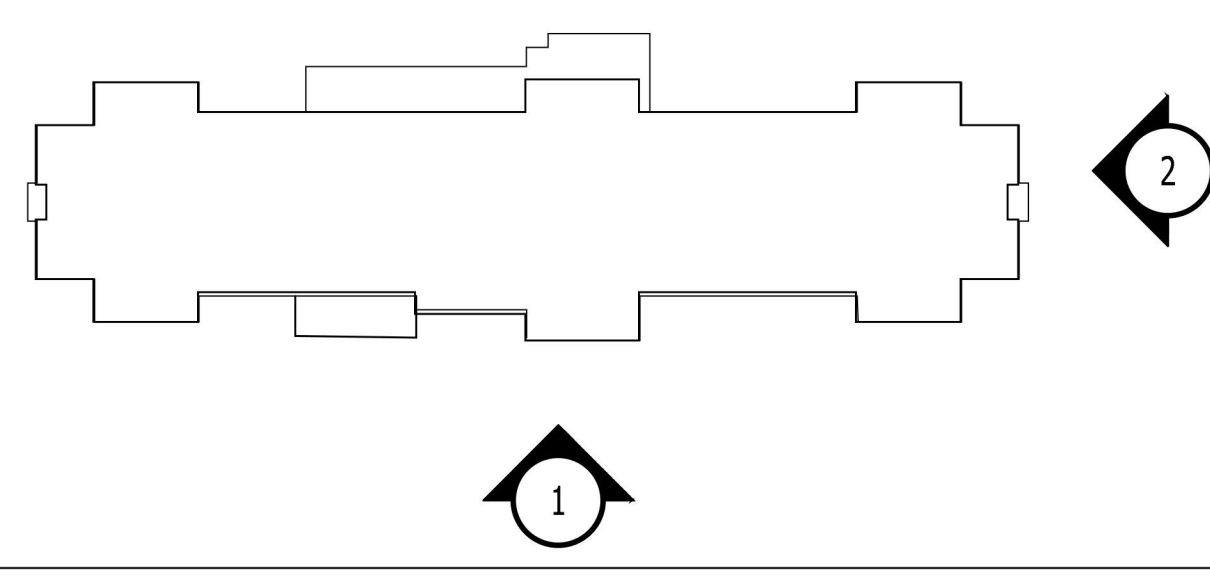
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**TOWNEPLACE SUITES BY MARRIOTT**  
SUNRISE VILLAGE  
CHICO, CA

PROJECT NO.: 19-005  
DATE: 08-26-19  
DRAWN BY: AL  
CHKD BY: MWT

**EXTERIOR ELEVATIONS**

**SHEET A200**  
19005\_A200.DWG



**IMPORTANT NOTICE TO ALL CONTRACTORS AND OR SUBCONTRACTORS (COMPLETE BUILDING SYSTEMS AND OR INSTALLATIONS)**

IF AWARDED A CONTRACT FOR ANY PORTION OF WORK NEEDED TO COMPLETE THIS PROJECT YOU ARE AGREEING TO THE FOLLOWING TERMS:

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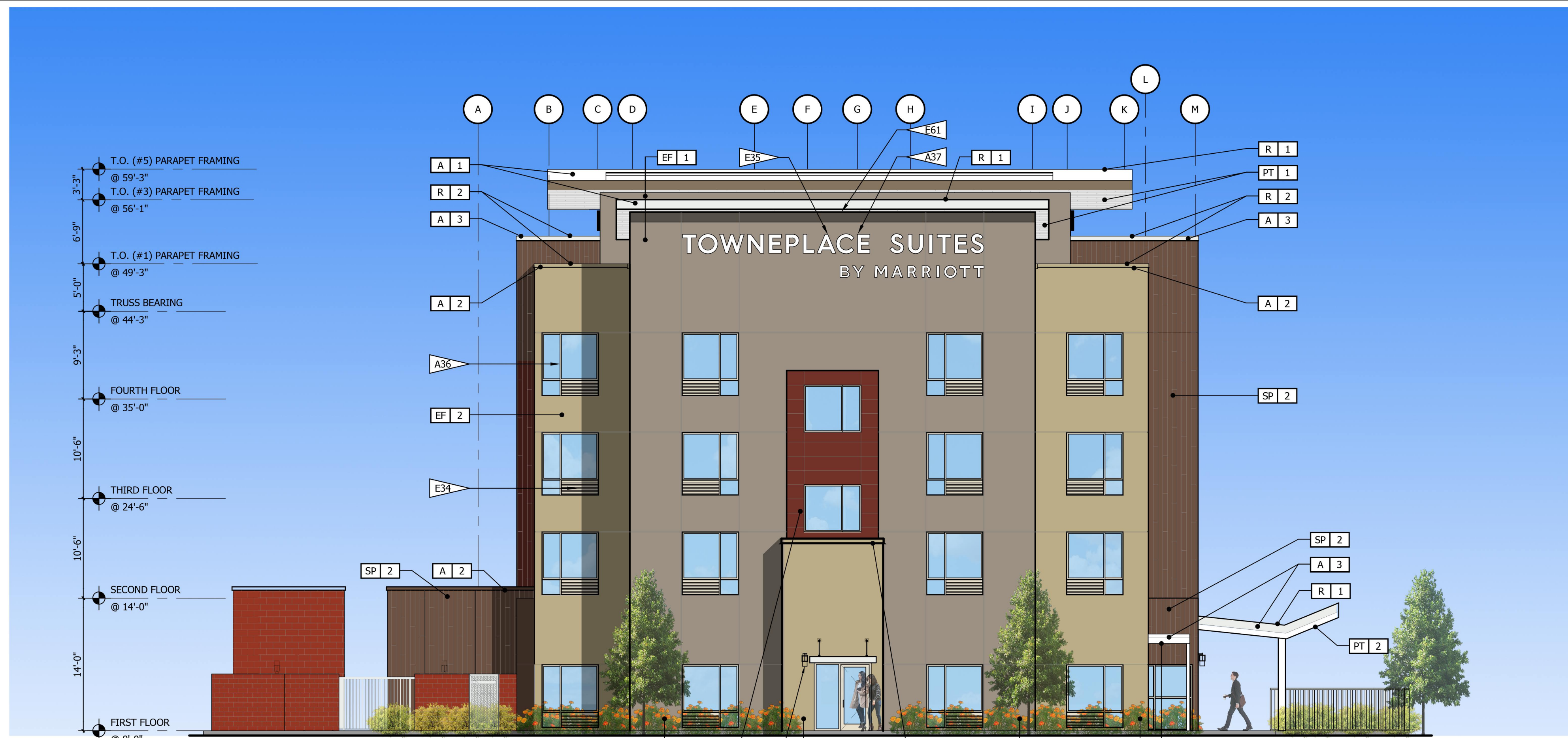
2. YOU AGREE TO PROVIDE A COMPLETE DESIGN SOLUTION BASED ON THE ARCHITECT'S INTENTED DESIGN FOR THIS PROJECT.

3. YOU WILL BE RESPONSIBLE FOR ALL THE STEPS, PROCESS, MATERIAL, PRODUCTS, EQUIPMENT, PARTS AND APPROVALS NECESSARY TO PROVIDE A COMPLETE AND WARRANTY SYSTEM OR INSTALLATION THAT DOES NOT REQUIRE ANOTHER CONTRACTOR OR PRODUCT TO COMPLETE YOUR SCOPE OF WORK.

4. YOU WILL NOT JUST RELY ON THE DESIGNER'S DESIGN, BUT YOU WILL CONDUCT YOUR OWN INVESTIGATION OF ALL THE DESIGN INFORMATION AND ON PLUMBING DRAWINGS, ALSO NOTE THESE DRAWINGS ARE SCHEMATIC AND ARE INTENDED TO SHOW ONLY BASIC CONCEPTS AND GENERAL INFORMATION. THE COMPLETION OF THE PROJECT WILL REQUIRE THE DESIGNER'S DESIGN, PROCESS, MATERIAL, PRODUCTS, LABOR, EQUIPMENT, PARTS AND OR APPROVALS NECESSARY TO PROVIDE A COMPLETE SYSTEM INSTALLATION WHETHER SUGGESTED OR NOT.

5. YOUR DESIGN AND INSTALLATION SHALL BE YOUR RESPONSIBILITY. ANY WORK NOT COMPLETED IN A TIMELY MANNER, OR INCOMPLETE WORK WILL BE BILLED BACK TO THE CONTRACTOR AND OR SUBCONTRACTOR, AT THE OWNER'S DISCRETION.





2 EXTERIOR ELEVATION  
1/8" = 1'-0"

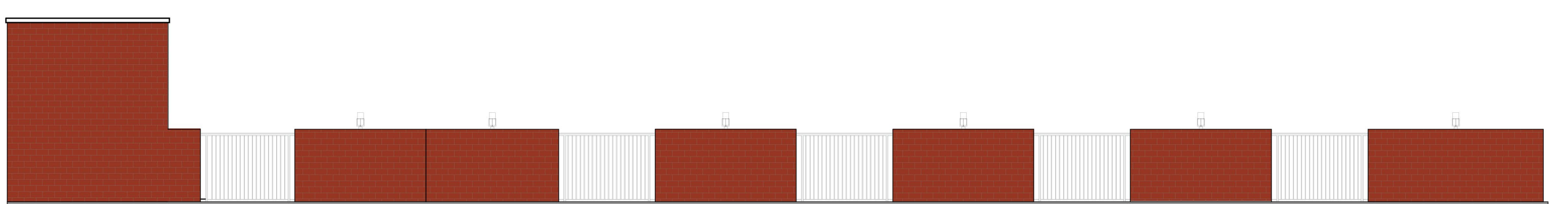
| EXTERIOR FINISH INDEX   |   | REFERENCE NOTES   |   |
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| EF1                     | DRYVIT SYSTEMS - FINE FINISH (BM : WEIMARANER AF-155)   | A.  | REFER TO DESIGN STANDARDS "SITE & BUILDING EXTERIOR" CHAPTER FOR MAIN REQUIREMENTS RELATED TO THIS SHEET.   |
| EF2                     | DRYVIT SYSTEM - FINE FINISH (BM : SHAKESPEARE TAN - 228)  | B.  | REFER TO EXTERIOR FINISH INDEX FOR MATERIALS AND COLORS; AND ALSO FOR TYPICAL FINISHES FOR EXTERIOR ELEMENTS SUCH AS TYPICAL LOUVER COLOR, DOOR FRAMES, PORTE COCHERE AND CANOPY STRUCTURE FINISHES, ETC. |
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| SP1                     | NICHHA - FIBER CEMENT SIDING PANEL (CARRIAGE RED)   | <b>GENERAL NOTES</b>  |   |
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| PT1                     | PAINTED SURFACE (TO MATCH: WHITE DIAMOND)   | <b>CRITERIA NOTES</b>   |   |
| PT2                     | PAINTED SURFACE   | CRITERIA NOTES ARE SHOWN CATEGORIZED BY DISCIPLINE AND COULD BE INTERPRETED TO APPLY TO MORE THAN ONE DISCIPLINE. FOR EFFICIENCY, NOTES ARE ONLY SHOWN UNDER ONE PREDOMINANT DISCIPLINE. CONSULTANTS SHOULD FAMILIARIZE THEMSELVES WITH ALL CRITERIA NOTES. |   |
| <b>AD ARCHITECTURAL</b> |   |   |   |
| A22                     | REFER TO GUESTROOM PLANS FOR HOLD-TO DIMENSIONS BETWEEN PTAC UNIT AND INTERIOR WALL NOTED   |   |   |
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| <b>EO ENGINEERING</b>   |   |   |   |
| E34                     | PTAC LOUVER INTEGRAL WITH WINDOW FRAME. FINISH TO MATCH WINDOW FRAME.   |   |   |
| E35                     | AT EXTERIOR SIGNAGE PROVIDE ACCESS FOR MOUNTING ELECTRICAL COMPONENTS AND MAKING FINAL ELECTRICAL CONNECTIONS. PROVIDE ADEQUATE DEDICATED CIRCUITRY BROUGHT TO SIGN LOCATION FROM ELECTRICAL PANEL. |   |   |
| E61                     | FOR STANDARD LINEAR LIGHT FIXTURE (294), PROVIDE CONTINUOUS BAND OF LIGHT TO ILLUMINATE BOTTOM OF SOFFIT.   |   |   |

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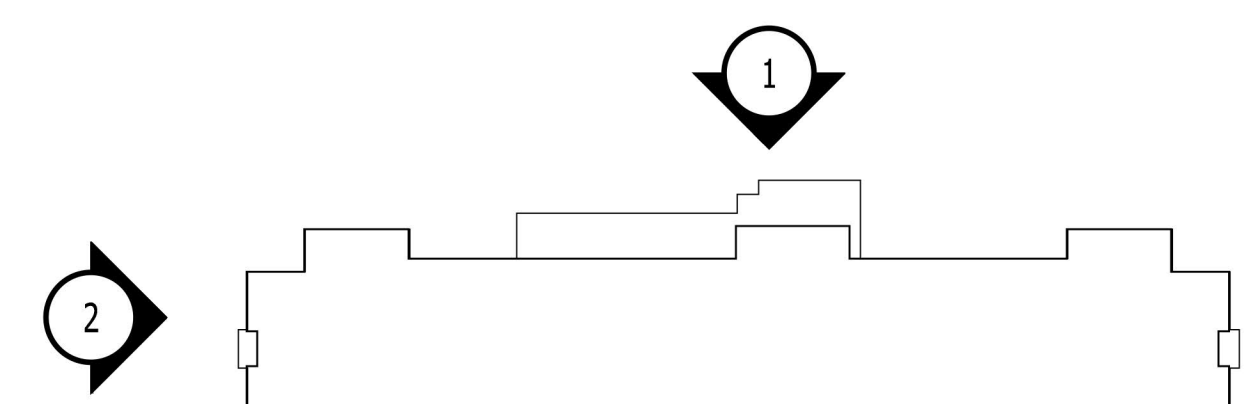
CONSTRUCTION DRAWING REVIEW (90%)



1 EXTERIOR ELEVATION  
1/8" = 1'-0"



1.1 POOL WALL ELEVATION  
1/8" = 1'-0"



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- YOUR BID INCLUDES ALL THE COSTS: MATERIALS, PRODUCTS, COVERING PERMITS AND APPROVALS NECESSARY TO PROVIDE A COMPLETE AND WARRANTY SYSTEM OR INSTALLATION THAT DOES NOT REQUIRE ANOTHER CONTRACTOR OR PRODUCT TO COMPLETE YOUR SCOPE OF WORK.
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- AS AN EXAMPLE ITEMS AND/OR TASKS LIKE: FIRE CAULK, DRAFT STOP, FASTENERS, ANCHORS, EMBLEMMENTS, DUMPSTERS, DEBRIS CLEAN UP, ETC. ARE YOUR RESPONSIBILITY. ANY WORK NOT COMPLETED IN A TIMELY MANNER, OR INCOMPLETE WORK WILL BE HELD BACK TO THE CONTRACTOR AND/OR SUBCONTRACTOR, AT THE OWNER'S DISCRETION.



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TOWNEPLACE SUITES  
BY MARRIOTT

SUNRISE VILLAGE  
CHICO, CA

PROJECT NO.: 19-005  
DATE: 08-26-19  
DRAWN BY: AL  
CHKD. BY: MWT

EXTERIOR ELEVATION

SHEET A201  
19005\_A201.DWG

Attachment G





TOWNEPLACE SUITES

HANDICAPPED  
PARKING ONLY





TOWNEPLACE SUITES  
BY

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PARKING  
ONLY

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PARKING  
ONLY

Attachment H



TOWNEPLACE SUITES  
BY MARRIOTT







TOWNEPLACE SUITES  
BY MARRIOTT





TOWNEPLACE SUITES  
BY MARRIOTT





TOWNEPLACE SUITES

ACCESSIBLE PARKING ONLY

ACCESSIBLE PARKING ONLY

ACCESSIBLE PARKING ONLY

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TOWNEPLACE SUITES  
BY MARRIOTT

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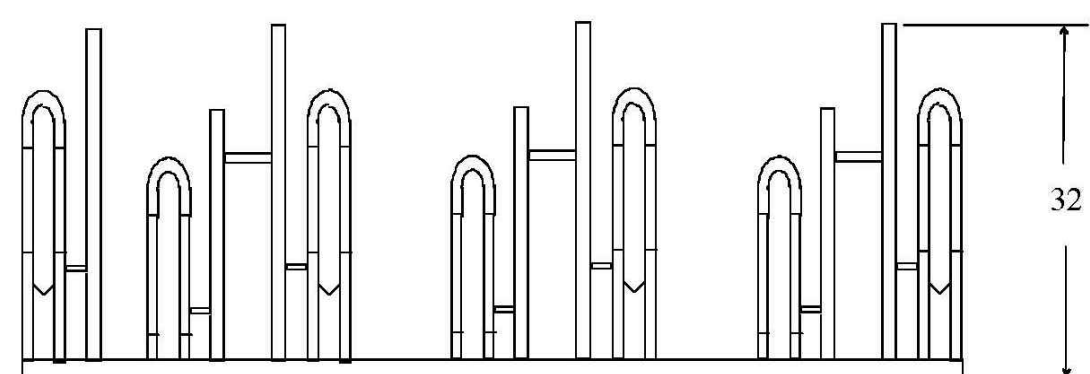
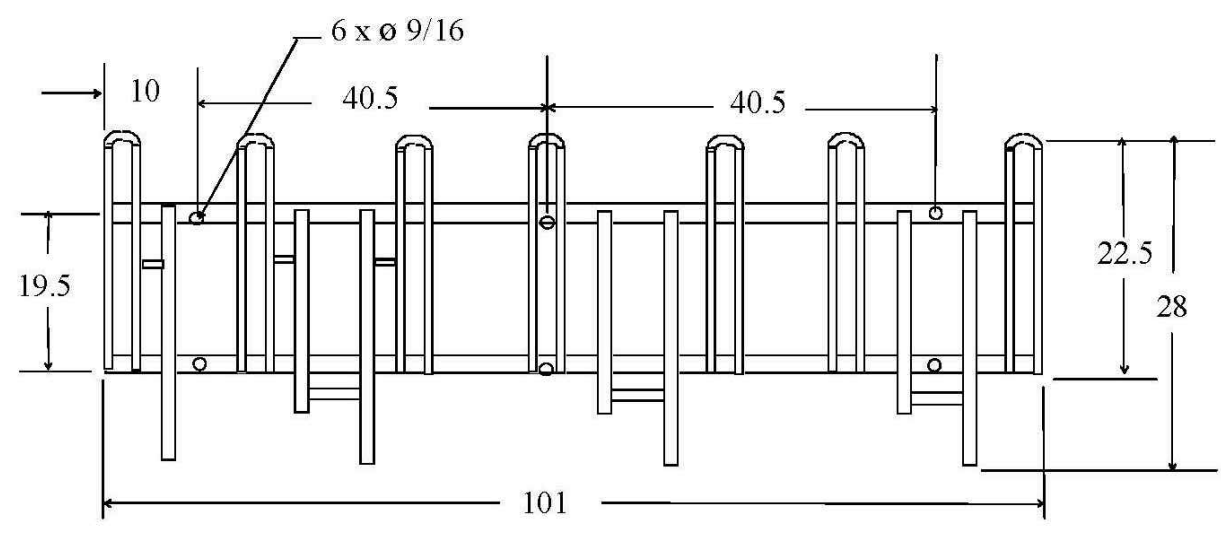
TOWNEPLACE SUITES  
BY MARRIOTT





### Specifications

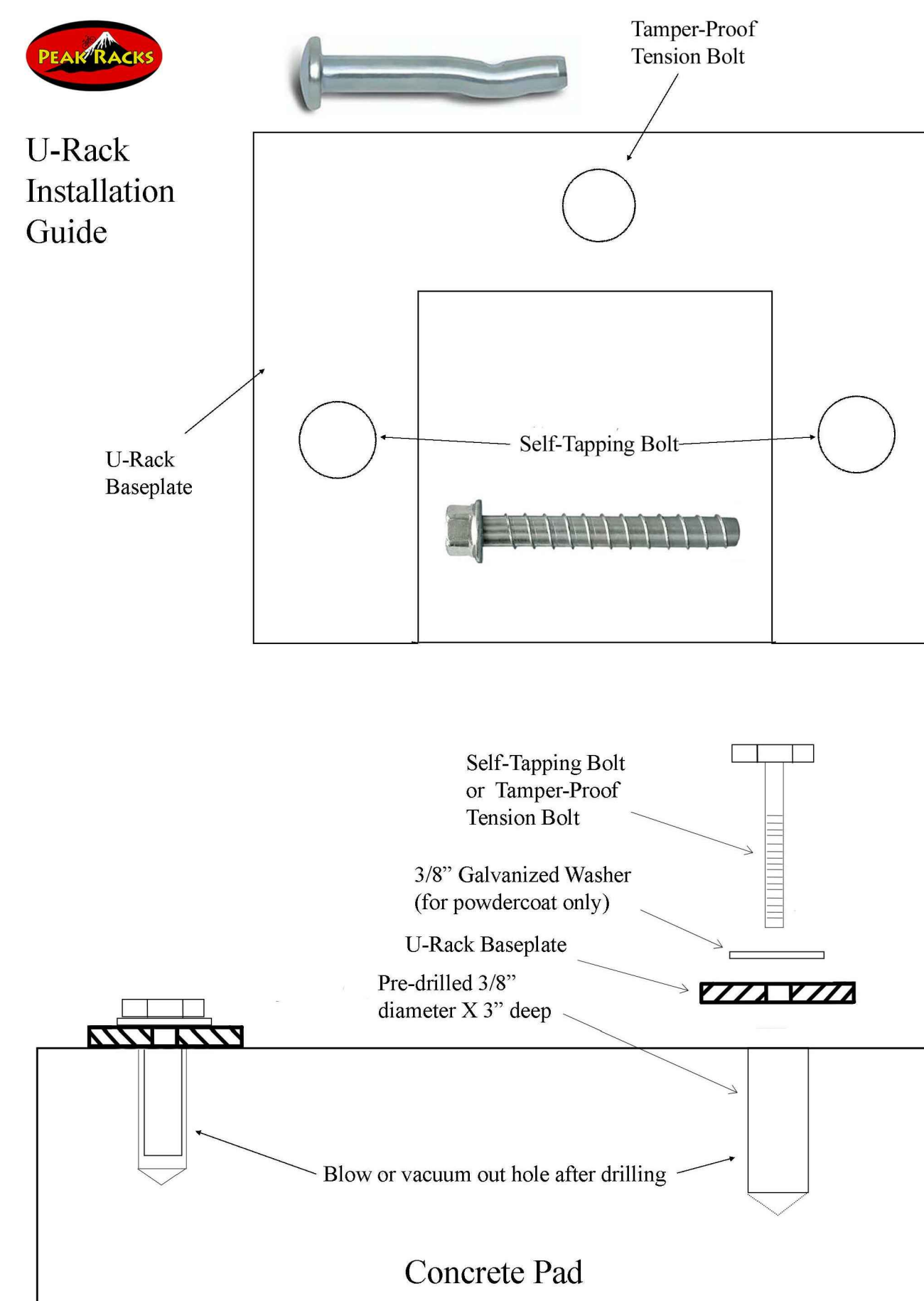
**7-bike Capacity:** Minimum Footprint with Bikes Inserted: 125" x 72"  
 Campus Standard 117 Lbs (allows 12" on each side for handlebars)  
 Rack with Plaque 141 Lbs  
 Rack with Plaque Heavy Duty Base 150 Lbs



| Materials  | Finish  | Installation   |
|--|---|--|
| Hot rolled ASTM A36 solid steel bars welded with GMAW (MIG).   | Standard Finish is Hot Dipped Galvanized for superior durability. Optional powder coat is available with sand blasting, primer, color and clear coat (for additional cost). | This rack is free standing and can function without mounting, however, it is recommended that it be secured on concrete with six tamper-resistant stainless steel anchors. (available from Peak Racks) |
| Standard Base: 1/8" angle<br>Heavy Base: 3/16" angle<br>Wheel Holders: 1/2" diameter<br>Wheel Troughs: 1/8" angle<br>Locking Bars: 5/8" diameter |   |  |

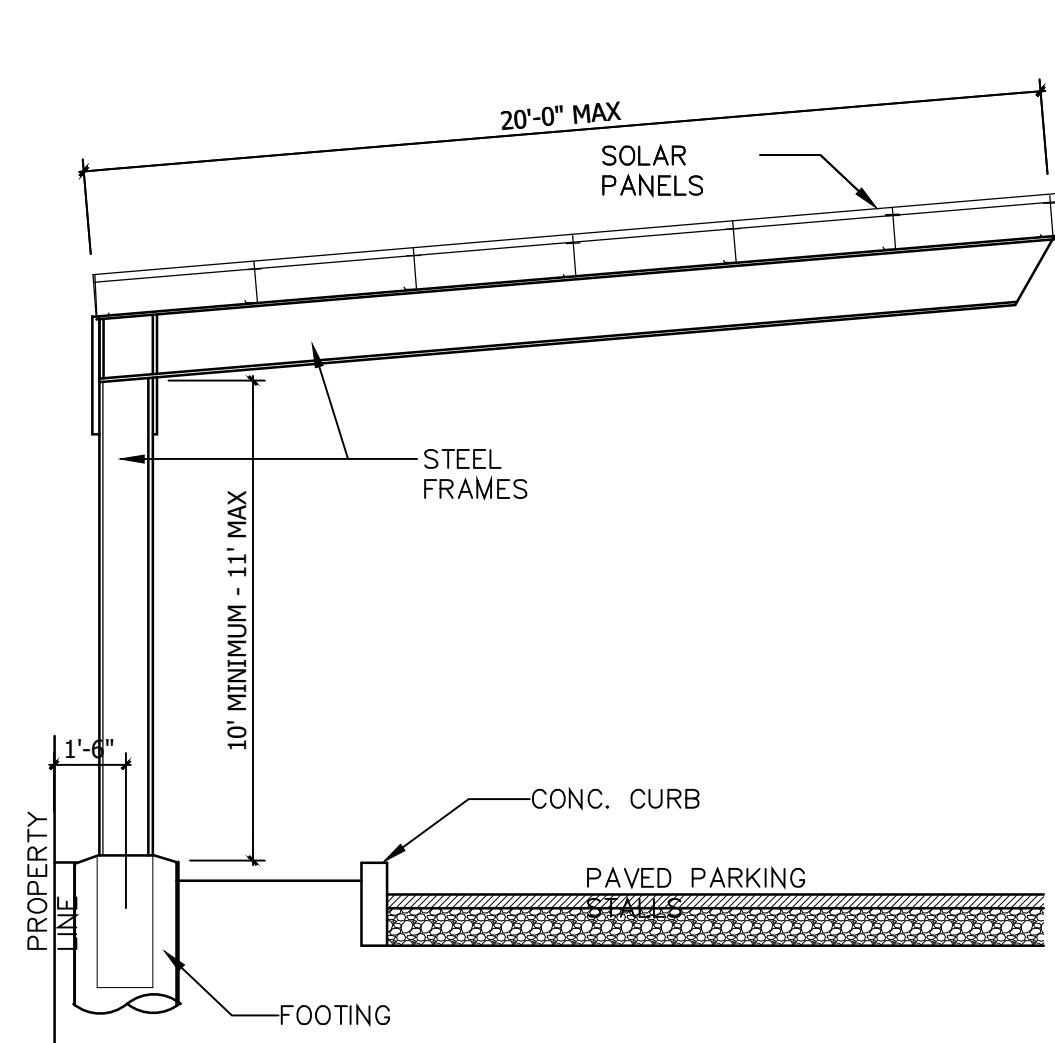


### U-Rack Installation Guide



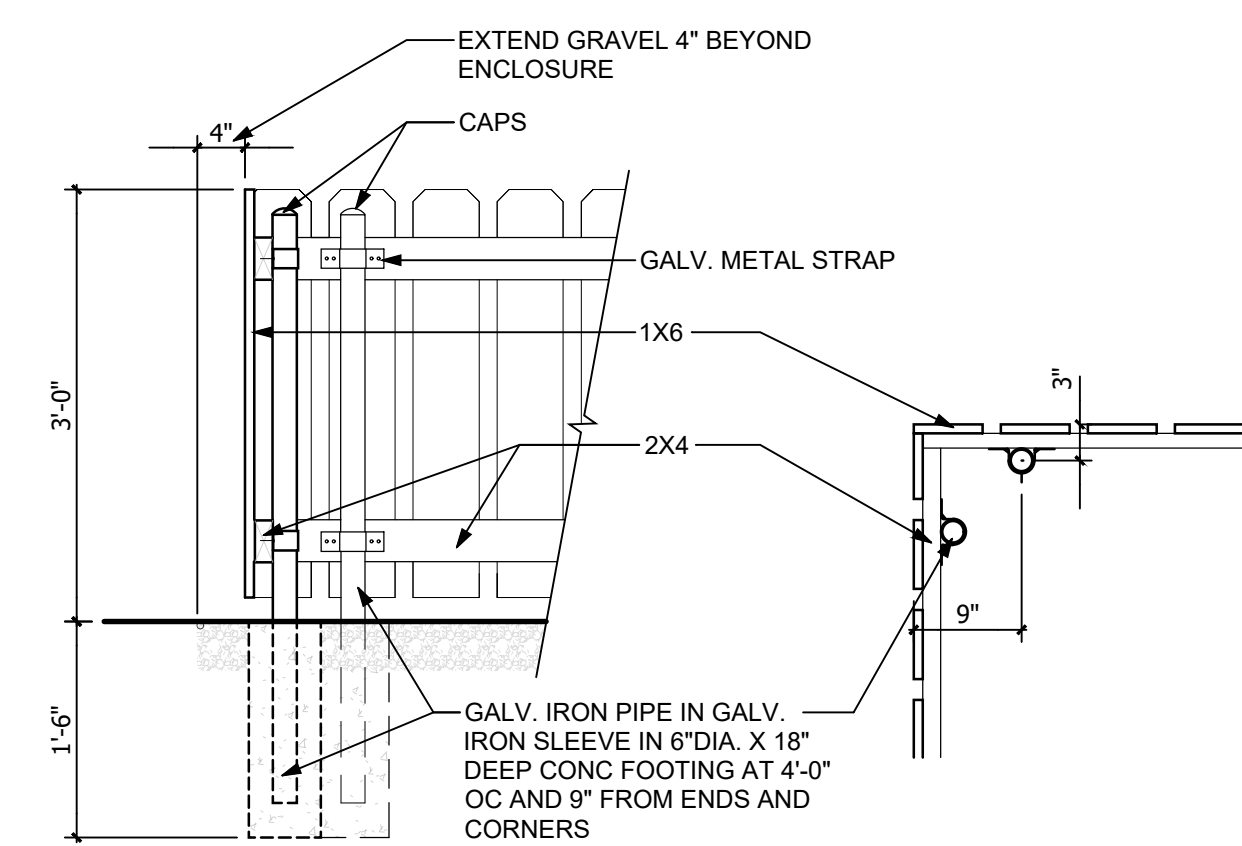
### 12 BICYCLE RACK SPECIFICATION

SCALE: NTS



### 10 SOLAR CAR CANOPIES (OPTIONAL)

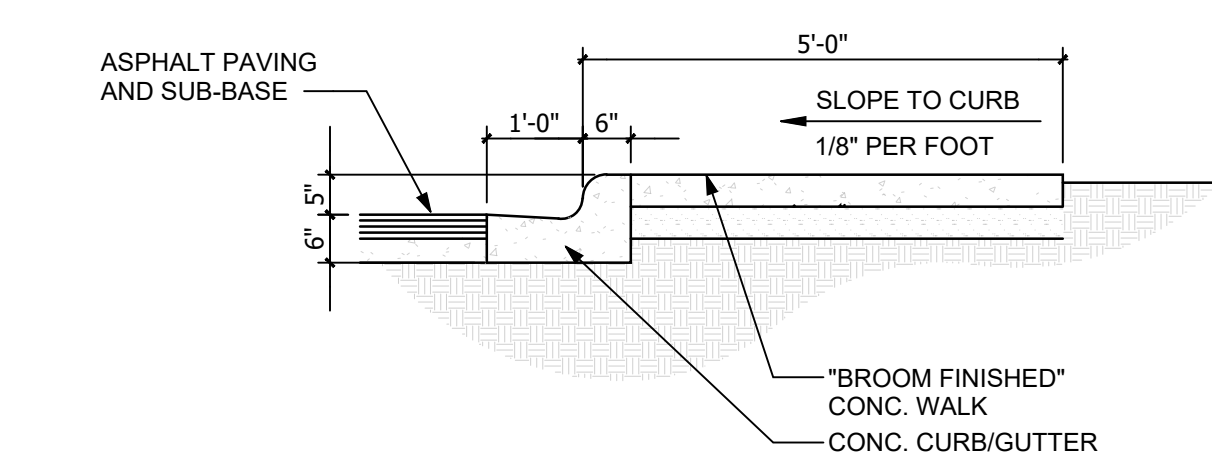
1/4" = 1'-0"



ELEVATION PLAN

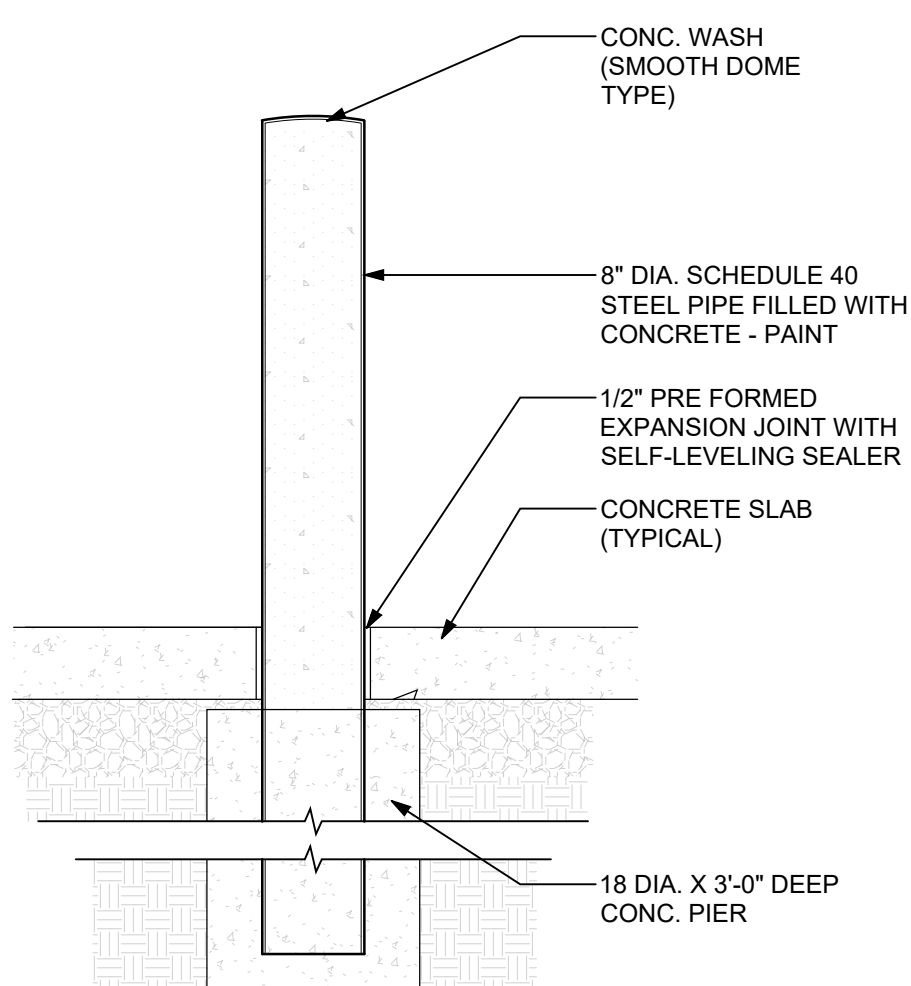
### 9 CONDENSER FENCE ELEVATION

3/4" = 1'-0"



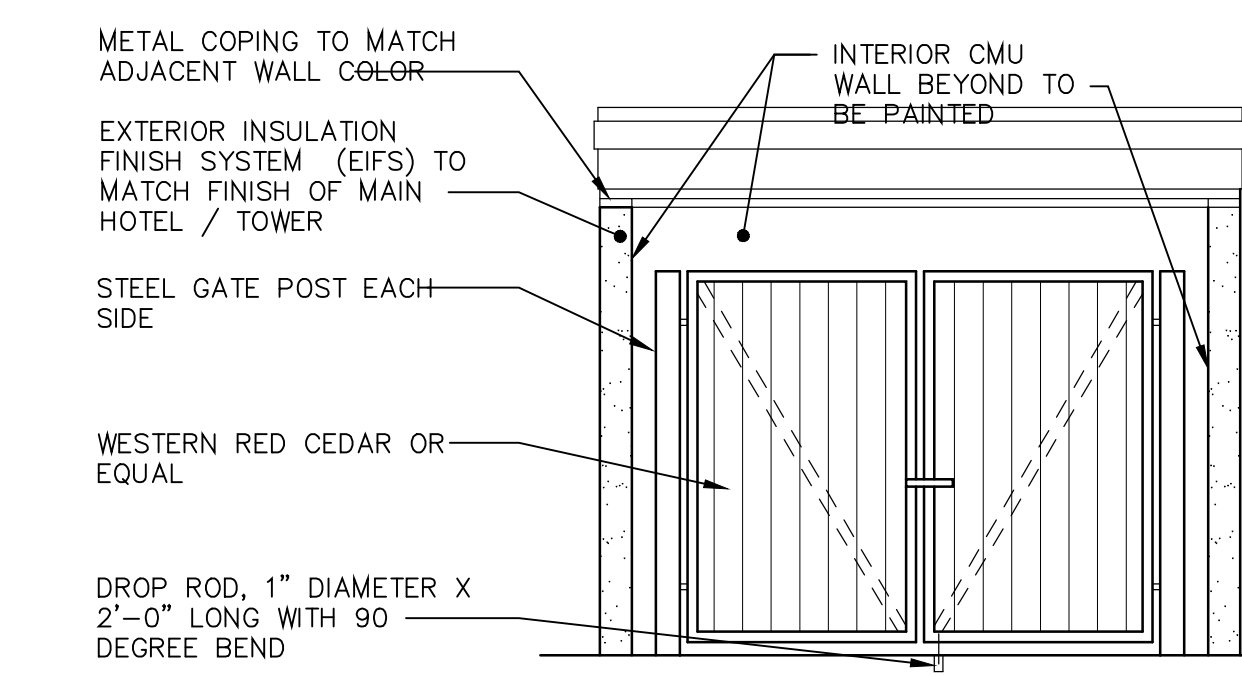
### 8 SIDEWALK CURB DETAIL

1/2" = 1'-0"



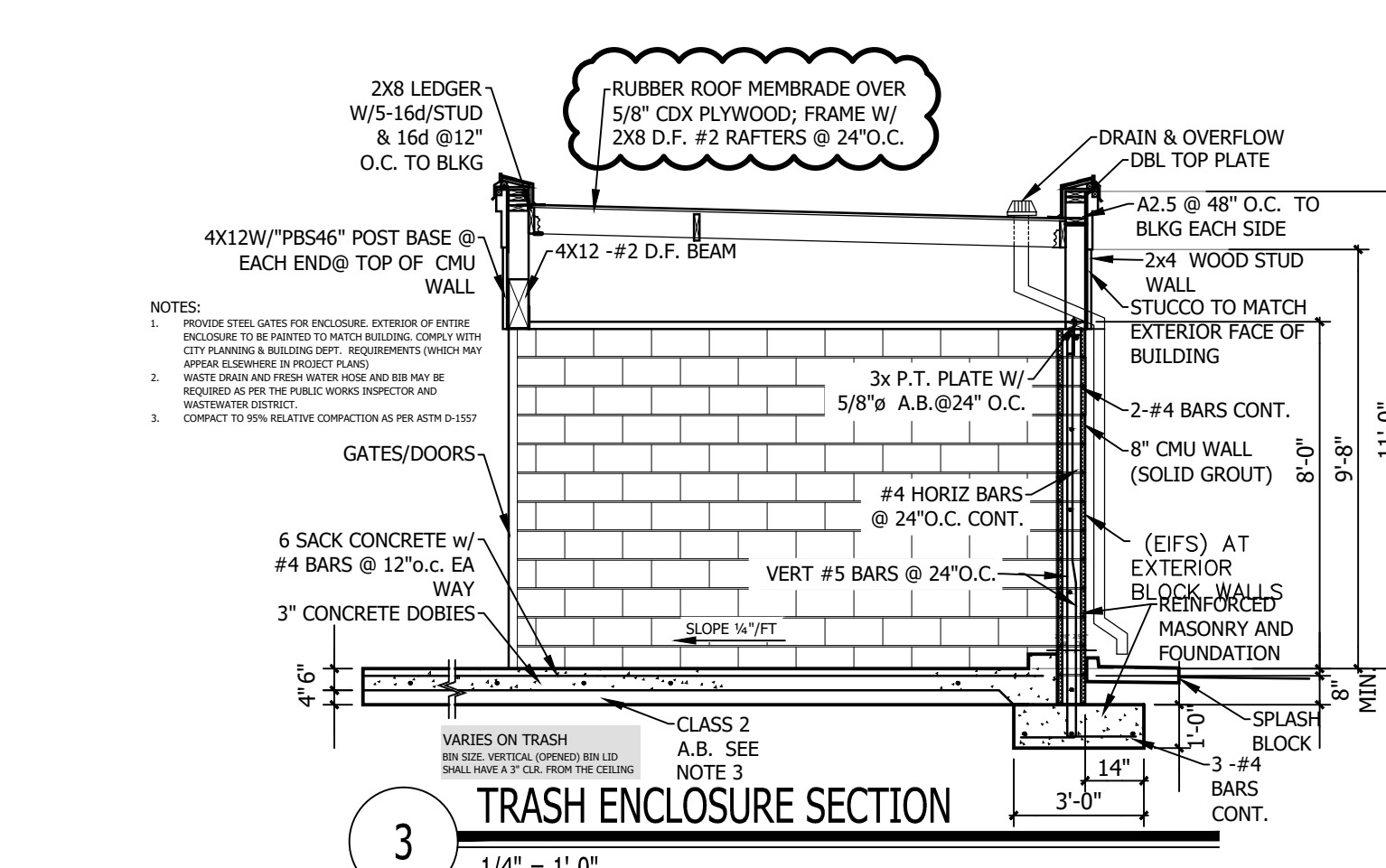
### 5 BOLLARD DETAIL

3/4" = 1'-0"



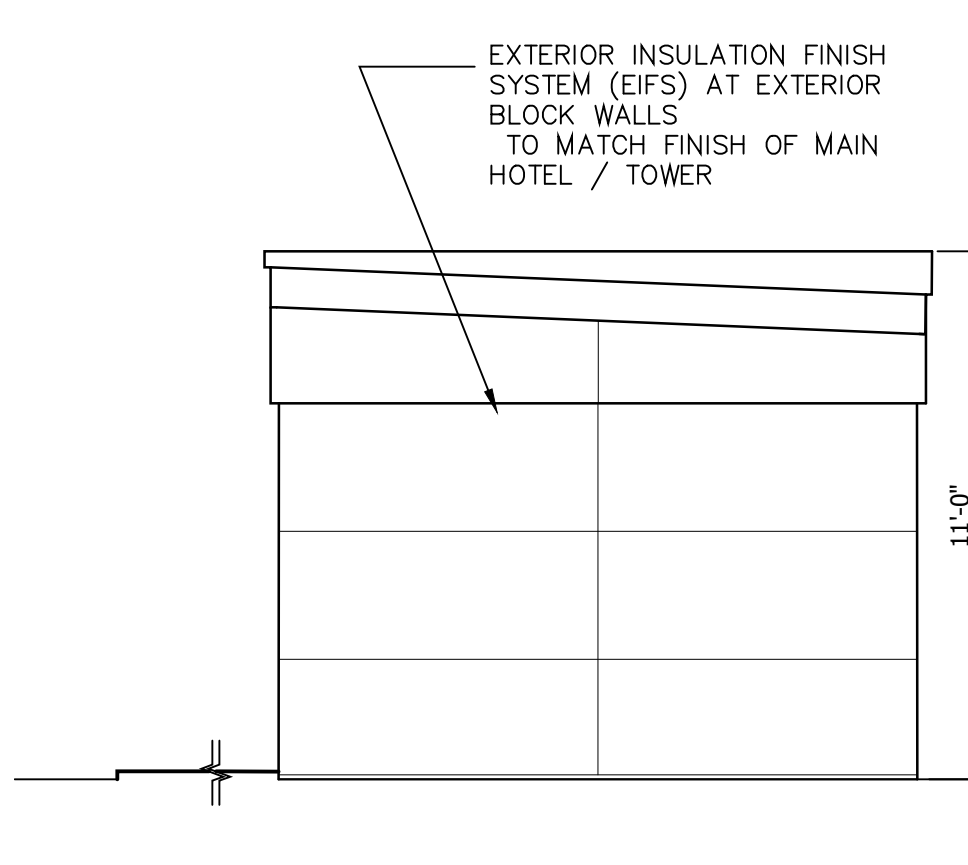
### 4 TRASH ENCLOSURE GATE OPEN

1/4" = 1'-0"



### 3 TRASH ENCLOSURE SECTION

1/4" = 1'-0"

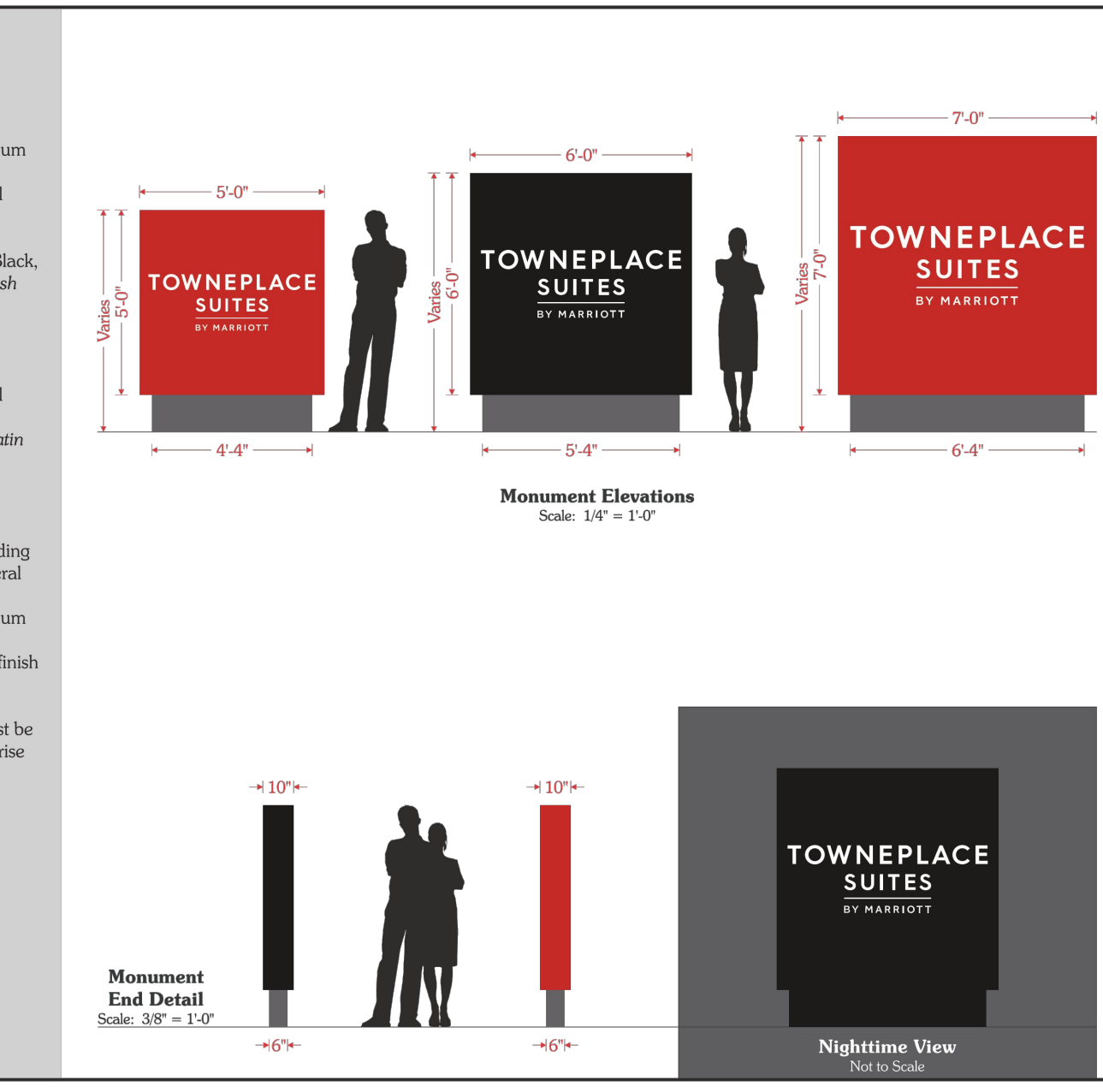


### 2 TRASH ENCLOSURE ELEVATION

1/4" = 1'-0"

### 3 monument signs

**MONUMENT SPECIFICATIONS**  
**Monument Specifications:**  
 Construction: Aluminum angle frame with .125" aluminum .800x.800x.800 face  
 Face Construction: Routed aluminum with backer panel  
 Backer Panel: Beaded face  
 Illumination: GE White LED's  
 Exterior Finish: Paint Mathews MAP/LVS929 Carbon Black, satin finish (or Pantone® 1795 C red, satin finish for red option)  
 Interior Finish: Paint reflective white  
**Monument Face Specifications:**  
 Face Construction: Routed aluminum with backer panel  
 Face Decoration: Paint Mathews MAP/LVS929 Carbon Black, satin finish (or Pantone® 1795 C red, satin finish for red option)  
 Backer Panel: .118" white solar grade polycarbonate  
**Pole Cover Specifications:**  
 \*Patented method to have monument base match building hardcap (stone, brick, etc., provided by general contractor)  
 Construction: Aluminum angle frame with .080" aluminum slat  
 Exterior Finish: Paint Pantone® Cool Gray 11 C, satin finish  
 \*\* If brand color is used, all other freestanding signs must be the brand color as well. This includes mid-rise signs, hi-rise signs and directional signs.



TOWNEPLACE SUITES

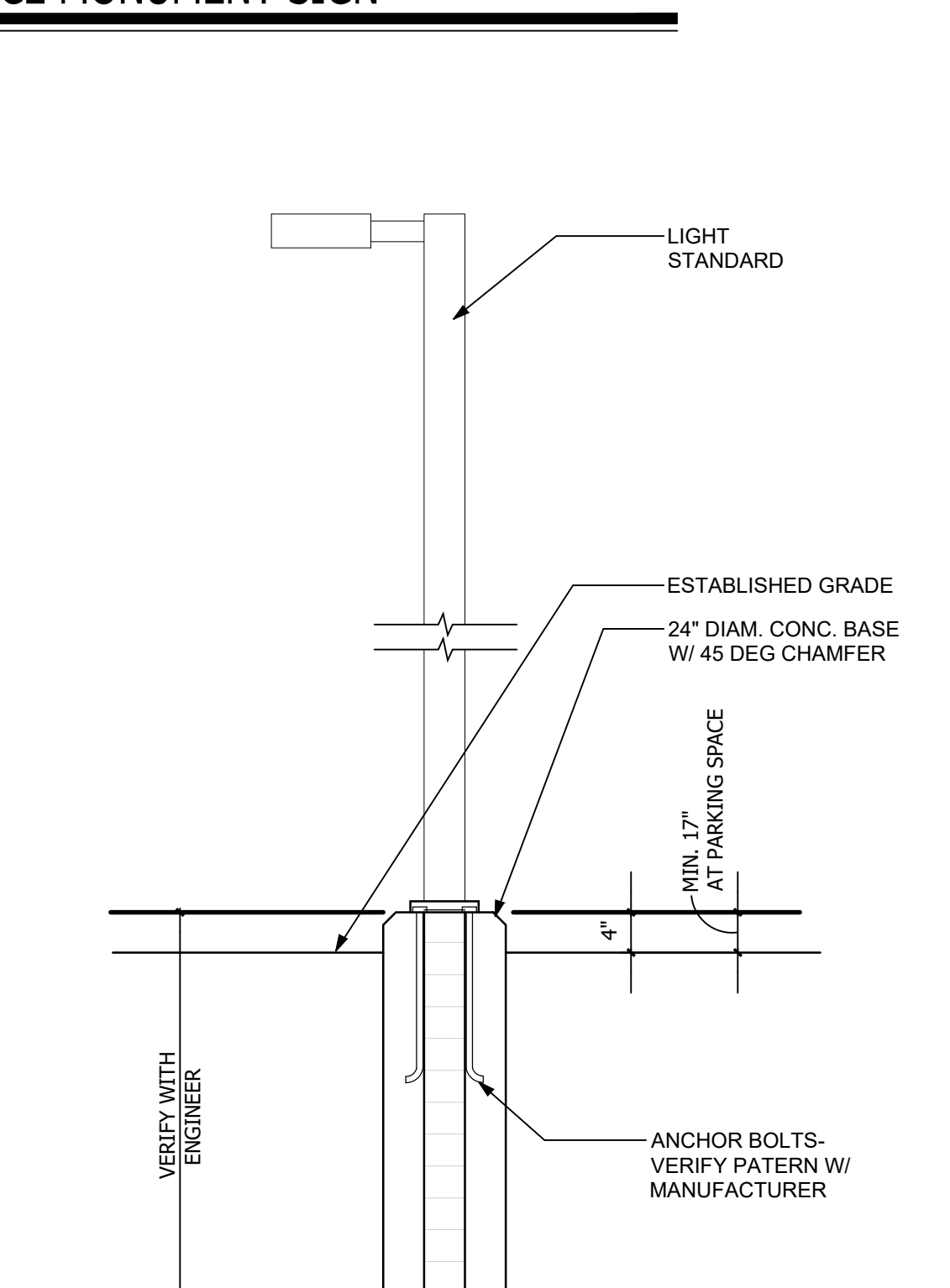
TOWNEPLACE SUITES

### 7 color specifications

- Paints**
  - Mathews MAP/LVS929 Carbon Black, satin finish
  - Pantone® 1795 C red, satin finish
  - Pantone® Cool Gray 11 C, satin finish
- Scotchcal® Films**
  - 3M 3635-222 black dual-color film
  - 3M 7725-22 black opaque film
  - 3M 3630-2528 red translucent film
  - 3M 3635-20B white blackout film
- Flexible Face Materials**
  - 3M Panagraphics III flexible face
- Acrylics**
  - 3/16" 2447 white acrylic
  - 3/16" 7328 white acrylic
- Polycarbonates**
  - .118" white solar grade polycarbonate

### 11 ENTRANCE MONUMENT SIGN

SCALE: NTS

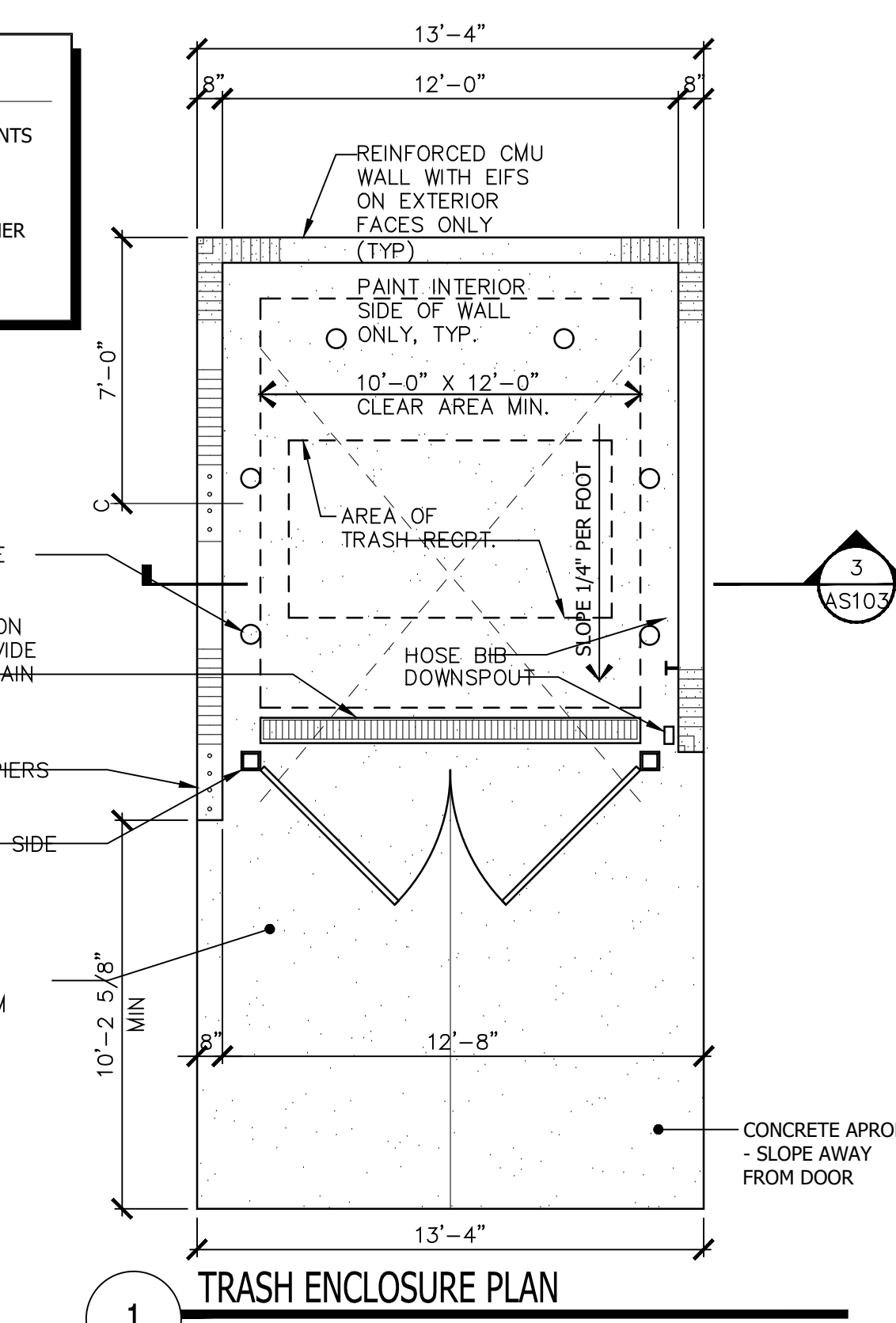


### 7 SITE LIGHT POLE

3/4" = 1'-0"

### 6 TRASH ENCLOSURE SECTION

3/4" = 1'-0"



### 1 TRASH ENCLOSURE PLAN

1/4" = 1'-0"

- NOTE:**
- VERIFY DRAINAGE REQUIREMENTS WITH HEALTH INSPECTOR
  - ADDITIONAL SPACE MAY BE NEEDED FOR RECYCLING. DESIGNER TO COORDINATE REQUIREMENTS WITH OWNER

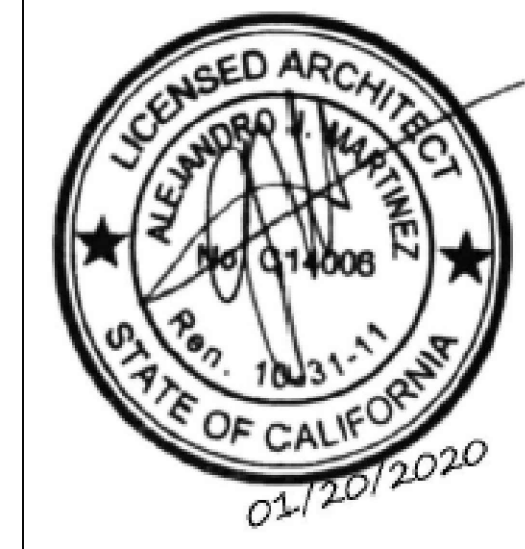
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CONSTRUCTION DRAWING REVIEW (90%)



HOTELS, HOUSING & MIXED USE

DRAWING QUESTIONS  
 CA@MWTUSA.COM  
 1-800-717-5816



IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS ACTING UNDER THE DIRECTION OF THE ARCHITECT, TO ALTER OR REPRODUCE THESE DRAWINGS IN ANY WAY.

**TOWNEPLACE SUITES**  
 BY HARRIOTT

SIERRA SUNRISE TERRACE  
 CHICO, CA

PROJECT NO.: 19-005  
 DATE: 08-26-19  
 DRAWN BY: AL  
 CHKD. BY: MWT

SITE DETAILS

**IMPORTANT NOTICE TO ALL CONTRACTORS AND OR SUBCONTRACTORS: (COMPLETE BUILDING SYSTEMS AND OR INSTALLATIONS)**  
 IF AWARDED A CONTRACT FOR ANY PORTION OF WORK NEEDED TO COMPLETE THIS PROJECT YOU ARE AGREEING TO THE FOLLOWING TERMS:  
 1. YOU ARE ASSUMED TO HAVE RECEIVED ALL NECESSARY SPECIFICATIONS AND INFORMATION FROM THE ARCHITECT FOR THE TYPE OF WORK AND SCOPE OF THE PROJECT.  
 2. YOU ARE ASSUMED TO HAVE RECEIVED ALL NECESSARY PERMITS AND APPROVALS FROM THE ARCHITECT AND TO OBTAIN ALL NECESSARY PERMITS AND APPROVALS FOR THE TYPE OF WORK AND SCOPE OF THE PROJECT.  
 3. YOU ARE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE ARCHITECT AND TO OBTAIN ALL NECESSARY PERMITS AND APPROVALS FOR THE TYPE OF WORK AND SCOPE OF THE PROJECT.  
 4. YOU ARE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE ARCHITECT AND TO OBTAIN ALL NECESSARY PERMITS AND APPROVALS FOR THE TYPE OF WORK AND SCOPE OF THE PROJECT.  
 5. YOU ARE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE ARCHITECT AND TO OBTAIN ALL NECESSARY PERMITS AND APPROVALS FOR THE TYPE OF WORK AND SCOPE OF THE PROJECT.  
 6. YOU ARE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE ARCHITECT AND TO OBTAIN ALL NECESSARY PERMITS AND APPROVALS FOR THE TYPE OF WORK AND SCOPE OF THE PROJECT.



# TOWNSHIP SUITES

## BUILDING & CAMERAS LOCATIONS

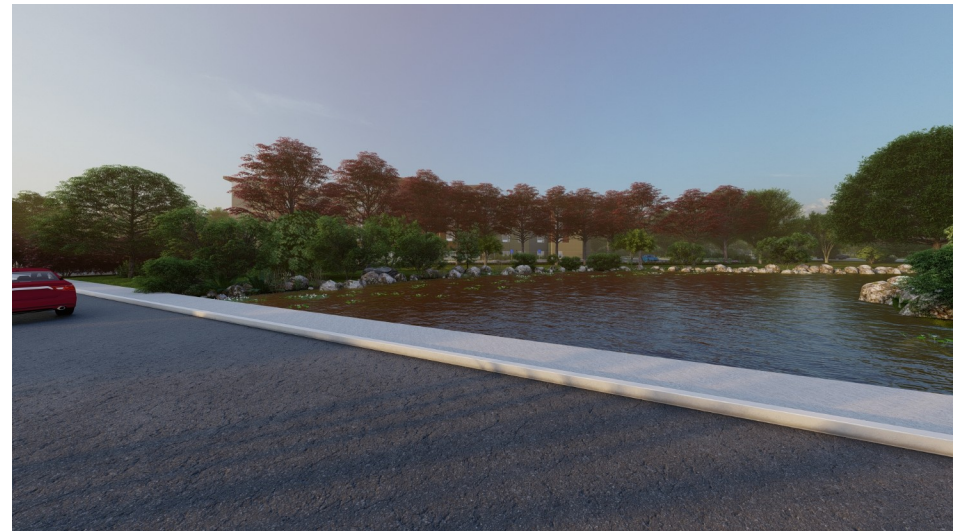
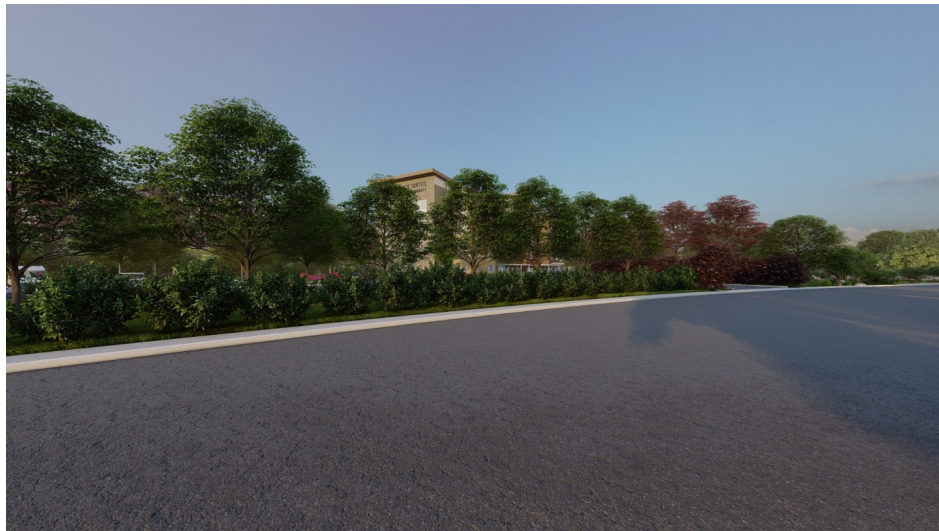
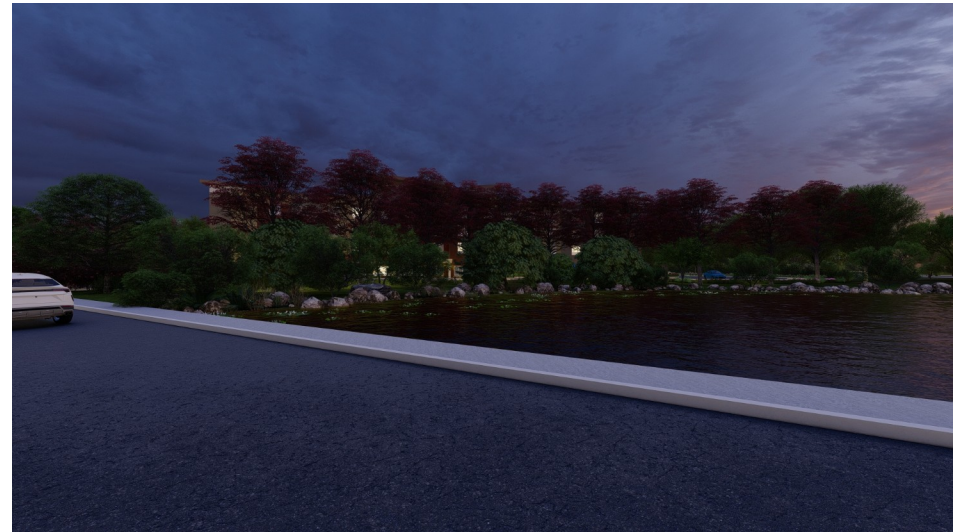




VIEW 1

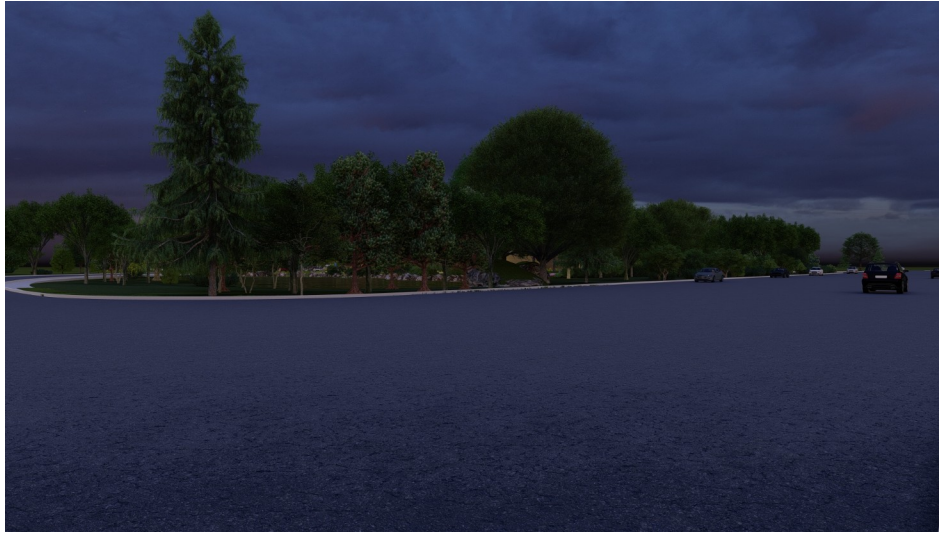


VIEW 2





VIEW 3



VIEW 4



VIEW 5





**PLANT SCHEDULE**

| SHRUBS                           | BOTANICAL NAME                            | COMMON NAME                 | SIZE  | QTY |
|----------------------------------|---|-----------------------------|-------|-----|
| ⊙                                | ARCTOSTAPHYLOS DENSIFLORA 'HOWARD MCHINN' | HOWARD MCHINN MANZANTA      | 5 GAL | 28  |
| ⊙                                | BERBERIS THUNBERGII 'CRIMSON PYGMY'       | CRIMSON PYGMY BARBERRY      | 5 GAL | 17  |
| ⊙                                | CISTUS X PURPUREUS                        | ORCHID SPOT ROCKROSE        | 5 GAL | 33  |
| ⊙                                | LOROPETALUM CHINENSE RUBRUM 'RAZZLEBERRI' | RAZZLEBERRI FRINGE FLOWER   | 5 GAL | 15  |
| ⊙                                | MAHONIA AQUIFOLIUM 'COMPACTA'             | COMPACT OREGON GRAPE        | 5 GAL | 36  |
| ⊙                                | NANDINA DOMESTICA 'GULF STREAM' TM        | GULF STREAM HEAVENLY BAMBOO | 5 GAL | 66  |
| ⊙                                | OLEA EUROPAEA 'LITTLE OLLIE' TM           | LITTLE OLLIE OLIVE          | 5 GAL | 10  |
| ⊙                                | RHAPHOLEPIS INDICA 'BALLERINA'            | BALLERINA INDIAN HAWTHORN   | 5 GAL | 192 |
| ⊙                                | ROSA X 'FLOWER CARPET RED'                | ROSE                        | 2 GAL | 154 |
| GRASSES                          | BOTANICAL NAME                            | COMMON NAME                 | SIZE  | QTY |
| ⊙                                | FESTUCA IDAHOENSIS 'SISKIYOU BLUE'        | SISKIYOU BLUE FESCUE        | 1 GAL | 26  |
| ⊙                                | MUHLENBERGIA RIGIDA                       | PURPLE MUHLY                | 1 GAL | 14  |
| ⊙                                | FENISETUM ORIENTALE                       | ORIENTAL FOUNTAIN GRASS     | 1 GAL | 3   |
| PERENNIALS                       | BOTANICAL NAME                            | COMMON NAME                 | SIZE  | QTY |
| ⊙                                | ACHILLEA MILLEFOLIUM 'MOONSHINE'          | YARROW                      | 1 GAL | 13  |
| ⊙                                | AGAPANTHUS AFRICANUS 'PETER PAN'          | DWARF BLUE LILY OF THE Nile | 1 GAL | 38  |
| ⊙                                | DIETES VEGETA                             | AFRICAN IRIS                | 1 GAL | 22  |
| ⊙                                | ERIGERON KARVINSKIANUS                    | FLEABANE                    | 1 GAL | 106 |
| ⊙                                | HEMEROCALLIS X 'STELLA DE ORO'            | STELLA DE ORO DAYLILY       | 1 GAL | 44  |
| ⊙                                | SALVIA GREGGII                            | AUTUMN SAGE                 | 1 GAL | 31  |
| ⊙                                | TULBAGHIA VIOLACEA                        | SOCIETY GARLIC              | 1 GAL | 91  |
| ⊙                                | ZAUSCHNERIA CALIFORNICA                   | CALIFORNIA FUCHSIA          | 5 GAL | 13  |
| STORM WATER MANAGEMENT PLANTINGS | BOTANICAL NAME                            | COMMON NAME                 | SIZE  | QTY |
| ⊙                                | CAREX BARBERAE                            | SANTA BARBARA SEDGE         | 1 GAL | 67  |
| ⊙                                | JUNCUS PATENS 'CARMAN'S GREY'             | SPREADING RUSH              | 5 GAL | 67  |
| VINES                            | BOTANICAL NAME                            | COMMON NAME                 | SIZE  | QTY |
| ⊙                                | FARTHENOCISSUS TRICUSPIDATA 'VEITCHI'     | BOSTON IVY                  | 5 GAL | 6   |

**PLANT SCHEDULE**

| TREES         | BOTANICAL NAME                           | COMMON NAME                    | SIZE   | CONTAINER | QTY      |        |
|---------------|--|--------------------------------|--------|-----------|----------|--------|
| ⊙             | ARBUTUS X 'MARINA'                       | ARBUTUS STANDARD               | 15 GAL |           | 2        |        |
| ⊙             | LAGERSTROEMIA X 'NATCHEZ'                | CRAPE MYRTLE                   | 15 GAL | STANDARD  | 6        |        |
| ⊙             | PISTACIA CHINENSIS 'KEITH DAVEY'         | KEITH DAVEY CHINESE PISTACHE   | 15 GAL |           | 26       |        |
| ⊙             | QUERCUS ILEX                             | HOLLY OAK                      | 15 GAL |           | 13       |        |
| ⊙             | ULMUS PARVIFOLIA 'TRUE GREEN'            | TRUE GREEN ELM                 | 15 GAL |           | 2        |        |
| ⊙             | ZELKOVA SERRATA 'VILLAGE GREEN'          | SAWLEAF ZELKOVA                | 15 GAL |           | 20       |        |
| GROUND COVERS | BOTANICAL NAME                           | COMMON NAME                    | SIZE   | CONTAINER | SPACING  | QTY    |
| ⊙             | ANNUALS MIXED SEASONAL                   | FLOWERS                        | 4"     |           | 12" O.C. | 219    |
| ⊙             | ARCTOSTAPHYLOS UVA-URSI 'EMERALD CARPET' | EMERALD CARPET MANZANTA        | 1 GAL  |           | 36" O.C. | 1,362  |
| ⊙             | BACCHARIS PILULARIS 'TWIN PEAKS #2'      | TWIN PEAKS COYOTE BRUSH        | 1 GAL  |           | 36" O.C. | 836    |
| ⊙             | MAHONIA REPENS                           | CREeping MAHONIA               | 1 GAL  |           | 36" O.C. | 162    |
| ⊙             | ROSMARINUS OFFICINALIS 'PROSTRATUS'      | DWARF ROSEMARY                 | 1 GAL  |           | 36" O.C. | 218    |
| ⊙             | TRACHELOSPERMUM ASIATICUM                | ASIAN JASMINE                  | 1 GAL  |           | 36" O.C. | 39     |
| MATERIALS     | BOTANICAL NAME                           | COMMON NAME                    | SIZE   | CONTAINER | SPACING  | QTY    |
| ⊙             | 3/4 CRUSHED ROCK/SONOMA GOLD OR EQUAL    | 2" DEPTH OVER LANDSCAPE FABRIC | ---    |           |          | 333 SF |

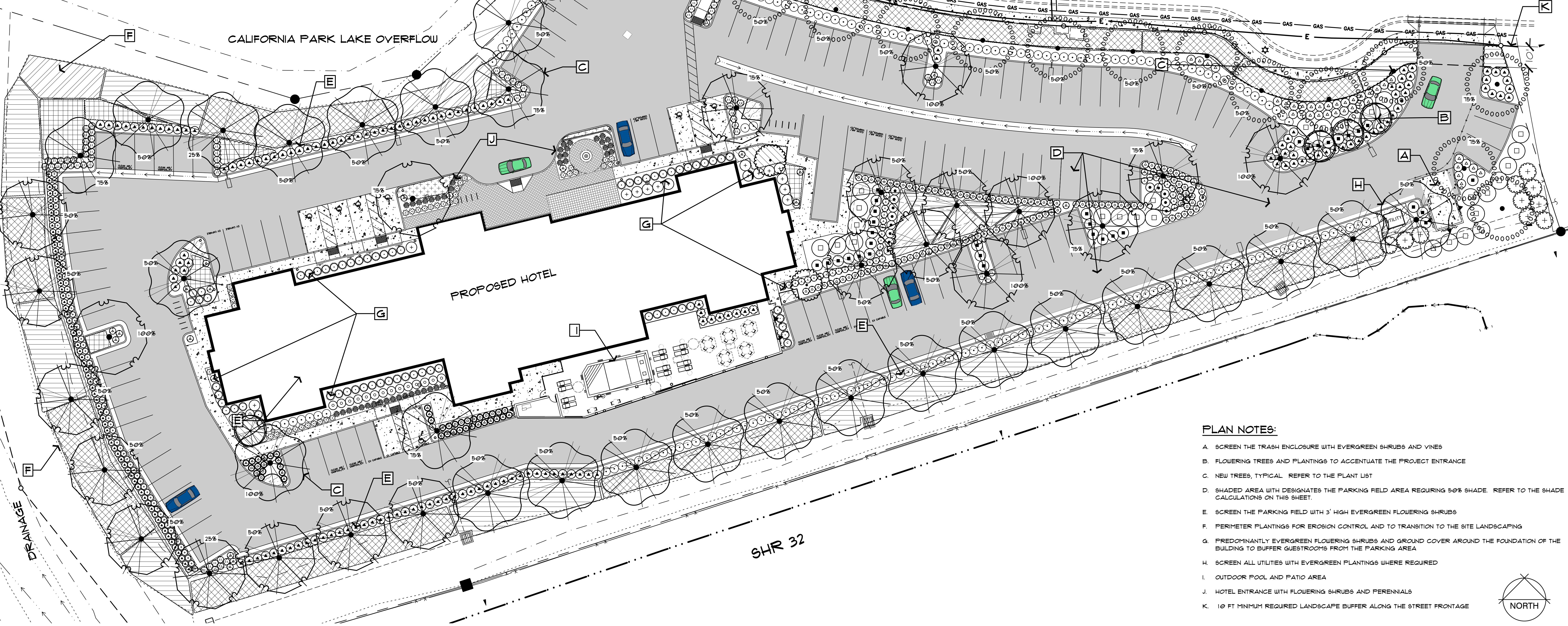
**SHADE CALCULATIONS**

Shade Calculations for The Bruce Road Hotel Project rev 03.10.21

| Botanical Name                   | Common Name           | Quantity  | Shade allowed | at 25%    | at 50%   | at 75%   | at 100% | Total            |
|----------------------------------|-----------------------|-----------|---------------|-----------|----------|----------|---------|------------------|
| Pistacia chinensis 'Keith Davey' | Chinese Pistache      | 26        | 1,256         | 1         | 21       | 3        | 1       | 17,584.00        |
| Quercus ilex                     | Holly Oak             | 11        | 1,256         | 0         | 9        | 2        | 0       | 5,652.00         |
| Ulmus parvifolia                 | Evergreen Chinese Elm | 2         | 1,256         | 0         | 0        | 1        | 1       | 2,198.00         |
| Zelkova serrata 'Village Green'  | Japanese Zelkova      | 20        | 1,256         | 1         | 12       | 3        | 4       | 15,700.00        |
| <b>Total Shade Allowed</b>       |                       | <b>59</b> | <b>2</b>      | <b>42</b> | <b>9</b> | <b>6</b> |         | <b>41,134.00</b> |

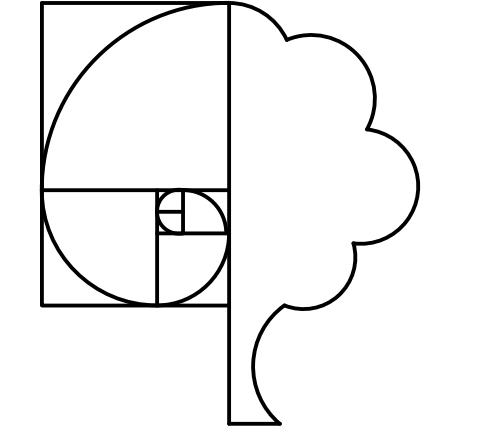
parking lot area to be shaded (excludes areas as indicated (per CMC 19.070.060 E2))  
 50% shade required 52,412.00  
 % Shade Provided 78.48%

15% shade from evergreen tree species required  
 7,850 sf provided = 15% (! indicates evergreen tree species)



**PLAN NOTES:**

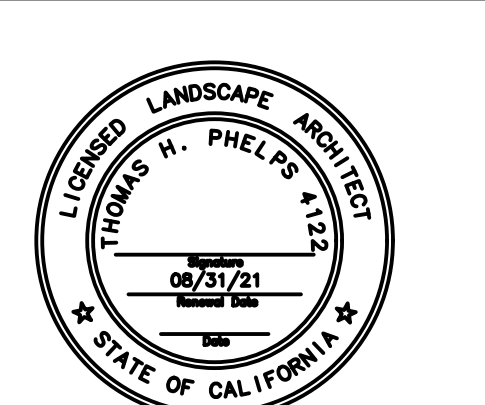
- A. SCREEN THE TRASH ENCLOSURE WITH EVERGREEN SHRUBS AND VINES
- B. FLOWERING TREES AND PLANTINGS TO ACCENTUATE THE PROJECT ENTRANCE
- C. NEW TREES, TYPICAL REFER TO THE PLANT LIST
- D. SHADED AREA WITH DESIGNATES THE PARKING FIELD AREA REQUIRING 50% SHADE. REFER TO THE SHADE CALCULATIONS ON THIS SHEET.
- E. SCREEN THE PARKING FIELD WITH 3' HIGH EVERGREEN FLOWERING SHRUBS
- F. PERIMETER PLANTINGS FOR EROSION CONTROL AND TO TRANSITION TO THE SITE LANDSCAPING
- G. PREDOMINANTLY EVERGREEN FLOWERING SHRUBS AND GROUND COVER AROUND THE FOUNDATION OF THE BUILDING TO BUFFER GUESTROOMS FROM THE PARKING AREA
- H. SCREEN ALL UTILITIES WITH EVERGREEN PLANTINGS WHERE REQUIRED
- I. OUTDOOR POOL AND PATIO AREA
- J. HOTEL ENTRANCE WITH FLOWERING SHRUBS AND PERENNIALS
- K. 10' FT MINIMUM REQUIRED LANDSCAPE BUFFER ALONG THE STREET FRONTAGE



**THOMAS H. PHELPS**  
 LANDSCAPE ARCHITECTURE  
 California Landscape Architect #4122  
 P.O. BOX 170129  
 Boise, Idaho 83717  
 thphelps@sbcglobal.net  
 THPLARCH.com

**TONEPLACE SUITES**  
**CONTINUUM HOSPITALITY, LLC**  
 BRUCE ROAD AT SIERRA SUNRISE TERRACE  
 CHICO, CALIFORNIA

Sheet Title  
**PLANTING PLAN**



| No. | Date | Revision |
|-----|------|----------|
| △   |      |          |
| △   |      |          |
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| △   |      |          |

Project Mgr: THP Sheet No:  
 Drawn By: THP  
 Scale: 1" = 20'  
 Date: 03.10.2021  
 File Name: of \_\_\_\_\_ sheets





## COMMUNITY DEVELOPMENT DEPARTMENT

411 Main Street – 2<sup>nd</sup> Floor  
P.O. Box 3420  
Chico, CA 95927

530-879-6800  
Fax 530-895-4726  
[www.chicoca.gov](http://www.chicoca.gov)

### RESPONSES TO COMMENTS FOR THE TOWNE PLACE SUITES PROJECT

The City of Chico received comments on the proposed Towne Place Suites project. As a result of the high number of comments received, the City's consultant, Michael Baker International assisted the City with preparation of four master responses. The master responses relate to emergency evacuation, Dead Horse Slough (biological resources), traffic impacts, and general environmental impacts and conclusions. City staff have provided master responses to the Use of Common Areas in the California Park Subdivision. Each master response is provided in the sections following below.

Based on review of the proposed project, supporting technical studies/assessments, and comments received on the Categorical Exemption, staff concluded the project meets the requirements for Categorical Exemption under Section 15332 and 15300.2 of the CEQA Guidelines. There is nothing peculiar about the proposed project or the project site that would result in any significant effects relating to traffic, noise, air quality, or water quality and the site has no value as habitat for endangered, rare, or threatened species. In addition, all recommended actions of the project-specific technical studies have been incorporated into the project design itself and, as a result, mitigation measures are not required.

#### Emergency Evacuation

Comments were received specifically related to the proposed project's potential to effect emergency evacuation. The following list provides a few of the specific comments received and provides a general overview of all comments related to emergency evacuation:

- As survivors of the Campfire, we have significant concerns regarding evacuation should there be an emergency. The California Park area sits just below the foothills, and wildfires are to be expected; it's simply a reality for California. Overly-impacting the area with residents without proper evacuation routes is asking for disaster.
- It [Sierra Sunrise Terrace] is also a primary emergency evacuation route for all the residents of Cal Park, Lake Vista and Canyon Oaks.
- State audit on lack of emergency preparedness in Butte County---This certainly is a very timely and frightening subject. All Californians wonder where the next fire will be this summer. The CA state audit found 3 counties not prepared for a disaster such as a large, fastmoving forest fire. If a fire were to come down the hills from the east. Chico would be in big trouble, especially East Chico, with all cars trying to move west. And the residents of CALPark Sierra Sunrise would be in dangerous difficulty due to age, lack of car, disabilities, inability to walk, confusion etc. And as noted, Butte County HAS NOT DEVELOPED AN EMERGENCY EVACUATION PLAN. How can a large building with many people be built where there is no emergency evacuation plan to leave the entire east side of the city?
- During a fire evacuation there would be a bottle neck from the traffic flowing on to Hwy 32. We learned a lot about this areas fire vulnerability during the Campfire. Sierra Sunrise Terrace, Senior Citizen Community, located on Sierra Sunrise Rd, next to the proposed hotel has special fire evacuation and ambulance needs because they are a vulnerable

community. Putting in a hotel is in conflict with the safety and fire evacuation requirements of this senior community.

- When evacuation becomes necessary due to a future wildfire, vehicles exiting Cal Park will join those coming down Highway 32 as well as from the development along Bruce Road, which continues to grow. Residents from within Cal Park will take all possible routes out, including Sierra Sunrise Terrace, to get to Bruce Road. Yet Sierra Sunrise Terrace will have to be clear enough to bring in emergency vehicles and buses, safely load elderly patients and residents, and take them out to safety. Families assisting older relatives will add to the congestion, and a high percentage of evacuees will be non-ambulatory or have other functional or cognitive disabilities. Placing a hotel at the base of Sierra Sunrise Terrace, closest to Bruce Road, and adding in hotel guests and staff will bottleneck and further delay the evacuation process, creating a dangerous situation.

A high level of concern has been identified in the community regarding emergency evacuation, particularly with many residents experiencing firsthand the challenges and inadequacy of evacuation during the Camp Fire. The Camp Fire brought to bear how having primarily only one way out of Paradise for residents fleeing the fire, the four-lane road known as Skyway, quickly became paralyzed by traffic. Paradise had five two-lane roads and one four-lane road leading out of town, but the fire forced officials to close three of those routes which resulted in clogging the remaining roads. Evacuation was also complicated by the topography of Paradise where Skyway runs along a ridge, thereby placing evacuees in the direct path of flames and making escape even more dangerous.

In response to the 2018 Camp Fire, the 2017 Sonoma Complex fires, and the 2017 Thomas Fire that occurred in Butte County, Sonoma County, and Ventura County, respectively, the California State Auditor prepared a report to assess the preparedness of each county to protect vulnerable populations before, during, and after a natural disaster (*California Is Not Adequately Prepared to Protect Its Most Vulnerable Residents From Natural Disasters*, December 2019). The assessment concluded the three counties have not adequately followed key practices for emergency planning, including having emergency plans for alerting, evacuating, and sheltering residents and assessing the needs of their communities in advance of disaster events. As a result, the assessment stated the counties are less prepared for future natural disasters, which may place the residents for whom they are responsible at greater risk of harm.

As a result of the assessment, Butte County issued a response (dated October 22, 2019) stating most of FEMA's guidelines and best practices do not consider large-scale, catastrophic wildfires in counties with limited financial and public safety resources, such as Butte County. FEMA's guidance historically focuses on hurricanes and flooding, which allow for advanced notice and planning, whereas wildfires occur quickly and with short notice. The County identifies that new regulations, guidelines, and best practices for devastating wildfires experienced by many California counties in the past two years will be published in the future. Butte County identifies its commitment to improve response efforts after every disaster, and to consider the assessment's recommendations along with input from other valued partners. Butte County acknowledges implementing, or began implementing after the Camp Fire, many of the protocols regarding alerts, warnings, and evacuations identified in the assessment. For the practices not already implemented and that are appropriate for Butte County, the County intends to implement them over time as resources become available.

Currently, the Butte County Sheriff's Office utilizes a *Local Alert and Warning Plan* which establishes guidelines for using the Sheriff's alert and warning program in partnership with the cities within Butte County. The alert and warning program provides public notification of protective

actions to take before, during, and after threats or emergencies and disseminates other messages to community members who have opted in to receive such messages. The *Local Alert and Warning Plan* supports the Butte County Emergency Operations Plan and incorporates the recommendations noted in the *2019 California Office of Emergency Services Alert & Warning Guidelines*, such as alert and warning program that incorporates multiple methods and technologies to accomplish the goal of reaching the largest percentage of the target population.

Policy 432 of the Butte County Sheriff's Office Policy Manual specifically identifies actions related to evacuating the public due to an imminent threat to life and property, such as wildfires. When the Butte County Sheriff's Office becomes aware that evacuations are necessary, available on-duty personnel are required to immediately respond and begin evacuating the affected population. Special considerations are required to be made for people with access and functional needs. In addition, the Sheriff's Office is required to use any/all of the following methods of notifying the public of potential/actual threats: emergency mass notifications via cellular and landline systems, local media, social media platforms, community radio systems, amateur and General Mobile Radio Systems (GMRS), hi-lo siren, and door-to-door notifications.

The City of Chico's General Plan, Safety Element, also includes Actions that are required to be considered in approving proposed projects. These Actions include the following:

**Action S-4.1.1 (Fire Response Time)** – Strive to obtain an initial response time of five and a half minutes or less for at least 90 percent of fire emergency response calls in urbanized areas.

**Action S-4.2.1 (Interagency Programs)** - Continue to work with CalFire and the Butte County Fire Department on programs that will enhance fire protection and firefighting capabilities in the Planning Area, including maintaining aid agreements.

**Action S-4.3.3 (Project Design)** - As part of the project review process in wildland fire areas, require consideration of emergency evacuation routes and defensible buffer areas.

The proposed project has been reviewed and commented on by the City's Fire Department and Police Department to ensure any concerns with evacuation are vetted and resolved prior to project approval. Both departments did not identify any concerns with wildland fires or with inhibiting evacuation capabilities with implementation of the proposed hotel. In addition, the City continues to design and phase-in an Intelligent Transportation System to address concerns about evacuation routing. Overall, Butte County and the City of Chico continue to update and prepare for the eventuality of future wildland fires and, if needed, the safe evacuation of all residents.

Lastly, the project area provides multiple exit routes beyond Bruce Road, Highway 32, and Sierra Sunrise Terrace alone, compared to primarily one exit experienced during the Camp Fire. Specifically, Sierra Sunrise Terrace not only provides access to Bruce Road but also provides access to a residential neighborhood to the north which expands into additional multiple exit routes (e.g., Lakewest Drive, Yosemite Drive, California Park Drive).

In conclusion, the City determined that there are no unusual circumstance associated with the project or project site as related to emergency evacuation. The anticipated significant impacts, or lack thereof, associated with a project governs the level of determination or CEQA document to be prepared. As a result of the proposed project not having an unusual circumstance, the project would qualify for a Class 32 exemption and preparation of another CEQA document type with additional environmental analysis (e.g., EIR, negative declaration) is not required.

## Dead Horse Slough - Biological Resources

Comments were received specifically related to the proposed project's potential to effect biological resources. The following list provides a few of the specific comments received and provides a general overview of all comments related to biological resources:

- ... have seen many ducks, egrets, herons, and other bird and aquatic life. It is obvious that animal and bird and aquatic life will be affected by more cars, more noise, more lighting throughout the night, and a building 59.3 feet tall. Migration and nesting changes may likely occur.
- This project would certainly impact the surrounding natural environment and the adjacent riparian area.
- Environmental concerns relative to the proximity to a local waterway.
- The runoff into the natural waterways from a high impact building will have an unavoidable impact on the environment.
- Environmental impact...there are Herons and other birds in the wetland near that corner, as well as turtles and other life. Disturbing the drainage of the area (especially during construction) will damage the area. I have witnessed what collateral damage occurs with excavation to the surrounding area ...
- The environmental impact of this hotel with its 112 rooms, and its proximity to the creek, and the runoff it will produce, which will go directly into the Waterways of California, is incompatible with the state of California and any thoughts of sustainability. California Park's lakes and ponds are constantly monitored by Fish and Game to make sure we comply with chemical and run off regulations.

As identified in a report titled, *Environmental Permit Due Diligence for Cal Park Hotel Site Project*, prepared by ECORP Consulting (dated July 12, 2021), the project site was assessed for potential biological resources constraints (e.g., aquatic resources, special-status species) and was reviewed to identify any regulatory requirements for future development. The assessment identified the project site as a graded and leveled lot, primarily consisting of a ruderal plant community dominated by nonnative species. The assessment identified an active nesting colony of cliff swallows located above the pond (California Park Lake) immediately adjacent to the project site; however, no other active nests were observed in the vicinity.

The assessment acknowledges the proposed project received authorization for Nationwide Permit (NWP) 18, Minor Discharges Regulatory Division by the United States Army Corp of Engineers (USACE) Sacramento District and received a CWA Section 401 Certification from the Central Valley Regional Water Quality Control Board (RWQCB). The assessment concludes that no additional aquatic resources were identified within the project site and that graded areas did not contain any aquatic resources. The assessment recommends construction activities in the grading areas can be conducted without further approval from USACE or Central Valley RWQCB and that any construction activities taking place outside of previous work areas should be evaluated by a biologist to ensure no incidental impacts to aquatic resources occur. Lastly, the assessment recommends best management practices (BMPs) (i.e., pre-construction surveys, evaluation of any construction activities taking place outside of previous work areas) to ensure that there no impacts would occur to sensitive species or waters on or adjacent to the project site.

As shown in Figure 4 of the Categorical Exemption, all construction activities and proposed development features (e.g., parking lot, buildings) would be located a minimum of 30 feet from

the top of slope for the adjacent water body (i.e., Dead Horse Slough, California Park Lake). In addition, as shown in Figure 4, the project would be located outside the nearest flood zone.

As identified in the Categorical Exemption, much of the land surrounding the project site is developed. The project site is adjacent to major roadways including State Highway 32 to the south, Bruce Road to the west, and Sierra Sunrise Terrace to the north and east. The project site is located adjacent to existing urban development including residential to the northwest and offices to the east. Therefore, the proposed project would be a logical extension of existing urban development and would not threaten any habitat for endangered, rare, or threatened species.

To control runoff from the development, the proposed project would include landscaping covering 57 percent of the site, as well as constructing a bioswale to direct stormwater flows into the storm drain system located along Highway 32. In addition, the proposed project has been designed to comply with the City's Creekside Development Standards by providing a minimum 25-foot setback from the top of bank adjoining the creek to protect the creek and to avoid any potential impacts to creek habitat. In conformance with the City's Creekside Development Standards, no grading or filling, planting of non-native or non-riparian plant species, or removal of native vegetation would occur within the 25-foot creek setback (Section 19.60.030 of the City of Chico Municipal Code).

The analysis provided in the Categorical Exemption and technical study, described previously, provide substantial evidence that the proposed project meets the provisions for being exempt from the provisions of CEQA pursuant to the CEQA Guidelines Section 15332 as an in-fill development project. Specifically, the project site has no value as habitat for endangered, rare, or threatened species (CEQA Guidelines Section 15332(c)) and the proposed project design would not impact any said habitat.

## **Traffic Impacts**

Comments were received specifically related to the proposed project's potential to effect traffic. The following list provides a few of the specific comments received and provides a general overview of all comments related to traffic:

- Sunrise Terrace is a narrow road and this hotel doesn't fit the community, a neighborhood. Service vehicles and delivery trucks will impact the road/traffic.
- Traffic concerns are already an issue in this area, and a hotel of this size would exacerbate these problems.
- The road ways are over-taxed with the amount of use for people in this area. Then the luxury apartments were added to the corner of Bruce and California Park Drive, which impacted the traffic immensely. Now, you've added multiple apartment complexes along Hwy 32 and Bruce Road. These residents will be using Bruce Road as a daily route, which is now impacting the traffic even more. During the raining season, Bruce Road often floods on the same corner where the hotel is being proposed, and the traffic has to be diverted through our neighborhoods. This would mean that during these flood times, all of the hotel traffic would go through our private neighborhoods, adding wear and tear to our neighborhood streets and additional to our neighborhoods.
- ... why has it been determined that it would not cause additional traffic concerns, when their own estimate is for 500 car trips per day into the ever increasing congestion at Bruce Road and Hwy 32?
- I read the traffic study for the present and future 2040 prediction. I agree there may be very little growth coming from the CALPark area (unless the City of Chico changes more

land use zoning). However, I see BIG discrepancies in the Traffic Report regarding traffic along the Bruce Road (which should include Manzanita) and HWY 32. Personally, living on Hooker Oak, I use the Manzanita-Bruce corridor sometimes 1-4 times per day. Many people choose to drive this "back way" rather than head west and get on the freeway. Remember that we have the huge apartment development and homes being built on Eaton Road and side streets. Eaton, Lassen, East, Hooker Oak, Vallambrosa, Hwy 32, 20th Street and Lower Skyway cars all use this corridor as a way to avoid driving "into town". What will happen to the intersections and stop lights at 32 and Sierra Sunrise then?

- A four story hotel at this location is a major hinderance to residential traffic as well as emergency vehicles as the hotel is not accessible from any street other than the one belonging to the retirement community.
- ... there will be 20 vehicles per hour during peak traffic hours coming from the new construction when completed and the delay at Sierra Sunrise Terrace and Bruce Road would be 3.2 minutes. ... Let's round this out to 3 minutes delay per vehicle, and 20 vehicles make that a one hour delay. ... vehicles backed up to Humboldt Road waiting to get through the junction and if 20 percent go left and 20 percent go right at the intersection 60 percent are going to continue on Bruce Road and be stopped at the proposed traffic control light at Sierra Sunrise Terrace making a mess at Deer Creek Hwy and Bruce Road. However, if the dating of the document is even close to when the analysis was made, two thirds of the traffic on Bruce Road did NOT exist because of the "lockdown" during the COVID19 state and country emergency. Therefore, the information submitted is inconclusive, or maybe more simplistically stated, absolutely wrong as a definitive analysis.
- ... a traffic signal will impact the Hwy 32/Bruce Road intersection especially during the am/pm commute. If the Sierra Sunrise/Bruce Rd trafficking light is on green for an exit from Sierra Sunrise, the traffic will backup on Bruce Rd back to Hwy 32 and beyond. The traffic on Bruce Road east of Hwy 32 at 5:00 pm already backs up to nearly Humboldt Rd.

As identified in a report titled, *Traffic Analysis & Signal Warrant Evaluation for TownePlace Suites – Chico, CA*, prepared by Headway Transportation (dated January 28, 2020), the project site was assessed for potential traffic impacts on local intersections associated with Towne Place Suites development. In addition, the assessment included a warrant evaluation to identify any off-site impacts and determine if a traffic signal would be justified at the Bruce Road / Sierra Sunrise Terrace intersection.

The traffic analysis used the California Department of Transportation (Caltrans) Manual on Uniform Traffic Control Devices (MUTCD) to assess the need for a traffic control signal at the Bruce Road / Sierra Sunrise Terrace intersection using four-hour vehicular volume and the peak hour volume warrants. The analysis did not find any unusual cases at the intersection and concludes a signal is not justified with implementation of the proposed project alone. However, as community population and development growth causes traffic to increase over time, the traffic analysis recommends a signal at the intersection for traffic moving onto Bruce Road from Sierra Sunrise Terrace.

Since the warrant analyses were completed, a few collisions occurred at the Bruce Road / Sierra Sunrise Terrace intersection. After reviewing the traffic signal warrant analyses, recent collision data, and the Development Impact Fee Program, City staff recommended installing a traffic signal at this intersection to improve safety. The developer is supportive of the safety improvement and is willing to install a traffic signal as part of the proposed project. The City of Chico collects fair share cost of circulation improvements from developer their necessary to address cumulative transportation impacts, including those to state highways, local roadways, transit, pedestrian, and



bicycle facilities, through the City's Development Impact Fee Program. Therefore, it is a requirement that developers provide funding for improvements as needed. Maintenance of the transportation network would be built-in to the scope of the project (Circulation Element of the City of Chico General Plan, Policy CIRC-1.3).

The proposed traffic signal at the Bruce Road / Sierra Sunrise Terrace intersection would not affect traffic conditions (e.g., delay, stacking) to the south of Highway 32, including the Humboldt Road / Bruce Road intersection. Traffic controlled to the south of Highway 32 by the existing traffic signal at the Highway 32 / Bruce Road intersection would not change with operation of a new traffic signal at the Bruce Road / Sierra Sunrise Terrace intersection.

The project site is currently zoned Community Commercial (CC) which permits, by right, a variety of land uses including shopping centers, medical offices, general offices, restaurant/bar/tasting rooms, grocery stores, pharmacies, and other retail and mixed residential uses, many of which generate substantially more vehicle trips than the proposed hotel. The existing CC zoning for the project site also indicates local-serving retail uses are purposefully envisioned for the project site. As assessed in the traffic analysis, implementation of the proposed project would generate approximately 40 total trips during the highest one-hour period of the day (am or pm peak hour) and approximately 500 total trips per day. To illustrate the difference in the number of trips that could occur with other allowable uses on the project site such as a medical office and a restaurant/retail mixed use, the traffic analysis identifies the number of trips generated by each of these uses. Based on these numbers, the traffic analysis concludes that operation of a hotel would generate substantially fewer daily trips (500 daily trips estimated) compared with the other allowable uses (between 2,088 and 2,921 daily trips estimated). Operation of a hotel on the project site is estimated to result in less than one-fourth the trips generated by other allowable uses. This reduced trip generation would work towards preventing or substantially reducing exacerbation of any potential future traffic congestion in the project area.

The analysis provided in the Categorical Exemption and technical study, described previously, provide substantial evidence that the proposed project meets the provisions for being exempt from the provisions of CEQA pursuant to the CEQA Guidelines Section 15332 as an in-fill development project. Specifically, the project would not result in any significant effects relating to traffic (CEQA Guideline Section 15332(d)) with implementation of the proposed project as designed, including installation of a traffic signal.

### **General Environmental Impacts and Conclusions**

Comments were received specifically related to the proposed project's general environmental impacts and conclusions. The following list provides a few of the specific comments received and provides a general overview of the comments related to general environmental impacts and conclusions:

- ... the project has not gone through a full environmental impact review.
- I would like to see the environmental impact report.

CEQA provides a process for evaluating the potential environmental effects of a proposed project. CEQA also identifies projects that are considered exempt from analyzing their potential environmental effects. There are two kinds of CEQA exemptions: statutory and categorical exemptions. Statutory exemptions are projects specifically excluded from CEQA by the California Legislature, regardless of their potential impacts to the environment. Categorical exemptions

apply to classes of projects that generally are considered not to have potential impacts on the environment.

Projects that are specifically identified as urban infill development are exempt from CEQA and referred to as Class 32 categorical exemption. The Class 32 infill exemption was adopted in the 1990s and is intended to promote infill development within urbanized areas. "Infill development" refers to building within unused and underutilized lands within existing urban areas that are already largely developed. Under the Class 32 infill exemption, as defined in the CEQA Guidelines, a project is exempt from CEQA if:

- a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations.
- b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.
- c) The project site has no value as habitat for endangered, rare or threatened species.
- d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.
- e) The site can be adequately served by all required utilities and public services.

It is also important to note that categorical exemptions, including the Class 32 infill exemption, are subject to exceptions under CEQA Guidelines Section 15300.2. Therefore, a project that fits under a categorical exemption may not qualify for an exemption if it is subject to an exception. Exceptions applicable to a Class 32 infill exemption include if the project would result in a cumulative impact, would have significant environmental impacts due to unusual circumstances, is located along a scenic highway, is located on a hazardous waste site, or would impact historical resources.

The analysis provided in the Categorical Exemption for the Towne Place Suites project, supported by technical studies, provides substantial evidence that the proposed project qualifies for an exemption under CEQA Guidelines Section 15332 as an urban in-fill development, and would not have a significant effect on the environment. Taking into consideration the above requirements and exceptions the project must meet to qualify for the Class 32 infill exemption, the Categorical Exemption describes the contents and conclusions for each of the nine technical studies/assessments prepared specifically for the proposed project. The Categorical Exemption also provides conclusions regarding the adequacy of each study/assessment to support the proposed 112-suite hotel qualifies for an exemption from CEQA. The anticipated significant impacts, or lack thereof, associated with a project governs the level of determination or CEQA document to be prepared. As a result of the proposed project qualifying for a Class 32 exemption, preparation of another CEQA document type (e.g., EIR, negative declaration) is not required.

### **Use of Common Areas in the California Park Subdivision**

Comments were received specifically related to the proposed project's potential to affect the use, enjoyment and financial burden of patrolling the common areas within the California Park and ability to enforce traffic and parking regulations on the private streets Subdivision. The following list provides a few of the specific comments received and provides a general overview of all comments related to concerns with access rights to Sierra Sunrise Terrace (private street) and the ability to enforce traffic and parking regulations on the private streets, and visitors and use of the common areas in California Park Subdivision:



- Trespassing on CALPark private prop--This is an issue for the CALPark residents. They live in a private community and the lakes and walking paths are not open to the public. From the front of the hotel, it's a 2 minute walk across the road to trespass on beautiful CALPark property. How will the hotel prevent that problem?
- I am also concerned about the added financial burden that would be placed on our residents for the cost of additional security patrol. The safety of our seniors would be a concern in the face of an ever changing, transient hotel clientele. California Park lakes and trails are private and would have to be protected from hotel trespassers as well.
- Cal Park residents pay an annual fee to keep our paths, lakes and ponds clean AND private, but hotel guests will not respect that, causing additional revenue from the homeowners for patrol, keeping our area safe and clean.
- The lake is private. The homeowners pay HOA for the upkeep and security of this lake. I walk Idylwild, Sunrise Terrace and Lakewest. While I would like to walk around the lake, since I don't pay HOA dues to have a pass, I'm not able to. The lake is private. I see the private security keeping people off the paths and the lake. The hotel would bring visitors to this private community and there would be trespassers.
- I'm guessing the lake is what is making this land appealing to the company. I'm sure the hotel's promotional material will feature the lake. The lake is private property and guests and staff will not have access to it or to the walking paths around it. This will require HOA members to spend substantially more for additional security measures to prevent trespassing, which pose liability and maintenance issues for the organization.
- Are those vehicles allowed to be there and since it's a private drive do we issue ticket there?
- Would or does PD currently enforce trespassing or similar activities at Cal Park?
- There are also safety and security issues with vehicles parking on the street. Vehicles parking on the street are likely to be oversized and won't fit in on-site parking spaces. These vehicles are heavier<sup>1</sup> as defined by GVWR<sup>2</sup> louder and will carry items ripe for theft. These vehicles will likely attract unsavory characters who look to profit off of others. In today's climate everything is for the taking. Residents throughout Chico report vehicle vandalism, ransacking, broken windows, stolen catalytic converters and vehicle theft.

All of the properties within California Park are entitled to access the common areas throughout the California Park Subdivision and this right extends to their invitees, tenants and vendors. Sierra Sunrise Terrace was created and is referenced as COMMON AREA "I" (Private Streets). The recorded map for "Sierra Sunrise Village Unit 2" *dedicated "An easement for emergency access and public purposes, including water, sewer, drainage, gas and communication facilities, over, on and under that land shown on this map as "Common Area I."* The City Clerk's Certificate references this as *"Also an easement shown as an easement for emergency access and public utility purposes over, on, and under "Common Area I" as shown on the annexed map and offered for dedication to the public was accepted on behalf of the public for emergency access purposes and public utility purposes."*

In particular, Article III, Common Areas, Declaration of Restrictions, California Park, Chico, California, states *"Each lot owner shall have a right and easement to use and enjoy all of the common areas within the boundaries of the property in common with all other lot owners. Such right to use the common area shall pass with the title to every lot and extend to the Declarant and the invitees of Declarant, and to each lot owner and the tenants and contract vendees of each lot owner, and to such other classes of persons as to whom the Board of Directors may, from time to time and subject to published rules and regulations, extend the privilege of use and enjoyment of the common areas. The right of Declarant and each lot owner, and of such owner's tenants*

*and contract vendees, to use and possess the common areas as set forth herein, shall be subject and governed by the provisions of this Declaration, the Articles and the By-Laws, and such other rules and regulations as may be hereafter be adopted by the Board of Director's from time to time. The corporation shall have the authority to lease or to grant licenses or concessions with respect to all or any part of the common area, subject only to the provisions of this Declaration, the Articles and By- Laws; provided, however, that any charges levied against the general public for any use of any particular facilities shall not be less than charges levied against lot owners for the same use of the same facilities."*

The Police Department has continually enforced the traffic safety and parking laws on all the streets in California Park, regardless of the private street status. They have also confirmed they will continue to ticket illegally parked vehicles and trucks on the streets. The Police Department reiterates that they will always respond to complaints of suspicious persons, trespassing, parking violations, unpermitted vehicles, and the like.

Much of the concern expressed by the commentors revolve around hotel guests accessing and walking around California Park. The hotel clientele is expected to include business travelers, medical professionals, tradespeople, and those looking for a convenient place to stay while visiting relatives in the surrounding community. Outdoor walking paths and trails are not a common amenity that hotel guests look for or expect in the typical hotel experience. There would be more of an expectation for a destination-oriented hotel to provide "outdoor" and wellness activities/amenities (e.g., yoga classes, guided hikes, walking areas, spa treatments), which does not appear to be the intent of the proposed hotel.

It is expected that guests visiting relatives in California Park would have a higher likelihood of using an adjacent park and trail area would be families or those, which are likely to account for a small portion of hotel guests. The proposed hotel is not expected to be a hotel with lower economy room rates with a high-volume turnover, nor a destination resort type with guests expecting an abundance of easily accessible amenities and activities. The project site is also proximal to medical facilities off Bruce Road which provide outpatient services for Enloe Hospital including an urgent care and surgery center, making this an appropriate and desirable location for travelling healthcare and medical professionals.



**CITY OF CHICO**  
**NOTICE OF EXEMPTION**  
**TOWNE PLACE SUITES**

**To:** County Clerk  
County of Butte  
25 County Center Drive  
Oroville, CA 95965

**From:** City of Chico  
Community Development Department  
Planning Division  
411 Main Street  
Chico, CA 95928

**Project Address:** Northeast corner of Deer Creek Highway (SR 32) and Bruce Road (UP 19-25 & AR 19-22)

**City:** Chico

**County:** Butte

**PROJECT DESCRIPTION**

**Project Location**

The proposed Towne Place Suites project (UP 19-25 & AR 19-22) includes the construction of a 4-story, 112-room hotel, with a 16,655 square foot footprint, on a 4.09 acre site located south of Sierra Sunrise Terrace and northeast of the intersection of Deer Creek Highway (State Route 32 (SR 32)) and Bruce Road. See **Figure 1**. California Park Lake is north of the proposed project site, while land to the south and west is currently vacant, and an office building and residential neighborhood is to the east. Access to the project site would be taken from Sierra Sunrise Terrace, a two-lane collector road. See **Figure 2**.

**Surrounding Land Use Designations and Zoning**

The proposed project site is designated for Commercial Mixed Use (CMU) on the City of Chico General Plan Land Use Diagram and surrounding land use designations are Primary Open Space (POS) to the north, and CMU to the east, south, and west. Immediately beyond the CMU land uses to the east of the project site is land designated Medium Density Residential (MDR). The development of a hotel is consistent with the CMU Land Use Designation.

The project site is zoned Community Commercial (CC), while the land to the north is zoned Primary Open Space (OS1), and land to the south, west, and east is zoned CC. Immediately beyond the area zoned OS1 to the east of the project site is land zoned Medium Density Residential (R2). The proposed land use (hotel) is a conditionally permitted use within the CC zone. The purpose of conditional use permits is to allow for activities and uses that are unique and whose effect on the surrounding environment cannot be determined prior to being proposed for a particular location. In addition, conditions of approval may be placed on a project to ensure that the proposed use is compatible with existing and designated uses in the general vicinity (City of Chico Municipal Code Section 19.44).



## **Project Site**

The approximate 4.09 acre project site is in the northeast corner of the intersection of Bruce Road and Deer Creek Highway (SR 32) and may be identified by Assessor's Parcel Number (APN) 018-230-001. The site has been graded and a previous stockpile of soil and construction rubble and debris removed in 2020 for which a mitigated negative declaration (MND) was prepared in 2018 (**Attachment A**). The site topography features a gentle southeasterly slope with an elevation of approximately 250 feet above sea level. A contributory drainage channel of the Dead Horse Slough forms the western and southern property boundaries and serves as an overflow to the human-made California Park Lake, located to the north of the property. See **Figure 3**.

The Geotechnical Report prepared for the project refers to aerial photographs that indicate the site was not previously developed or occupied by any structures and that the parcel shape was formed by the construction of California Park Lake between 1984 and 1988 (**Attachment B**).

## **Proposed Project**

The proposed Project includes the construction of a four-story, 112-room hotel with a 16,655 square foot (SF) footprint. Other components of the Project include 59,265 SF of paved parking areas composed of 117 off-street parking spaces, 20 bicycle parking spaces, drive aisles, and 102,241 SF of landscaping. See **Figure 4 (Attachment C)**. Development associated with the proposed project will be setback a minimum of 18 feet from the northern property line, 30 feet from the western property line, and 26 feet from the southern property line avoiding any potential impacts to the existing drainage features along the southern and western boundaries of the proposed project site (i.e., Dead Horse Slough). The project has been designed to comply with the City's Creekside Development Standards (CMC Sec. 19.60.303) by providing a minimum 25-foot setback from the top of bank adjoining the creek.

Utilities to the Project site will be provided by existing facilities within Sierra Sunrise Terrace. Utility providers are as follows: wastewater – City of Chico, water – California Water Company, Electricity and Natural Gas – Pacific Gas and Electric, telephone – AT&T, and Cable TV – Comcast. Stormwater from the site will be screened by the proposed landscaping to the south and a proposed bioswale to the west before being directed to an existing concrete storm drain system along Highway 32.

Vehicular access to the Project site is via Sierra Sunrise Terrace, a two-lane collector road which currently runs west to terminate at Bruce Road, a four-lane arterial roadway with accompanying Class II Bicycle/Pedestrian facilities. Approximately 500 feet south of the intersection of Bruce Road and Sierra Sunrise Terrace, Bruce Road intersects with Deer Creek Highway (State Highway 32).

As discussed above, although there was a mitigated negative declaration (MND) prepared in 2018 for the grading and removal of a debris stockpile from the site in 2020, this proposed Project involves a new site plan for the development of a hotel that was not included in the MND. This proposed Project, based upon the analysis below, is eligible for and will be reviewed under Section 15332, In-fill Development Projects, of the California Environmental Quality Act (CEQA) Guidelines.

**Name of Public Agency Approving Project:** City of Chico

**Name of Person or Agency Carrying Out Project:** Continuum Hospitality, LLC

**Exemption Status:** (check one)

- Ministerial [PRC, Sec. 21080(b)(1); CCR, Sec. 15268]
- Declared Emergency [PRC, Sec. 21080(b)(3); CCR, Sec. 15269(a)]
- Emergency Project [PRC, Sec. 21080(b)(4); CCR, Sec. 15269(b)(c)]
- Categorical Exemptions [CCR Title 14, Sec. 15332]
- Statutory Exemptions [State Code Section Number]
- Common Sense Exemption [CCR, Sec. 15061(b)(3)]

**Exemption Title:** In-fill Development Projects

**Reasons Why Project is Exempt:** Article 19 of the CEQA (CEQA Guidelines Section 15300 to 15333) includes a list of classes of projects that have been determined to not have a significant impact on the environment, and as a result, are exempt from review under CEQA. The analysis described below, supported by the attached technical studies, provides substantial evidence that the proposed Project is Categorically Exempt from the provisions of CEQA pursuant to the CEQA Guidelines Section 15332 as an in-fill development project, and implementation of the proposed Project would not result in any significant impact to the environment.

The proposed Project’s consistency with the in-fill development criteria, as defined by CEQA Guidelines Section 15332, is discussed below following each criterion, (a) - (e):

*(a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as the applicable zoning designation and regulations.*

The proposed project site is designated for Commercial Mixed Use (CMU) on the City of Chico General Plan Land Use Diagram. The CMU designation encourages the integration of retail and service commercial uses with office and/or residential uses. The designation also allows for other public/quasi-public uses allowing for the proposed project to be consistent with the CMU land use designation. The Project meets policies that support the goals of the General Plan. The Project aligns with City policies for development compatible with the existing urban form while supporting conservation of natural resources, such as the creek and by avoiding outward growth. Additionally, the Project fulfills City goals for providing a mix of uses, access to jobs, and community services. The Project is also consistent with the General Plan because it would not displace residents or conflict with the Land Use Diagram’s intended uses in the City (Land Use and Community Design, City of Chico General Plan, 2011).

The proposed project site is zoned Community Commercial (CC) on the City of Chico Municipal Code. The construction of a hotel is listed as a conditionally permitted use in the CC zone of the Chico Municipal Code. In accordance with the City’s municipal code regarding conditional use permits (Section 19.24), the City will conduct a review of the proposed project’s configuration, design, location, and potential impact of the proposed use (hotel) by comparing it to established development standards. The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses. See **Table 1** for development standards for the CC zoning district.



**Table 1. Commercial and Office Zone General Development Standards, Section 19.44.030**

| Development Feature                     | Requirement by Zoning District   |            |
|---|--|------------|
|   | 19.44.030 Commercial and office zone general development standards.  |            |
|   | Community Commercial (CC)  | Proposed   |
| Minimum Lot Size<br>Minimum area        | 6,000 sq. ft., interior lots<br>7,000 sq. ft., corner lots   | 4.09 acres |
| Landscaping                             | Minimum 10% of Site Area<br>Required to be Landscaped  | 57 %       |
| Building Setbacks Required (1)<br>Front | None required, except where<br>the block is partly within an R<br>zoning district, the same front<br>setback shall be required as in<br>the R district.  | 89 feet    |
| Sides                                   | 10 ft. where the side of the<br>parcel abuts an R district; lesser<br>setbacks may be approved<br>through the Design Review<br>process when abutting an alley.<br>No setback required elsewhere. | 344 feet   |
| Street side                             |  | 157 feet   |
| Rear                                    | 10 ft. where the rear of the<br>parcel abuts an R district; none<br>elsewhere.   | 80 feet    |
| Site Coverage, Maximum                  | 95% (2)  | 10%        |
| Height Limits                           | 57 ft. Lesser height may be<br>required through the Design<br>Review process where the<br>parcel abuts an R district.  | 4 stories  |

Source: City of Chico Municipal Code, 2021.

(b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.

The proposed project site is within the City of Chico city limits and encompasses approximately 4.09 acres. Current land uses surrounding the site include a human-made lake to the north, an existing office building to the east, undeveloped land to the west that is designated and zoned for mixed use or commercial development, and currently undeveloped land to the south that will be the site for an approved 24-hour fueling station, convenience store, and drive-through car wash. The project site is

adjacent to major roadways, including State Highway 32 to the south, Bruce Road to the west, and Sierra Sunrise Terrace to the north and east.

*(c) The project site has no value as habitat for endangered, rare, or threatened species.*

The proposed Project site has no value as a habitat for endangered, rare, or threatened species. Nor are there protected trees at the site. The site consists of non-native plants growing amongst waste (refuse) on ground previously disturbed by grading and leveling activities. There is no presence of aquatic resources in the graded areas. Additionally, active nests have not been observed at the proposed project site. Construction and operation of the proposed hotel would not result in significant impacts to any habitat for endangered, rare, or threatened species (ECORP Consulting 2021, see **Attachment D**).

Though critical habitat for vegetation is located south of SR 32, none exists at the Project site. See **Figure 5**. Based on the types of activities involved, the Project will have no effect on federally protected (listed or proposed) threatened or endangered Species (plants, animals, fish, or invertebrates), and will not adversely impact their critical habitats (United States Fish & Wildlife Service. Critical Habitat Portal, 2021).

Much of the land surrounding the project site is developed. The project site is immediately adjacent to the major roadways State Highway 32 to the south, Bruce Road to the west, and Sierra Sunrise Terrace to the north and east; intense residential development, RS-20 Suburban Residential 20,000 sq ft minimum, is located immediately to the northwest, and offices to the east. Parcels to the northeast and west are zoned Community Commercial. Therefore, the area has existing, as well as planned urban uses and will not constitute a threat to endangered habitat. Further, the project has been designed to comply with the City's Creekside Development Standards by providing a minimum 25-foot setback from the top of bank adjoining the creek thereby avoiding any potential impacts to creek habitat. In conformance with the City's Creekside Development Standards, no grading or filling, planting of non-native or non-riparian plant species, or removal of native vegetation will occur within the 25-foot creekside setback (Section 19.60.030 of the City of Chico Municipal Code).

*(d) The Project is consistent with City policies and goals to protect native habitats. As discussed, the site has no value as a habitat for endangered, rare, or threatened species. Aligning with City policies, the Project will be located in an area that does not impact access to open space. Additionally, the Project is an example of development directed to an urban setting - the site has been previously graded and zoned for commercial development. Furthermore, as the Project-related studies have shown, there will be impacts to habitat and aquatic resources that are considered less than significant (Open Space and Environment, City of Chico General Plan, 2011). Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.*

**Traffic.** A project-specific traffic analysis was prepared to determine if implementation of the proposed project would result in any significant effects to traffic. As analyzed, the project would have a less-than significant impact to traffic.

On September 27, 2013, Senate Bill (SB) 743 was into law and changed transportation impact analyses completed in support of CEQA documentation. SB 743 eliminated level of service (LOS) as a basis for determining significant transportation impacts under CEQA and provided a new performance metric,



vehicle miles travelled (VMT). A VMT-based analysis is provided below. In addition, a traffic warrant analysis is provided for the Bruce Road and Sierra Sunrise Terrace intersection.

*Vehicle Miles Traveled*

Vehicle Miles Traveled (VMT) is defined as the average number of miles traveled by a motor vehicle for commute trips. For VMT thresholds, the City of Chico utilizes the Technical Advisory on Evaluating Transportation Impacts in CEQA, December 2018, published by the Governor’s Office of Planning and Research (OPR). The City of Chico’s Circulation Element Policy CIRC-1.5 requires a VMT analysis that is consistent with the California Office of Planning and Research CEQA Guidelines. The VMT analysis also considered the 500 daily trips the project would generate (Headway Transportation, 2020, see **Attachment E**).

The project is below thresholds recommended by the OPR due to the destinations of hotel visitors, the level of employment that will be generated by the project, as well as the traffic generated by comparable retail service types. Further, the project conforms to OPR’s recommendations for reducing VMT by reducing average trip lengths, for example, trips to and from this project type are lower than average because the project is located near transit (served by the B-Line Route 7). Location near transit is a strategy recommended by OPR to reduce VMT (Headway Transportation 2020, see **Attachment E**).

**Table 2** below shows that “Constant Hotel Demand” Existing plus Project, based upon Project generated VMT as projected by the Butte County Association of Governments (BCAG) Travel Demand Model (TDM). The BCAG TDM projects that hotel visitors will travel to destinations on the east side of the City, which are short distances. Hotel visitors commonly choose a hotel based on proximity to their activities to reduce travel time. Adding service uses in a developed community distributes the service use, thereby improving service destination proximity, shortening trips, and reducing VMT. The use added to the existing distribution accounts for less VMT in the “Constant Hotel Demand” Existing plus Project scenario, as shown in **Table 2**. Though, a hotel use does not generate VMT as much as other, more intense commercial uses and is therefore, not a significant impact to traffic. Overall, hotel visitors will visit locations on the east side of the city, which will result in fewer VMT.

**Table 2. Project Effect on Visitor VMT**

| Scenario   | Total Model Boundary VMT |
|--|--------------------------|
| Existing   | 5,431,969                |
| “Constant Hotel Demand” Existing plus Project  | 5,431,754                |
| <b>Difference</b>  | <b>-215</b>              |
| Note: “Constant Hotel Demand” Existing plus Project TDM scenario assuming hotel demand in the City remained constant before and after Project completion. Constant hotel demand was calculated by reducing demand for the three existing hotels closest to the proposed Project. |                          |

Source: Headway Transportation, 2020.

As shown in **Table 3** below, the Project is below the VMT threshold. The analysis used the OPR Technical Advisory’s recommendation using 15 percent below the existing regional average VMT per employee as a threshold of significance for employment-based projects. As recommended by OPR, VMT per employee was utilized as the overall threshold for all VMT generated by both visitors and employees because the project is a commercial use, a use where VMT is primarily generated by employees of the project (Headway Transportation, 2020, see **Attachment E**).

**Table 3. VMT Threshold Comparison**

| Metric   | Value  |
|--|--------|
| Project Home-Based Work VMT  | 710    |
| Decrease in Visitor VMT due to the Project   | -215   |
| Net Change in Model Boundary VMT due to the Project  | 495    |
| Estimated Project Employees  | 67     |
| Existing Model Boundary Average VMT per Employee   | 11.3   |
| 85% of Existing Model Boundary Average VMT per Employee (VMT Threshold)  | 9.6    |
| Total Net Project VMT per Employee   | 7.4    |
| Percent Difference   | -22.9% |
| VMT Threshold Exceeded?  | No     |
| Note: VMT per Employee is the sum of all home-based work VMT generated by a project then divided by the total number of employees employed by that project or within that area. Existing model boundary average VMT per employee was calculated using the Existing conditions Butte County Association of Governments (BCAG) travel demand model (TDM) scenario. |        |

Source: Headway Transportation, 2020.

VMT is defined as the total number of miles a person travels to reach a destination. As discussed above, it is likely that destinations will be short for visitors of the project and the nature of the use would also keep VMT lower. **Table 4** below illustrates average trip lengths of the project. Again, the threshold is 85% below the average trip length for the model boundary. The average trip lengths generated by the project, based upon the analysis, would not exceed the threshold recommended by OPR.



**Table 4. Average Trip Lengths**

| Metric   | Model Boundary Average | 85% of Model Boundary Average (Threshold) | Project |
|--|------------------------|---|---------|
| HBW Average Trip Length (without IXXI Trips), miles        | 5.8                    | 4.9                                       | 5.5     |
| HBW Average Trip Length (with IXXI Trips), miles           | 6.6                    | 5.6                                       | 6.2     |
| Total Average Trip Length, miles                           | 6.7                    | 5.7                                       | 5.7     |
| Note: IXXI = internal-to-external and external-to-internal |                        |   |         |

Source: *Headway Transportation, 2020.*

The Institute of Transportation Engineers’ (ITE) Trip Generation Manual, 10th Edition, was used to estimate daily trips. Implementation of the Project (hotel) would generate approximately 40 total trips during the highest one-hour period of the day and approximately 500 total trips per day. For comparison purposes, trips generated by a hotel were compared with other allowable uses for the project site such as a medical office and a restaurant/retail mixed use. See **Table 5** below.

**Table 5. Trip Generation Comparison to Permitted Land Uses**

| Land Use   | ITE Code  | Quantity                                       | Daily | AM    |     |     | PM    |     |     |
|--|---|--|-------|-------|-----|-----|-------|-----|-----|
|  |   |  |       | Total | In  | Out | Total | In  | Out |
| Hotel  | All-Suites Hotel (311)                                  | 112 Rooms                                      | 500   | 38    | 20  | 18  | 40    | 19  | 21  |
| Medical Office   | Medical- Dental Office (720)                            | 60,000 sq ft                                   | 2,088 | 167   | 130 | 37  | 208   | 58  | 150 |
| Restaurant/ Retail Mix   | Fast Food w/ Drive-thru (934) and Shopping Center (820) | 5,000 sq ft Restaurant and 15,000 sq ft Retail | 2,921 | 215   | 111 | 104 | 220   | 112 | 108 |
| Note: Trip rates obtained from the ITE Trip Generation Manual, 10th Edition. |   |  |       |       |     |     |       |     |     |

Source: *Headway Transportation, 2020.*

As described earlier, the project site is zoned for Community Commercial (CC) uses. However, hotel use generates significantly less intense VMT as compared to other commercial uses, such as retail, office, restaurants, which are permitted by right on the project site.

The traffic study concludes that the project would create a less-than-significant impact to VMT, based upon the findings summarized above, and would not warrant a traffic signal. Overall, potential traffic impacts from the project would be considered less than significant.

#### *Traffic Signal Warrant*

The traffic analysis (**Attachment F**) used the California Department of Transportation (Caltrans) Manual on Uniform Traffic Control Devices (MUTCD) to assess the need for a traffic control signal at the Bruce Road and Sierra Sunrise Terrace intersection using Four-Hour Vehicular Volume and the Peak Hour volume warrants.

The study found the following:

- Existing conditions would not require a signal.
- Existing Plus Project condition warrant is met marginally, and a traffic signal may be considered at the intersection.
- 2040 Baseline Conditions finds a traffic signal may be warranted under the Four-Hour Vehicular Volume and PM peak hour warrants.
- The 2040 Baseline Plus Project finds that two analysis hours may meet the Four-Hour Vehicular Volume warrants; the PM peak hour volume meets the Peak Hour volume warrant.

The analysis cites that CA MUTCD states Peak Hour warrant should only be applied in “unusual cases.” Unusual cases would include manufacturing plants, industrial complexes, or high-occupancy vehicle facilities attracting or discharging large numbers of vehicles over a short time. The analysis does not find any unusual cases at the intersection and concludes a signal is not justified because of the project. The analysis only suggests there may be the need for a traffic signal in the future if application is made for development on the west side of Bruce Road, which would not be a part of this project. As community population and development growth causes traffic increases over time, the traffic analysis still recommends a signal at the intersection for traffic moving onto Bruce Road from Sierra Sunrise Terrace (Headway Transportation 2020, see **Attachment F**; Proposed Traffic Signal Plans, see **Attachment G**).

Regarding Citywide Circulation Improvements – it is required that the City of Chico collects from the developer of a project their fair share cost of circulation improvements necessary to address cumulative transportation impacts, including those to state highways, local roadways, transit, pedestrian, and bicycle facilities, through the City’s Development Impact Fee Program. Therefore, it is a requirement that developers provide funding for improvements as needed. Maintenance of the transportation network would be built-in to the scope of the project (Circulation Element of the City of Chico General Plan, Policy CIRC-1.3).

Since the warrant analyses were completed, there were few collisions at the intersection of Bruce Road and Sierra Sunrise Terrace. After reviewing the traffic signal warrant analyses, recent collision data, and the Development Impact fee Program, staff recommends installing a traffic signal at this intersection to improve intersection safety. The developer is supportive of the safety improvement and willing to install a traffic signal as part of the Project. It is estimated that the traffic signal will cost about \$375,000. The



Development Impact Fee Program budgeted only \$280,000 for the traffic signal at the time of program adoption. The construction costs have increased significantly since the adoption of the Development Impact Fee Program. The City will reimburse up to \$280,000 for the traffic signal, and the developer will be responsible for the remaining cost to install a traffic signal.

**Noise.** A project specific noise assessment was prepared to determine if the implementation of the proposed project would result in any significant impacts from noise (**Attachment H**). Noise-sensitive uses, residential parcels, are located to the northeast of the Project parcel. The assessment concluded that the primary noise source on the project site is transportation noise emanating from Bruce Road and State Highway 32, and that future transportation noise levels at interior areas of the project site are not predicted to exceed the City of Chico's interior noise level standard of 45 dBA  $L_{dn}$ , which is the average noise level over a 24-hour period.

The noise assessment predicted that construction noise generated by construction activities from the proposed project would comply with the requirements of the City of Chico Noise ordinance because construction activities are exempted during certain hours and days (10:00 a.m. to 6:00 p.m. on Sundays and holidays, 7:00 a.m. to 9:00 p.m. Monday through Saturday) (Section 9.38.060(b) of the City of Chico Municipal Code). However, if construction activities occur outside of exempt hours, the assessment predicted maximum noise levels will range from 72 to 84 dBA at 100 feet. Construction activity is required to remain below 86 dBA at any point outside of a property line. Based on the typical setback distance of 300 feet, construction noise levels are predicted to be in the range of 60-74 dBA  $L_{max}$ , the highest sound level measured during a single noise event, which meets the 86 dBA criteria. Construction activities associated with parking lot pavement will occur at closer distances to nearby residences ranging between approximately 50 to 100 feet distance. At this distance, parking lot paving will generate noise levels of approximately 71 to 77 dBA  $L_{max}$  which also meets the 86 dBA criteria.

The proposed Project is designed to ensure that noise-sensitive land uses, such as residential uses, are not exposed to noise that exceeds dBA levels as required by City policies and zoning. The proposed project will not result in any significant noise effects because noise generated by construction activities (considered to be the project's loudest noise source) will comply with the City's Noise Ordinance.

**Air Quality.** A project-specific assessment was prepared to determine the project's impact to air quality and greenhouse gas emissions (GHG) (**Attachment I**). The assessment used methods and assumptions recommended from Butte County Air Quality Management District (BCAQMD), the California Air Control Officers Association (CAPCOA), and the California Air Resources Board (CARB). Using significant levels, as recommended by BCAQMD and CAPCOA, the assessment estimated the criteria air pollutants (particulate matter, ozone, carbon monoxide, sulphur oxides, nitrogen oxides, and lead) and GHG emissions generated by the project that could potentially impact the environment. The existing ambient (outdoor) air quality in the Project area is used and federal, state, and local regulations related to criteria air pollutants were identified (ECORP Consulting 2021b, see **Attachment I**).

Construction emissions originate primarily from operation of vehicles, dust from grading, and asphalt and last only during construction activities. Emissions from construction activities were calculated using the CalEEMod computer program. Results found that air emissions generated by construction of the Project will not exceed the BCAQMD's thresholds of significance for any criteria air pollutants (ECORP Consulting 2021b, see **Attachment I**). See **Table 6**.

**Table 6. Construction-Related Project Emissions**

| Construction Year       | ROG         |               | NO <sub>x</sub> |               | CO          |               | PM <sub>10</sub> |               | PM <sub>2.5</sub> |               |
|-------------------------|-------------|---------------|-----------------|---------------|-------------|---------------|------------------|---------------|-------------------|---------------|
|                         | Daily (lbs) | Annual (tons) | Daily (lbs)     | Annual (tons) | Daily (lbs) | Annual (tons) | Daily (lbs)      | Annual (tons) | Daily (lbs)       | Annual (tons) |
| Year 2022               | 3.247       | 0.280         | 33.123          | 2.434         | 21.028      | 2.496         | 21.370           | 0.269         | 11.613            | 0.167         |
| Year 2023               | 127.01      | 1.1561        | 16.063          | 0.116         | 19.100      | 0.162         | 1.402            | 0.009         | 0.858             | 0.006         |
| <i>BCAQMD Threshold</i> | <i>137</i>  | <i>4.5</i>    | <i>137</i>      | <i>4.5</i>    | <i>None</i> | <i>None</i>   | <i>80</i>        | <i>None</i>   | <i>None</i>       | <i>None</i>   |
| <b>Exceeded</b>         | <b>No</b>   | <b>No</b>     | <b>No</b>       | <b>No</b>     | NA          | NA            | <b>No</b>        | NA            | NA                | NA            |

ROG = reactive organic gases; NO<sub>x</sub> = nitrogen oxides; CO = carbon monoxide; SO<sub>x</sub> = sulfur oxides; PM<sub>10</sub> = particulate matter up to 10 microns; PM<sub>2.5</sub> = particulate matter up to 2.5 microns.

Note: Emissions were calculated using the CalEEMod version 2020.4.0 computer program. Thresholds were used as recommended by BCAQMD for levels of significance for any criteria air pollutants.

Source: Air Quality and Greenhouse Gas Assessment Towne Place Suites Project, ECORP Consulting, August 2021.

The CalEEMod calculation of air emissions also determined that daily operations of the hotel would not exceed any BCAQMD thresholds of significance (ECORP Consulting 2021b, see **Attachment I**). See **Table 7**. As for cumulative impacts, the Project will not result in substantial net increase of criteria pollutants for areas in the region that do not meet federal or state ambient air quality standards.



**Table 7. Operation-Related Project Emissions**

| Operational Emissions   | ROG Daily (lbs) |             | NO <sub>x</sub> Daily (lbs) |             | CO Daily (lbs) |              | PM <sub>10</sub> Daily (lbs) |             | PM <sub>2.5</sub> Daily (lbs) |             |
|-------------------------|-----------------|-------------|-----------------------------|-------------|----------------|--------------|------------------------------|-------------|-------------------------------|-------------|
|                         | Summer          | Winter      | Summer                      | Winter      | Summer         | Winter       | Summer                       | Winter      | Summer                        | Winter      |
| Area                    | 4.12            | 4.12        | 0.00                        | 0.00        | 0.02           | 0.02         | 0.00                         | 0.00        | 0.00                          | 0.00        |
| Energy                  | 0.12            | 0.12        | 1.09                        | 1.09        | 0.92           | 0.92         | 0.08                         | 0.08        | 0.08                          | 0.08        |
| Mobile                  | 2.14            | 1.51        | 1.92                        | 2.20        | 11.66          | 11.86        | 1.59                         | 1.59        | 0.44                          | 0.44        |
| Stationary              | 0.57            | 0.57        | 1.61                        | 1.61        | 1.46           | 1.46         | 0.08                         | 0.08        | 0.08                          | 0.08        |
| <b>Total</b>            | <b>6.96</b>     | <b>6.33</b> | <b>4.61</b>                 | <b>4.90</b> | <b>14.07</b>   | <b>14.27</b> | <b>1.76</b>                  | <b>1.76</b> | <b>0.61</b>                   | <b>0.61</b> |
| <i>BCAQMD Threshold</i> | <i>25</i>       | <i>25</i>   | <i>25</i>                   | <i>25</i>   | <i>None</i>    | <i>None</i>  | <i>80</i>                    | <i>80</i>   | <i>None</i>                   | <i>None</i> |
| <b>Exceeded</b>         | <b>No</b>       | <b>No</b>   | <b>No</b>                   | <b>No</b>   | <b>NA</b>      | <b>NA</b>    | <b>No</b>                    | <b>No</b>   | <b>NA</b>                     | <b>NA</b>   |

ROG = reactive organic gases; NO<sub>x</sub> = nitrogen oxides; CO = carbon monoxide; SO<sub>x</sub> = sulfur oxides; PM<sub>10</sub> = particulate matter up to 10 microns; PM<sub>2.5</sub> = particulate matter up to 2.5 microns.

Note: Emissions were calculated using the CalEEMod version 2020.4.0 computer program. Thresholds were used as recommended by BCAQMD for levels of significance for any criteria air pollutants.

*Source: Air Quality and Greenhouse Gas Assessment Towne Place Suites Project, ECORP Consulting, August 2021.*

The air quality assessment also discusses the Northern Sacramento Valley Air Basin 2018 Air Quality Attainment Plan that focuses on a project’s impacts to air quality for the long term. The 2018 Air Quality Attainment Plan uses growth data from the general plans of cities and counties to plan for emissions reduction. Because the project is consistent with the City of Chico General Plan and its projections for growth, it is consistent with the 2018 Air Quality Attainment Plan. The assessment determined the Project is consistent with the plan because it will not inhibit the region in meeting state and federal air quality standards. The assessment also determined the Project will not create a carbon monoxide hot spot (ECORP Consulting 2021b, see **Attachment I**).

In addition, the Project will not affect or prohibit the Regional Transportation Plan/Sustainable Communities Strategy (the product of Senate Bill 375 that directs the California Air Resources Board to set regional targets for reducing greenhouse gas emissions) targets for emission reduction (ECORP Consulting 2021b, see **Attachment I**).

At preparation of the air quality assessment for the Project, the City of Chico was preparing a qualified greenhouse gas reduction plan (City of Chico 2020 Climate Action Plan), though it had not yet been adopted. Currently, neither the City of Chico nor the BCAQMD have established GHG emission thresholds. A review of the draft Climate Action Plan identified no proposed CEQA-related significance thresholds for GHG emissions generated by land use development or any specific performance

standards. Instead, Project GHG emissions are quantified and compared to the thresholds issued by the California Air Pollution Control Officers Association (CAPCOA), under this assessment. CAPCOA is an association of air pollution control officers from all 35 local air quality agencies throughout California, including the BCAQMD. Due to the project’s size, CAPCOA recommends a significance threshold of 900 metric tons annually which is based upon a capture rate of 90 percent of land use development projects and, therefore, a 90 percent capture rate of all GHG emissions. The 900 metric ton threshold is the lowest promulgated in any region in the state. CAPCOA considers the 900 metric ton threshold low enough to capture a large portion of future projects that would accommodate statewide population and economic growth, while at the same time, setting the emission threshold high enough to exclude small projects that will, in aggregate, contribute a relatively small fraction of the cumulative statewide GHG emissions.

Using the threshold described above, construction emissions related to the project are projected to be 672 metric tons annually, as shown in **Table 8** below. The Project will produce greenhouse gas emissions with the ability to cause global warming at levels below the CAPCOA significance threshold of 900 metric tons annually at 672 metric tons annually.

**Table 8. Construction Related Greenhouse Gas Emissions**

| Description   | CO <sub>2</sub> e Emissions (Metric Tons/Year) |
|---|--|
| Construction in 2022  | 438  |
| Construction in 2023  | 234  |
| <b>Project Construction Total</b>   | <b>672</b>                                     |
| <i>CAPCOA Threshold</i>   | 900  |
| <b>Exceed Threshold?</b>  | <b>No</b>                                      |
| Note: Emissions were calculated using the CalEEMod version 2020.4.0 computer program. |  |

*Source: Air Quality and Greenhouse Gas Assessment Towne Place Suites Project, ECORP Consulting, August 2021.*

The assessment also concludes that operational activities related to the Project will produce greenhouse gas emissions with the ability to cause global warming at 696 metric tons annually, which is below the CAPCOA significance threshold of 900 metric tons annually. See **Table 9** below.



**Table 9. Operational-Related Greenhouse Gas Emissions**

| <b>Description</b>   | <b>CO<sub>2</sub>e Emissions (Metric Tons/Year)</b> |
|--|---|
| Area Source Emissions  | 0   |
| Energy Emissions   | 334   |
| Mobile Source Emissions  | 293   |
| Stationary Source Emissions  | 27  |
| Waste Emissions  | 37  |
| Water Emissions  | 6   |
| <b>Project Operations Total</b>  | <b>696</b>  |
| CAPCOA Threshold   | 900   |
| <b>Exceed Threshold?</b>   | <b>No</b>   |
| Notes: Emission projections are predominantly based on CalEEMod model defaults for Butte County. Onroad Source emissions data used in CalEEMod is based on average daily trip data from Headway Transportation (2020). |   |

*Source: Air Quality and Greenhouse Gas Assessment Towne Place Suites Project, ECORP Consulting, August 2021.*

Under Title 24, Part 6, of the California Code of Regulations (Title 24), it is required that the project is built to the Energy Efficiency Standards for Residential and Nonresidential Buildings. The 2019 Energy Standards focus on several key areas to improve the energy efficiency of newly constructed buildings and additions and alterations to existing buildings. According to the City’s updated Climate Action Plan, Title 24 Energy Standards compliance would anticipate progressive savings in carbon emissions over time, totaling 4,705 metric tons of CO<sub>2</sub> emissions by 2045. The update to Title 24 supports the City of Chico Climate Action Plan’s goals of reducing GHG emissions to net zero by 2045, consistent with State targets. The project will be required to comply with California Green Building Standards Code—Part 11, Title 24, California Code of Regulations— also referred to as CALGreen and is a mandatory green building standards code. CALGreen was developed to meet the goals of California’s AB 32, establishing a program of cost-effective reductions of GHG to 1990 levels by 2020. CALGreen enhancements include higher energy efficiency, better air quality, and improved daylighting. The first CALGreen Code was adopted in 2009 and is updated every three years with stricter policies; the most recent update was in 2019.

In alignment with the City of Chico Climate Action Plan, the project provides active transportation infrastructure to support the City in achieving greater than 6% bicycle mode share by 2030 and 12% bicycle mode share by 2045. The project includes three locations for 20 bicycle parking spaces (two locations on the north side of the building and one on the west side). In addition, the project provides a

total of 18 parking spaces that are designated for electric vehicles. Lastly, the main entrance to the hotel faces Sierra Sunrise Terrace which provides convenient access to transit, bicycle, and pedestrian facilities. Fixed route transit service and a bus stop is located on Bruce Road, immediately south of Sierra Sunrise Terrace, at the project site (City of Chico, Climate Action Plan Update, 2021).

The assessment also concludes the Project will not expose sensitive receptors or populations susceptible to health effects (e.g., medical patients residing in health care facilities, children, users of athletic facilities, daycare facilities, schools) to substantial air contaminants because none of these facilities are located near the project site. The Project also would not increase population figures over those that have been planned for the area and would not result in a long-term impact on the region's ability to meet State and Federal air quality standards (ECORP Consulting 2021b, see **Attachment I**).

Furthermore, the Project's features support all applicable City policies designed to improve air quality. For example, the project contributes to a reduction in VMT by adding a new service use. The project also implements parking for bicycles and electric vehicles and is conveniently adjacent to public transit (Open Space and Environment, City of Chico General Plan, 2011). Impacts to air quality have been carefully evaluated and impacts are considered less than significant.

**Water Quality.** A site-specific biological resources assessment analyzed the potential for the Project to affect water quality. The assessment took into consideration the fact that Project received authorization for a Nationwide Permit (NWP) 18, Minor Discharges Regulatory Division by the United States Army Corp of Engineers (USACE) Sacramento District, which certifies the Project will result in only minor discharges of material into waters and will not exceed minor thresholds that would negatively impact water quality. Though California Park Lake is approximately 240 feet north of the Project, the NWP ensures the Project activity would not substantially disrupt aquatic species at the lake; the Project will avoid impacts to breeding areas for migratory birds; and the Project will not use toxic materials for construction or operation that could be detrimental to waters, such as the lake (ECORP Consulting 2021a, see **Attachment J**).

The Project also received a Clean Water Act, Section 401 Certification from the Central Valley Regional Water Quality Control Board (RWQCB) (**Attachment K**), which certifies that activities as related to the Project can comply with water quality standards, effluent limitations, new source performance standards, as well as toxic pollutants restrictions (ECORP Consulting 2021a, see **Attachment J**).

A geotechnical study of the site prepared for the proposed project did not discover groundwater. However, if groundwater is encountered during construction activities, the project would be required to avoid any adverse impacts caused by groundwater encountered in excavations. Therefore, the project would have a less-than-significant impact to groundwater (CGI Technical Services 2015, see **Attachment B**).

The Project complies with the City's goals and policies for water conservation. In support of such policies, the Project has complied with the California Regional Water Quality Control Board's regulations and standards to maintain, protect, and improve water quality. Also, to control runoff from the new development, the project will include landscaping that will cover 57 percent of the site, as well as a bioswale for stormwater to flow to the storm drain system along Highway 32. Lastly, as mentioned, a 25-foot setback will be provided to protect the creek ([Open Space and Environment, City of Chico General Plan, 2011](#)).



In summary, the Project will not violate standards for wastewater discharge, neither to surface water nor groundwater. Further, the project will comply with NPDES and RWQCB requirements, and will not have the capacity to conflict with or be harmful to a water quality control plan or groundwater management plan for the region.

The project's hydrology and water quality effects would not result in significant water quality impacts because it must comply with stormwater permit requirements including implementing a Stormwater Control Plan for the project (ECORP Consulting 2021a, see **Attachment J**).

*(e) The site can be adequately served by all required utilities and public services.*

Utilities and public services to the project site are existing and will not require improvements outside of the project site and area adjacent to the project site. A summary of service providers and any potential impacts to water supply, wastewater service, solid waste service, and energy and natural gas service are analyzed below.

#### *Water Supply*

Water service to the project site is provided by the Chico District of the California Water Service Company (CalWater). The 2020 Urban Water Management Plan (UWMP) prepared by the California Water Service Company for the Chico-Hamilton City District, estimated that the water use for the service area is 185 gallons per capita daily (gpcd).

The proposed project includes 112 hotel rooms which, at full occupancy and two occupants per room and ten staff members on site, would result in an overall daily water consumption of 43,290 gallons (0.13-acre feet (af)). The 2020 UWMP states that the overall deliveries by CalWater to the district were approximately 62 af of water daily. The proposed project would result in a 0.21 percent increase in water demand within Chico-Hamilton City Water service area. This anticipated increased demand for water is not considered substantial and the UWMP has sufficient capacity to meet the needs of the proposed project.

#### *Wastewater Service*

Wastewater treatment for the project site is provided by the City of Chico Water Pollution Control Plant (WCPC) located approximately 4.0 miles southwest of the city in the western portion of Butte County. WCPC currently has a 12 million gallon per day (mgd) capacity with plans to expand to 15 mgd in the future.

The projected amount of water consumption of the proposed project has been used to predict the quantity of wastewater that will be generated by the proposed project. Estimating that that the proposed project would generate 43,290 gallons of wastewater per day would result in a 0.36 percent increase in daily treatment capacity at the WCPC. This anticipated increased demand for wastewater service is not considered substantial and the WCPC has sufficient capacity to meet the needs of the proposed project.

### *Solid Waste Service*

Solid waste removal service for the proposed project will be provided by Recology and solid waste generated by the proposed project will be disposed of at the Neal Road Recycling and Waste (NRRWF).

The NRRWF has a permitted capacity of approximately 25.3 million cubic yards and a remaining capacity of 20.8 million cubic yards. Currently, the maximum amount of the solid waste accepted by NRRWF is 1,500 tons with daily amounts rarely exceeding 1,200 tons. (California Integrated Waste Management / Board Neal Road Recycling and Waste Facility)

Cal Recycle estimates that hotel generates two pounds of solid waste per room per day (2006 Waste Disposal and Diversion Findings for Selected Industry Groups). The proposed project includes the development of a 112-room hotel which would generate a maximum estimated 224 pounds of solid waste daily. This anticipated increased demand for solid waste service is not considered substantial and the NRRWF has sufficient capacity to meet the needs of the proposed project.

### *Energy and Natural Gas Service*

Pacific Gas and Electric (PG&E) provides electricity and natural gas to the project site. An Energy Consumption Assessment completed for the proposed project (**Attachment L**) analyzed the potential direct and indirect environmental impacts associated with any energy consumption, including the depletion of nonrenewable resources of the proposed project during its construction and operational phases. The assessment quantified the estimated amount of energy and natural gas that will be necessary for the proposed project to conduct operations and then compared that estimate to the energy and natural gas consumed by all non-residential land uses in Butte County. Energy consumption associated to be used by the proposed project was 1,236,620-kilowatt hours annually, which would represent a 0.170 percent increase to total energy consumption in Butte County. The assessment further found that the proposed project would consume 40,617 therms of natural gas annually which would represent an increase of 0.245 percent to the total natural gas consumption in Butte County. This anticipated increased demand for energy and natural gas is not considered substantial and PG&E has sufficient capacity to meet the needs of the proposed project.

The proposed project can be adequately served by all required utilities and public services. The site is located within an urbanized area with adjacent utilities available and with sufficient capacity to serve the proposed project.

### **Exceptions**

The applicability of all Categorical Exemptions is qualified by the exceptions listed in CEQA Guidelines Section 15300.2. In the discussion below each subsection (*italicized*) is followed by an explanation of why these exceptions do not apply to the proposed project.

- (a) Location. Classes 3,4,5,6, and 11 are qualified by consideration of where the project is to be located – a project that is ordinarily insignificant in its impact on the environment may in a particularly sensitive environment be significant. Therefore, these classes are considered to apply in all instances, except where the project may impact on an environmental resource of hazardous or critical concern where designated, precisely mapped, and officially adopted pursuant to law by federal, state, or local agencies.*



Categorical Exemptions under Section 15332 (In-Fill Development Projects) are Class 32 activities. As such, this exception is not applicable. In addition, the Project would not result in significant effects on the environment due to the project location, or the project design. The project location is not a sensitive environmental site, as it is a disturbed site based on previous grading activities and surrounded by existing roadways and urbanized uses.

*(b) Cumulative Impact. All exemptions for these classes are inapplicable when the cumulative impact of successive projects of the same type in the same place, over time is significant.*

The Project will contribute to cumulative impacts that could occur under the full buildout assumed by the City's General Plan, but the Project will not result in any new or more significant cumulative impacts than those already assumed in the City's General Plan, which assumed increased density of development at the intersection of State Highway 32 and Bruce Road and other areas of the city. The Project is consistent with the assumptions and impacts identified in the City's General Plan and accompanying Environmental Impact Report (EIR).

*(c) Significant Effect. A categorical exception shall not be used for any activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances.*

Implementation of the Project, with the applicable City's Standard Conditions of Approval, would not result in any significant environmental effects. As described below, there are no unusual circumstances associated with the proposed project that will result in any significant effects on the environment.

As previously mentioned, California Park Lake is located approximately 240 feet north of the Project. However, the lake is surrounded by existing development consisting of low-density residential, high-density residential for seniors, and areas of commercial development immediately adjacent to the lake. The lake is also used for motor boating and other recreational purposes, such as canoeing. Though the lake is near the Project, the NWP ensures activities from the Project would not substantially disrupt aquatic species at the lake. The Project will also not use toxic materials that could be detrimental to waters, such as the lake (ECORP Consulting 2021a, see **Attachment J**).

Additionally, the site is currently vacant, and development of the Project will change the overall setting at the site. However, as mentioned, the Project site is previously graded and zoned for development of commercial uses to implement City goals and policies. For example, the location of the project aligns with the City's goals for compact development that does not spread toward the foothills or agricultural areas (Land Use and Community Design, City of Chico General Plan, 2011).

*(d) Scenic Highways. A categorical exception shall not be used for a project which may result in damage to scenic resources, including but not limited to, trees, historic buildings, rock outcroppings, or similar resources, within a highway officially designated as a state scenic highway. This does not apply to improvements which are required as mitigation by an adopted negative declaration or certified EIR.*

The Project will not result in damage to scenic resources, including but not limited to, trees, historic buildings, rock outcroppings, or similar resources, within a highway officially designated as a state scenic highway. According to the California State Scenic Highway System Map, the project site is not located

near, or adjacent to, a highway officially designated as a state scenic highway, and the project site itself does not contain any scenic resource.

### *Effects on Scenic Resources*

The Project will not damage a scenic resource. The site is not located near a scenic resource, designated or otherwise. Rather, the site is adjacent to existing commercial and residential development and major roads. The Project site is graded and zoned and designated for commercial development. The Project complies with the General Plan and Zoning Ordinance, both of which are designed to preserve the scenic views of the neighboring topographies: foothills to the east, agriculture to the west, ravines, and the City's many creeks. The hotel Project would be subject to review by the Planning Commission, Architectural Review and Historic Preservation Board, and the Planning staff for consistency with the City's design guidelines and development standards. The implementation of the General Plan's policy provisions and the Municipal Code ensures that no adverse impact to a scenic resource would occur. Therefore, the project's impact to scenic resources is less than significant. Additionally, the Municipal Code and city's Design Guidelines Manual instructs new development to be compact, infill near existing development to prevent encroachment on the neighboring landscapes, while also harmonizing with the City's natural setting, thereby avoiding impacts to scenic views (4.13 Visual Resources and Aesthetics, General Plan Update, Draft Environmental Impact Report, 2010 and Chapter 19.18 of the City of Chico Municipal Code).

Post-construction views would be similar in character to current views with implementation of City policies and guidelines designed to integrate new development with existing structural and natural settings. As shown in **Figures 6a – 6d**, visual simulations were obtained that show five viewpoints, allowing an assessment of impacts to viewsheds. As shown, no scenic resources would be obstructed.

- View 1- looking southwest from Sierra Sunrise Terrace toward the hotel's northeast elevation, nighttime views of the hotel and lighting are minimal and without glare. See **Figure 6b**.
- View 2- looking south from Sierra Sunrise Terrace toward the hotel's north elevation, views of the hotel and lighting are minimal and without glare; the hotel is minimally visible in daytime. See **Figure 6b**.
- View 3- looking southeast from the intersection of Bruce Road and Sierra Sunrise Terrace toward the northwest corner of the parcel, which is covered by a mass of trees; the hotel is not visible in nighttime or the daytime. See **Figure 6c**.
- View 4- looking northeast from the southeast corner of Bruce Road and SR 32 toward the southwest corner of the parcel, the hotel is not visible in nighttime or daytime. See **Figure 6c**.
- View 5- looking northwest from SR 32 toward the hotel's southeast elevation, the hotel is not visible in nighttime or the daytime view. See **Figure 6d**.

Overall, the foothills are far in the distance and, as such, are almost indiscernible and minimally obstructed by existing development near the site.

Building setbacks exceed what is required by development standards for the CC zoning district (minimum 10 feet side and rear): 157 feet from Bruce Road, 89 feet from Sierra Sunrise Terrace, 80 feet from SR 32, and 344 feet from its eastern property line. Standards also require a minimum of 10% of the site area to be landscaped - the Project will landscape 57% of the site. The building's setbacks, combined



with a landscape buffer, will reduce visual impacts of the Project to levels that are considered less than significant.

Building colors will be neutral, which will not draw attention. All building elevations are articulated, and the architecture is modern. Large, shaded seating areas are provided for the use of visitors and employees at the hotel's main entrance. Trees and colorful landscaping are adjacent to the building. The site is surrounded by trees and landscaping, providing a strong buffer between the site's building and parking and adjacent roads and properties. Overall, renderings of the Project's design would complement structures that presently exist in the project vicinity, including the proposed building materials, massing, and scale; see **Figures 7a – 7h**.

Further, City policies to protect creeks applies to the Project because of the creek adjacent to the west side of the parcel. The General Plan's Parks, Public Facilities, and Services Element requires that development adjacent to creeks address setbacks, building orientation, security measures, and lighting to promote and protect the creeks (Action PPFS-2.1.2). The Project will provide a 30-foot setback from the top of the bank adjoining the creek to comply with the City's Creekside Development Standards, which requires a minimum setback of 25 feet from the top of bank in all zoning districts. "Top of bank" refers to the upper elevation of land, having a slope not exceeding 10 percent, which confines the channel waters flowing in a watercourse in their normal winter flow (4.13 Visual Resources and Aesthetics, General Plan Update, Draft Environmental Impact Report, 2010 and 19.60.030, E. of the City of Chico Municipal Code).

Construction activities and associated equipment will be exposed to surrounding uses, motorists, pedestrians, and bicyclists. However, construction will be temporary in nature and cease upon completion of construction. Impacts from construction will be lessened as proscribed by conditions of project approval. For example, conditions may include the installation of temporary visual barriers, such as chain link fencing with privacy slats, windscreen material, etc., around work areas to obstruct undesirable views of construction activities and minimizing project-related light and glare to the maximum extent feasible.

#### *Damage to Scenic Resources*

There are no state scenic highways in the Planning Area, therefore, the project would have no impact on scenic resources in this regard. The Planning area is defined by the City of Chico as all land within the City limits, land within the City's designated Sphere of Influence (SOI), and other land in unincorporated Butte County outside of these boundaries which, in the City's judgment relates to the City's planning efforts (4.13 Visual Resources and Aesthetics, General Plan Update, Draft Environmental Impact Report, 2010).

Further, the area of work for the Project is flat, vacant land that has been previously graded and would not require the removal of tree resources. There are no historic buildings, nor history of historic buildings on the site and no historic buildings in the vicinity that would be affected by the Project. Therefore, there would be no impacts to scenic resources (Section 19.68.050 of the City of Chico Municipal Code).

#### *Conflict with Regulations Governing Scenic Quality*

The Project is in an urbanized area, east of SR 99 where much commercial and higher density residential has been developed. The Project site is designated and zoned for uses high in intensity. The building

height, massing, and scale will be similar to existing commercial and multi-family residential uses near the site.

As can be seen in **Figures 6b – 6d**, the Project will be visually buffered by trees and landscaping. The Project exceeds City requirements for landscaping. A total of 102,241 SF of the site will be landscaped and consist of trees, shrubs, and ground cover. Landscaping will occur around the site's perimeter, along roads, as well as within parking islands and medians in the parking area. The final landscape plan will be subject to approval. To avoid impacts to the creek, no grading or filling, planting of non-native or non-riparian plant species, or removal of native vegetation is allowed to occur within the 25-foot creekside setback (Section 19.60.030 of the City of Chico Municipal Code).

The Project's impacts to the area's visual character will be less than significant due to existing intense development nearby. Additionally, the Project will not conflict with City zoning or policies. The Project complies with the City's zoning ordinance, which is designed to protect scenic character, while at the same time meeting policy goals of the General Plan for new compact commercial development.

Project construction activities will change existing views of the project site. However, the Project will be conditioned to reduce to such impacts to levels that would be less than significant during construction. Further, such activities will be short-term and would cease when construction is complete.

#### *Light and Glare*

The Project will not create a new source of substantial light or glare that would adversely affect day or nighttime views in the area. The hotel would be considered a stationary source of lighting. Stationary sources of nighttime light include structure illumination, landscape lighting, lighted signs, and streetlights (4.13 Visual Resources and Aesthetics, General Plan Update, Draft Environmental Impact Report, 2010).

It will be required by the City that Project lighting be compatible with the existing surroundings. Lighting and glare are required to be architecturally oriented and directed to the area inside the Project's property lines to limit the spill over of light and glare onto adjacent properties. For example, exterior lighting must be shielded, recessed, and directed away from adjacent properties. Lighting plans that exhibit a footcandle map with the intensity of all lighting at the Project are also subject to review (Section 19.60.050 of the City of Chico Municipal Code).

Trees and landscaping will surround the Project site, and the building material is predominantly stucco rather than glass, all of which will reduce potential glare. Minimally sized windows, trees, and window treatments will also help reduce potential light and glare to level that is considered less than significant. See **Figures 6a – 6d and 7a – 7h**.

Because the Project site is in an urban area, light and glare emanates from existing uses near the proposed Project, including the office commercial uses to the north and east, as well as from the high- and low-density residential developments to the north and east. Major roads run adjacent to the Project, Sierra Sunrise Terrace to the north, Bruce Road to the west, and SR 32 to the south, creating light and glare from existing streetlighting, stop lights at the intersection of Bruce Road and SR 32, and the traffic on the adjacent roads. However, as a result of building materials, landscaping, and low amount of Project traffic, glare from the Project would be less than that of existing uses nearby.



(e) Lighting impacts from construction are also considered to be less than significant because construction would not occur at nighttime and construction activities are exempted during certain hours and days (10:00 a.m. to 6:00 p.m. on Sundays and holidays, 7:00 a.m. to 9:00 p.m. Monday through Saturday) (Section 9.38.060(b) of the City of Chico Municipal Code). *Hazardous Waste Sites. A categorical exemption shall not be used for a project located on a site which is included on any list compiled pursuant to Section 65962.5 of the Government Code.*

A Phase 1 Environmental Site Assessment (ESA) determined that the Project site does not have the potential to contain hazardous substances and does not exhibit any Recognized Environmental Conditions (RECs) where hazardous substances or petroleum-based products may have been released into the environment or where there is potential for future releases of hazardous substances. Additionally, there are no past recorded releases of hazardous substances or use restrictions for the site (Historical Recognized Environmental Conditions (HRECs)). There are also no risks currently identified for material environmental or environmentally driven impacts on the business associated with the planned use of the Project site (Business Environmental Risks (BERs)), for example, no hazardous waste has been left onsite that would require disposal, no underground storage tank that would require removal due to potential hazards, and no lead-based paint containing lead, a highly toxic metal that may cause a range of health problems, especially in young children, has been discovered at the site (Sierra Delta Consultants 2019, see **Attachment M**).

(f) *Historical Resources. A categorical exemption shall not be used for a project which may cause a substantial adverse change in the significance of a historical resource.*

The project will not cause a substantial adverse change in the significance of a historical resource. There are no structures existing at Project site. The project site is on an undeveloped parcel. The site is within the Community Commercial zoning district and is designated by the General Plan as a Commercial Mixed Use land use and would anticipate commercial development, and there would be no impacts to a historical resource within or adjacent to the Project site. Further, the site and surrounding structures are not currently listed on the National Register of Historic Places (NRHP) and are not located within an historic district.

There will also be no significant impact on other cultural resources. The City of Chico General Plan lists the project site as an area of high archeological sensitivity. However, the Initial Study for the Gonzales Grading Plan references an Archaeological Inventory study conducted in 2016 that did not reveal any historical, archaeological, or paleontological resources at the Project site. The study included a records search at the Northeastern Information Center, an intensive level pedestrian survey, consultation with the Native American Heritage Commission (NAHC), and consultation with local Native American tribes to identify cultural resources, or the potential for resources in the project area (see **Attachment N**).

Pursuant to State Health and Safety Code Section 7050.5, upon the discovery of human remains during development and construction of the Project, the County Coroner must be notified immediately. No further disturbance shall occur in the area of discovery or in areas suspected of overlying remains until the coroner has decided the origin and disposition of the remains. Pursuant to Public Resources Code Section 5097.98, which states that “no person shall obtain or possess any Native American artifacts or human remains which are taken from a Native American grave or cairn,” it is required that the coroner notify the Native American Heritage Commission (NAHC) within 24 hours of determining the remains are not subject to his authority and could, therefore, be Native American. The NAHC will determine and

notify a Most Likely Descendant (MLD). With the landowner's permission, the MLD may inspect the site of the discovery, and may make recommendations to the NAHC on the disposition of the remains; additionally, if the remains cannot be identified, the landowner shall inter the Native American human remains on the property in a location not subject to further and future subsurface disturbance.

## FIGURES

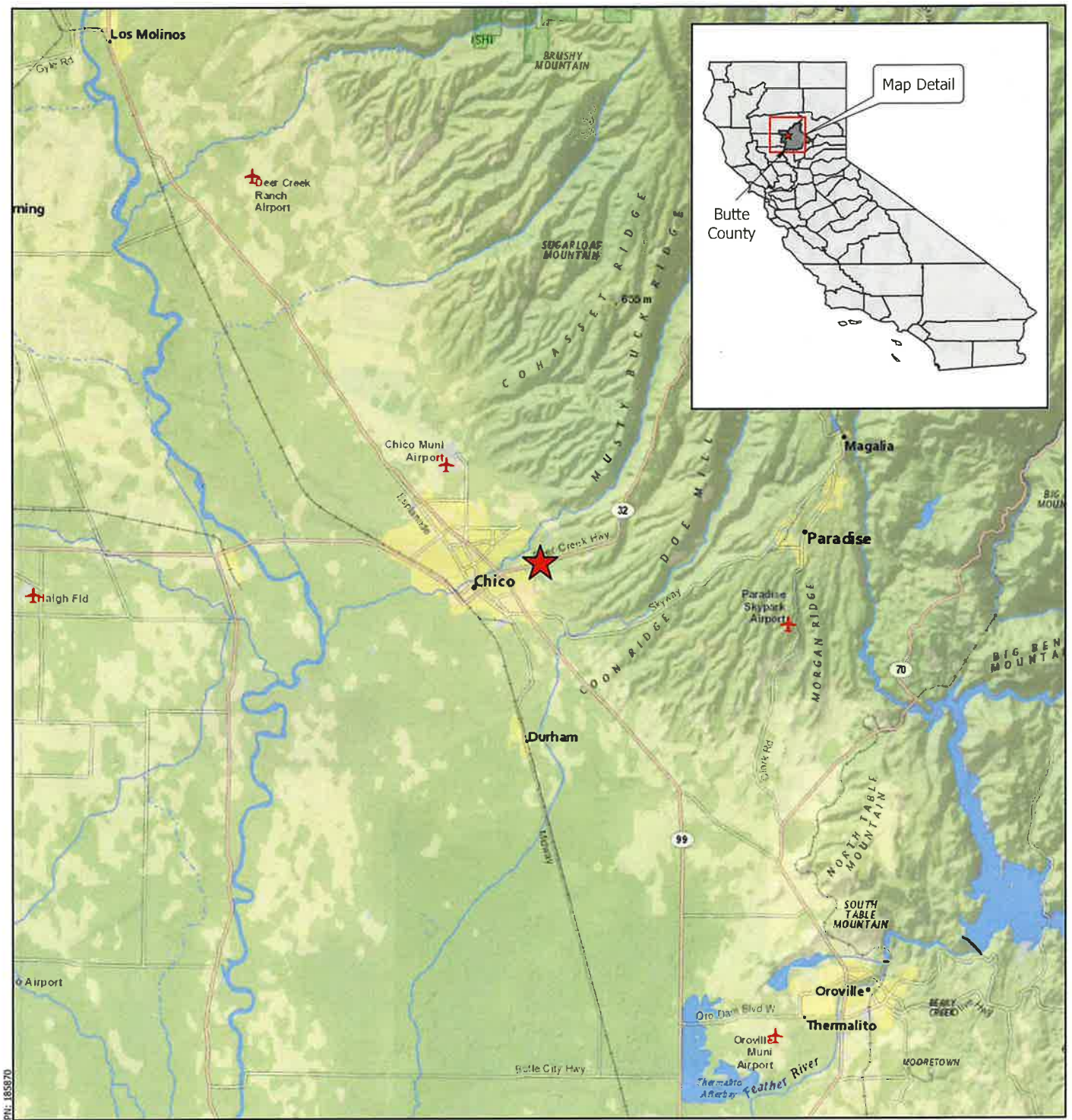
1. Regional Map
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3. Project Area Map
4. Project Site Plan
5. Critical Habitat Map
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  - c. View 3 and View 4
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  - e. Rear Building View of Pool Area Perspective (Facing Highway 32)
  - f. Front Building View (Facing Sierra Sunrise Terrace)
  - g. Rear Building View (Facing Highway 32)
  - h. Side and Rear Building View (Facing East and Highway 32)



## ATTACHMENTS

- A. Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program, Gonzales Grading Plan ER 18-01
- B. Geotechnical Report, CGI Technical Services, August 31, 2015
- C. Towne Place Suites Site Plan, Northstar, November 22, 2021
- D. Towne Place Suites Biological Assessment Update, ECORP Consulting, July 2, 2021
- E. VMT Analysis for Towne Place Suites, Headway Transportation, November 11, 2020
- F. Traffic Analysis and Signal Warrant Evaluation for Towne Place Suites, Headway Transportation, January 28, 2020
- G. Bruce Road at Sierra Sunrise Terrace Proposed Traffic Signal Plans, Headway Transportation, September 7, 2021
- H. Environmental Noise Assessment Chico Towne Place Suites, Saxelby Acoustics, September 7, 2021
- I. Air Quality and Greenhouse Gas Assessment Towne Place Suites Project, ECORP Consulting, August 2021
- J. Environmental Permit Due Diligence for Cal Park Hotel Site Project, ECORP Consulting, July 12, 2021
- K. Section 401 Water Quality Certification, California Water Board, August 29, 2018
- L. Energy Consumption Assessment Towne Place Suites Project, ECORP Consulting, August 2021
- M. Phase 1 Environmental Site Assessment, Sierra Delta Consultants, May 23, 2019
- N. City of Chico Draft Initial Study, Gonzales Grading Plan, April 2018
- O. Towne Place Suites Civil Plan Set, Northstar, July 1, 2021
- P. Towne Place Suites Landscape Plans, Thomas Phelps, June 10, 2021
- Q. Towne Place Suites Architectural Drawings, June 10, 2021

To view attachments- Please visit the City of Chico website at <https://chico.ca.us/post/towne-place-suites-hotel-proposed-bruce-road-hwy-32-and-sierra-sunrise-terrace>



PH: 155870

 Project Location

TOWNE PLACE SUITES PROJECT

**Michael Baker**  
INTERNATIONAL



Source: Esri, ArcGIS Online, National Geographic World Map: Chico, California

# Regional Map

Figure 1





 Project Location





PN: 165570

 Project Area



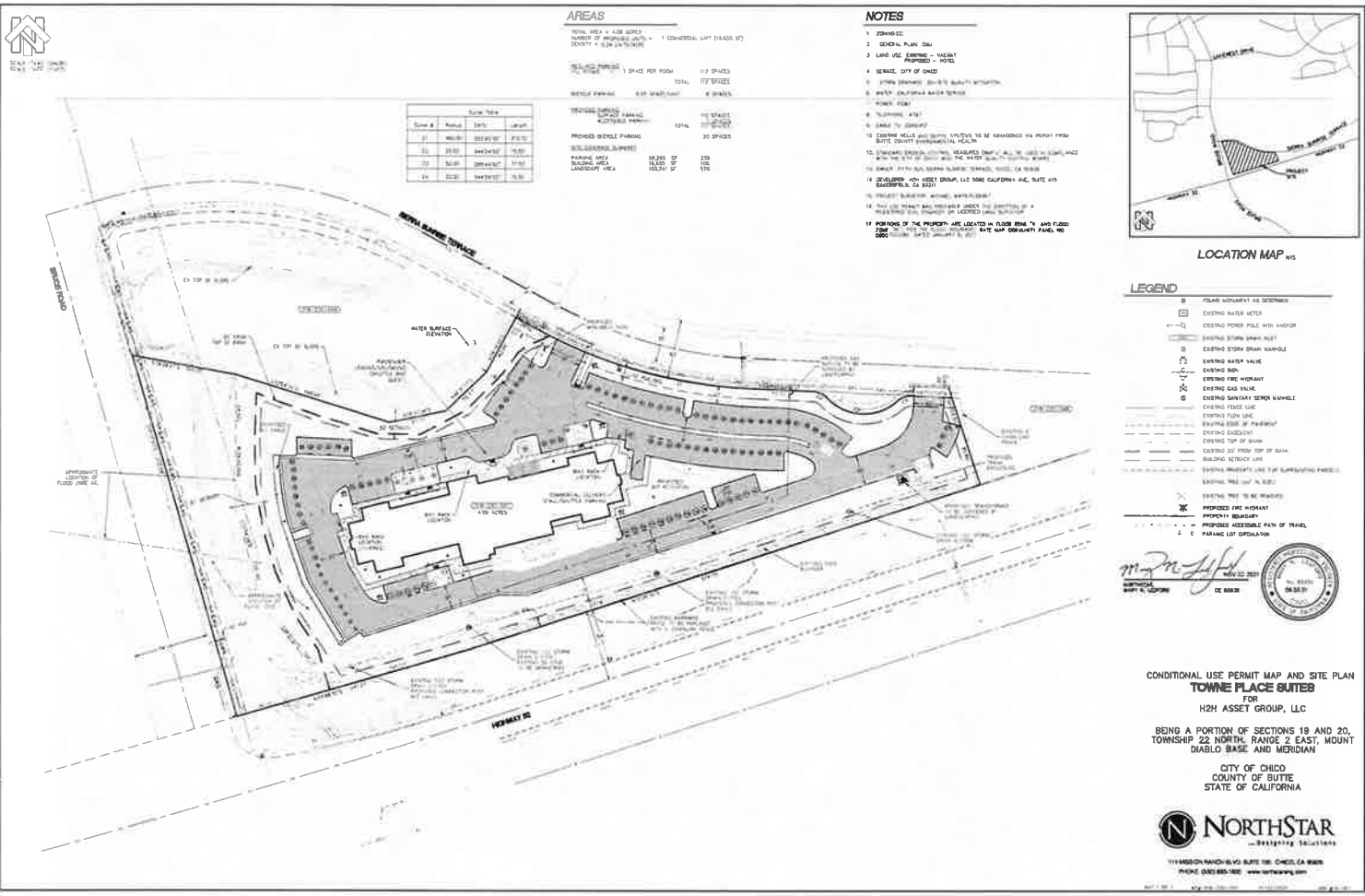


Figure 4

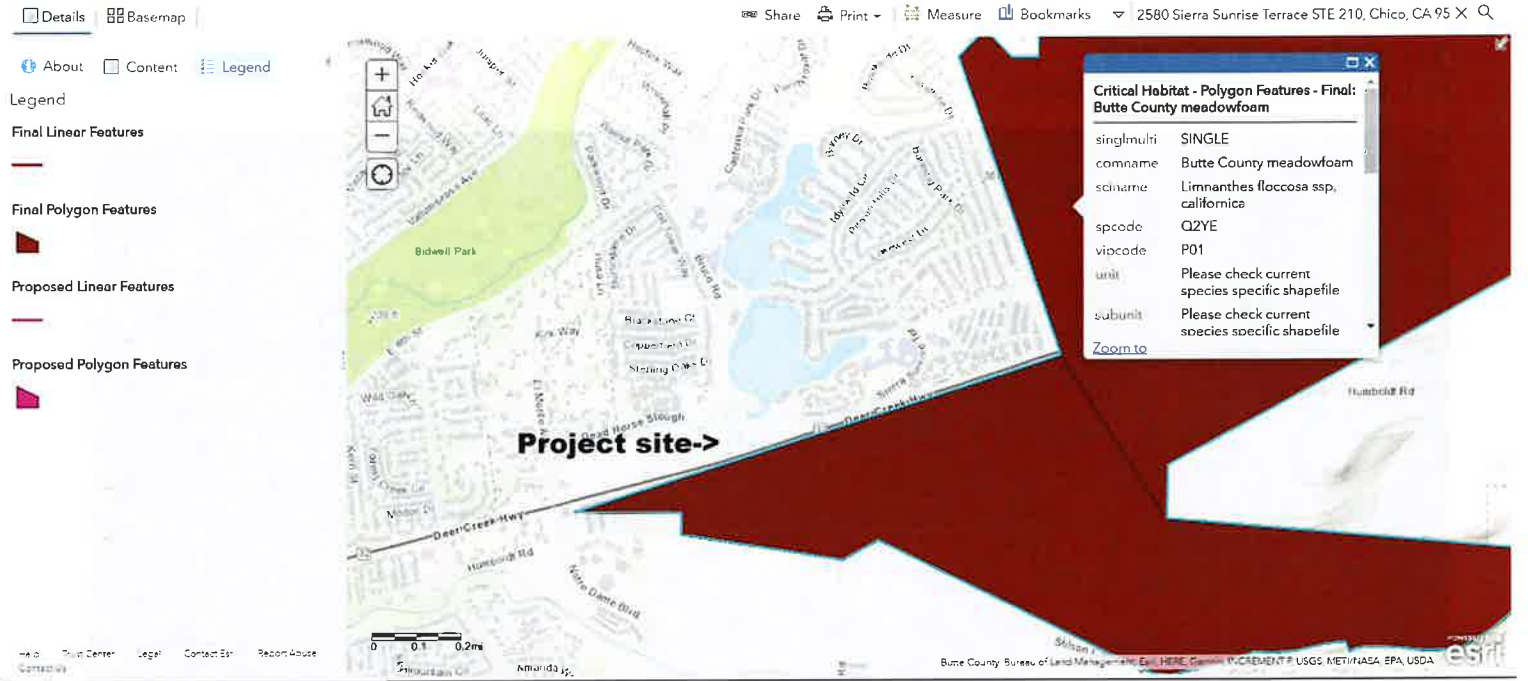


Figure 5

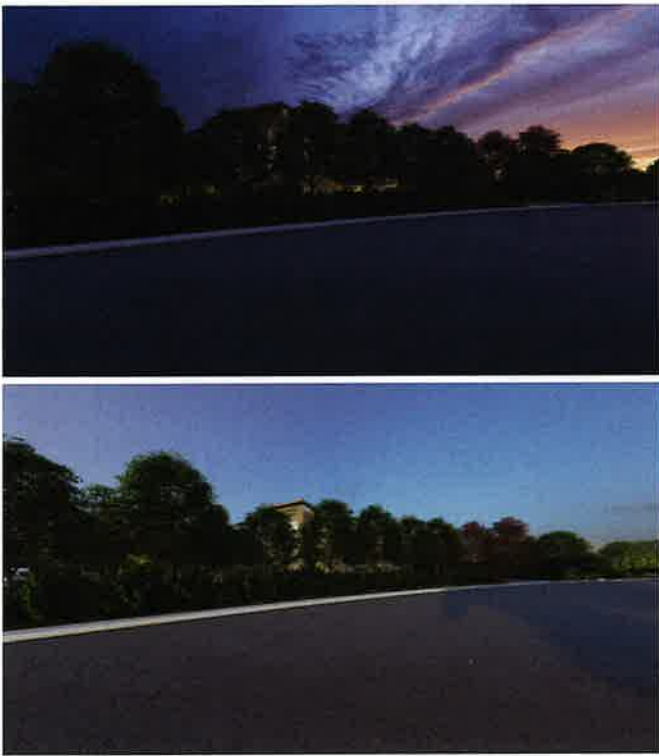


**TOWNSHIP SUITES**  
BUILDING & CAMERAS LOCATIONS

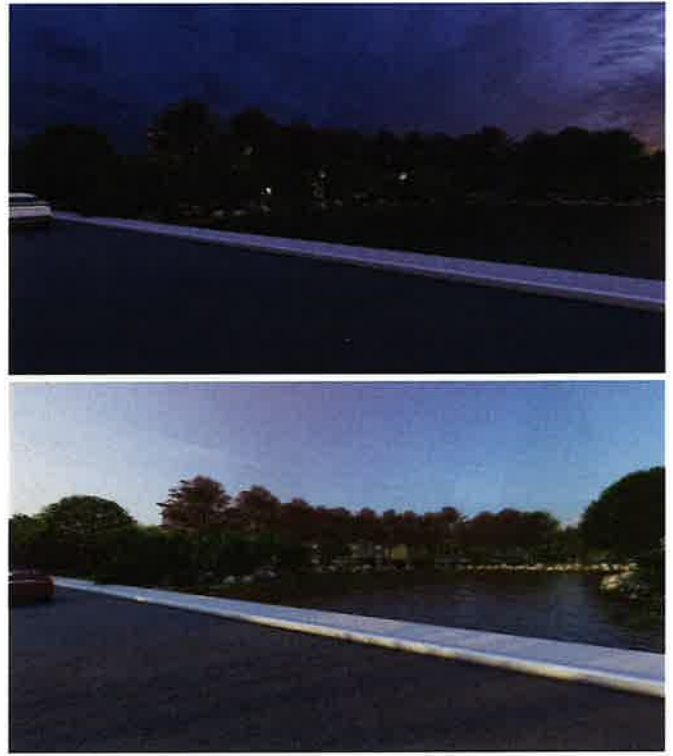


**Figure 6a**

VIEW 1



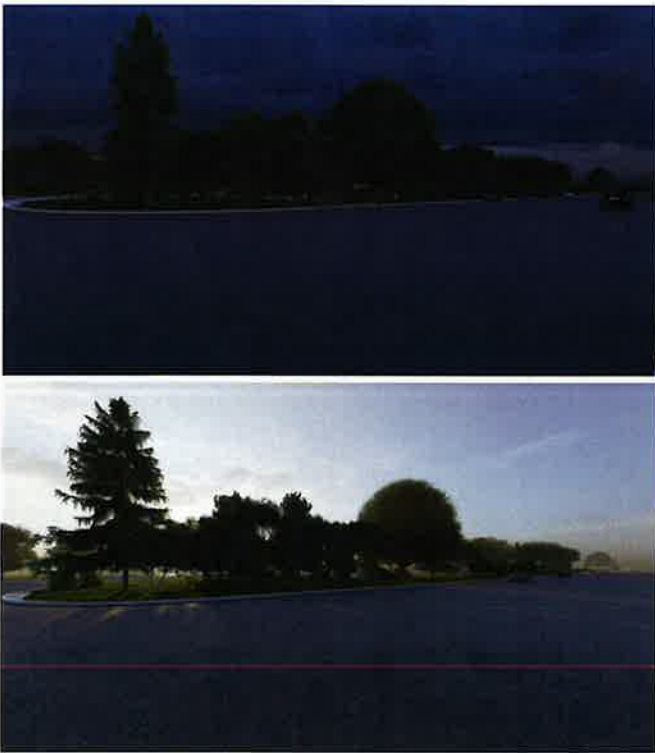
VIEW 2



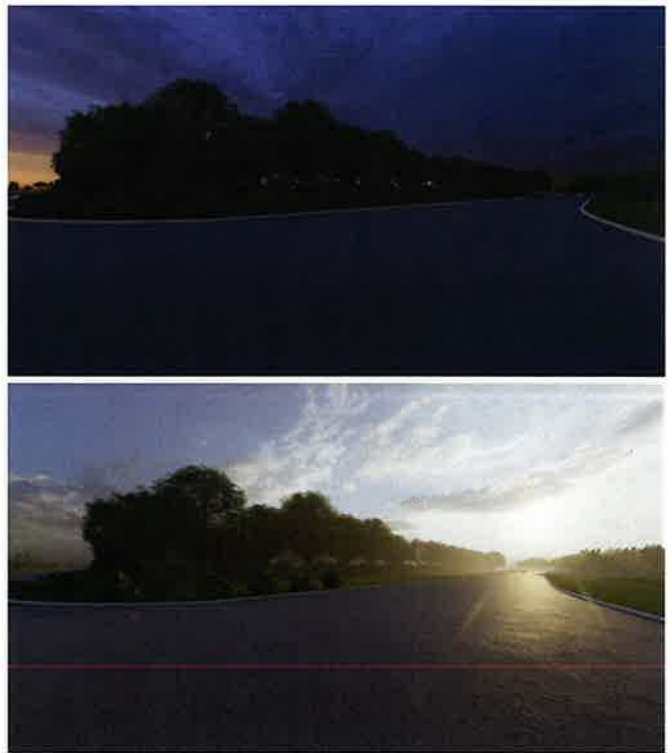
**Figure 6b**



VIEW 3



VIEW 4



**Figure 6c**

VIEW 5



**Figure 6d**





**Figure 7a**



**Figure 7b**





**Figure 7c**



**Figure 7d**





**Figure 7e**



**Figure 7f**





**Figure 7g**



**Figure 7h**





James Smith  
5 Valley Lake Commons  
Chico CA 95928  
559-286-6805

RECEIVED

JUL 14 2022

CITY OF CHICO  
PLANNING SERVICES

July 11, 2022

City of Chico Community Development Department  
PO Box 3420  
Chico, CA 95927

Dear Commission:

I am writing to you regarding my request to speak at the City of Chico's Planning Commission's public meeting on July 21 regarding the following matter:

There is a single line in the Traffic Analysis study submitted to the City of Chico's Planning Commission in January 2020 addressing pedestrian traffic:

*"It is anticipated that little, if any, project traffic will travel east on Sierra Sunrise Terrace into the residential neighborhoods. There is little desire for hotel visitors to wander through residential backstreets considering the close proximity to the Bruce Road / Sierra Sunrise Terrace intersection." (A four-story hotel with a direct view of a beautiful Sierra private lake, with nowhere else in sight to walk!).*

It appears that North Star Engineering submitted the survey without completing the pedestrian and bicycle survey that were part of its own declared professional standard. The attachments to the Study have pedestrian traffic data greyed out and absent with a value of "zero." The pedestrian volume does not appear on any of the reports and does not appear to have been collected by the engineer. Yet North Star Engineering specifically cites the Highway Capacity Manual as its professional methodologic standard which would require the presence of pedestrian studies.

The 2010 Highway Capacity Manual was most noteworthy compared to previous versions because it added pedestrian and bicycle traffic to its methodology to gauge the effect on cyclists and walkers in studies. The report's *Foreword* noted, "It is the first highway capacity manual to provide an integrated multimodal approach to the analysis of urban streets from the points of view of automobile drivers, transit passengers, **bicyclists and pedestrians. This is the first manual to take into account the effect of cars on bicycles and pedestrians.**" And yet, North Star Engineering did not include it.



There is also inconsistency between the consultant engineer and North Star Engineering, as to which Highway Capacity Manual served as its professional standard at the time of submission. While Attachments A-D of the report cite HCM Sixth Edition, which was published in 2016 and contains improved pedestrian and cyclist data collection. However, Table 1 of the report indicates that Level of Service calculations were performed “in accordance the current HCM 2010 methodology,”<sup>1</sup>

I believe these matters are serious and need to be resolved. For this reason, we ask that you pause this process to complete a professional pedestrian analysis as was indicated by the engineers who failed to complete it.

Sincerely,



James Smith

5 Valley Lake Commons  
Chico, CA 95928

---

<sup>1</sup> *Traffic Analysis & Signal Warrant Evaluation for Towne Place Suites—Chico, CA*, submitted by North Star Engineering to the City of Chico on 1/28/20 PDF page 2, Analysis Methodology Level of Service section. Table 1: “Level of Service Calculations were performed using the Vistro 2020 software package with the analysis and results reported with the current 2010 methodology.”

James Smith  
5 Valley Lake Commons  
Chico, CA 95928

July 12, 2022

Mr. Bruce Ambo  
City of Chico  
Planning Division  
411 Main Street, 2nd Floor  
P.O. Box 3420  
Chico, CA 95927

Mr. Ambo:

Attached is a copy of the State of California Department of Transportation "Guide for the Preparation of Impact Studies." You will note on page 5 of the Guide that Traffic Impact Analysis Methodologies should include (I), Pedestrians, and (J), Bicycles. The Guide indicates that "The most current edition of the Highway Capacity Manual, National Transportation Research Council, should be used" as is the current professional standard of practice.

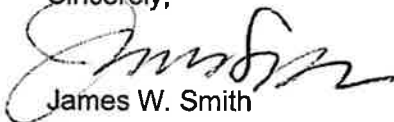
The last two versions of the HCM, published in 2010 and 2016, include pedestrian and bicycle methodologies to be followed. In fact, the defining characteristic of both recent versions is the updated methodologies relating to pedestrians and bicycle studies.

North Star Engineering, the firm that submitted a Traffic Analysis to your office in January 2020, **did not include this methodology in its recommendations concerning the approval of use of the corner of Hwy 32 and Bruce Rd as the location for a four-story Marriott hotel.** Further, NorthStar referred to **outdated, previous versions of the HCM in its report (HCM 2010), while the data tables on the 6<sup>th</sup> Edition released in 2016, and was inconsistent regarding which version served as its reference. All data tables on appendices of the report lack pedestrian data. This is indicated by the numeral "0" where it is required, and it is grayed out of the report itself.**

Further, the only reference to pedestrians and bicycles in the North Star study is a statement by the engineer consultant regarding hotel guests' lack of desire to walk into nearby residential neighborhoods, which is speculation and opinion by the engineer and does not appear to be based on data collection or the engineer's training.

Prior to proceeding with discussions about this important project, I believe the Division needs to address this serious omission from the impact study **and request its revision and resubmission, and that further progress and discussions should cease.**

Sincerely,



James W. Smith  
Resident





---

**GUIDE FOR THE PREPARATION**

**OF**

**TRAFFIC IMPACT STUDIES**

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**STATE OF CALIFORNIA**  
**DEPARTMENT OF TRANSPORTATION**

**December 2002**

## PREFACE

*The California Department of Transportation (Caltrans) has developed this "Guide for the Preparation of Traffic Impact Studies" in response to a survey of cities and counties in California. The purpose of that survey was to improve the Caltrans local development review process (also known as the Intergovernmental Review/California Environmental Quality Act or IGR/CEQA process). The survey indicated that approximately 30 percent of the respondents were not aware of what Caltrans required in a traffic impact study (TIS).*

*In the early 1990s, the Caltrans District 6 office located in Fresno identified a need to provide better quality and consistency in the analysis of traffic impacts generated by local development and land use change proposals that effect State highway facilities. At that time, District 6 brought together both public and private sector expertise to develop a traffic impact study guide. The District 6 guide has proven to be successful at promoting consistency and uniformity in the identification and analysis of traffic impacts generated by local development and land use changes.*

*The guide developed in Fresno was adapted for statewide use by a team of Headquarters and district staff. The guide will provide consistent guidance for Caltrans staff who review local development and land use change proposals as well as inform local agencies of the information needed for Caltrans to analyze the traffic impacts to State highway facilities. The guide will also benefit local agencies and the development community by providing more expeditious review of local development proposals.*

*Even though sound planning and engineering practices were used to adapt the Fresno TIS guide, it is anticipated that changes will occur over time as new technologies and more efficient practices become available. To facilitate these changes, Caltrans encourages all those who use this guide to contact their nearest district office (i.e., IGR/CEQA Coordinator) to coordinate any changes with the development team.*

## ACKNOWLEDGEMENTS

*The District 6 traffic impact study guide provided the impetus and a starting point for developing the statewide guide. Special thanks is given to Marc Birnbaum for recognizing the need for a TIS guide and for his valued experience and vast knowledge of land use planning to significantly enhance the effort to adapt the District 6 guide for statewide use. Randy Treece from District 6 provided many hours of coordination, research and development of the original guide and should be commended for his diligent efforts. Sharri Bender Ehlert of District 6 provided much of the technical expertise in the adaptation of the District 6 guide and her efforts are greatly appreciated.*

*A special thanks is also given to all those Cities, Counties, Regional Agencies, Congestion Management Agencies, Consultants, and Caltrans Employees who reviewed the guide and provided input during the development of this Guide for the Preparation of Traffic Impact Studies.*



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## I. INTRODUCTION

Caltrans desires to provide a safe and efficient State transportation system for the citizens of California pursuant to various Sections of the California Streets and Highway Code. This is done in partnership with local and regional agencies through procedures established by the California Environmental Quality Act (CEQA) and other land use planning processes. The intent of this guide is to provide a starting point and a consistent basis in which Caltrans evaluates traffic impacts to State highway facilities. The applicability of this guide for local streets and roads (non-State highways) is at the discretion of the effected jurisdiction.

Caltrans reviews federal, State, and local agency development projects<sup>1</sup>, and land use change proposals for their potential impact to State highway facilities. The primary objectives of this guide is to provide:

- guidance in determining if and when a traffic impact study (TIS) is needed,
- consistency and uniformity in the identification of traffic impacts generated by local land use proposals,
- consistency and equity in the identification of measures to mitigate the traffic impacts generated by land use proposals,
- lead agency<sup>2</sup> officials with the information necessary to make informed decisions regarding the existing and proposed transportation infrastructure (see Appendix A, Minimum Contents of a TIS)
- TIS requirements early in the planning phase of a project (i.e., initial study, notice of preparation, or earlier) to eliminate potential delays later,
- a quality TIS by agreeing to the assumptions, data requirements, study scenarios, and analysis methodologies prior to beginning the TIS, and
- early coordination during the planning phases of a project to reduce the time and cost of preparing a TIS.

## II. WHEN A TRAFFIC IMPACT STUDY IS NEEDED

The level of service<sup>3</sup> (LOS) for operating State highway facilities is based upon measures of effectiveness (MOEs). These MOEs (see Appendix “C-2”) describe the measures best suited for analyzing State highway facilities (i.e., freeway segments, signalized intersections, on- or off-ramps, etc.). Caltrans endeavors to maintain a target LOS at the transition between LOS “C” and LOS “D” (see Appendix “C-3”) on State highway facilities, however, Caltrans acknowledges that this may not always be feasible and recommends that the lead agency consult with Caltrans to determine the appropriate target LOS. If an existing State highway facility is operating at less than the appropriate target LOS, the existing MOE should be maintained.

---

<sup>1</sup> "Project" refers to activities directly undertaken by government, financed by government, or requiring a permit or other approval from government as defined in Section 21065 of the Public Resources Code and Section 15378 of the California Code of Regulations.

<sup>2</sup> "Lead Agency" refers to the public agency that has the principal responsibility for carrying out or approving a project. Defined in Section 21165 of the Public Resources Code, the "California Environmental Quality Act, and Section 15367 of the California Code of Regulations.

<sup>3</sup> "Level of service" as defined in the latest edition of the Highway Capacity Manual, Transportation Research Board, National Research Council.

## A. Trip Generation Thresholds

The following criterion is a starting point in determining when a TIS is needed. When a project:

1. Generates over 100 peak hour trips assigned to a State highway facility
2. Generates 50 to 100 peak hour trips assigned to a State highway facility – and, affected State highway facilities are experiencing noticeable delay; approaching unstable traffic flow conditions (LOS “C” or “D”).
3. Generates 1 to 49 peak hour trips assigned to a State highway facility – the following are examples that may require a full TIS or some lesser analysis<sup>4</sup>:
  - a. Affected State highway facilities experiencing significant delay; unstable or forced traffic flow conditions (LOS “E” or “F”).
  - b. The potential risk for a traffic incident is significantly increased (i.e., congestion related collisions, non-standard sight distance considerations, increase in traffic conflict points, etc.).
  - c. Change in local circulation networks that impact a State highway facility (i.e., direct access to State highway facility, a non-standard highway geometric design, etc.).

Note: A traffic study may be as simple as providing a traffic count to as complex as a microscopic simulation. The appropriate level of study is determined by the particulars of a project, the prevailing highway conditions, and the forecasted traffic.

## B. Exceptions

Exceptions require consultation between the lead agency, Caltrans, and those preparing the TIS. When a project’s traffic impact to a State highway facility can clearly be anticipated without a study and all the parties involved (lead agency, developer, and the Caltrans district office) are able to negotiate appropriate mitigation, a TIS may not be necessary.

## C. Updating An Existing Traffic Impact Study

A TIS requires updating when the amount or character of traffic is significantly different from an earlier study. Generally a TIS requires updating every two years. A TIS may require updating sooner in rapidly developing areas and not as often in slower developing areas. In these cases, consultation with Caltrans is strongly recommended.

## III. SCOPE OF TRAFFIC IMPACT STUDY

Consultation between the lead agency, Caltrans, and those preparing the TIS is recommended before commencing work on the study to establish the appropriate scope. At a minimum, the TIS should include the following:

### A. Boundaries of the Traffic Impact Study

All State highway facilities impacted in accordance with the criteria in Section II should be studied. Traffic impacts to local streets and roads can impact intersections with State highway facilities. In these cases, the TIS should include an analysis of adjacent local facilities, upstream and downstream, of the intersection (i.e., driveways, intersections, and interchanges) with the State highway.

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<sup>4</sup> A “lesser analysis” may include obtaining traffic counts, preparing signal warrants, or a focused TIS, etc.



## B. Traffic Analysis Scenarios

Caltrans is interested in the effects of general plan updates and amendments as well as the effects of specific project entitlements (i.e., site plans, conditional use permits, sub-divisions, rezoning, etc.) that have the potential to impact a State highway facility. The complexity or magnitude of the impacts of a project will normally dictate the scenarios necessary to analyze the project. Consultation between the lead agency, Caltrans, and those preparing the TIS is recommended to determine the appropriate scenarios for the analysis. The following scenarios should be addressed in the TIS when appropriate:

1. When only a general plan amendment or update is being sought, the following scenarios are required:
  - a) Existing Conditions - Current year traffic volumes and peak hour LOS analysis of effected State highway facilities.
  - b) Proposed Project Only with Select Zone<sup>5</sup> Analysis - Trip generation and assignment for build-out of general plan.
  - c) General Plan Build-out Only - Trip assignment and peak hour LOS analysis. Include current land uses and other pending general plan amendments.
  - d) General Plan Build-out Plus Proposed Project - Trip assignment and peak hour LOS analysis. Include proposed project and other pending general plan amendments.
2. When a general plan amendment is not proposed and a proposed project is seeking specific entitlements (i.e., site plans, conditional use permits, sub-division, rezoning, etc.), the following scenarios must be analyzed in the TIS:
  - a) Existing Conditions - Current year traffic volumes and peak hour LOS analysis of effected State highway facilities.
  - b) Proposed Project Only - Trip generation, distribution, and assignment in the year the project is anticipated to complete construction.
  - c) Cumulative Conditions (Existing Conditions Plus Other Approved and Pending Projects Without Proposed Project) - Trip assignment and peak hour LOS analysis in the year the project is anticipated to complete construction.
  - d) Cumulative Conditions Plus Proposed Project (Existing Conditions Plus Other Approved and Pending Projects Plus Proposed Project) - Trip assignment and peak hour LOS analysis in the year the project is anticipated to complete construction.
  - e) Cumulative Conditions Plus Proposed Phases (Interim Years) - Trip assignment and peak hour LOS analysis in the years the project phases are anticipated to complete construction.
3. In cases where the circulation element of the general plan is not consistent with the land use element or the general plan is outdated and not representative of current or future forecasted conditions, all scenarios from Sections III. B. 1. and 2. should be utilized with the exception of duplicating of item 2.a.

<sup>5</sup> "Select zone" analysis represents a project only traffic model run, where the project's trips are distributed and assigned along a loaded highway network. This procedure isolates the specific impact on the State highway network.

## IV. TRAFFIC DATA

Prior to any fieldwork, consultation between the lead agency, Caltrans, and those preparing the TIS is recommended to reach consensus on the data and assumptions necessary for the study. The following elements are a starting point in that consideration.

### A. Trip Generation

The latest edition of the Institute of Transportation Engineers' (ITE) TRIP GENERATION report should be used for trip generation forecasts. Local trip generation rates are also acceptable if appropriate validation is provided to support them.

1. Trip Generation Rates – When the land use has a limited number of studies to support the trip generation rates or when the Coefficient of Determination ( $R^2$ ) is below 0.75, consultation between the lead agency, Caltrans and those preparing the TIS is recommended.
2. Pass-by Trips<sup>6</sup> – Pass-by trips are only considered for retail oriented development. Reductions greater than 15% requires consultation and acceptance by Caltrans. The justification for exceeding a 15% reduction should be discussed in the TIS.
3. Captured Trips<sup>7</sup> – Captured trip reductions greater than 5% requires consultation and acceptance by Caltrans. The justification for exceeding a 5% reduction should be discussed in the TIS.
4. Transportation Demand Management (TDM) – Consultation between the lead agency and Caltrans is essential before applying trip reduction for TDM strategies.

NOTE: Reasonable reductions to trip generation rates are considered when adjacent State highway volumes are sufficient (at least 5000 ADT) to support reductions for the land use.

### B. Traffic Counts

Prior to field traffic counts, consultation between the lead agency, Caltrans and those preparing the TIS is recommended to determine the level of detail (e.g., location, signal timing, travel speeds, turning movements, etc.) required at each traffic count site. All State highway facilities within the boundaries of the TIS should be considered. Common rules for counting vehicular traffic include but are not limited to:

1. Vehicle counts should be conducted on Tuesdays, Wednesdays, or Thursdays during weeks not containing a holiday and conducted in favorable weather conditions.
2. Vehicle counts should be conducted during the appropriate peak hours (see peak hour discussion below).
3. Seasonal and weekend variations in traffic should also be considered where appropriate (i.e., recreational routes, tourist attractions, harvest season, etc.).

### C. Peak Hours

To eliminate unnecessary analysis, consultation between the lead agency, Caltrans and those preparing the TIS is recommended during the early planning stages of a project. In general, the TIS should include a morning (a.m.) and an evening (p.m.) peak hour analyses. Other peak hours (e.g., 11:30 a.m. to 1:30 p.m., weekend, holidays, etc.) may also be required to determine the significance of the traffic impacts generated by a project.

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<sup>6</sup> "Pass-by" trips are made as intermediate stops between an origin and a primary trip destination (i.e., home to work, home to shopping, etc.).

<sup>7</sup> "Captured Trips" are trips that do not enter or leave the driveways of a project's boundary within a mixed-use development.

#### D. Travel Forecasting (Transportation Modeling)

The local or regional traffic model should reflect the most current land use and planned improvements (i.e., where programming or funding is secured). When a general plan build-out model is not available, the closest forecast model year to build-out should be used. If a traffic model is not available, historical growth rates and current trends can be used to project future traffic volumes. The TIS should clearly describe any changes made in the model to accommodate the analysis of a proposed project.

#### V. TRAFFIC IMPACT ANALYSIS METHODOLOGIES

Typically, the traffic analysis methodologies for the facility types indicated below are used by Caltrans and will be accepted without prior consultation. When a State highway has saturated flows, the use of a micro-simulation model is encouraged for the analysis (please note however, the micro-simulation model must be calibrated and validated for reliable results). Other analysis methods may be accepted, however, consultation between the lead agency, Caltrans and those preparing the TIS is recommended to agree on the data necessary for the analysis.

- A. Freeway Segments – Highway Capacity Manual (HCM)\*, operational analysis
- B. Weaving Areas – Caltrans Highway Design Manual (HDM)
- C. Ramps and Ramp Junctions – HCM\*, operational analysis or Caltrans HDM, Caltrans Ramp Metering Guidelines (most recent edition)
- D. Multi-Lane Highways – HCM\*, operational analysis
- E. Two-lane Highways – HCM\*, operational analysis
- F. Signalized Intersections<sup>8</sup> – HCM\*, Highway Capacity Software\*\*, operational analysis, TRAFFIX<sup>TM\*\*</sup>, Synchro\*\*, see footnote 8
- G. Unsignalized Intersections – HCM\*, operational analysis, Caltrans Traffic Manual for signal warrants if a signal is being considered
- H. Transit – HCM\*, operational analysis
- I. Pedestrians – HCM\*
- J. Bicycles – HCM\*
- K. Caltrans Criteria/Warrants – Caltrans Traffic Manual (stop signs, traffic signals, freeway lighting, conventional highway lighting, school crossings)
- L. Channelization – Caltrans guidelines for Reconstruction of Intersections, August 1985, Ichiro Fukutome

\*The most current edition of the Highway Capacity Manual, Transportation Research Board, National Research Council, should be used.

\*\*NOTE: Caltrans does not officially advocate the use of any special software. However, consistency with the HCM is advocated in most but not all cases. The Caltrans local development review units utilize the software mentioned above. If different software or analytical techniques are used for the TIS then consultation between the lead agency, Caltrans and those preparing the TIS is recommended. Results that are significantly different than those produced with the analytical techniques above should be challenged.

<sup>8</sup> The procedures in the Highway Capacity Manual "do not explicitly address operations of closely spaced signalized intersections. Under such conditions, several unique characteristics must be considered, including spill-back potential from the downstream intersection to the upstream intersection, effects of downstream queues on upstream saturation flow rate, and unusual platoon dispersion or compression between intersections. An example of such closely spaced operations is signalized ramp terminals at urban interchanges. Queue interactions between closely spaced intersections may seriously distort the procedures in" the HCM.



## **VI. MITIGATION MEASURES**

The TIS should provide the nexus [Nollan v. California Coastal Commission, 1987, 483 U.S. 825 (108 S.Ct. 314)] between a project and the traffic impacts to State highway facilities. The TIS should also establish the rough proportionality [Dolan v. City of Tigard, 1994, 512 U.S. 374 (114 S. Ct. 2309)] between the mitigation measures and the traffic impacts. One method for establishing the rough proportionality or a project proponent's equitable responsibility for a project's impacts is provided in Appendix "B." Consultation between the lead agency, Caltrans and those preparing the TIS is recommended to reach consensus on the mitigation measures and who will be responsible.

Mitigation measures must be included in the traffic impact analysis. This determines if a project's impacts can be eliminated or reduced to a level of insignificance. Eliminating or reducing impacts to a level of insignificance is the standard pursuant to CEQA and the National Environmental Policy Act (NEPA). The lead agency is responsible for administering the CEQA review process and has the principal authority for approving a local development proposal or land use change. Caltrans, as a responsible agency, is responsible for reviewing the TIS for errors and omissions that pertain to State highway facilities. However, the authority vested in the lead agency under CEQA does not take precedence over other authorities in law.

If the mitigation measures require work in the State highway right-of-way an encroachment permit from Caltrans will be required. This work will also be subject to Caltrans standards and specifications. Consultation between the lead agency, Caltrans and those preparing the TIS early in the planning process is strongly recommended to expedite the review of local development proposals and to reduce conflicts and misunderstandings in both the local agency CEQA review process as well as the Caltrans encroachment permit process.

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# **APPENDIX “A”**

## **MINIMUM CONTENTS**

### **OF A**

## **TRAFFIC IMPACT STUDY**

# MINIMUM CONTENTS OF TRAFFIC IMPACT STUDY REPORT

- I. EXECUTIVE SUMMARY
- II. TABLE OF CONTENTS
  - A. List of Figures (Maps)
  - B. List of Tables
- III. INTRODUCTION
  - A. Description of the proposed project
  - B. Location of project
  - C. Site plan including all access to State highways (site plan, map)
  - D. Circulation network including all access to State highways (vicinity map)
  - E. Land use and zoning
  - F. Phasing plan including proposed dates of project (phase) completion
  - G. Project sponsor and contact person(s)
  - H. References to other traffic impact studies
- IV. TRAFFIC ANALYSIS
  - A. Clearly stated assumptions
  - B. Existing and projected traffic volumes (including turning movements), facility geometry (including storage lengths), and traffic controls (including signal phasing and multi-signal progression where appropriate) (figure)
  - C. Project trip generation including references (table)
  - D. Project generated trip distribution and assignment (figure)
  - E. LOS and warrant analyses - existing conditions, cumulative conditions, and full build of general plan conditions with and without project
- V. CONCLUSIONS AND RECOMMENDATIONS
  - A. LOS and appropriate MOE quantities of impacted facilities with and without mitigation measures
  - B. Mitigation phasing plan including dates of proposed mitigation measures
  - C. Define responsibilities for implementing mitigation measures
  - D. Cost estimates for mitigation measures and financing plan
- VI. APPENDICES
  - A. Description of traffic data and how data was collected
  - B. Description of methodologies and assumptions used in analyses
  - C. Worksheets used in analyses (i.e., signal warrant, LOS, traffic count information, etc.)



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# **APPENDIX “B”**

## **METHODOLOGY FOR**

## **CALCULATING EQUITABLE**

## **MITIGATION MEASURES**

## METHOD FOR CALCULATING EQUITABLE MITIGATION MEASURES

The methodology below is neither intended as, nor does it establish, a legal standard for determining equitable responsibility and cost of a project's traffic impact, the intent is to provide:

1. A starting point for early discussions to address traffic mitigation equitably.
2. A means for calculating the equitable share for mitigating traffic impacts.
3. A means for establishing rough proportionality [Dolan v. City of Tigard, 1994, 512 U.S. 374 (114 S. Ct. 2309)].

The formulas should be used when:

- A project has impacts that do not immediately warrant mitigation, but their cumulative effects are significant and will require mitigating in the future.
- A project has an immediate impact and the lead agency has assumed responsibility for addressing operational improvements

NOTE: This formula is not intended for circumstances where a project proponent will be receiving a substantial benefit from the identified mitigation measures. In these cases, (e.g., mid-block access and signalization to a shopping center) the project should take full responsibility to toward providing the necessary infrastructure.

### EQUITABLE SHARE RESPONSIBILITY: Equation C-1

NOTE:  $T_E < T_B$ , see explanation for  $T_B$  below.

$$P = \frac{T}{T_B - T_E}$$

Where:

P = The equitable share for the proposed project's traffic impact.

T = The vehicle trips generated by the project during the peak hour of adjacent State highway facility in vehicles per hour, vph.

$T_B$  = The forecasted traffic volume on an impacted State highway facility at the time of general plan build-out (e.g., 20 year model or the furthest future model date feasible), vph.

$T_E$  = The traffic volume existing on the impacted State highway facility plus other approved projects that will generate traffic that has yet to be constructed/opened, vph.

### EQUITABLE COST: Equation C-2

$$C = P (C_T)$$

Where:

C = The equitable cost of traffic mitigation for the proposed project, (\$). (Rounded to nearest one thousand dollars)

P = The equitable share for the project being considered.

$C_T$  = The total cost estimate for improvements necessary to mitigate the forecasted traffic demand on the impacted State highway facility in question at general plan build-out, (\$).

### NOTES

1. Once the equitable share responsibility and equitable cost has been established on a per trip basis, these values can be utilized for all projects on that State highway facility until the forecasted general plan build-out model is revised.
2. Truck traffic should be converted to passenger car equivalents before utilizing these equations (see the Highway Capacity Manual for converting to passenger car equivalents).

3. If the per trip cost is not used for all subsequent projects, then the equation below will be necessary to determine the costs for individual project impact and will require some additional accounting.

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**Equation C-2.A**

$$C = P (C_T - C_C)$$

Where:

C = Same as equation C-2.

P = Same as equation C-2.

C<sub>T</sub> = Same as equation C-2.

C<sub>C</sub> = The combined dollar contributions paid and committed prior to current project's contribution. This is necessary to provide the appropriate cost proportionality. Example: For the first project to impact the State highway facility in question since the total cost (C<sub>T</sub>) estimate for improvements necessary to mitigate the forecasted traffic demand, C<sub>C</sub> would be equal to zero. For the second project however, C would equal P<sub>2</sub>(C<sub>T</sub> - C<sub>1</sub>) and for the third project to come along C would equal P<sub>3</sub>[C<sub>T</sub> - (C<sub>1</sub> + C<sub>2</sub>)] and so on until build-out or the general plan build-out was recalculated.



# **APPENDIX “C”**

## **MEASURES OF EFFECTIVENESS**

**BY**

**FACILITY TYPE**

## MEASURES OF EFFECTIVENESS BY FACILITY TYPE

| TYPE OF FACILITY           | MEASURE OF EFFECTIVENESS (MOE)                         |
|----------------------------|--|
| Basic Freeway Segments     | Density (pc/mi/ln)                                     |
| Ramps                      | Density (pc/mi/ln)                                     |
| Ramp Terminals             | Delay (sec/veh)  |
| Multi-Lane Highways        | Density (pc/mi/ln)                                     |
| Two-Lane Highways          | Percent-Time-Following<br>Average Travel Speed (mi/hr) |
| Signalized Intersections   | Control Delay per Vehicle (sec/veh)                    |
| Unsignalized Intersections | Average Control Delay per Vehicle (sec/veh)            |
| Urban Streets              | Average Travel Speed (mi/hr)                           |

Measures of effectiveness for level of service definitions located in the most recent version of the Highway Capacity Manual, Transportation Research Board, National Research Council.

**Transition between LOS "C" and LOS "D" Criteria**  
(Reference Highway Capacity Manual)

**BASIC FREEWAY SEGMENTS @ 65 mi/hr**

| LOS | Maximum Density (pc/mi/ln) | Minimum Speed (mph) | Maximum v/c | Maximum Service Flow Rate (pc/hr/ln) |
|-----|----------------------------|---------------------|-------------|--------------------------------------|
| A   | 11                         | 65.0                | 0.30        | 710                                  |
| B   | 18                         | 65.0                | 0.50        | 1170                                 |
| C   | 26                         | 64.6                | 0.71        | 1680                                 |
| D   | 35                         | 59.7                | 0.89        | 2090                                 |
| E   | 45                         | 52.2                | 1.00        | 2350                                 |

**SIGNALIZED INTERSECTIONS and RAMP TERMINALS**

| LOS | Control Delay per Vehicle (sec/veh) |
|-----|-------------------------------------|
| A   | ≤ 10                                |
| B   | > 10 - 20                           |
| C   | > 20 - 35                           |
| D   | > 35 - 55                           |
| E   | > 55 - 80                           |
| F   | > 80                                |

**MULTI-LANE HIGHWAYS @ 55 mi/hr**

| LOS | Maximum Density (pc/mi/ln) | Minimum Speed (mph) | Maximum v/c | Maximum Service Flow Rate (pc/hr/ln) |
|-----|----------------------------|---------------------|-------------|--------------------------------------|
| A   | 11                         | 55.0                | 0.29        | 600                                  |
| B   | 18                         | 55.0                | 0.47        | 990                                  |
| C   | 26                         | 54.9                | 0.68        | 1430                                 |
| D   | 35                         | 52.9                | 0.88        | 1850                                 |
| E   | 41                         | 51.2                | 1.00        | 2100                                 |

..... Dotted line represents the transition between LOS "C" and LOS "D"



### TWO-LANE HIGHWAYS

| LOS | Percent Time-Spent-Following | Average Travel Speed (mi/hr) |
|-----|------------------------------|------------------------------|
| A   | 35                           | > 55                         |
| B   | > 35 - 50                    | > 50 - 55                    |
| C   | > 50 - 65                    | > 45 - 50                    |
| D   | > 65 - 80                    | > 40 - 45                    |
| E   | > 80                         | 40                           |

### URBAN STREETS

| Urban Street Class | I                            | II             | III            | IV             |
|--------------------|------------------------------|----------------|----------------|----------------|
| Range of FFS       | 55 to 45 mi/hr               | 45 to 35 mi/hr | 35 to 30 mi/hr | 35 to 25 mi/hr |
| Typical FFS        | 50 mi/hr                     | 40 mi/hr       | 35 mi/hr       | 30 mi/hr       |
| LOS                | Average Travel Speed (mi/hr) |                |                |                |
| A                  | > 42                         | > 35           | > 30           | > 25           |
| B                  | > 34 - 42                    | > 28 - 35      | > 24 - 30      | > 19 - 25      |
| C                  | > 27 - 34                    | > 22 - 28      | > 18 - 24      | > 13 - 19      |
| D                  | > 21 - 27                    | > 17 - 22      | > 14 - 18      | > 9 - 13       |
| E                  | > 16 - 21                    | > 13 - 17      | > 10 - 14      | > 7 - 9        |
| F                  | 16                           | 13             | 10             | 7              |

\*\*\*\*\* Dotted line represents the transition between LOS "C" and LOS "D"

**GRAY DAVIS**  
Governor

**MARIA CONTRERAS-SWEET**  
Secretary  
Business, Transportation and Housing Agency

**JEFF MORALES**  
Director  
California Department of Transportation

**RANDELL H. IWASAKI**  
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**JOHN A. (Jack) BODA**  
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**JOAN SOLLENBERGER**  
Chief  
Division of Transportation Planning

Additional copies of these guidelines can be copied from the internet at,  
<http://www.dot.ca.gov/hq/traffops/developserv/operationalsystems/>





JOHN JEFFERY CARTER  
LAW OFFICE

July 7, 2022

**VIA HAND DELIVERY**

City of Chico  
Community Development Planning Division  
411 Main Street  
Chico, CA 95928

**Re: Towne Place Suites Hotel Proposed at Bruce Road, Hwy 32 and Sierra Sunrise Terrace**

Dear Sir or Madam:

For so many reasons, the Towne Place Suites Hotel proposed at Bruce Road, Hwy 32, and Sierra Sunrise Terrace (the "Project") is inconsistent with the Chico 2030 General Plan (the "Plan") and must be rejected. These include the following:

**Inconsistencies with Chico 2030 General Plan**

1. At its outset, the Plan sets forth on Page 1-2 its Overall Vision for Chico for 2030, which states as follows:

Chico, in the year 2030, is a livable, healthy, and sustainable community that offers a high quality of life **with a strong sense of community and place.** (Emphasis added.) Chico maintains its small-town character while providing opportunities for future generations to thrive. Government is transparent and politics are open and engaging. The City is characterized by a vibrant Downtown, a healthy economy, compact urban form, identifiable neighborhoods with diverse housing choices, convenient access to locally-produced goods, and a focus on alternative transportation and healthy lifestyles. Historic places and buildings are prominent, and Chico is celebrated for its diversity, arts, culture, outdoor access, recreational opportunities, and parks. It is known as a leader in innovative technology and education. Above all, Chico is a place we're proud to call home.

To achieve such, the Plan identifies and promotes certain development patterns, including compact urban development, infill development and redevelopment, and mixed-use development, complete neighborhoods, and a variety of housing types. None of these patterns trumps any other, although one reading the Project Description, Notice of Exemption, and Mitigated Negative Declaration for the Project would think that the Project's self-characterization as "infill development," trumps all other principles and patterns of the Plan.

To facilitate its achievement of this Vision, the Plan is informed by guiding principles set forth beginning at Page 1-3. All goals, policies, and actions of the Plan originate from these Guiding Principles.

Among these, the Guiding Principles of (i) Enhanced Character and Identity, (ii) Complete Neighborhoods as Community Foundation, and (iii) Development Patterns that Offer Alternatives to

More importantly, later in its section entitled “Infill and Redevelopment,” at Page 3-8, it is noted that “[i]t is important to focus infill and redevelopment in the Downtown, along transit corridors, and at other key locations in the City...In other areas of the community, infill and redevelopment needs to be more closely scrutinized to ensure compatibility with existing neighborhoods, as directed by policies in this and the Community Design Element.” In other words, infill and redevelopment needs must give way to neighborhood compatibility.

4. The Goals, Policies, and Actions intended to implement and achieve the policies and goals set forth in the Plan are set forth beginning at Page 3-28. Goal LU-3 at Page 3-32 requires the City to “[e]nhance existing neighborhoods and create new neighborhoods with walkable access to recreation, places to gather, jobs, daily shopping needs, and other community services.” Policies implementing this Goal include LU-3.1, which “[d]irect[s] growth into complete neighborhoods with a land use mix and distribution intended to reduce auto trips and support walking, biking, and transit use, LU-3.2, which [p]romote[s] the development of strategically located neighborhood serving centers that incorporate commercial, employment, cultural or entertainment uses and are within walking distance of surrounding residents, LU-3.3 which requires the City to “[r]ecognize existing neighborhoods and support neighborhood-level planning in partnership with residents and property owners to preserve and enhance neighborhood character, identity, and livability,” and LU-3.4 which requires the City to “[s]trengthen the character of existing residential neighborhoods and districts.”

Policy LU-4.1 requires the City to facilitate infill development, consistent with Policy LU-4.2., which requires the City to “[s]upport infill development, redevelopment, and rehabilitation projects that are compatible with surrounding properties and neighborhoods.

Finally, Policy LU-4.3 provides that “[f]or residential infill projects outside of Opportunity Sites and Special Planning Areas, maintaining neighborhood character may take precedence over meeting density goals.”

(The Hotel meets none of these goals or policies and in fact, likely will prevent them from being realized in Sierra Sunrise.)

5. The **VISION** of Chapter 5, Community Design states at Page 5-1, that “Chico retains its distinct small town charm in the year 2030, attractive to longtime and new residents as well as the dynamic student population at California State University Chico, all of who (sic) continue to replenish Chico’s multi-generational vitality and culture... Visitors easily recognize the community’s cultural pride exhibited by timeless architecture, the beauty of the natural environment, public art, gathering places, and neighborhood landmarks that establish Chico’s identifiable sense of place.”

In its introduction, this Chapter notes, significantly, that “[q]uality, compatible design is highly valued by the community.” It notes at Page 5-2 that the Community Design element is an optional element of the Plan that is related to several other Plan elements:

Specifically, this element prioritizes the importance of a compact urban form and **of creating, preserving and enhancing the distinct neighborhoods of Chico**. In addition to the community design policies established by this element, the City of Chico Design Guidelines Manual (the “Manual”) provides more specific architectural design guidance and solutions for a wide spectrum of project types. The Manual is intended to guide the aesthetic qualities of development in Chico and maintain its dignified visual character by integrating timeless architectural and landscape design with the natural beauty of its surrounding environment. (Emphasis added.)

neighborhoods with design elements reflected in the streetscape, landmarks, public art, and natural amenities.”

Consistent with the foregoing, Goal CD-5, at Page 5-14, requires the City to “[s]upport infill and redevelopment compatible with the surrounding neighborhood,” consistent with Goal Policies CD-5.1, to “[e]nsure that new development and redevelopment reinforces the desirable elements of its neighborhood including architectural scale, style, and setback patterns,” and CD-5.2, to “[e]ncourage context sensitive transitions in architectural scale and character between new and existing residential development.” Finally, Goal CD-6 requires the City to “[e]nhance gateways and wayfinding elements for an improved sense of arrival and orientation for residents and visitors throughout Chico.” Policies CD-6.1 and 6.1.1 require under this Goal that the City “[c]reate a sense of arrival to Chico and develop prominent community focal points at key locations within the City” and “[a]s part of roadway improvement projects, **integrate artistic gateway features at the following locations, including without limitation, Hwy 32 near Yosemite Drive.**” (Emphasis added.)

And finally, and significantly, Chapter 10. Open Space and Environment of the Plan requires in its Policy OS-2.4 at Page 10-19 that the City “[p]reserve the foothills as a natural backdrop to the urban form.”

### **Inconsistency with Design Guidelines Manual**

8. In addition to the Community Design policies set forth in the Plan, the City of Chico Design Guidelines Manual (the “Manual”) was created “to guide the aesthetic qualities of development in Chico and maintain its dignified visual character by integrating timeless architecture and landscape design with the natural beauty of its surrounding environment. Use of the Manual is intended to continue the high priority that the community places on the design of development since Chico adopted its first architectural design ordinance in 1955.” Manual, Page ii. Significantly, the Manual dictates that the Planning Commission, Architectural Review Board, and the Planning Services Director “...refer to any design guidelines adopted by the City, and to base determinations of development proposals on certain findings that include consistency with adopted design guidelines..

The stated purpose of the Manual, set forth at Page 1-2, is as follows:

Chico is a distinctive California community situated in the northern Central Valley between the western foothills of the Sierra Nevada Mountains and the rich agricultural land east of the Sacramento River. The City’s historic downtown and its adjacent neighborhoods have served as a nucleus and model of growth for the City for over 160 years. Longtime residents combined with the changing student population at California State University, Chico continues (sic) to replenish a multi-generational vitality that characterizes Chico’s culture.

Each development in the City should complement and advance the positive qualities of the City’s culture and the beauty of its surrounding natural environment. Although there are a variety of architectural styles that contribute to the community’s character, the design of the community on a City-wide scale should embody the vision of the General Plan, encouraging a hierarchy of mixed-use districts that foster neighborliness and the design of environments that emphasize pedestrian, bicycle, and transit mobility over auto dependence.

9. Chapter 1 of the Manual addresses Site Design. Its intent is to “[c]ontribute to defining the sense of place of the surrounding neighborhood, environment, or the greater Chico community.” Manual, Page 1-4. Among its Design Guidelines (the “DG’s”) are 1.1.11 and 1.1.12 at Page 1-5 which require the



2.1.25 Screen parking areas from street views and avoid elevating parking area grade above street grade;

2.1.26 Minimize the visual impact of vehicles by locating parking areas to the rear or sides of developments rather than along street frontages;

2.1.27 Minimize views of parking areas to allow the architectural significance of the buildings and landscaping to take precedence;

2.1.36 Maximize screening and buffering of trash enclosures, storage areas, expansive paving, service yards, and utility equipment from public view.

2.2.13 Minimize trademark building design elements that create “sameness”. Incorporate design elements that create unique identity and are complementary to surrounding architectural styles; and,

2.2.34 Avoid obscuring the scenic beauty of foothill and riparian backdrop locations with buildings that are oversized, extremely tall, or have materials or colors that draw attention from the natural view.

### **The Project**

**12.** The Project proposed is the construction of a 4-story, 112 room Towne Place Suites Hotel, with a 16,655 square foot footprint on a 4.09 acre site south and west of Sierra Sunrise Terrace and northeast of the intersection of Highway 32, also known as Deer Creek Highway, and Bruce Road, the eastern gateway (the “Gateway”) to Chico. Adjacent to it to the east and north is the neighborhood of Sierra Sunrise Terrace consisting primarily of single family homes, condominiums, apartments, and congregate living facilities serving senior citizens aged 55 and older. Immediately north and across Sierra Sunrise Terrace from the Project is a two story office building consisting of 30,000 square feet presently occupied by six businesses and organizations. There are no commercial businesses in the neighborhood providing goods and services to its residents.

Nothing could be more incongruent with the Sierra Sunrise Terrace neighborhood than the Project. Too, nothing could be more incongruent with the Gateway to Chico from the scenic foothills and canyons to the east than the Project.

Nothing could say less “welcome to Chico,” than the Project, decked out in its corporate architecture typical of “Anywhere America.”

And nothing could be more incongruent with the City of Chico 2030 General Plan and its vision of Chico in 2030 and beyond, than the Project.

In sum, whatever ultimately is to be planned and built on the site, to be consistent with the Plan and the Manual, must be on a scale and of a degree consistent with the neighborhood and emblematic of the Gateway envisioned in the Plan. The Project is none of these.

### **The Categorical Exemption**

**13.** Determination by the City that the Project is categorically exempt from the California Environmental Quality Act is, like the Project itself, wrongheaded for a project of this expanse in an

**Mr. Bruce Ambo, AICP**  
Principal Planner  
City of Chico, Community Development  
411 Main Street, Chico, CA 95928  
P.O Box 3420, Chico, CA 95927

**Mr. F. P. Menet, B.Sc., M. Sc.**  
350 Stratheden Drive  
Burlington, Ontario  
Canada  
L7R 2W4

July 7, 2022

Subject: Environmental Noise Assessment, Chico Towne Place Suites, City of Chico, CA, September 7, 2021, Project #210801, by Saxelby Acoustics LLC, prepared for Continuum Hospitality, LLC, Bakersfield, CA.

Dear Mr. Ambo,

The conclusion given on page 11 of the above noise assessment is that “future transportation noise levels at interior areas are predicted to be 42 dBA  $L_{dn}$  or less, which meets the City of Chico’s interior noise level standard of 45 dBA  $L_{dn}$ ” as required for “Transient Lodging” as shown on page 13-16, Table N-1, Chico General Plan, 13. Noise.

This conclusion appears to have been reached by the use of a short-term noise measurement, ten minutes in duration, at the intersection of Bruce Road and Highway 32. The Chico General Plan, 13. NOISE, requires that the noise measurement to be used is a “Day-Night Average Noise Level” of “24-hour” duration as stated on page 13-5.

In their noise assessment Saxelby Acoustics has not provided a justification for using a ten-minute noise measurement in place of a 24-hour noise measurement. An explanation is needed in order to accept the conclusion given in their noise assessment.

Noise monitoring tests were done at three locations on the subject land and these monitoring sites are shown on Figure 2 of the noise assessment as LT-1, LT-2 and ST-1. Continuous noise measurements, lasting 24-hours, were made at sites LT-1 and LT-2. Site ST-1 is where a short-term noise measurement, lasting ten minutes, was taken. Figure 2 shows the location of site ST-1 at the intersection of Highway 32 and Bruce Road.

The test results for sites LT-1, Lt-2 and ST-1 are presented in Appendices B1, B2 and B3 respectively of the noise assessment. Continuous noise monitoring tests at sites LT-1 and LT-2 are presented in Appendices B1 and B2 respectively. The short-term noise measurement at site ST-1 is presented in Appendix B3.

On page nine of the noise assessment is a discussion of “On-Site Traffic Noise Prediction Methodology”. Saxelby Acoustics used the computer model “SoundPLAN” to predict noise levels at the building façade facing the intersection of Bruce Road and Highway 32. Figure 3 shows the results of the computer model at the façade of the proposed building. Unfortunately, Saxelby does not provide the input data used with the computer model. The short-term noise measurement at site ST-1 is the only data provided for the intersection of Bruce Road and Highway 32. Was a short-term noise measurement used instead of a 24-hour long-term measurement in the computer model “SoundPLAN” to predict noise levels at the building façade facing the intersection of Bruce Road and Highway 32? An explanation in the noise assessment is needed.

“Interior Traffic Noise” is discussed on page 11 of the noise assessment. The “SoundPLAN” model predicted a “maximum of 67 dBA  $L_{dn}$  at the fourth-floor building façade facing the intersection of Bruce Road and Highway 32”. Saxelby Acoustics then applied “a typical 25 dB exterior-to-interior noise level reduction” and reported that an “interior noise level of 42 dBA  $L_{dn}$  would be expected” on the fourth floor. The conclusion was then reached that this “would meet the City’s 45 dBA  $L_{dn}$  interior noise level standard”. This conclusion may be incorrect because a 24-hour noise measurement was not used in the computer program “SoundPLAN” for the intersection of Bruce Road and Highway 32. A ten- minute short-term noise measurement appears to have been used and this may have resulted in a significantly lower predicted noise level than would have been predicted had a 24-hour long-term noise measurement been used. It is likely that using a 24-hour noise measurement would result in a predicted value higher than the “maximum of 67 dBA  $L_{dn}$ ” predicted with a ten-minute noise measurement.

The noise assessment provided Saxelby Acoustics needs to include an explanation as to how short-term noise measurements can be used instead of long-term 24-hour noise measurements at the intersection of Bruce Road and Highway 32. If necessary, a long-term 24-hour noise measurement needs to be performed so that predicted noise levels are in compliance with the Chico General Plan, 13. NOISE. It may be necessary to withhold approval of the subject plan until Saxelby Acoustics has revised their noise assessment of September 7, 2021.

Sincerely,



F. P. Menet, B.Sc., M.Sc.

Footnotes:

Three publications have been relied upon. They are:

1. Chico General Plan, 13. NOISE 13.
2. Traffic Noise Analysis Protocol for New Highway Construction, Reconstruction, and Retrofit Barrier Projects, California Department of Transportation, Division of Environmental Analysis, Sacramento April 2020
3. Technical Noise Supplement to the Caltrans Traffic Noise Analysis Protocol, California Department of Transportation, Division of Environmental Analysis, Sacramento, CA, September 2013



# MARSHA A. BURCH

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July 1, 2022

*Via Electronic Mail*

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Re: **Towne Place Suites Hotel Proposed at Bruce Road,  
Hwy 32 and Sierra Sunrise Terrace**  
Infill Exemption – CEQA Guidelines section 15332

Dear Planning Commissioners and Mr. Ambo:

This office represents California Park Homeowners Association and Sierra Sunrise Village Property Owners Association (“Associations”) with respect to the above-referenced hotel project (“Project”) and the City of Chico’s intent to find the Project exempt from CEQA<sup>1</sup> review pursuant to CEQA Guidelines section 15332. Members of the Associations and others have submitted comments regarding the Project, and these comments are meant to supplement, not replace, previous comments by Associations’ members, the comments of other members of the public, or of other experts or agencies.

After reviewing the “Notice of Exemption”<sup>2</sup> for the Project, we have concluded that the analysis in the document falls short of compliance with the CEQA. As an initial matter, the Project review has been improperly piecemealed, and further piecemealing is proposed. Additionally, the Project description used by the City in its analysis is unstable and misleading, and does not meet CEQA’s standards for a finite, stable Project Description. Finally, the

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<sup>1</sup> California Environmental Quality Act: Public Resources Code § 21000 et seq. and the CEQA Guidelines, California Code of Regulations, title 14, 15000 et seq.

<sup>2</sup> A Notice of Exemption is a brief notice filed at the County Clerk’s office, which states the Lead Agency has approved or will carry out a project that is exempt from the requirements of CEQA. (CEQA Guidelines § 15062.) The document identified by the City as “Notice of Exemption” is the City’s CEQA analysis.

City cannot make all the findings necessary to support the use of the Infill Exemption. The evidence does not support a finding that the Project is consistent with the General Plan or that the Project will not have a significant effect on water quality, and the “unusual circumstances” exception applies and precludes use of the exemption.

**A. The Project Description is unstable and misleading and the Project review has been improperly piecemealed.**

The Mitigated Negative Declaration for the previous grading of the Project site (“MND”) included *only* a description of the grading and leveling of a spoil pile to “facilitate the future sale of the site.” In the environmental analysis for the hotel Project the City and its consultants include the grading as part of the “project” under review. (See July 12, 2021, Memorandum from Dave Krolick; and “Notice of Exemption” p. 15, relying upon Nationwide Permit 18 and Clean Water Act, Section 401 Certification from the Central Valley Regional Water Quality Control Board, both issued *solely* for the grading activity.)

A CEQA document must accurately describe the proposed project. (Guidelines §15071(a).) “An accurate, stable and finite project description is the *sine qua non* of an informative and legally sufficient [CEQA document].” (*County of Inyo v. City of Los Angeles* (1977) 71 Cal.App.3d 185, 193.) The Project Description for the hotel Project at times includes the grading work done under a previous approval by the City, and at other times it does not. This is not a small matter and is confusing to the public and the decision makers. The confusion is compounded by the fact that the City is deferring analysis of the design of the Project.<sup>3</sup> The City acknowledges that detailed architectural and landscaping plans have been provided by the developer, but the City has determined to break the review into pieces. Such an approach violates CEQA.

“CEQA requires public agencies to undertake an environmental review of proposed projects that require their discretionary approval.” (*Tuolumne County Citizens for Responsible Growth, Inc. v. City of Sonora* (2007) 155 Cal.App.4th 1214, 1222.) “The scope of the environmental review conducted for the initial study must include the entire project.” (*Ibid.*) “Project” is defined broadly in the CEQA Guidelines as “the whole of an action, which has a potential for resulting in either a direct physical change in the environment, or a reasonably foreseeable indirect physical change in the environment . . . .” (Guidelines §15378(a).)

Deferring review of the final design of the Project to the future precludes analysis of the whole of the project. Notably, the City makes conclusions regarding the hotel Project’s scenic and aesthetic impacts, and yet it has

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<sup>3</sup> This deferral of review of the design appears in a second document labeled “Notice of Exemption” that appears on the City’s website as “Project Description”.

determined to leave the architectural review “on hold” pending approval of construction of the hotel.

The City has broken the overarching project into three small pieces, and has thereby avoided the appropriate level of environmental review for the whole of the Project. There “is no dispute that CEQA forbids ‘piecemeal’ review of the significant environmental impacts of a project.” (*Berkeley Keep Jets Over the Bay Com. V. Board of Port Commissioners* (2001) 91 Cal.App.4<sup>th</sup> 1344, 1358.)

The piecemeal environmental review has resulted in confusion regarding the description of the “project” being reviewed at this time. The City may not move forward in its review of the hotel Project *separate* from the architectural and landscape plans. Further, the record must be clarified to prevent the public and the decision makers from continuing to believe that the water quality permits issued for the previous grading project are somehow applicable to the hotel Project. They are not.

**B. The Project is not exempt from CEQA.**

If an agency determines that a project falls within a categorical exemption, the findings necessary to support that exemption must be made. In this case, the City intends to approve the Project and file a Notice of Exemption relying upon the Infill Exemption. (Guidelines § 15332.) The Infill Exemption requires the approving agency to apply the following 4 criteria:

- (a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations.
- (b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.
- (c) The project site has no value as habitat for endangered, rare or threatened species.
- (d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.

At this time, the City cannot make finding (a) because the City has deferred review of the architectural and landscaping plans for the Project, and so there is no basis for concluding that the Project is consistent with General Plan policies regarding Community Design. The City also cannot make finding (d) because there is no information (and no analysis) of the impacts to water quality that will most assuredly result from the development and paving of several acres adjacent to a sensitive water body.



If the four requisite findings can be made (and in this case they cannot), then the agency must determine whether any of the “exceptions” to the CEQA exemption apply. Exceptions to the exemptions add back in a measure of consideration to the process. (CEQA Guidelines, § 15300.2(b), (c)–(f).)

A categorical exemption cannot be used for an activity that has a reasonable possibility of resulting in a significant impact on the environment due to “unusual circumstances.” (Guidelines § 15300.2(c).)

The “unusual circumstances” exception precludes the use of any categorical exemption when there is a “reasonable possibility” that the project “will have a significant effect on the environment due to unusual circumstances.” (Guidelines § 15300.2(c).) In reviewing a lead agency’s determination as to whether the exemption applies and if the effects will be significant, the Supreme Court has applied a two-prong test wherein an agency must answer: (1) are there unusual circumstances? And if so, (2) would these unusual circumstances create a potential for significant impact? The second prong of the test is subject to the “fair argument” standard. If an agency determines there is an “unusual circumstance,” then the “fair argument” standard requires an EIR when it can be fairly argued based on substantial evidence that “due to” the unusual circumstances of the project, it *may* have a significant effect on the environment. (*Berkeley Hillside Preservation v. City of Berkeley* (2015) 60 Cal.4th 1086, 1114).

In the present case, an “unusual circumstance” exists that requires additional environmental review of the Project. The Project site is located on the edge of Dead Horse Slough, a water body identified by the City in its Code as worthy of specific regulation (Chico Municipal Code §19.60.030(B)(6)), and it is regulated by the U.S. Army Corps of Engineers and the Central Valley Regional Water Quality Control Board. There are no permits in place, no analysis of stormwater runoff has been done, and the City’s own documentation shows that mitigation is required to prevent contamination of Dead Horse Slough. Thus, there is a fair argument that the Project may have a significant impact on the environment.

**1. There is no basis to conclude that the Project is consistent with General Plan**

The “Notice of Exemption” notes that the Project is consistent with the General Plan Land Use Diagram and the zoning (Community Commercial). This letter will not go into the details of the inconsistency of the Project with the General Plan but refers the Commissioners to the letter submitted by John Jeffery Carter. This letter focuses on the fact that the findings necessary to support the use of the CEQA Exemption cannot be made for the Project, and one deficiency is that the City cannot conclude that the Project is “consistent with the applicable general plan designation and all applicable general plan policies as well as the

applicable zoning designation and regulations.” (Guidelines § 15332(a).) Noting that the hotel use is allowed under the current zoning designation with a use permit is not sufficient. The Community Design Chapter of the General Plan governs community design through policies meant to be used with the City of Chico Design Guidelines Manual. In the section “Infill Design Capability” the General Plan states that the Community Design Element establishes policies and actions to guide the design of infill development to be compatible with its neighborhood. (General Plan, p. 5-2.)

The “Notice of Exemption” prepared by the City does not even mention the Community Design Element. The City cannot make a conclusion that the Project is consistent with the Community Design Element or the City of Chico Design Guidelines Manual because the City has deferred review of the architectural design and the landscaping plan for the Project. The finding is unsupported.

**2. The Project will result in significant effects to water quality.**

In making Infill Exemption finding (d), the City relies upon water quality permits issued by the U.S. Army Corps of Engineers (“USACE”) and the Central Valley Regional Water Quality Control Board (“RWQCB”) to support the conclusion that the Project will not have biological impacts or impacts to water quality. Neither of these two permits cover the hotel Project.

The August 28, 2018, letter from USACE to Dan Gonzales provides that the “Grading project involves the discharge of fill material into 0.03 acres of waters of the U.S. (WOUS) for the grading and leveling of a large spoil pile subject to Section 404 of the Clean Water Act.” The letter authorized the work under Nationwide Permit 18, pending certification under Section 401 of the Clean Water Act. The verification *expired* on March 18, 2022.

On August 29, 2018, the Central Valley RWQCB sent a letter to Mr. Gonzales for the “Bruce Road and Highway 32 Grading Plan Project.” The Section 401 certification and order was issued, stating that “[t]he proposed project involves grading and leveling the site in anticipate of selling the site for future development.” The “Project Description” in the certification describes the grading and leveling that will occur during the dry season. The permit also requires the applicant to notify the RWQCB when the work is completed. Presumably Mr. Gonzales provided the required notice when he completed his work. The permit does not allow for modifications to the project (such as the construction of a hotel) without an amendment to the permit. Additionally, the Order is not transferrable to a new owner without following the procedures of the RWQCB.

The USACE and RWQCB approvals are no longer in place, and even if they were, the “project” those approvals were issued for was grading only and

would not extend to the construction and operation of a hotel with “59,265 SF of paved parking areas” and other impervious surfaces adjacent to Dead Horse Slough. The City has not analyzed the amount of stormwater runoff that will end up in the Slough. The City acknowledges that a “contributory drainage channel of the Dead Horse Slough forms the western and southern project boundaries and serves as an overflow to the human-made California Park Lake, located at the north of the property.” The City has done no analysis of how (or if) storm flows will be prevented from entering these water bodies. In a short statement about utilities, the City notes that “Stormwater from the site will be screened by the proposed landscaping to the south and a proposed bioswale to the west before being directed to an existing concrete storm drain system along Highway 32.” It is interesting that the City can make this conclusion when it has decided *not* to review the architectural and landscaping plan at this time.

In response to the grading plan that was undertaken to prepare the Project site for sale, the USACE and the RWQCB determined that significant mitigation was required to reduce impacts to water quality. The City Code requirement of a 25-foot setback from the Slough is not going to prevent contamination by stormwater runoff from the tremendous amount of impervious surface that will be added to the site. All of this is compounded by the fact that the City is not even planning to review the landscaping plan, and “proposed bioswale” until some point after Project approval.

The Section 401 Water Quality Certification for the grading of the Project site (page 4) described Dead Horse Slough as follows:

The Project is located within the jurisdiction of the Central Valley Water Board. Receiving waters and groundwater potentially impacted by this Project are protected in accordance with the Water Quality Control Plan for the Sacramento River and San Joaquin River Basins, Fifth Edition, revised May 2018 (Basin Plan). The plan for the region and other plans and policies may be accessed online at: [http://www.waterboards.ca.gov/plans\\_policies/](http://www.waterboards.ca.gov/plans_policies/). The Basin Plan includes water quality standards, which consist of existing and potential beneficial uses of waters of the state, water quality objectives to protect those uses, and the state and federal antidegradation policies.

The grading work on the site required an NPDES permit because of possible impacts to the Slough, and yet the City has decided not to analyze hotel construction impacts to water quality, and has deferred analysis of the stormwater flows that will leave the hotel site once it is in operation; flows that will include landscaping runoff potentially laden with chemicals, as well as contaminated parking lot runoff.

The “evidence” relied upon by the City to conclude that there will be no significant effects to water quality includes two permits that do not apply to the



City of Chico Planning Commission  
Bruce Ambo, Principal Planner  
July 1, 2022  
Page 7 of 7

Project. There is no evidence that the design of the Project or some mitigation measures or best management practices will avoid water quality impacts. Infill Exemption finding (d) cannot be made for water quality.

**3. The Project's proximity to Dead Horse Slough is an "unusual circumstance" and there is a fair argument that the Project may have significant impacts to water quality**


The City included Dead Horse Slough in its Creekside Development Standards for a reason: because there is environmental value in preventing impacts to this water body. The City also included discussion of the sensitive nature of the Project site and the Slough in the MND for the grading project, including mitigation measures to "protect sensitive natural resources and water quality from project impacts and ensure that the project will not jeopardize the continued existence of special-status species." (MND, p. 2.) The MND identified several special-status species, terrestrial and aquatic, that triggered the need for several mitigation measures.

The MND also required a Section 404 permit from USACE and a Section 401 Water Quality Certification from the RWQCB. The mitigation measure requiring these water quality permits were necessary for "protected wetlands" on the Project site. The 404 and 401 permits included myriad mitigation measures and best management practices to prevent runoff from entering Dead Horse Slough. As noted above, these permits are either expired or do not apply to the present Project, and there is no basis for a conclusion that the grading, construction, paving, and operation of the hotel will not result in significant impacts to water quality. In fact, the City's own documentation shows that there is a fair argument the Project may have a significant impact on water quality.

**C. Conclusion**

Because of the issues raised above, we believe that the Infill Exemption does not apply to the Project. The evidence before the City reveals a fair argument that the Project may have significant environmental impacts, and the proposal should be denied pending the preparation of an Environmental Impact Report.

Sincerely,



Marsha A. Burch  
Attorney

cc: California Park Homeowners Association  
Sierra Sunrise Village Property Owners Association  
Vincent C. Ewing, City Attorney ([vincent.ewing@chicoca.gov](mailto:vincent.ewing@chicoca.gov))

Valerie Priola  
7 Valley Lake Commons  
Chico, CA 95928

June 14, 2022

City of Chico Community Development Department  
% Bruce Ambo, Chico City Principal Planner  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927

Attn: City Planners

Re: Sierra Sunrise Village-Village Plaza  
TownePlace Suites (Lot 59) APN 018-230-001

I'm a resident and President of the Sierra Sunrise Village HOA a planned Senior Community<sup>1</sup>. I'm writing on behalf of the Senior Citizens who live in Sierra Sunrise Village and our NO HOTEL supporters. Both groups took to social media, signed petitions, wrote letters and postcards to the City Planning Department to express their reasons to deny the Use Permit for the proposed TownePlace Suites. I, too, did my due diligence by personally writing a number of letters to the Chico Planning Department. Those letters further defined community concerns and overall anxiety residents feel about the prospect of a 4 story 112 room hotel being built on Sierra Sunrise Terrace; a private road, adjacent to Chico's largest senior community.

Sierra Sunrise Village was carefully planned from its inception wherein all parties shared a common goal, which was to create a retirement community offering a wider range of housing and lifestyles. Les Wahl<sup>2</sup> led a group of seniors to act as Planners and Advisors to the Developers to create a retirement campus that would meet many residential, social, and healthcare needs. This resulted in a retirement campus offering all levels of living: individual homes, condominiums, apartments, congregate living, assisted living, rehabilitation and memory care facilities. This senior community is bordered by a man-made lake with an exclusive use path, with lots of trees and lush landscaping to give seniors a safe place to enjoy the fresh air and keep active.

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<sup>1</sup> A planned community is any community that was carefully planned from inception and is typically constructed on undeveloped land. This contrasts to settlements that scale in a more ad hoc or organic fashion.

<sup>2</sup> Sierra Sunrise Edition, October 1987, Newsletter 9

The vision for the Village Plaza, the site of the proposed hotel, that would sit just outside Sierra Sunrise Village, was expected to provide services for the seniors i.e.; a pharmacy, restaurant, travel agent, convenience store, brokerage, and financial institutions. Thirty years later we'd still love to have a restaurant, adult day care, hospice care, supporting services, vision, dental, chiropractic care or even a full service salon. The Village Plaza plan was to provide services to seniors within walking distance because as we age our worlds get smaller and smaller, and access to services get harder and harder. I know of residents that don't drive, others who do drive but are afraid to get on the freeway, or won't go downtown because they can't parallel park, can't maneuver the parking garages, and the parking lots aren't close enough to where they want to go. Others have said that Dr Martin Luther King Jr Pkwy is too far to go and it's only 3.4 miles and 9 minutes away.

There's no shortage of hotels or rooms in south Chico. There are 422 rooms available in 3 different hotels less than 2.5 miles **from the proposed Towne Place Suites site** and another 171 rooms at 2 Marriott locations approx 3.5 miles away. That's 599 rooms, so we really don't need another hotel!

Southeast of HWY 99

- Holiday Inn Express & Suites on 20th (93 Rooms 1.2 miles)
- Hampton Inn & Suites on Springfield Dr (148 Rooms 1.5 miles)
- Oxford Suites on Business Lane (184 Rooms 2.5 miles)

Southwest of HWY 99

- Residence Inn by Marriott on Carmichael Dr. (78 rooms 3.5 miles)
- Courtyard by Marriott on Carmichael Dr (93 rooms 3.5 miles)

All the commercial lots in California Park were defined as CN-Neighborhood Commercial or comparable zoning, and were to be developed in accordance with said zoning designation<sup>3</sup>. The proposed hotel site is zoned CC-Community Commercial<sup>4</sup>. A hotel is not a Community Commercial enterprise. The permitted zone for a hotel is CS. CS-Community Services is applied to areas appropriate for commercial businesses not allowed in other commercial areas without a Use Permit because they attract high volumes of traffic or may have adverse impacts on other surrounding uses per 19.40.010.

Consider the area demographics: Seniors to the east and families with children to the west. Neither are compatible with an extended stay hotel.

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<sup>3</sup> The hotel grossly exceeds the maximum CC density development standard of 6-22 units gross acre as per 19.44.030.

<sup>4</sup> A business in a (CC) Community Commercial District is intended to serve the local community; not to attract others to it.



An extraordinary effort went into the planning of Sierra Sunrise Village making every effort to accommodate senior needs. It was a plan that the City of Chico signed off on and people bought into. People bought their forever homes here with the peace of mind that they'd live in a private community, where they could age in place, be with people their own age, and be governed by community seniors that understand resident needs. This hotel, if built, would betray the trust of this entire community.

The City of Chico Housing Element 2022-2030-Update The document details the specific need of seniors and how future neighborhoods should include safe walkable neighborhoods near services and planned housing communities where seniors can age in place.<sup>5</sup> That's exactly what the visionaries of Sierra Sunrise Village wanted and it appears from this document that the needs of seniors remain unchanged. So, if the City of Chico wants to stand behind this updated version of the Housing Element document then they should feel compelled to deny the Use Permit application for the TownePlace Suites project.

In accordance with Title 19, Land Use and Development Regulations, Chapter 19.24.040 - Decision and Finding, we believe the required findings for can not be met by this project.

Valerie Priola

Sierra Sunrise Village, President

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<sup>5</sup> The City of Chico Housing Element 2022-2030-Draft Focus Group (p26) Design senior complexes to foster community and residents can be active. Workshop #1 (pg27): The special need for senior housing close to services and allows residents to age in place.

26 April 2022

To: Planning Commission, City of Chico  
From: B T Chapman, California Park Community

Subj: Granting or Not Granting California Park Hotel Use Permit Application; comments on

Members of the Commission:

I write in strong opposition to granting subject permit.

First of all, to get a permit there should be an explanation of the prime motivating factor to place a 4-story hotel in a retiree and residential district? The answer to this sets the stage for ongoing discussion.

My comments focus on 3 of 5 findings, all five of which must be made in order for the Commission to approve the use permit application.

1. The proposed use is out of character and detrimental to the general welfare of persons residing in the neighborhood. The immediate area is a quiet, restful community of retirees and those requiring assisted living in their own homes. A hotel or other significant structure was never intended in the California Park General Plan. In fact the area under consideration was designed for one or two story office or other small commercial use as exists now across the street on Sierra Sunrise Terrace.
2. The proposed use is not consistent with any applicable specific plan and applicable neighborhood area plan. California Park is designed for residential housing with no non-compatible structure or uses as the hotel would be.
3. The design, location, size and operating characteristics of the proposed use are not compatible with existing and future land uses in the vicinity. Pure and simple, a hotel is fully out of character to the surrounding buildings in the community. And what is the potential impact on nearby home values?

To the point of future single dwelling and multi-unit construction nearby driving more residents and the associated potential need for more hotel rooms over and above the 589 units and other short stay options that now exist with in two miles of California Park, 117 rooms for a hotel is a "drop in the bucket" and totally out of character with surrounding buildings. The VMT Analysis portion of the Environmental Analysis identifies a large underserved area East of Highway 99 and the limited supply of hotel rooms on the east side of Chico. How can 589 existing rooms be "limited supply"? Fully defining the term is critical to the overall analysis of this project. 589 existing rooms does not quite seem a "limited supply".

If additional rooms are truly needed, (although this is debatable), find a larger property that accommodates more rooms than the property in question and at the same time has space allowing for future expansion as needed. The CalPark property has no space to expand. A perfect location to investigate, before approving anything in CalPark, is the recently announced Stonegate development at 20<sup>th</sup> Street, Bruce and The Skyway.

Before proceeding, if the previous owner of the property thought an additional hotel was so important to the area the question needing a response is, why did he not plan ahead to build a hotel in Miriam Park where lots of room for expansion exists.

Finally, the Commission should ask what alternate designs for a hotel were considered that might more appropriately fit in California Park. If none were considered, it shows a lack of due diligence on the part of the developer and a disregard of the negative impact the project would have on the surrounding residential community and the potential negative impact on near by home values.

Respectfully submitted, s/s B T CHAPMAN  
041422hotelusepermit

Valerie Priola  
7 Valley Lake Commons  
Chico, CA 95928

April 7, 2022

City of Chico Community Development Department  
c/o Bruce Ambo, Chico City Principal Planner  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927

Attn: City Planners

RE: Health and Safety, TownePlace Suites Hotel for APN 018-230-001

I'm writing today to add additional information regarding how hotel vehicle parking on Sierra Sunrise Terrace could affect the Health and Safety of the Sierra Sunrise Village Residents.

It's no secret that we are opposed to the Hotel, but parking on Sierra Sunrise Terrace, especially within Sierra Sunrise Village, goes way beyond that. I live in Sierra Lakeside, the abutting R-Zoned property. My living, dining and master bedroom face south toward Sierra Sunrise Terrace and Hwy 32. That side of the unit has 8 windows and two skylights. Neither are comparable to today's STC standards: it's a noisy place to live.

The Sierra Sunrise Village, CC&R'S prohibit residents from parking on the street, limiting vehicle types and weight. **CC&R'S also restrict hours for commercial service providers in order to keep from disturbing seniors.**

The Continuous Noise Monitoring Report found in the Hotel project documents reported on Lmax (dbl) levels at LT-1: Day Average of 75 dbl (low/high 71/81) and Night Average of 73 dba (low/high 68/78). **In reality it does not require a high decibel level or a running vehicle to disturb residents.**

The problem I foresee is that as we transition from day to evening we'll be swapping one kind of noise with another. **TownePlace Suites guests with late check in and early departures as well as those who park on the street are likely to disturb Sierra Lakeside Seniors.**

**Medical News Today**, reports that **interrupted sleep impacts mood more than the lack of sleep.**

**WebMD** reports that not enough sleep can cause **changes in mood, increased irritability, anxiousness, or depression.** One might find it **hard to think straight or**



**remember things.** In the long run, **lack of sleep can lead to conditions like obesity, high blood pressure, diabetes, and heart disease.**

**Sleep Foundation** reports that Sleep disruption has also been **associated with neurodegenerative disease**, including age-related cognitive impairment, **Azzheimer's dementia, and Parkinson's Disease.** Fragmented sleep is considered to be an early symptom of the conditions, but research suggests that it may also be a **contributing factor to their development and/or progression.**

There are also **safety and security** issues with vehicles parking on the street. Vehicles parking on the street are likely to be oversized and won't fit in on-site parking spaces. These vehicles are heavier<sup>1</sup> as defined by GVWR<sup>2</sup> louder and will carry items ripe for theft. These vehicles will likely attract unsavory characters who look to profit off of others. In today's climate everything is for the taking. Residents throughout Chico report vehicle vandalism, ransacking, broken windows, stolen catalytic converters and vehicle theft.

Chico Police Department seeing an increase in reported car break-ins  
Feb 8, 2022 Action News Now

[https://www.actionnewsnow.com/news/chico-police-department-seeing-an-increase-in-reported-car-break-ins/article\\_79e6b320-8931-11ec-970b-43e1298a3acc.html](https://www.actionnewsnow.com/news/chico-police-department-seeing-an-increase-in-reported-car-break-ins/article_79e6b320-8931-11ec-970b-43e1298a3acc.html)

Action New Now reports just under 900 car thefts were reported last year, according to the Chico Police Department. Crime prevention officer Ed Nelson for the Chico Police Department said car break-ins are high reward low risk crimes and are why they happen so often. Nelson also said, "It's easy to break a window or steal a purse or backpack or anything out of a vehicle and it is hard to get caught" and that, "There are not enough eyes looking at people committing burglaries."

Chico Police arrest a man for Cadillac converter theft

April 02, 2022 <https://www.actionnewsnow.com/news/chico-police>

Chico vehicle theft arrest

Feb 27, 2022 <https://www.newskudo.com/california/chico/crime-safety/8606920-chico-v>

Chico Burglaries with Stolen Vehicle - City of Chico

Feb 16, 2022 <https://www.chico.ca.us/post/vehicle-burglaries-stolen-vehicle>

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<sup>1</sup> Chico Municipal Code, Section, 10.20 160 States that no person who owns or has possession, custody or control of any commercial vehicle having a manufacturer's gross weight rating (GVWR) of 10,000 pounds or more shall park said vehicle in an R-1, R-2, R-3, R-P, RD-1, or RDH residential district of the city between the hours of 2 a.m. and 6 .a.m.

<sup>2</sup> The gross vehicle weight rating (GVWR) is assigned by the vehicle manufacturer and **represents the maximum weight of the vehicle and what it can carry when fully loaded.** The GVWR includes the weight of the vehicle itself plus fuel, passengers, cargo, and trailer tongue weight. It may sometimes be listed as gross weight. Ref: GVWR Examples

After two nights of at least 13 car burglaries, Chico police make arrest  
Feb 22, 2022 <https://www.chicoer.com/2022/02/22/man-suspected-of-breaking-into-cars>

Here's an infamous line from the movie, Field of Dreams, in which Ray Kinsella, played by Kevin Costner, hears a strange whisper from a corn field: **"If you build it, he will come."** Build what? In the movie it refers to a baseball field... In our case, I hear whispers: **"If you build it, they will come."** Build what? **Build the TownePlace Suites in Sierra Sunrise Village and those unsavory characters who profit off of others will come to the largest senior community in the City of Chico!**

In accordance with Title 19, Land Use and Development Regulations, Chapter 19.24.040 - Decision and Finding, we believe the required finding for B<sup>3</sup> can not be met by this project!

So again, we ask the City Planners to consider the health, safety and security of the Sierra Sunrise Village Seniors when considering the Use Permit for this project.

Valerie Priola

Sierra Sunrise Village, President

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<sup>3</sup> Title 19 Chico Municipal Code - Land Use and Development Chapter 19-24-040 Decision and findings. Following a public hearing, the Zoning Administrator or Planning Commission may approve a use permit application, with or without conditions, only if all the following findings can be made: **B. The Proposed use would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed use.**

## GVWR TRUCK CLASSES

- **Light Duty Trucks** - vehicle with GVWR under 10,000 pounds
- **Medium Duty Trucks** - vehicle with a GVWR between 10,001 and 26,000 pounds
- **Heavy Duty Trucks** - vehicle with a GVWR over 26,001 pounds

## TRUCK SIZE CHART

### TRUCK WEIGHT CLASSES

| Class | Gross Vehicle Weight Rating (GVWR) | GVWR Category | Examples  |
|-------|------------------------------------|---------------|---|
| 1     | < 6,000 lbs.                       | Light Duty    | Jeep Wrangler, Ford Ranger, Chevy Colorado, GMC Canyon, Toyota Tacoma, Ford Bronco  |
| 2     | 6,001 to 10,000 lbs.               | Light Duty    | <b>Class 2A:</b> Ford F-150, Chevy Silverado 1500, GMC Sierra 1500, Toyota Tundra, Ram 1500<br><br><b>Class 2B:</b> Ford F-250, Chevy Silverado 2500, GMC Sierra 2500, Ram 2500 |
| 3     | 10,001 to 14,000 lbs.              | Medium Duty   | Ford F-350, Chevy Silverado 3500, GMC Sierra 3500   |
| 4     | 14,001 to 16,000 lbs.              | Medium Duty   | Ford F-450, Chevy Silverado 4500, Ram 4500  |
| 5     | 16,001 to 19,500 lbs.              | Medium Duty   | Ford F-550, Chevy Silverado 5500, Ram 5500  |
| 6     | 19,501 to 26,000 lbs.              | Medium Duty   | Ford F-650, Chevy Silverado 6500  |
| 7     | 26,001 to 33,000 lbs.              | Heavy Duty    | Ford F-750, Tow Truck   |



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: NO HOTEL CAL PARK  
**Date:** Wednesday, March 30, 2022 4:00:51 PM

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**From:** Nan Tofanelli <[nan@nantuckethomeinc.com](mailto:nan@nantuckethomeinc.com)>  
**Sent:** Wednesday, March 30, 2022 3:59 PM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
**Subject:** NO HOTEL CAL PARK

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Dear Bruce,

I have lived in California Park since 1992. I have seen a lot of changes in this community. Most of the changes were well thought out and compatible with the existing development. I am writing to you because I think that the proposed 4 story hotel is definitely not something that would blend in nicely with what we currently have here. I am one of the members of the California Park Design Review Committee and this is something that I would NEVER allow to go through our process.

First there are the aesthetics. There are not many tall buildings in Chico and having one at that location will create a visual eyesore to the surrounding landscapes. The lights from the building will be a constant reminder to those that face it across the lake. I personally think that so many nighttime lights are bad for our psyche. I know of many people who live on the lake that don't use window coverings because they like to look at the lake day and night. The constant lights will force them to put up coverings and close their draperies.

I know that many who are opposed to this project are concerned about the traffic problem. They feel it would be substantial. The installation of another traffic light at the intersection of Bruce and

Sierra Sunrise Dr is too close to the current one at Bruce Rd and Hwy 32. There will be a traffic back up at peak times. And because most who use Sierra Sunrise Dr are senior citizens going to and from their homes, it will cause further upsets and confusion as to what is going on.

We live in a fragile eco-system. I am sure that the surrounding natural environment will be impacted by the building. We will see more illegal usage of our lake by people who don't have a vested interest in keeping it clean. I predict that the crime rate will also increase with the hotel there. And our homeowners fees will likely increase because of the added cost of more private patrols to keep the intruders out.

Bruce, I beg you to really reconsider this hotel's location. Is it really necessary to be at that location? Is there another place where it would make more sense? Do we really need another hotel?

Thank you very much for taking the time to read this letter.

Nan Tofanelli

Nan Tofanelli

NANTUCKET HOME  
603 Broadway  
Chico CA 95928  
530 895 1038  
[www.NantucketHomeInc.com](http://www.NantucketHomeInc.com)

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Planning Commission's Process  
**Date:** Friday, March 18, 2022 8:17:21 AM

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**From:** James Smith <fresnojimmy@yahoo.com>  
**Sent:** Friday, March 18, 2022 7:57 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Planning Commission's Process

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Mr. Ambo,

I am advised that the Chico City Planning Commission follows the Institute for Local Government's process as outlined in "*Understanding the Basics of Land Use and Planning*."

In consideration of the four-story Marriott hotel proposed for the corner of Bruce Rd. and Rt. 32, I ask the Planners to consider that this project **fails on three of the very basic underlying tenets of land use planning**, as outlined in *Understanding the Basics*:

**"Create a sense of Place and reinforce a sense of community."**

"Planning can ensure that architectural, environmental and aesthetic elements are incorporated into projects to connect people to their community and establish a sense of place – the features and characteristics that define the unique identity of the community." **The proposed project does not connect current residents to their community, nor does it provide features that establish a sense of place.**

**"Provide Public facilities and Infrastructure."** The proposed property will not provide, but rather detract from infrastructure, by adding to the traffic burden of the nearby private road, and by **further impacting evacuation routes in an area of demonstrated wildfire risk.**

**"Protect and Enhance Property values."** The proposed project would neither protect, nor enhance, the property values of existing Sierra Sunrise Village. It can be argued that a nearby commercial center would transform this community into a semi-urban eyesore with visitors and additional traffic that could deteriorate current property values.

For these reasons, I ask the Planners to determine that the proposed hotel does not meet the needs of our community.

James Smith  
5 Valley Lake Commons



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: TownePlace Suites Project  
**Date:** Tuesday, March 15, 2022 11:12:58 AM

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**From:** Susan Shafsky <[csucmimi@yahoo.com](mailto:csucmimi@yahoo.com)>  
**Sent:** Monday, March 14, 2022 1:37 PM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>; [sgoulart@sbcglobal.net](mailto:sgoulart@sbcglobal.net); [jiminchico@yahoo.com](mailto:jiminchico@yahoo.com)  
**Subject:** TownePlace Suites Project

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Dear Mr. Ambo and Associate Planners,

This particular Marriott project planned for the tiny 4.09 acre lot at the northeast corner of Bruce and HWY 32, is not the appropriate project for that site. There are so many reasons why this oversized hotel (4/stories, 112 rooms plus restaurant and pool) should not be built there. Here are the main reasons:

- Increased evacuation difficulties in a disaster (see State Audit)
- lack of architectural beauty for a serene, rural site
- environmental and wildlife concerns
- large busy building in a basically residential area
- insufficient roads and traffic controls
- not congruent/ appropriate with CALPark Senior Community with almost 700 residents within yards of the hotel
- probable trespassing of hotel guests on private CALPark properties
- large group of Chico residents against this project at this site.

The NO HOTEL Chico Group is getting larger every day. More and more Chico residents are against this project. How can the Planning Commission, in good conscience, even approve this project? Please listen to your constituents.

Sincerely,  
Susan Shafsky  
1857 Hooker Oak Ave  
Chico, CA. 95926

[Sent from Yahoo Mail for iPhone](#)

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Hotel in California Park  
**Date:** Monday, March 14, 2022 11:11:29 AM

---

**From:** dougcalbreath@aol.com <dougcalbreath@aol.com>  
**Sent:** Monday, March 14, 2022 10:57 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Hotel in California Park

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Hello Mr. Ambo,  
I am writing to voice my concerns about the proposed hotel in California park. I think the hotel is clearly not compatible with the surrounding area. Not only for aesthetic reasons but it would have a huge impact on the traffic. I believe this is why it is not Zoned for a project such as this. It is zoned residential and I strongly feel it should stay that way. Thank you for listening,  
Doug Calbreath

Valerie Priola  
7 Valley Lake Commons  
Chico, California 95928

RECEIVED

MAR 14 2022

CITY OF CHICO  
PLANNING SERVICES

March 11, 2022

City of Chico Community Development Department  
c/o Bruce Ambo, Chico City Principal Planner  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
[bruce.ambo@chico.gov](mailto:bruce.ambo@chico.gov)

Re: Use Permit APN 018-230-001 – Proposed 4-story 112-unit Hotel on Sierra Sunrise Terrace

Attention: City Planners

I'm writing on the behalf of the 800 +/- Senior Citizens living in Sierra Sunrise Village, Chico's Largest Senior Citizen Community and for all those who support the NO HOTEL effort. (Ref: Attachment 9-10) In previous letters to the City of Chico Planner's, I detailed the Sierra Sunrise Village demographics, our concerns and reasons for denying the use permit for the proposed hotel. This letter is intended to summarize those letters and our findings into a concise format.

**19.24.040 Decision and Findings**

**A. The proposed use is allowed within the subject zoning district and complies with all of the applicable provisions of Chapter 19.24 (Use Permits);**

1. Ref: E. 1-6 on this document.

**B. The proposed use would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed use;**

1. Access to the hotel would be from Sierra Sunrise Terrace, a private two-lane collector road, currently maintained by Sierra Sunrise Village, Chico's Largest Senior Citizen community. Sierra Sunrise Terrace is less than a half-mile-long and was intended to service 626 residences or approx. 800+/- seniors. This planned senior community has 163 homes, 190 independent living apartments, 84 residents in 2 memory care facilities, 99 in assisted living apartments and 90 beds in the California Park Rehabilitation Hospital. **Emergency responders are a common site within the development that requires unimpeded ingress and egress. Sierra Sunrise Terrace is a private two-lane road intended to service the senior community. In the event of**



**an emergency transport or fire evacuation, this street is our life line.** (Ref: Attachment 2-4 and 7).

2. Seniors who live here feel safe in this tucked away private community. They feel free to walk along streets and paths regularly. It's inevitable that hotel patrons will trespass onto the private areas of the adjacent lake and paths of the association which will give seniors pause. **19.44.010 recognizes that some commercial businesses may have adverse impacts on other surrounding uses and those are better suited in a CS Zoning District.**
3. 3.52.030 Defines a hotel as any structure, or any portion of any structure, which is occupied, or intended or designed for occupancy, **by transients for dwelling, lodging or sleeping purposes...** A 24-hour hotel by nature is unsuited for a residential zone, especially not one that abuts a residential community of 800 senior citizens. Seniors are vulnerable to crime, falls and health issues. Age and impaired immune systems make seniors vulnerable to types of injury and illnesses which can lead to higher mortality.
4. The corner of Bruce Road and Sierra Sunrise Terrace is an extremely dangerous intersection and is in need of traffic signal. The staff recognizes the issue and recommends a signal be a part of the Hotel project. Traffic speed on Bruce and Sierra Sunrise Terrace (SST) will continue to be a problem. Speed humps were added to SST to calm traffic but after making City mandated changes, cars drive down the center of the road to avoid them!
5. The hotel traffic would increase overall maintenance costs for the section used by the hotel. **19.44.010 recognizes hotels can attract higher volumes of vehicle traffic.** (Ref: Attachments 3, 5-6)
6. Sierra Sunrise Village CCR'S 4.14(a)(i) Restricts resident vehicle types and storage; (a)(ii) limits truck weight to 1 ton (2,000 lbs.) or less, as-well-as time of use restrictions for commercial service vehicles. 4.14(f) Prohibit residents from parking on Common Area private streets at any time. Sierra Sunrise Terrace though labeled private but that's only in the sense that we are responsible to maintain it. Sierra Sunrise Village residents and facilities located along Sierra Sunrise Terrace access those properties via exclusive use streets and/or driveways. To my knowledge, the lot for the proposed hotel has never been part of Sierra Sunrise Village and therefore not under SSV HOA CC&R control. Without the ability to control hotel vehicle types and weights it leaves the HOA to bear the brunt of increased maintenance costs, **it therefore necessitates the City of Chico to accept the pending Dedication for that proportion of the road.**

**C. The proposed use would not be detrimental and/or injurious to property and improvements in the neighborhood of the proposed use, as well as the general welfare of the City;**

1. California Park is a planned community built around a man-made lake with parks and lake paths. These amenities are owned and maintained by the California Park Homeowners Association. All the residents in California Park which includes Sierra Sunrise Village and others, have exclusive rights to parks, lake and lake paths; businesses do not. The costs to facilitate an association like ours are astounding; landscaping, parks, lakes and paths and security and insurance... It would be wrong to believe that the hotel could prevent their patrons from entering into the private areas of the adjacent lake and paths. **19.44.010 recognizes that some commercial businesses may have adverse impacts on other surrounding uses and those are better suited in a CS Zoning District. The CS zoning district is applied to areas appropriate for business not allowed in other areas because they attract high volumes of vehicle traffic.** (Ref: Attachment 2, 4 and 6)
2. The Lake and Dead Horse Slough are managed under the watchful eye of the National Pollutant Discharge Elimination System (NPDES) and requires a very complex permit. It acknowledges that our lake empties into the "Waters of the US" and we cannot cause any type of contamination. The street drains along the hotel property drain directly into Dead Horse Slough, instead of the lake where it can be tested and treated. **We believe that the Hotel due to proximity to the slough needs its own permit as chemicals used on the property could end up in the slough and that they are held to the same standard as California Park in protecting the "Waters of the US".**

**D. The proposed entitlement is consistent with the General Plan, any applicable specific plan, and any applicable neighborhood or area plan;**

Community Commercial (CC) zoning districts are primarily residential in design are intended to implement the Commercial Mixed Use (CMU) designation in the city's General Plan. **A business in a (CC) Community Commercial District is intended to serve the local community; not to attract others to it.** The General Plan does not include hotels in a CC District; hence the requirement for such a proposal to qualify for a use permit.

**E. The design, location, size, and operating characteristics of the proposed use are compatible with the existing and future land uses in the vicinity.**

1. The design is not keeping with the surroundings. The enormous size of the building (59.25 feet) in relationship to its surroundings would be visually obtrusive. To think that the Landscaping Plan could screen or reduce its presence is delusional! This location is at the base of the Sierra Foothill; the

east side community deserves to see more than a 4-story hotel. **Within the 2030 General Plan, Chico pledged to protect the viewsheds from foothill development, through the careful location and design of roads, buildings, lighting, landscaping and other infrastructure; this project will test that commitment.** Ref: Chico 2030 General Plan, Community Design CD-2.4.

2. The hotel location abuts Residential District (R) never planned or intended for a large-scale Community Commercial hotel. Hotels of this size are permitted in Commercial Services (CS) and Regional Commercial (CR) Districts. **CS and CR Districts were created for a reason. The CS zoning district is applied to areas appropriate for commercial businesses not allowed in other commercial areas because they attract high volumes of vehicle traffic or may have adverse impacts on other surrounding uses per 19.40.010 (G. Purpose, applicability). The Regional Commercial (CR) zoning district is applied to sites appropriate for larger retail and service businesses that serve residents from the City and the Region. CR Mixed-Use projects integrating office and residential uses are allowed. The density range for residential development within the CR district is 6 to a maximum of 50 units per acre. The CR zoning district is primarily intended to implement the Regional Commercial land use designation of the General Plan. (Ref. 19.44.010 H. Purpose, applicability)**
3. 19.44.030 Commercial and office zone general development standards. The land use for the proposed hotel is Community Commercial (CC) and its General Plan designation is "CMU" (Commercial Mixed Use). **19.44.030 limits the residential density for CC to 6 to 22 units per gross acre: the hotel plan of 112 rooms exceeds the maximum units allowed per that development standard.**
4. 19.44.030 Commercial and office zone general development standards. Table 4-7 requires a 10ft where the side of the parcel abuts an R district. The 10ft eastern property line setback was removed when changes to the east driveway were made. Notes (1) A minimum 6ft landscape buffer shall be included along the rear or side property line abutting the residential use. **Towne Place Suite documents do not show required setback or 6ft landscape buffer.**
5. 19.70.060 Design and development standard for off-street parking. A fence or wall shall be constructed along the property, in compliance with Subsection J (Screening). J. Screening. Multi-family, commercial, manufacturing, and public parking areas abutting residentially zoned parcels shall have a wood or masonry wall, not less than 6 feet high, to properly screen the parking areas. **TownePlace Suites site plan does not show required wood or masonry wall.**



6. The proposed hotel height is 59.25' which exceeds the Height Limit Standard allowed in a CC District. The Height Limit for CC is 57', **a lesser height may be required through the Design Review process where a parcel abuts a R district per 19.44.030.** Comment: NorthStar, in a written statement said that the proposed development is 49.25' in height (59.25' to the top of the parapet) the allowed height in the CC zone is 57 feet (with parapet walls extending not more than 6 feet above the height limit of the structure. CMC 19.60.070 E.3). **Reference to parapet walls... do not apply here as CMC 19.60.070 E.3 only pertains to downhill lots! This lot is technically considered a Foothill Development only because it sits east of Bruce Road with a general elevation of 250 feet (19.66) with a small slope, certainly not enough for serious CMC 19.60.070 consideration.**
7. The parking for this hotel is off the front of the building. 19.70.060 Requires off-street parking areas shall be located in the following manner: In order to minimize the visual impact of vehicles, the parking areas other than single-family residential shall be located to the rear or side of the parcel, or appropriately screened from public views if located along the street frontages. The parking areas shall not be located within a sight distance area.
8. The hotel meets the minimum parking slot requirement but fails to address how it will/can accommodate vehicles that don't fit into a standard vehicle space. Currently there is no off-site parking to accommodate oversized vehicles. **Sierra Sunrise Terrace is 36' wide, which meets City of Chico standard for on street parking but parking availability on that section of the road would be contingent road Dedication to the City of Chico.**

In accordance with Title 19, Land Use and Development Regulation, Charter 19.24.040 – Decision and Findings, we believe the required findings can NOT be met by this project.

So again, we ask the City Planners to consider the health, safety and security of the Sierra Sunrise Village Seniors and their need to protect their private street, bridge, common property, lake views and overall tranquility of their neighborhood when considering the Use Permit for this project.

Sincerely,








Valerie Priola  
Sierra Sunrise Village, President

**Attachments:**

1. City of Chico Memorandum dated January 11, 2019, Camp Fire Traffic Impacts  
[http://www.chico.ca.us/building\\_development\\_services/traffic/documents/ME\\_MOCampFireTrafficImpacts.pdf](http://www.chico.ca.us/building_development_services/traffic/documents/ME_MOCampFireTrafficImpacts.pdf)
2. Auditor of the State of California report dated December 2019, California Is Not Adequately Prepared to Protect Its Most Vulnerable Residents from Natural Disasters <http://auditor.ca.gov/reports/2019-103/summary.html>

**Letters to City of Chico Community Development Department**

3.  Proposed Hotel APN 018-230-001.docx
4.  East Valley Evacuation Plan.docx
5.  Hotel Parking.docx
6.  Traffic at Bruce and SST.docx
7.  Evacuating Seniors.docx
8. No Hotel Supporters (Change.Org Petition) PDF
9. No Hotel Supporters (Paper Petitions) PDF
10. Lorrane Kurtela (Letter/Petition) PDF

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Opposition to Proposed Hotel in Cal Park  
**Date:** Thursday, March 10, 2022 3:08:04 PM

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-----Original Message-----

From: Terry M Battle <[TBattle@csuchico.edu](mailto:TBattle@csuchico.edu)>  
Sent: Monday, February 28, 2022 6:51 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Opposition to Proposed Hotel in Cal Park

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Dear Mr. Ambo,

I would like to express my concern regarding the proposed hotel in California Park:

1. As I understand the property is not zoned for this type of commercial property. Please deny the use permit as it clearly will not meet the requirements to serve the local residents. The size, design, location and other characteristics of the proposed 4-story hotel are clearly no compatible with the surrounding community.
2. We already have an issue with traffic concerns that have been expressed to the City numerous times. This will only add to the problem. A cluster of hundreds of apartments on California Park Drive have already impacted traffic.
3. I feel you should be concerned for safety and ability to evacuate of seniors living in Sierra Sunrise and other surrounding communities. The development would be detrimental to the largest senior community in Chico.
4. This project would certainly impact the surrounding natural environment and the adjacent riparian area.
5. As homeowners of California Park Association this will have a financial impact on us as will be required to spend substantial increased dollars to provide significantly more hours of patrol services to protect us against the inevitable detrimental impacts from hotel patrons, visitors, and staff trespassing onto our private lakes and walkways.
6. It makes no sense to have a hotel where there are no amenities such as shopping, restaurants, or entertainment venues. This is a neighborhood!

Thank you for your time and consideration,

Theresa M. Battle

530-513-2887



tbattle@csuchico.edu

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: No Hotel California Park  
**Date:** Wednesday, March 9, 2022 3:36:10 PM

---

-----Original Message-----

From: Gail Brown <[gbiski@aol.com](mailto:gbiski@aol.com)>  
Sent: Wednesday, March 9, 2022 3:28 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: No Hotel California Park

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Absolutely not! Too many retired folks, lots of dog walkers, trails, the lake etc. There couldn't be a worst spot. 4 stories next to a retirement home!  
I'm sure there r better sites like Miriam Park area, etc. Near restaurants and shopping.

Gail Brown  
Skylake Ranch  
530-521-3891

**From:** [Lauri Burelson](#)  
**To:** [Nicole Acain](#)  
**Subject:** Town Place Suites  
**Date:** Tuesday, March 8, 2022 7:13:55 PM

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As a long time resident (27 years) of California Park and a part of the coalition to STOP the planned hotel on Sierra Sunrise Village Road - I beg your attention to this matter.

Why on earth would this parcel [AP#018-230-001-00] be deemed fit for this use? The road that this parcel sits on is a private road, at the entrance to a massive senior neighborhood. There is another parcel in front of that parcel that is actually owned by the HOA - how can this proposed hotel be built without crossing over that parcel? Isn't this hotel use going to have to apply for a "use permit"? And actually cutting in another road to gain access to the hotel off Bruce Road may be problematic.

There has to be another piece of property in Chico, that is better suited for this 3 story hotel. Not to mention the impact on the California Park HOA walking paths, Lake, and the traffic congestion caused on that road from this huge hotel.

All the residents of California Park pay a yearly fee to belong to and partake of all these wonderful amenities that are available to them. Add this hotel with approximately 300 rooms, and do those staying there get FREE use of our walking paths, and Lake. I don't think so - but who's going to stop them?

Appreciate your time and attention to my concerns.

Lauri Burelson  
1235 Banning Park Dr  
Chico, Ca 95928



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: TownePlace Suites Chico, CA  
**Date:** Monday, March 7, 2022 7:57:59 AM

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**From:** Susan Shafsky <csucmimi@yahoo.com>  
**Sent:** Sunday, March 6, 2022 10:18 AM  
**To:** gpresley@hotelequities.com; Sandy Goulart <s.goulart@sbcglobal.net>; Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** TownePlace Suites Chico, CA

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Dear Mr. Presley,

I am writing to you with the sincere hope that you, your company and Marriott will change your plans to build the TownePlace Suites at the corner of Hwy 32 ( Deer Creek Hwy) and Bruce Road in Chico, CA.

That 4.09 acre parcel is not the right location for your hotel - for so many reasons. You yourself heard many of the reasons at the standing-room-only meeting on 3/2/3020 at Canyon Oaks Golf Club. I was there also. You must remember the outright negative atmosphere ( sometimes hostile) of the meeting by residents of CALPark. Not one person in the audience wanted your hotel in that location. Even more Chico residents are against your hotel in 2022.

I've seen and even stayed at Towne Place Suites before and they are pleasant hotels - just not there! If you walked the parcel and area, you must have noticed the semi- rural, peaceful environment . Not any of us who live in the area can even conceive of 112 plus cars, hundreds of guests, workers' cars, trucks, deliveries, employees, and shift changes coming in and out of your entrance and exit at all times of the day and night. And then the 4 story height and bright lights and signage ? To us, it's inconceivable.

I do not even live in CALPark but I live on a nearby street (Hooker Oak Ave). I drive the Bruce Rd. ( and Manzanita) corridor several times PER DAY. Often I sit at the light at 32 and Bruce add I stare at that beautiful parcel of land, imagining how the whole ambience of the area will be changed in a very negative way by your hotel.

Please please please do not build your hotel there. There are other large vacant parcels in Chico. A good place for Towne Place would be on West East Avenue. There are no hotels near there and there is lots of shopping, restaurants, and a hospital nearby.

Thank you for your time and consideration in this very serious ( to local Chicoans) matter.

Sincerely,  
Susan Shafsky  
1857 Hooker Oak Ave.  
Chico, CA 96926

Sent from Yahoo Mail for iPhone

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW:  
**Date:** Wednesday, March 2, 2022 1:13:33 PM

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**From:** Heather Nicole <mizdomerese@gmail.com>  
**Sent:** Wednesday, March 2, 2022 11:43 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:**

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Mr. Ambo,

I would like to add my voice to others who are completely opposed to having a hotel this close to my safe and quiet neighborhood.

With a hotel nearby, I would have to think twice about going on the bike path alone, letting my children out to play, having crowds of people in the park/lake areas, possible uptick in litter and non-residents upsetting the lovely animals that reside in and near the lake. The lake residents will also have to deal with these things on a daily basis.

I am unsure how exactly this even became an idea. It's a very unusual, not to mention unwise prospect to consider putting individuals, families, the elderly, children and our natural habitats at risk with people coming through who will not be as caring as we are to our own environment and fellow neighbors.

If you care about the community and keeping our children and neighborhoods safe, please put a stop to this plan. While it may bring city revenue, there are other more appropriate places within the city to place a hotel.

Thank you,  
Heather

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**From:** [Bruce Ambo](#)  
**To:** [Gary Taylor](#); [Lee-Anne Calhoon](#)  
**Cc:** [Kelly Murphy](#); [Nicole Acain](#)  
**Subject:** RE: Expressing objection to the proposed Hotel  
**Date:** Wednesday, March 2, 2022 1:34:13 PM  
**Attachments:** [image001.png](#)

---

Hi Gary,

Thanks for keeping me in the loop. The only thing I would add is that there is no environmental review deadline or end date. The purpose of the courtesy review is to give folks a chance to review and digest the vast amount of environmental analysis that has already completed and documented. We don't anticipate going to the Planning Commission until April or May 2022. The review of the project will continue to occur up to and during the Planning Commission hearing. Notices of the Planning Commission hearing will be sent to all residents within a 500-foot radius of the project site and all interested parties (100+ thus far) that sent letters or emails.

Thanks

**Bruce Ambo, AICP**

Principal Planner  
City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927



<http://www.ci.chico.ca.us/>

**From:** Gary Taylor <Gary@Hignell.com>  
**Sent:** Wednesday, March 2, 2022 11:54 AM  
**To:** Lee-Anne Calhoon  
<32fd0a866b784cbeee87ae6ec19703848ec642add49386947085dc82aacde0ad@hignellcohoa.mailer.appfolio.us>  
**Cc:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** RE: Expressing objection to the proposed Hotel

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I would say to address it to the Planning Commission. You can email it to: Bruce Ambo ad [bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov) and he can forward on to the planning commission. I will find out more information and send out another email blast to the community about this as well.

Regards,

Gary Taylor, CCAM-PM®

Property Manager

The Hignell Companies

1750 Humboldt Road

Chico, CA 95928

530-894-0404 ext.105 phone

888--304-4674 ext. 105 phone

530-894-6984 fax

Corp. B.R.E. Lic. # 317250 Cont. Lic. # 143778

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**From:** Lee-Anne Calhoon

[\[mailto:32fd0a866b784cbeee87ae6ec19703848ec642add49386947085dc82aacde0ad@hignellcoho.a.mailer.appfolio.us\]](mailto:32fd0a866b784cbeee87ae6ec19703848ec642add49386947085dc82aacde0ad@hignellcoho.a.mailer.appfolio.us)

**Sent:** Wednesday, March 2, 2022 11:41 AM

**To:** Gary Taylor <[Gary@Hignell.com](mailto:Gary@Hignell.com)>

**Subject:** Re: Expressing objection to the proposed Hotel

Gary, Thank you for all this great info.

Do you have an address to mail our letter? Should we address it to someone specific or to The Planning Commission?

Gracias,

Lee-Anne Calhoon 2619 Amanecida Commons

Sent from Yahoo Mail for iPhone

On Monday, February 28, 2022, 3:36 PM, Gary Taylor wrote:

Cal Park owners,

There may be some misunderstanding about the process of the consideration of the proposed Hotel at the corner of Bruce Road and Highway 32. Currently there is a courtesy public review of the Environmental Analysis that ends today.

Subsequent to this, the project must come before the Planning Commission to be considered for a 'Use Permit'. A 'Use Permit' is required because the property is not zoned for the Hotel unless a 'Use Permit' is granted. The Planning Commission meeting has not been scheduled

yet but is expected to take place in April or May. Please see the criteria below under Decisions and Findings that must be considered in granting a 'Use Permit'.

These are the required findings necessary for the approval or denial.

#### 19.24.040 Decision and findings.

Following a public hearing, the Zoning Administrator or Planning Commission may approve a use permit application, with or without conditions, only if all of the following findings can be made:

- A. The proposed use is allowed within the subject zoning district and complies with all of the applicable provisions of Chapter 19.24 (Use Permits);
- B. The proposed use would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed use;
- C. The proposed use would not be detrimental and/or injurious to property and improvements in the neighborhood of the proposed use, as well as the general welfare of the City;
- D. The proposed entitlement is consistent with the General Plan, any applicable specific plan, and any applicable neighborhood or area plan;
- E. The design, location, size, and operating characteristics of the proposed use are compatible with the existing and future land uses in the vicinity.

(Ord. 2185)

For those wanting to write to the city and object to the project, please do so according to objections related to the categories listed above. These letters will go into the packets of the Planning Commission representatives when they consider this project. We will be sending out more information soon, but wanted you to be aware of the information above.

Have a great week!

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW:  
**Date:** Tuesday, March 1, 2022 8:12:09 AM

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**From:** Mari Moore <mars2009bar@gmail.com>  
**Sent:** Monday, February 28, 2022 7:20 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:**

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Dear Bruce:

Re: Hotel Project on Cal Park property, corner of Highway 32 and Bruce Rd..

There appears to be quite a bit of evidence that this project does not fit into the Cal Park property it is seeking. The whole idea that the project is required to have a specific "use permit" to even be considered screams inappropriate. It is my understanding that within the requirements of consideration for new development that the proposed project would be one that enhances the area by adding a possible business that benefits the good of the people in the Cal Park Community. How does a noisy, light-polluting hotel do that? There are all of the traffic complaints, parking complaints and trespassing complaints wrapped up in this one debacle.

Those of us here in the Cal Park Community were duped a few years ago when suddenly a "deluxe" apartment complex ended up at the corner of Cal Park Dr. and 8th St. I heard there were Cal Park HOA meetings that mentioned the possibility of this project going forward but I absolutely don't remember getting an email about them. I could only find vaguely mentioned topics on this project in the Cal Park HOA meeting agenda but nothing specific that would cause me to suspect this eye sore to actually go through, not to mention a complete hazard for evacuation purposes.

I don't believe there was a thorough environmental review of the health of Dead Horse Slough. I want a reputable environmental specialist to tell us there's absolutely no problem with putting a huge building practically on top of the slough with no repercussions like flooding, collapsing infrastructure, etc..

My main objection is that this project in no way benefits the people of our community. This project lowers property values, increases human traffic around the lakes ( We pay for keeping this area up. Lately we've had increased foot traffic around the lakes by strangers, urinating publicly into the lakes, garbage left around that can injure the wildlife), and, of course, more noise, dirt, and smelliness. That means we need more security because there's been more break-ins (me included). I can't even imagine a large building housing limited-stay occupants who will feel it's their right to use property we pay for. It's not their fault. It would be our fault for allowing this project to even be suggested in this area. Perhaps farther down Manzanita Ave. and East Ave. would be a more appropriate location, maybe even off Eaton.

I hope you will consider those of us living in Cal Park. It's our home. It's not a commercial district.

Regards,  
Mari Moore  
2893 Pennyroyal Dr.  
Chico



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: No Hotel, California Park  
**Date:** Tuesday, March 1, 2022 8:14:40 AM

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**From:** Gene Glocksen <nuttin01@hotmail.com>  
**Sent:** Monday, February 28, 2022 6:58 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** No Hotel, California Park

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Dear Mr. Ambo,

This letter is in regards to the possible construction of a Marriott's Towne Place Suites hotel in Chico on the corner of Bruce Road and Rt 32 Deer Creek Highway.

I am very displeased with all of the people of Chico involved with even considering the possibility of placing an oversized monument of capitalism in a residential area without FIRST getting an opinion from the people that you are working for and protecting from such atrocities in their front and back yards.

The second reason I am writing to you is about the traffic analysis given to review, which on paper seems quite complete and quite within acceptable limits of meeting regulations. The traffic on Sierra Sunrise Terrace, which I have been trying to figure out, but if the analysis is correct there will be 20 vehicles per hour during peak traffic hours coming from the new construction when completed and the delay at Sierra Sunrise Terrace and Bruce Road would be 3.2 minutes. I've been there during rush hour and I have been held there a bit longer. Let's round this out to 3 minutes delay per vehicle, and 20 vehicles make that a one hour delay. Are you saying that this is acceptable when we haven't even put the residents who live here into this equation? What these visitors will do is turn around and drive through out residential streets to find a quicker way out, putting "unknowns" into our quiet and safe residential streets. The access to the proposed construction is on Sierra Sunrise Terrace which gives a beautiful view of our beautiful 64 acre lake complex which California Park residents walk, admire, use, fish from and use for boating because they pay for this. I know I'm speaking for a lot of residents but I don't want people using something I pay for to keep in good condition to be making it difficult to keep in good condition. We have security here trying to keep things safe for us, I wouldn't feel safe knowing that the possibility of 200 or more customers of Marriott could be walking around on "our" property without the right to be there. There was mention of installing a traffic light at the junction of Sierra Sunrise Terrace and Bruce Road to mitigate this problem of delay getting onto the main roads, which in most cases solves the issue, but not here. Why don't you come to this intersection during peak time and take a look at Bruce Road south of the intersection. You will be able to see vehicles backed up to Humboldt Road waiting to get through the junction and if 20 percent go left and 20 percent go right at the intersection 60 percent are going to continue on Bruce Road and be stopped at the proposed traffic control light at Sierra Sunrise Terrace making a mess at Deer Creek Hwy and Bruce Road. However, if the dating of the document is even close to when the analysis was made, two thirds of the traffic on Bruce Road did NOT exist because of the "lockdown" during the COVID19 state and country emergency. Therefore, the information

submitted is inconclusive, or maybe more simplistically stated, absolutely wrong as a definitive analysis.

The third reason for writing to you is the impact on the residents of California Park to which I will comment that to my knowledge you haven't notified anyone personally, by social media, letter, phone call or announcing on public TV, until just recently, of what your intent was for this property. Only by due diligence of concerned residents in California Park have the majority of us have been notified. Looking through "City of Chico Draft Initial Study" I found it very interesting that the study was concerned about nesting fowl, the Western Pond Turtle and the Western Spadefoot so that they could assist them on their way out of the property, but not mention one thing about the impact of the 20 to 30 foot high light poles installed around the constructions parking lot for customers, flooding the area with night time bright light for all the residents across the street or across the lake or the increase in human noise made by the travelers coming and going on Sierra Sunrise Terrace road at all and anytime of the day and night.

It is with great concern that I take the time to present to you my feelings on this issue, which will greatly change my feelings about the place I have chosen, in Chico, to live in because of its beauty and comfort and safety that it provides to me if you so choose to issue a "use permit".

With Respect,  
Eugene Glocksen

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: No Hotel California Park  
**Date:** Monday, February 28, 2022 4:19:49 PM

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-----Original Message-----

From: VERDA MACKAY <verdamackay7114@comcast.net>  
Sent: Monday, February 28, 2022 11:33 AM  
To: Bruce Ambo <bruce.ambo@Chicoca.gov>  
Subject: No Hotel California Park

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Dear Mr. Ambo:

I bought my model home in Nob Hill just across the street from California Park 22 years ago and have lived in it since. I have seen the extreme traffic grow as the new buildings are completed. My home backs up to Bruce Rd. across from the Enloe Hospital speciality clinics and it has been peaceful--until this traffic grows.

Please do what you can to disallow the building of a large hotel building in this peaceful housing area. Thank you for any help you are able to provide.

Verda Mackay  
853 Coit Tower Way  
Chico, CA 95928

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Opposition to Proposed Hotel in Cal Park  
**Date:** Thursday, March 10, 2022 3:08:04 PM

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-----Original Message-----

From: Terry M Battle <[TBattle@csuchico.edu](mailto:TBattle@csuchico.edu)>  
Sent: Monday, February 28, 2022 6:51 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Opposition to Proposed Hotel in Cal Park

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Dear Mr. Ambo,

I would like to express my concern regarding the proposed hotel in California Park:

1. As I understand the property is not zoned for this type of commercial property. Please deny the use permit as it clearly will not meet the requirements to serve the local residents. The size, design, location and other characteristics of the proposed 4-story hotel are clearly no compatible with the surrounding community.
2. We already have an issue with traffic concerns that have been expressed to the City numerous times. This will only add to the problem. A cluster of hundreds of apartments on California Park Drive have already impacted traffic.
3. I feel you should be concerned for safety and ability to evacuate of seniors living in Sierra Sunrise and other surrounding communities. The development would be detrimental to the largest senior community in Chico.
4. This project would certainly impact the surrounding natural environment and the adjacent riparian area.
5. As homeowners of California Park Association this will have a financial impact on us as will be required to spend substantial increased dollars to provide significantly more hours of patrol services to protect us against the inevitable detrimental impacts from hotel patrons, visitors, and staff trespassing onto our private lakes and walkways.
6. It makes no sense to have a hotel where there are no amenities such as shopping, restaurants, or entertainment venues. This is a neighborhood!

Thank you for your time and consideration,

Theresa M. Battle

530-513-2887



tbattle@csuchico.edu

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: No Hotel California Park!  
**Date:** Monday, February 28, 2022 4:17:40 PM

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**From:** Tally Sturm <[ttreyster@gmail.com](mailto:ttreyster@gmail.com)>  
**Sent:** Monday, February 28, 2022 4:09 PM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
**Subject:** No Hotel California Park!

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February 28, 2022

Dear Mr. Ambo:

Though not a Paradise fire victim, the horror and ramifications have touched me and all in Chico. Living in California Park on the edge of Highway 32 which was closed as the fire blazed down the hill and we prepared for evacuation, I was healthy and nimble enough to pack my car and be ready to drive out...if a road was available.

This brings me to a major concern about a proposed hotel to be built on the corner of Highway 32 and Bruce Road. Marriott is requesting use of our private road, Sierra Sunrise Terrace, which is the main access to the largest retirement community in Chico. Numerous residents are not ambulatory and mayhem ensued during the fire as evacuation of this fragile group was necessary. Fire trucks and ambulances are a daily occurrence for this population and quick access is mandatory. A four story hotel at this location is a major hinderance to residential traffic as well as emergency vehicles as the hotel is not accessible from any street other than the one belonging to the retirement community. This is a horrendous fit for the neighborhood and without thoughtful and safe planning, many lives will be in peril.

We have over 1500 signatures on the petition to fight this abuse of land, but need your intervention to assist us in any way you can. After two meetings where our grievances were aired to Marriott spokespeople and the City of Chico, the firm has gone ahead to file a request to build and a use permit with the Chico Planning Commission.

Chico is in a building frenzy at the moment, however, development needs to be suitable to an existing neighborhood. As you are aware, if a proposed use is detrimental to the health, safety

and general welfare of persons residing or working in the neighborhood, a permit should not be issued.

Thank you for your time and consideration in this matter.

Sincerely,

**Tally Sturm**  
2859 Pennyroyal Drive  
Chico, CA 95928

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: TownePlace Suites Proposal  
**Date:** Monday, February 28, 2022 12:38:26 PM

---

**From:** Susan Shafsky <csucmimi@yahoo.com>  
**Sent:** Monday, February 28, 2022 12:21 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Cc:** Mike Wolcott <mwolcott@chicoer.com>; Susan Shafsky <csucmimi@yahoo.com>; sgoulart@sbcglobal.net; jiminchico@yahoo.com; Hannah Shafsky <hanniemay@gmail.com>; Leah Goodliffe <leahgoodliffe@gmail.com>  
**Subject:** TownePlace Suites Proposal

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Dear Mr. Ambo,

As a resident of a neighboring area of CALPark (Hooker Oak Avenue), I have deep concerns about the proposal to build TownePlace Suites at the northeast corner of Bruce and Hwy 32. I have no financial concern regarding this project but I do have important concerns. I have reviewed many of the documents regarding this proposal. I also want to add that I was a participant at the standing room only meeting regarding this hotel in March 2020 at the Canyon Oaks Club House. The City Planning Dept. and representatives of Marriott and approx 200 audience members were at that meeting. I do not know if you were there so let me just say that there was palpable tension, dismay and anger from the audience. Not one person in the audience raised their hand when asked if anyone wanted the hotel built.

I could write pages and pages on each of the following concerns - I will try to be as brief as possible. I am sure you have already received emails and letters regarding some of the following concerns.

The main concerns are:

Question of what "Commercial Mixed Use" actually means ----In looking for definitions of this term, there is ambiguity. I could find no clear definition. Mostly definitions described buildings as "mixed use", not areas. I did see that "commercial mixed use" could include housing, services, parks, even industrial. I saw no mention of "hotels". I saw there was a different zoning designation on a 2020 city zoning map.

General safety --- Just the fact the entire area was originally planned as a relaxed lifestyle environment (see old CALPark promotions and pamphlets from 1983) is an indication that times



have definitely changed and our planners now seem to be pro-growth and not in keeping with the original plan. Seniors and CALPark residents walk along the road to visit the lakes and restaurants in the area. There are almost no sidewalks along Sierra Terrace Road. A hotel will cause much more pedestrian traffic along the road. What happened to the nursery, cafe, park, gift shop? See more safety issues below.

Traffic--- I read the traffic study for the present and future 2040 prediction. I agree there may be very little growth coming from the CALPark area (unless the City of Chico changes more land use zoning). However, I see BIG discrepancies in the Traffic Report regarding traffic along the Bruce Road (which should include Manzanita) and HWY 32. Personally, living on Hooker Oak, I use the Manzanita-Bruce corridor sometimes 1-4 times per day. Many people choose to drive this "back way" rather than head west and get on the freeway. Remember that we have the huge apartment development and homes being built on Eaton Road and side streets. Eaton, Lassen, East, Hooker Oak, Vallambrosa, Hwy 32, 20th Street and Lower Skyway cars all use this corridor as a way to avoid driving "into town". What will happen to the intersections and stop lights at 32 and Sierra Sunrise then?

Environmental---I see that this project has an exemption from an Environmental Study due to the size of the project (4.09 acres) and other factors. As Church Lady would say, "How convenient!" There are animal and bird life concerns. A biologist will be required to oversee all grading and building. I do see that the biologist did note swallows, doves, a bald eagle and red slider turtles. I have seen many ducks, egrets, herons, and other bird and aquatic life. It is obvious that animal and bird and aquatic life will be affected by more cars, more noise, more lighting throughout the night, and a building 59.3 feet tall. Migration and nesting changes may likely occur.

Architectural drawing discrepancies--- While the hotel is being built, there will be traffic concerns with large trucks and semis delivering materials along Sierra Sunrise Road. Trucks backing up and such may cause road delays and confusion for senior drivers. After the hotel is built, after studying the architectural plans, I see there is a discrepancy in the number of parking spaces. The hotel will have 112 rooms. I counted 91 hotel parking places (including 3 handicapped). There might be 3 more on the very eastern side of the property. I do not see areas for deliveries and the necessary truck parking and unloading - food, linens, materials. Also if any semi truck drivers want to stay at the hotel, where will they park? What about people with trailers?

State audit on lack of emergency preparedness in Butte County---This certainly is a very timely and frightening subject. All Californians wonder where the next fire will be this summer. The CA state audit found 3 counties not prepared for a disaster such as a large, fastmoving forest fire. If a fire were to come down the hills from the east. Chico would be in big trouble, especially East Chico, with all cars trying to move west.. And the residents of CALPark Sierra Sunrise would be in dangerous difficulty due to age, lack of car, disabilities, inability to walk, confusion etc. And as noted, Butte County HAS NOT DEVELOPED AN EMERGENCY EVACUATION PLAN. How can a large building with many people be built where there is no emergency evacuation plan to leave the entire east side of the city?

Trespassing on CALPark private prop--This is an issue for the CALPark residents. They live in a private

community and the lakes and walking paths are not open to the public. From the front of the hotel, it's a 2 minute walk across the road to trespass on beautiful CALPark property. How will the hotel prevent that problem?

General beauty of the area--Personally this is the one that concerns me the most. Basically the intersection of 32 and Bruce is the GATEWAY to the rolling hills and mountains of our beautiful area. The hotel is visibly incongruous with anything in the entire area. The architecture does not blend in with any building nearby - partly because of height and mainly because of how large it is and what it is. Most of the area is residential and the few businesses that are there are relatively small and only one or two stories. If you've ever driven in Palm Springs or Santa Barbara, for example, you have seen what could city planning can look like. I do not want our town to have a big sore thumb with bright lights sitting on the GATEWAY TO THE MOUNTAINS.

I sincerely appreciate your time reading this and I hope you will share it with your co-planners.

Thank you,

Susan Shafsky  
1857 Hooker Oak Avenue  
Chico, CA 95926  
[Sent from Yahoo Mail on Android](#)

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Hotel at hyw32 and Bruce rd.  
**Date:** Monday, February 28, 2022 12:42:15 PM

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**From:** Rochelle Caldwell <rockic267@gmail.com>  
**Sent:** Monday, February 28, 2022 12:40 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>; Mike Wolcott <mwolcott@chicoer.com>  
**Subject:** Hotel at hyw32 and Bruce rd.

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ed. Live in Sierra Sunrise Village which is a senior community in California Park. The village borders the property the proposed residence hotel wishes to build on. As well as private homes and private condos the village also includes senior apartments, assisted living, rehab and skilled nursing care and a memory care facility. Quick access by emergency medical vehicles to our residents is often a matter of life and death. As residence of both Sierra Sunrise village and California Park and Lakeside village we pay HOA dues. These dues maintain the ponds, our trails and surrounding areas, our roads, lighting, etc. as well as our private security patrols. Currently the village is a quiet, friendly secure place to our last years. However I foresee our life in the village about to change and not for the better if the hotel is allowed to be built at the proposed location. There is no way our combined HOAs can prevent the people from the hotel from accessing our communities our private spaces thus opening our area to the noise, litter and criminal element that resident hotels incur. Attempting to do so by the HOA associations will mean higher HOA fees for our residents who are already trying to maintain thru high inflation on a fixed income. I feel as both a tax payer and voter that it is fair for me to ask my supervisors to get in their cars and drive out to the village. Drive our streets and better yet get out and walk our pond, talk to us about our security concerns, physically look at the proposed hotel location in relationship to our senior community. Don't just look at your map. The drawing of the hotel looks like it is designed to fit Merium Park, not Cal Park. Merium Park would make more sense to me as it would provide access to food and shopping both for the hotel residence and more customers for local businesses.. I could see it benefiting both the market place, businesses along both Forest Ave, Del North and the Skyway thus benefiting the city by generating many more tax dollars than a residential area could. Please remember that we in California Park also pay taxes and contribute to our wonderful community of Chico. Sincerely,  
Rochelle Caldwell

To: Bruce Ambo, Chico City Principal Planner  
Community Development Dept., Planning Division

From: Mary Jensen, 45 Edgewater Court  
Chico. Ca. 95928  
February 27, 2022

Shortly before the pandemic began, Chico residents packed a large meeting hall to vehemently protest the insertion of a Marriott Towne Place hotel into the senior residence and services neighborhood, Sierra Sunrise Village (SSV). Many in the audience had been in Chico only a short time since fleeing their town of Paradise under fire. Naturally the topic of fire evacuation was heavy on their minds, as was the sense that what they thought was a safe senior area could become a victim of thoughtless, dangerous planning.

Such planning to date would erect a four story, 112-unit hotel at the major SSV evacuation road; would every day divert traffic onto the SSV private road; would expose their lake and paths to need for 24-hour security---all at the expense and liability of residents in SSV and Cal Park the Rehabilitation Hospital and the assisted living facilities.

There are many reasons why the original plan for our senior community prohibited commercial structures of this size and impact, yet that original plan has been ignored in this process. Like the voices at the community meeting, I ask that a full environmental impact report be required, and an honest, thorough review put the needs of yourcitizens ahead of the desires of a massive corporation. Apparently no one in City Hall was listening the last time our neighbors were eloquent and impassioned in their opposition. Let's start again, perhaps with a consideration of why, if this is such a valuable project, it is not slated for Merriam Park where its architectural design, audience and 24-hour services would be compatible.



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Opposition to the Marriott Towne Place Suites Hotel Project  
**Date:** Monday, February 28, 2022 12:34:27 PM

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**From:** Martha Ellington <martha.ellington@gmail.com>  
**Sent:** Monday, February 28, 2022 12:19 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Opposition to the Marriott Towne Place Suites Hotel Project

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Dear Mr. Ambo,

I am a resident of California Park. I am very much opposed to the building of a hotel in our residential neighborhood. A four story, 112 room hotel has no place on Sierra Sunrise Terrace, a private road that leads to Chico's largest senior community. As you know, this community consists of senior housing, assisted living, memory care facilities, and a rehab hospital. Emergency vehicles rely on this private road for quick access to the community. It is also a primary emergency evacuation route for all the residents of Cal Park, Lake Vista and Canyon Oaks.

I am also concerned about the added financial burden that would be placed on our residents for the cost of additional security patrol. The safety of our seniors would be a concern in the face of an ever changing, transient hotel clientele. California Park lakes and trails are private and would have to be protected from hotel trespassers as well.

Those reasons alone should be enough to stop this ill-conceived project from being built. I am very curious to know why this project has been allowed to advance without a full review of the environmental impacts upon the riparian areas surrounding the bordering Dead Horse Slough, dam and spillway. Also, why has it been determined that it would not cause additional traffic concerns, when their own estimate is for 500 car trips per day into the ever increasing congestion at Bruce Road and Hwy 32?

I urge you to please help protect the health and safety of our community. There is a reason why this property is not zoned for this type of development - it Does Not belong here. The city's general plan indicates that the areas near Upper Bidwell Park and the foothills on the eastern edge of the community "are particularly prone to wildland fire".

This project is not consistent with the city's general plan. It would negatively impact the health, safety, and general welfare residents. The design, location, size and operating characteristics of a hotel are not compatible with an existing residential community.

Please do not allow a hotel to be built here.

Thank you,  
Martha Ellington  
President, California Park Association

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Proposed Hotel  
**Date:** Monday, February 28, 2022 1:52:30 PM

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-----Original Message-----

From: Garry Small <[garrynjackie@gmail.com](mailto:garrynjackie@gmail.com)>  
Sent: Monday, February 28, 2022 1:34 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Proposed Hotel

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As residents of Husa Ranch, we wish to express our strong opposition to the proposed hotel at Bruce and Sierra Sunrise. The traffic at that intersection is extremely dangerous and not easily resolved. This proposed project does not fit with the residential nature of the neighborhood. Hopefully the concerns of the residents will be given major consideration.

Thank you,  
Garry and Jackie Small  
128 Copperfield Dr.  
Chico, CA

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: No Hotel California Park  
**Date:** Monday, February 28, 2022 4:19:49 PM

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-----Original Message-----

From: VERDA MACKAY <[verdamackay7114@comcast.net](mailto:verdamackay7114@comcast.net)>  
Sent: Monday, February 28, 2022 11:33 AM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: No Hotel California Park

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Dear Mr. Ambo:

I bought my model home in Nob Hill just across the street from California Park 22 years ago and have lived in it since. I have seen the extreme traffic grow as the new buildings are completed. My home backs up to Bruce Rd. across from the Enloe Hospital speciality clinics and it has been peaceful--until this traffic grows.

Please do what you can to disallow the building of a large hotel building in this peaceful housing area. Thank you for any help you are able to provide.

Verda Mackay  
853 Coit Tower Way  
Chico, CA 95928



## Nicole Acain

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**From:** Bruce Ambo  
**Sent:** Monday, February 28, 2022 4:29 PM  
**To:** Nicole Acain; Kelly Murphy  
**Subject:** FW: NOE Town Suites  
**Attachments:** City of Chico\_NOE\_TownSuites.pdf

**From:** Anjanette Shadley <anjibabe7@gmail.com>  
**Sent:** Monday, February 28, 2022 4:26 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** NOE Town Suites

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Mr. Ambo:

Please see attached letter regarding findings of the NOE for the proposed Town Place Suites.

Respectfully,

--

Anjanette Shadley

*sent from mobile gmail*

February 28, 2022

City of Chico  
Attn: Bruce Ambo, Principal Planning

**RE: Notice of Exemption Town Place Suites – Environmental Analysis**

I am writing to object to the findings of the NOE for the Town Place Suites proposal at HWY 32/Bruce Road/Sierra Sunrise Terrace.

Specifically, to the findings below:

19.24.040 Decision and findings.  
Following a public hearing, the Zoning Administrator or Planning Commission may approve a use permit application, with or without conditions, only if all of the following findings can be made:

B. The proposed use would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed use;

**RESPONSE:** The proposed project is located adjacent to numerous senior housing, medical rehabilitation, supported living, 55+ community and medical facilities which would make it nearly impossible to evacuate the 1,000s of residents adjacent to or with in the same area in case of an emergency (flood, fire or earthquake etc). Traffic is congested at best, unpassable at worse during morning and evening commute hours at the most immediate intersection of Sierra Sunrise Terrace and Bruce Road. North, South, East and West traffic has most recently (over the last two years) backed up to Meriam Park development at Concord Ave on most weekdays between 4 pm and 6 pm. Current development adjacent to the proposed project in East Chico between Skyway and Hwy 32 will only add to the congestion and unsafe vehicle and pedestrian interface especially in the area of Marsh Junior High School and a proposal for a new high school on Bruce Road. Current development in the Oak Valley subdivision of more single family homes and apartments without improved roadways i.e. four-lane and new traffic lights and ingress and egress for this project, under construction developments and future need to be addressed as soon as possible.

C. The proposed use would not be detrimental and/or injurious to property and improvements in the neighborhood of the proposed use, as well as the general welfare of the City;

**RESPONSE:** See comment above under findings (B). Furthermore, the adjacent property to the proposed project of the new hotel is private property, protected by and for the homeowners in California Park, California Park Lakeside and Sierra Sunrise Terrace communities.

D. The proposed entitlement is consistent with the General Plan, any applicable specific plan, and any applicable neighborhood or area plan;

**RESPONSE:** The adjacent property to the proposed project of the new hotel is private property, protected by and for the homeowners in California Park, California Park Lakeside and Sierra Sunrise Terrace communities and therefore not compatible with the neighborhood.

I respectfully request the project not move forward and my comments submitted for the record in any future deliberations by the City of Chico, commissions and departments.

Sincerely,

Anjanette Shadley

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: NO Hotel  
**Date:** Monday, February 28, 2022 8:00:36 AM

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**From:** Robert Fraga <rfraga@sbcglobal.net>  
**Sent:** Sunday, February 27, 2022 8:44 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** NO Hotel

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Dear Mr. Ambo,

February 27, 2022

As residents of California Park, we are writing to ask you to deny the building permit for the proposed hotel in the California Park area. As a city planner, you should know that this type of building/business does not belong in this zone.

First, a hotel placed right next to an assisted-living facility is completely inappropriate. The said property was originally designed to be used for additional medical facilities that would house doctors' offices, pharmacies, etc. that would benefit the local community. Building a large hotel with its lighted signage and 24-hour-a-day use would be an eyesore and nuisance to our community. Furthermore, the noise and traffic it would bring would impact the quiet neighborhood that these seniors enjoy.

Not only would it impact the current aesthetics of the community, it would impact the already overcrowded parking situation. Knowing that the hotel would not be able to provide enough parking for the guests and employees, they would be forced to park in the already-existing parking lots of the current business on Lakewest Ave and the area around The Terraces. These businesses already have problems keeping people from parking in their lots that shouldn't be parking there. Adding the hotel guests would make this problem even worse.

Because the lake and the surrounding walking trails belong to the California Park Homeowners Association, we pay for the upkeep, maintenance and security. Our security teams are already taxed to the limit monitoring and keeping non-members from using the facilities. The hotel guests would add many more people to this mix, which would increase the cost of these security services, placing a financial burden on the homeowners.

Additionally, the said property is not zoned for hotel use. The road ways are over-taxed with the amount of use for people in this area. Then the luxury apartments were added to the corner of Bruce and California Park Drive, which impacted the traffic immensely. Now, you've added multiple apartment complexes along Hwy 32 and Bruce Road. These residents will be using Bruce Road as a daily route, which is now impacting the traffic even more. During the raining season, Bruce Road often floods on the

same corner where the hotel is being proposed, and the traffic has to be diverted through our neighborhoods. This would mean that during these flood times, all of the hotel traffic would go through our private neighborhoods, adding wear and tear to our neighborhood streets and additional to our neighborhoods. This is completely unacceptable. The new apartments in Miriam Park and the surrounding area also use Bruce Road, and this is causing an additional impact. The city has been talking about widening Bruce Road for years, and it still hasn't happened. The roads are in terrible condition and adding more traffic to the already dilapidated roads is not a step in the right direction.

As survivors of the Campfire, we have significant concerns regarding evacuation should there be an emergency. The California Park area sits just below the foothills, and wildfires are to be expected; it's simply a reality for California. Overly-impacting the area with residents without proper evacuation routes is asking for disaster.

I am a General Contractor, and I'm all for growth as long as it is in right place and at the right times. As a City Planner, you should know that this is not a good fit for our community. There are plenty of other properties in Chico that could house this type of business, places that would provide more amenities for the hotel guests and support the surrounding businesses. We are strongly asking you to deny the special use permit that will be needed for this hotel and preserve this property for the original intended purposes.

Sincerely,

Robert and Dyanne Fraga

1118 Yosemite Drive

Chico, CA 95928

CC Mike Wolcott, Chico Enterprise Record Editor, Chico News & Review



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: No Hotel  
**Date:** Monday, February 28, 2022 4:20:51 PM

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-----Original Message-----

From: MARTIN JIMENEZ <mjart11@hotmail.com>  
Sent: Sunday, February 27, 2022 7:20 PM  
To: Bruce Ambo <bruce.ambo@Chicoca.gov>  
Subject: No Hotel

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February 27, 2022

ATTN: Bruce Ambo, Chico City Principal Planner

We're writing to express our opposition to the proposed hotel at California Park. We have lived on Jordanne Court with our two sons for nearly 10 years. Our home directly faces the Sierra Sunrise Village senior community. We are concerned about the proposed hotel, and its probable negative impact on our neighborhood for several reasons.

1. We are concerned with our ability to evacuate in the face of a wildfire, and a large hotel at our main exit point seems unsafe.
2. We chose our neighborhood because of the safety we feel, and the freedom our boys have to explore our trails and lake areas. With a large hotel in our neighborhood, trespassing is likely, and security of our private lakes and walkways will be negatively impacted, with added fees to increase patrol services of these areas likely.
3. From what we understand, the property is not zoned for this type of commercial building and will require a special "use permit" from the City of Chico. The land was intended to be developed for the needs of the local residents, and clearly not for this type of commercial development.
4. The design, location, size, and other characteristics of the proposed 4-story hotel are clearly not compatible with the surrounding community. In addition, it would be an eyesore with its nightly business lights, enormous size in relation to surroundings, and its generally obtrusive nature.
5. Traffic concerns are already an issue in this area, and a hotel of this size would exacerbate these problems.
6. We have been informed that the project has not gone through a full environmental impact review. There would certainly be impacts to the surrounding natural environment, and the adjacent riparian area.

Thank you for your consideration.

Sincerely,

Martin and Nicole Jimenez

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Towne Place Suites Hotel Proposed at Bruce Road, Hwy 32 and Sierra Sunrise Terrace  
**Date:** Monday, February 28, 2022 8:01:36 AM

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**From:** Cindy Rogers <530cindy.rogers@gmail.com>  
**Sent:** Sunday, February 27, 2022 11:52 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Towne Place Suites Hotel Proposed at Bruce Road, Hwy 32 and Sierra Sunrise Terrace

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Hi,

I've been following this proposal for some time now and attended the meeting at Canyon Oaks Country Club. I don't live on the lake but I am located in the Luciano development off of Yosemite. I enjoy the closeness to the lake and the quiet neighborhood during my walks.

From the hotel proposal that day, I was amazed at how many neighbors attended - it was standing room only. Not one community member wanted a hotel at this location. In looking at the proposal and the pictures the hotel presented, the lake was a huge presence. I am shocked why a hotel would want to build where they are not welcome. I believe they were overwhelmed by the attendance and the adamant NO to this hotel. My key thoughts:

- The lake is private. The homeowners pay HOA for the upkeep and security of this lake. I walk Idylwild, Sunrise Terrace and Lakewest. While I would like to walk around the lake, since I don't pay HOA dues to have a pass, I'm not able to. The lake is private. I see the private security keeping people off the paths and the lake. The hotel would bring visitors to this private community and there would be trespassers.
- The footprint of the hotel is a huge negative to those that live on the lake that would face the hotel and those that live on Sunrise Terrace. The hotel next to Kohl's is 4 stories tall and blocks the view.
- The light and noise would change the community. This is a quiet neighborhood. A streetlight was just installed at 32 and Yosemite last week. Part of this was new streetlights at that corner. The extra light generated is significant and negatively affects the homes on the north side corner.
- The hotel has 112 rooms with 122 parking spaces. If the hotel is fully booked, this gives employees 10 parking spaces. This means they will park on the streets or park where they shouldn't.

- Sunrise Terrace is a narrow road and this hotel doesn't fit the community, a neighborhood. Service vehicles and delivery trucks will impact the road/traffic. I heard this road is private, but I don't know if this is true, is it?
  - When I stay at a hotel I want to be close to restaurants I can walk to.
  - Why do you need a hotel here? There is the hotel at Winco, the one next to Kohls is about to open, one is being built at Meriam park, and Oxford Suites is due to expand.
- Please reconsider this proposal, this is a bad choice for our neighborhood.

Cindy Rogers  
A Cal Park neighbor.



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: No Hotel Calif Park  
**Date:** Monday, February 28, 2022 8:02:57 AM

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-----Original Message-----

From: Sylvia Seagle <[SSeagle@csuchico.edu](mailto:SSeagle@csuchico.edu)>  
Sent: Saturday, February 26, 2022 8:27 AM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: No Hotel Calif Park

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Dear Mr. Ambo,

Like many others that I hope have written to you, my husband and I are extremely against building of the proposed hotel on the corner of Bruce Road and Sierra Sunrise Blvd. The additional traffic and congestion will be a nightmare, especially to the seniors living in that area, and many are considering leaving the best senior community in this area, if the hotel is built. Where will they go in this economy? This property is NOT zoned for commercial use. By admission of a representative from the hotel, in a neighborhood meeting that was held over year ago at Canyon Oaks Golf Course, there will NOT be enough parking spaces available on their lot, if the hotel is full. Consequently, cars would be lining the streets, causing further congestion, especially in an emergency evacuation situation. Over 100 residents attended that meeting, and ALL of them were opposed to this hotel. Cal Park residents pay an annual fee to keep our paths, lakes and ponds clean AND private, but hotel guests will not respect that, causing additional revenue from the homeowners for patrol, keeping our area safe and clean.

We URGE you to please voice our concerns to your committee regarding these issues, and many others that I have not mentioned, but I'm sure, many have.

Thank you for your time and consideration,

Dr. and Mrs. Edward Seagle

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: No Hotel California Park!  
**Date:** Monday, February 28, 2022 4:17:40 PM

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**From:** Tally Sturm <[ttreyster@gmail.com](mailto:ttreyster@gmail.com)>  
**Sent:** Monday, February 28, 2022 4:09 PM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
**Subject:** No Hotel California Park!

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February 28, 2022

Dear Mr. Ambo:

Though not a Paradise fire victim, the horror and ramifications have touched me and all in Chico. Living in California Park on the edge of Highway 32 which was closed as the fire blazed down the hill and we prepared for evacuation, I was healthy and nimble enough to pack my car and be ready to drive out...if a road was available.

This brings me to a major concern about a proposed hotel to be built on the corner of Highway 32 and Bruce Road. Marriott is requesting use of our private road, Sierra Sunrise Terrace, which is the main access to the largest retirement community in Chico. Numerous residents are not ambulatory and mayhem ensued during the fire as evacuation of this fragile group was necessary. Fire trucks and ambulances are a daily occurrence for this population and quick access is mandatory. A four story hotel at this location is a major hinderance to residential traffic as well as emergency vehicles as the hotel is not accessible from any street other than the one belonging to the retirement community. This is a horrendous fit for the neighborhood and without thoughtful and safe planning, many lives will be in peril.

We have over 1500 signatures on the petition to fight this abuse of land, but need your intervention to assist us in any way you can. After two meetings where our grievances were aired to Marriott spokespeople and the City of Chico, the firm has gone ahead to file a request to build and a use permit with the Chico Planning Commission.

Chico is in a building frenzy at the moment, however, development needs to be suitable to an existing neighborhood. As you are aware, if a proposed use is detrimental to the health, safety

and general welfare of persons residing or working in the neighborhood, a permit should not be issued.

Thank you for your time and consideration in this matter.

Sincerely,

Tally Sturm  
2859 Pennyroyal Drive  
Chico, CA 95928

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: TownePlace Suites Proposal  
**Date:** Monday, February 28, 2022 12:38:26 PM

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**From:** Susan Shafsky <[csucmimi@yahoo.com](mailto:csucmimi@yahoo.com)>  
**Sent:** Monday, February 28, 2022 12:21 PM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
**Cc:** Mike Wolcott <[mwolcott@chicoer.com](mailto:mwolcott@chicoer.com)>; Susan Shafsky <[csucmimi@yahoo.com](mailto:csucmimi@yahoo.com)>;  
[sgoulart@sbcglobal.net](mailto:sgoulart@sbcglobal.net); [jiminchico@yahoo.com](mailto:jiminchico@yahoo.com); Hannah Shafsky <[hanniemay@gmail.com](mailto:hanniemay@gmail.com)>; Leah  
Goodliffe <[leahgoodliffe@gmail.com](mailto:leahgoodliffe@gmail.com)>  
**Subject:** TownePlace Suites Proposal

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Dear Mr. Ambo,

As a resident of a neighboring area of CALPark (Hooker Oak Avenue), I have deep concerns about the proposal to build TownePlace Suites at the northeast corner of Bruce and Hwy 32. I have no financial concern regarding this project but I do have important concerns. I have reviewed many of the documents regarding this proposal. I also want to add that I was a participant at the standing room only meeting regarding this hotel in March 2020 at the Canyon Oaks Club House. The City Planning Dept. and representatives of Marriott and approx 200 audience members were at that meeting. I do not know if you were there so let me just say that there was palpable tension, dismay and anger from the audience. Not one person in the audience raised their hand when asked if anyone wanted the hotel built.

I could write pages and pages on each of the following concerns - I will try to be as brief as possible. I am sure you have already received emails and letters regarding some of the following concerns.

The main concerns are:

Question of what "Commercial Mixed Use" actually means ----In looking for definitions of this term, there is ambiguity. I could find no clear definition. Mostly definitions described buildings as "mixed use", not areas. I did see that "commercial mixed use" could include housing, services, parks, even industrial. I saw no mention of "hotels". I saw there was a different zoning designation on a 2020 city zoning map.

General safety --- Just the fact the entire area was originally planned as a relaxed lifestyle environment (see old CALPark promotions and pamphlets from 1983) is an indication that times



have definitely changed and our planners now seem to be pro-growth and not in keeping with the original plan. Seniors and CALPark residents walk along the road to visit the lakes and restaurants in the area. There are almost no sidewalks along Sierra Terrace Road. A hotel will cause much more pedestrian traffic along the road. What happened to the nursery, cafe, park, gift shop? See more safety issues below.

Traffic--- I read the traffic study for the present and future 2040 prediction. I agree there may be very little growth coming from the CALPark area (unless the City of Chico changes more land use zoning). However, I see BIG discrepancies in the Traffic Report regarding traffic along the Bruce Road (which should include Manzanita) and HWY 32. Personally, living on Hooker Oak, I use the Manzanita-Bruce corridor sometimes 1-4 times per day. Many people choose to drive this "back way" rather than head west and get on the freeway. Remember that we have the huge apartment development and homes being built on Eaton Road and side streets. Eaton, Lassen, East, Hooker Oak, Vallambrosa, Hwy 32, 20th Street and Lower Skyway cars all use this corridor as a way to avoid driving "into town". What will happen to the intersections and stop lights at 32 and Sierra Sunrise then?

Environmental---I see that this project has an exemption from an Environmental Study due to the size of the project (4.09 acres) and other factors. As Church Lady would say, "How convenient!" There are animal and bird life concerns. A biologist will be required to oversee all grading and building. I do see that the biologist did note swallows, doves, a bald eagle and red slider turtles. I have seen many ducks, egrets, herons, and other bird and aquatic life. It is obvious that animal and bird and aquatic life will be affected by more cars, more noise, more lighting throughout the night, and a building 59.3 feet tall. Migration and nesting changes may likely occur.

Architectural drawing discrepancies--- While the hotel is being built, there will be traffic concerns with large trucks and semis delivering materials along Sierra Sunrise Road. Trucks backing up and such may cause road delays and confusion for senior drivers. After the hotel is built, after studying the architectural plans, I see there is a discrepancy in the number of parking spaces. The hotel will have 112 rooms. I counted 91 hotel parking places (including 3 handicapped). There might be 3 more on the very eastern side of the property. I do not see areas for deliveries and the necessary truck parking and unloading - food, linens, materials. Also if any semi truck drivers want to stay at the hotel, where will they park? What about people with trailers?

State audit on lack of emergency preparedness in Butte County---This certainly is a very timely and frightening subject. All Californians wonder where the next fire will be this summer. The CA state audit found 3 counties not prepared for a disaster such as a large, fastmoving forest fire. If a fire were to come down the hills from the east. Chico would be in big trouble, especially East Chico, with all cars trying to move west.. And the residents of CALPark Sierra Sunrise would be in dangerous difficulty due to age, lack of car, disabilities, inability to walk, confusion etc. And as noted, Butte County HAS NOT DEVELOPED AN EMERGENCY EVACUATION PLAN. How can a large building with many people be built where there is no emergency evacuation plan to leave the entire east side of the city?

Trespassing on CALPark private prop--This is an issue for the CALPark residents. They live in a private

community and the lakes and walking paths are not open to the public. From the front of the hotel, it's a 2 minute walk across the road to trespass on beautiful CALPark property. How will the hotel prevent that problem?

General beauty of the area--Personally this is the one that concerns me the most. Basically the intersection of 32 and Bruce is the GATEWAY to the rolling hills and mountains of our beautiful area. The hotel is visibly incongruous with anything in the entire area. The architecture does not blend in with any building nearby - partly because of height and mainly because of how large it is and what it is. Most of the area is residential and the few businesses that are there are relatively small and only one or two stories. If you've ever driven in Palm Springs or Santa Barbara, for example, you have seen what could city planning can look like. I do not want our town to have a big sore thumb with bright lights sitting on the GATEWAY TO THE MOUNTAINS.

I sincerely appreciate your time reading this and I hope you will share it with your co-planners.

Thank you,

Susan Shafsky  
1857 Hooker Oak Avenue  
Chico, CA 95926  
[Sent from Yahoo Mail on Android](#)

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Hotel at hyw32 and Bruce rd.  
**Date:** Monday, February 28, 2022 12:42:15 PM

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**From:** Rochelle Caldwell <rockic267@gmail.com>  
**Sent:** Monday, February 28, 2022 12:40 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>; Mike Wolcott <mwolcott@chicoer.com>  
**Subject:** Hotel at hyw32 and Bruce rd.

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ed. Live in Sierra Sunrise Village which is a senior community in California Park. The village borders the property the proposed residence hotel wishes to build on. As well as private homes and private condos the village also includes senior apartments, assisted living, rehab and skilled nursing care and a memory care facility. Quick access by emergency medical vehicles to our residents is often a matter of life and death. As residence of both Sierra Sunrise village and California Park and Lakeside village we pay HOA dues. These dues maintain the ponds, our trails and surrounding areas, our roads, lighting, etc. as well as our private security patrols. Currently the village is a quiet, friendly secure place to our last years. However I foresee our life in the village about to change and not for the better if the hotel is allowed to be built at the proposed location. There is no way our combined HOAs can prevent the people from the hotel from accessing our communities our private spaces thus opening our area to the noise, litter and criminal element that resident hotels incur. Attempting to do so by the HOA associations will mean higher HOA fees for our residents who are already trying to maintain thru high inflation on a fixed income. I feel as both a tax payer and voter that it is fair for me to ask my supervisors to get in their cars and drive out to the village. Drive our streets and better yet get out and walk our pond, talk to us about our security concerns, physically look at the proposed hotel location in relationship to our senior community. Don't just look at your map. The drawing of the hotel looks like it is designed to fit Merium Park, not Cal Park. Merium Park would make more sense to me as it would provide access to food and shopping both for the hotel residence and more customers for local businesses.. I could see it benefiting both the market place, businesses along both Forest Ave, Del Norte and the Skyway thus benefiting the city by generating many more tax dollars than a residential area could. Please remember that we in California Park also pay taxes and contribute to our wonderful community of Chico. Sincerely,  
Rochelle Caldwell

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Opposition to the Marriott Towne Place Suites Hotel Project  
**Date:** Monday, February 28, 2022 12:34:27 PM

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**From:** Martha Ellington <martha.ellington@gmail.com>  
**Sent:** Monday, February 28, 2022 12:19 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Opposition to the Marriott Towne Place Suites Hotel Project

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Dear Mr. Ambo,

I am a resident of California Park. I am very much opposed to the building of a hotel in our residential neighborhood. A four story, 112 room hotel has no place on Sierra Sunrise Terrace, a private road that leads to Chico's largest senior community. As you know, this community consists of senior housing, assisted living, memory care facilities, and a rehab hospital. Emergency vehicles rely on this private road for quick access to the community. It is also a primary emergency evacuation route for all the residents of Cal Park, Lake Vista and Canyon Oaks.

I am also concerned about the added financial burden that would be placed on our residents for the cost of additional security patrol. The safety of our seniors would be a concern in the face of an ever changing, transient hotel clientele. California Park lakes and trails are private and would have to be protected from hotel trespassers as well.

Those reasons alone should be enough to stop this ill-conceived project from being built. I am very curious to know why this project has been allowed to advance without a full review of the environmental impacts upon the riparian areas surrounding the bordering Dead Horse Slough, dam and spillway. Also, why has it been determined that it would not cause additional traffic concerns, when their own estimate is for 500 car trips per day into the ever increasing congestion at Bruce Road and Hwy 32?

I urge you to please help protect the health and safety of our community. There is a reason why this property is not zoned for this type of development - it Does Not belong here. The city's general plan indicates that the areas near Upper Bidwell Park and the foothills on the eastern edge of the community "are particularly prone to wildland fire".

This project is not consistent with the city's general plan. It would negatively impact the health, safety, and general welfare residents. The design, location, size and operating characteristics of a hotel are not compatible with an existing residential community.



Please do not allow a hotel to be built here.

Thank you,  
Martha Ellington  
President, California Park Association

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Proposed Hotel  
**Date:** Monday, February 28, 2022 1:52:30 PM

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-----Original Message-----

From: Garry Small <[garrynjackie@gmail.com](mailto:garrynjackie@gmail.com)>  
Sent: Monday, February 28, 2022 1:34 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Proposed Hotel

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As residents of Husa Ranch, we wish to express our strong opposition to the proposed hotel at Bruce and Sierra Sunrise. The traffic at that intersection is extremely dangerous and not easily resolved. This proposed project does not fit with the residential nature of the neighborhood. Hopefully the concerns of the residents will be given major consideration.

Thank you,  
Garry and Jackie Small  
128 Copperfield Dr.  
Chico, CA

## Nicole Acain

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**From:** Bruce Ambo  
**Sent:** Monday, February 28, 2022 4:29 PM  
**To:** Nicole Acain; Kelly Murphy  
**Subject:** FW: NOE Town Suites  
**Attachments:** City of Chico\_NOE\_TownSuites.pdf

**From:** Anjanette Shadley <anjibabe7@gmail.com>  
**Sent:** Monday, February 28, 2022 4:26 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** NOE Town Suites

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Mr. Ambo:

Please see attached letter regarding findings of the NOE for the proposed Town Place Suites.

Respectfully,

--

Anjanette Shadley

*sent from mobile gmail*

February 28, 2022

City of Chico  
Attn: Bruce Ambo, Principal Planning

**RE: Notice of Exemption Town Place Suites – Environmental Analysis**

I am writing to object to the findings of the NOE for the Town Place Suites proposal at HWY 32/Bruce Road/Sierra Sunrise Terrace.

Specifically, to the findings below:

19.24.040 Decision and findings.

Following a public hearing, the Zoning Administrator or Planning Commission may approve a use permit application, with or without conditions, only if all of the following findings can be made:

B. The proposed use would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed use;

**RESPONSE:** The proposed project is located adjacent to numerous senior housing, medical rehabilitation, supported living, 55+ community and medical facilities which would make it nearly impossible to evacuate the 1,000s of residents adjacent to or with in the same area in case of an emergency (flood, fire or earthquake etc). Traffic is congested at best, unpassable at worse during morning and evening commute hours at the most immediate intersection of Sierra Sunrise Terrace and Bruce Road. North, South, East and West traffic has most recently (over the last two years) backed up to Meriam Park development at Concord Ave on most weekdays between 4 pm and 6 pm. Current development adjacent to the proposed project in East Chico between Skyway and Hwy 32 will only add to the congestion and unsafe vehicle and pedestrian interface especially in the area of Marsh Junior High School and a proposal for a new high school on Bruce Road. Current development in the Oak Valley subdivision of more single family homes and apartments without improved roadways i.e. four-lane and new traffic lights and ingress and egress for this project, under construction developments and future need to be addressed as soon as possible.

C. The proposed use would not be detrimental and/or injurious to property and improvements in the neighborhood of the proposed use, as well as the general welfare of the City;

**RESPONSE:** See comment above under findings (B). Furthermore, the adjacent property to the proposed project of the new hotel is private property, protected by and for the homeowners in California Park, California Park Lakeside and Sierra Sunrise Terrace communities.

D. The proposed entitlement is consistent with the General Plan, any applicable specific plan, and any applicable neighborhood or area plan;

**RESPONSE:** The adjacent property to the proposed project of the new hotel is private property, protected by and for the homeowners in California Park, California Park Lakeside and Sierra Sunrise Terrace communities and therefore not compatible with the neighborhood.

I respectfully request the project not move forward and my comments submitted for the record in any future deliberations by the City of Chico, commissions and departments.

Sincerely,

Anjanette Shadley



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acaín](#); [Kelly Murphy](#)  
**Subject:** FW: NO Hotel  
**Date:** Monday, February 28, 2022 8:00:36 AM

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**From:** Robert Fraga <rfraga@sbcglobal.net>  
**Sent:** Sunday, February 27, 2022 8:44 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** NO Hotel

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Dear Mr. Ambo,

February 27, 2022

As residents of California Park, we are writing to ask you to deny the building permit for the proposed hotel in the California Park area. As a city planner, you should know that this type of building/business does not belong in this zone.

First, a hotel placed right next to an assisted-living facility is completely inappropriate. The said property was originally designed to be used for additional medical facilities that would house doctors' offices, pharmacies, etc. that would benefit the local community. Building a large hotel with its lighted signage and 24-hour-a-day use would be an eyesore and nuisance to our community. Furthermore, the noise and traffic it would bring would impact the quiet neighborhood that these seniors enjoy.

Not only would it impact the current aesthetics of the community, it would impact the already overcrowded parking situation. Knowing that the hotel would not be able to provide enough parking for the guests and employees, they would be forced to park in the already-existing parking lots of the current business on Lakewest Ave and the area around The Terraces. These businesses already have problems keeping people from parking in their lots that shouldn't be parking there. Adding the hotel guests would make this problem even worse.

Because the lake and the surrounding walking trails belong to the California Park Homeowners Association, we pay for the upkeep, maintenance and security. Our security teams are already taxed to the limit monitoring and keeping non-members from using the facilities. The hotel guests would add many more people to this mix, which would increase the cost of these security services, placing a financial burden on the homeowners.

Additionally, the said property is not zoned for hotel use. The road ways are over-taxed with the amount of use for people in this area. Then the luxury apartments were added to the corner of Bruce and California Park Drive, which impacted the traffic immensely. Now, you've added multiple apartment complexes along Hwy 32 and Bruce Road. These residents will be using Bruce Road as a daily route, which is now impacting the traffic even more. During the raining season, Bruce Road often floods on the

same corner where the hotel is being proposed, and the traffic has to be diverted through our neighborhoods. This would mean that during these flood times, all of the hotel traffic would go through our private neighborhoods, adding wear and tear to our neighborhood streets and additional to our neighborhoods. This is completely unacceptable. The new apartments in Miriam Park and the surrounding area also use Bruce Road, and this is causing an additional impact. The city has been talking about widening Bruce Road for years, and it still hasn't happened. The roads are in terrible condition and adding more traffic to the already dilapidated roads is not a step in the right direction.

As survivors of the Campfire, we have significant concerns regarding evacuation should there be an emergency. The California Park area sits just below the foothills, and wildfires are to be expected; it's simply a reality for California. Overly-impacting the area with residents without proper evacuation routes is asking for disaster.

I am a General Contractor, and I'm all for growth as long as it is in right place and at the right times. As a City Planner, you should know that this is not a good fit for our community. There are plenty of other properties in Chico that could house this type of business, places that would provide more amenities for the hotel guests and support the surrounding businesses. We are strongly asking you to deny the special use permit that will be needed for this hotel and preserve this property for the original intended purposes.

Sincerely,

Robert and Dyanne Fraga

1118 Yosemite Drive

Chico, CA 95928

CC Mike Wolcott, Chico Enterprise Record Editor, Chico News & Review

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: No Hotel  
**Date:** Monday, February 28, 2022 4:20:51 PM

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-----Original Message-----

From: MARTIN JIMENEZ <mjart11@hotmail.com>  
Sent: Sunday, February 27, 2022 7:20 PM  
To: Bruce Ambo <bruce.ambo@Chicoca.gov>  
Subject: No Hotel

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February 27, 2022

ATTN: Bruce Ambo, Chico City Principal Planner

We're writing to express our opposition to the proposed hotel at California Park. We have lived on Jordanne Court with our two sons for nearly 10 years. Our home directly faces the Sierra Sunrise Village senior community. We are concerned about the proposed hotel, and its probable negative impact on our neighborhood for several reasons.

1. We are concerned with our ability to evacuate in the face of a wildfire, and a large hotel at our main exit point seems unsafe.
2. We chose our neighborhood because of the safety we feel, and the freedom our boys have to explore our trails and lake areas. With a large hotel in our neighborhood, trespassing is likely, and security of our private lakes and walkways will be negatively impacted, with added fees to increase patrol services of these areas likely.
3. From what we understand, the property is not zoned for this type of commercial building and will require a special "use permit" from the City of Chico. The land was intended to be developed for the needs of the local residents, and clearly not for this type of commercial development.
4. The design, location, size, and other characteristics of the proposed 4-story hotel are clearly not compatible with the surrounding community. In addition, it would be an eyesore with its nightly business lights, enormous size in relation to surroundings, and its generally obtrusive nature.
5. Traffic concerns are already an issue in this area, and a hotel of this size would exacerbate these problems.
6. We have been informed that the project has not gone through a full environmental impact review. There would certainly be impacts to the surrounding natural environment, and the adjacent riparian area.

Thank you for your consideration.

Sincerely,

Martin and Nicole Jimenez



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Towne Place Suites Hotel Proposed at Bruce Road, Hwy 32 and Sierra Sunrise Terrace  
**Date:** Monday, February 28, 2022 8:01:36 AM

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**From:** Cindy Rogers <530cindy.rogers@gmail.com>  
**Sent:** Sunday, February 27, 2022 11:52 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Towne Place Suites Hotel Proposed at Bruce Road, Hwy 32 and Sierra Sunrise Terrace

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Hi,

I've been following this proposal for some time now and attended the meeting at Canyon Oaks Country Club. I don't live on the lake but I am located in the Luciano development off of Yosemite. I enjoy the closeness to the lake and the quiet neighborhood during my walks.

From the hotel proposal that day, I was amazed at how many neighbors attended - it was standing room only. Not one community member wanted a hotel at this location. In looking at the proposal and the pictures the hotel presented, the lake was a huge presence. I am shocked why a hotel would want to build where they are not welcome. I believe they were overwhelmed by the attendance and the adamant NO to this hotel. My key thoughts:

- The lake is private. The homeowners pay HOA for the upkeep and security of this lake. I walk Idylwild, Sunrise Terrace and Lakewest. While I would like to walk around the lake, since I don't pay HOA dues to have a pass, I'm not able to. The lake is private. I see the private security keeping people off the paths and the lake. The hotel would bring visitors to this private community and there would be trespassers.
- The footprint of the hotel is a huge negative to those that live on the lake that would face the hotel and those that live on Sunrise Terrace. The hotel next to Kohl's is 4 stories tall and blocks the view.
- The light and noise would change the community. This is a quiet neighborhood. A streetlight was just installed at 32 and Yosemite last week. Part of this was new streetlights at that corner. The extra light generated is significant and negatively affects the homes on the north side corner.
- The hotel has 112 rooms with 122 parking spaces. If the hotel is fully booked, this gives employees 10 parking spaces. This means they will park on the streets or park where they shouldn't.

- Sunrise Terrace is a narrow road and this hotel doesn't fit the community, a neighborhood. Service vehicles and delivery trucks will impact the road/traffic. I heard this road is private, but I don't know if this is true, is it?
- When I stay at a hotel I want to be close to restaurants I can walk to.
- Why do you need a hotel here? There is the hotel at Winco, the one next to Kohls is about to open, one is being built at Meriam park, and Oxford Suites is due to expand.

Please reconsider this proposal, this is a bad choice for our neighborhood.

Cindy Rogers  
A Cal Park neighbor.

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Marriott Town Suites@Cal Park  
**Date:** Monday, February 28, 2022 8:02:24 AM

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-----Original Message-----

From: Jim Moon <[JMoon@csuchico.edu](mailto:JMoon@csuchico.edu)>  
Sent: Saturday, February 26, 2022 4:41 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Marriott Town Suites@Cal Park

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I have read the traffic study which appears to be comprehensive. I disagree with their conclusion that a traffic signal at Sierra Sunrise Terrace and Bruce Road is not needed at the time of the opening of the Marriott facility. That intersection is already compromised by the 45 mile per hour speed limit on Bruce Rd. which has some drivers traveling at speeds beyond that. This is problematic because there are seniors living in the Sierra Sunrise complex who have cars. It is a documented fact that reaction time and speed perception are affected by age. I did not see that accounted for in the Traffic Study.

In addition to this being true 24/7, it is exacerbated during am/pm rush hours.

While 112 units is not a large number, it is comprised of persons unfamiliar with the intersection.

So, I believe a traffic signal would help mitigate the danger of increased traffic and diminished reaction/perception capabilities of the seniors.

But, the Catch-22 here is that a traffic signal will impact the Hwy 32/Bruce Road intersection especially during the am/pm commute. If the Sierra Sunrise/Bruce Rd trafficking light is on green for an exit from Sierra Sunrise, the traffic will backup on Bruce Rd back to Hwy 32 and beyond. The traffic on Bruce Road east of Hwy 32 at 5:00 pm already backs up to nearly Humboldt Rd.

And, what happens with the completion of Bruce Rd to 4 lanes? This will accommodate and encourage more traffic on Bruce Rd.

I would like these remarks entered into the formal record regarding the traffic impact of the proposed Marriott facility.

Thank you,

Jim Moon, a Cal Park resident.

11 Glenview Ct.

Chico, CA

To: Bruce Ambo, Chico City Principal Planner  
Community Development Dept., Planning Division

From: Mary Jensen, 45 Edgewater Court  
Chico. Ca. 95928  
February 27, 2022

Shortly before the pandemic began, Chico residents packed a large meeting hall to vehemently protest the insertion of a Marriott Towne Place hotel into the senior residence and services neighborhood, Sierra Sunrise Village (SSV). Many in the audience had been in Chico only a short time since fleeing their town of Paradise under fire. Naturally the topic of fire evacuation was heavy on their minds, as was the sense that what they thought was a safe senior area could become a victim of thoughtless, dangerous planning.

Such planning to date would erect a four story, 112-unit hotel at the major SSV evacuation road; would every day divert traffic onto the SSV private road; would expose their lake and paths to need for 24-hour security---all at the expense and liability of residents in SSV and Cal Park the Rehabilitation Hospital and the assisted living facilities.

There are many reasons why the original plan for our senior community prohibited commercial structures of this size and impact, yet that original plan has been ignored in this process. Like the voices at the community meeting, I ask that a full environmental impact report be required, and an honest, thorough review put the needs of yourcitizens ahead of the desires of a massive corporation. Apparently no one in City Hall was listening the last time our neighbors were eloquent and impassioned in their opposition. Let's start again, perhaps with a consideration of why, if this is such a valuable project, it is not slated for Merriam Park where its architectural design, audience and 24-hour services would be compatible.



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Proposed Hotel in Cal Park  
**Date:** Friday, February 25, 2022 1:11:05 PM

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-----Original Message-----

From: Nona Kirk <[nonakirk@comcast.net](mailto:nonakirk@comcast.net)>  
Sent: Friday, February 25, 2022 1:03 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Proposed Hotel in Cal Park

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This Hotel would impact the daily lives of all seniors here in California Park, Sierra Sunrise Village , and Village Green. It would create safety, traffic, environmental, and maintenance issues beyond belief! Please do everything possible to stop this project. Nona Kirk, resident and HOA Board Member, Village Green

Sent from my iPhone

Dear Chico City Planners,

California Park, Sierra Sunrise Village and Sierra Lakeside Board of Director's oppose the proposed 4-story 112 unit hotel. If built, it would create a nightmare for the Senior's who live on Sierra Sunrise Terrace. The increase in traffic would increase traffic congestion to an already congested corner and erode the safety of our Seniors. The proposed parking for the hotel is insufficient to cover hotel staff and guests nor is there parking for RV's, vehicles with trailers or freight trucks...overflow parking along Sierra Sunrise Terrace would further erode safety, hamper access for First Responders and emergency evacuations. Sierra Sunrise Terrace from Bruce to the bulb including the Bridge are private and the HOA's bare the brunt of maintenance costs. The Boards are also concerned about protecting our common areas, lake and security of those living in that area. Lastly, Sierra Sunrise Terrace is a lakeside community and adding a 4-story 112 unit hotel would negatively affect lake views, light pollution, wildlife and overall tranquility of our neighborhood. Signing this petition and writing letters to the City Planners sends a message but attending the City Planning meetings has the most impact! Call Shannon Costa at the City of Chico Planning Department 530-879-6800 and put your name on the notification list so you don't miss a meeting.  
See less

I strongly oppose the Hotel that is proposed to be built at Sierra Sunrise Village.

Josie Whitehurst - Home owner in California Park -  
2863 Pennycroft Dr.  
Chico, CA 95920

RECEIVED

FEB 25 2022

CITY OF CHICO  
PLANNING SERVICES

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: Hotel  
**Date:** Friday, February 25, 2022 12:56:53 PM

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-----Original Message-----

From: Helen White <[wtpbwhite@comcast.net](mailto:wtpbwhite@comcast.net)>  
Sent: Friday, February 25, 2022 12:55 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Hotel

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Please no hotel in California Park

Sent from my iPhone

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: California Park Hotel  
**Date:** Friday, February 25, 2022 10:46:00 AM

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**From:** Diane Robel <[disco60zena@gmail.com](mailto:disco60zena@gmail.com)>  
**Sent:** Friday, February 25, 2022 10:44 AM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
**Subject:** California Park Hotel

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You will get feedback regarding opposition to the proposed hotel project because of its impact on California Park with the height, light and traffic. I agree, but also am alarmed that the city keeps approving housing, apartments and projects such as this when there is obviously a water shortage issue. We have been trying for the past years to continually cut back our water usage, yet the city keeps adding projects that do and will consume huge quantities of water. Please don't add this hotel to the list of this unrestrained growth.

Diane Robel

Bruce Ambo, Chico City Principal Planner  
[bruce.ambo@chicoca.gov](mailto:bruce.ambo@chicoca.gov)  
Community Development Department, Planning Division  
411 Main Street, 2<sup>nd</sup> Floor, Chico, CA 95928

2/25/2022

**Re: Towne Place Suites Hotel Project (UP 19-25 & AR 19-22) Proposed for Sierra Sunrise Terrace/California Park**

Dear Mr. Ambo,

**Based on the safety concerns that would be created and incompatibility with the surrounding senior neighborhood, I strongly oppose the approval of a use permit for this project.** I am a long time resident of California Park and have been deeply involved with senior services in Chico for over 36 years. During this time, I assisted my father while he resided in The Terraces, Wind Chime, and then California Park Rehabilitation Hospital. I also assisted seniors during the Camp Fire emergency, and noted what could happen in a future wildfire. At a community meeting with the Chico Fire Department after the Camp Fire emergency, the Chief described how a fire could cross Highway 32 by Canyon Oaks or above and travel down towards the senior residences in California Park. He emphasized the need to be prepared.

**The proposed hotel would add significant traffic to Sierra Sunrise Terrace, the primarily private road which serves as the key access and evacuation route for multiple senior care facilities and residences.** When evacuation becomes necessary due to a future wildfire, vehicles exiting Cal Park will join those coming down Highway 32 as well as from the development along Bruce Road, which continues to grow. Residents from within Cal Park will take all possible routes out, including Sierra Sunrise Terrace, to get to Bruce Road. Yet Sierra Sunrise Terrace will have to be clear enough to bring in emergency vehicles and buses, safely load elderly patients and residents, and take them out to safety. Families assisting older relatives will add to the congestion, and a high percentage of evacuees will be non-ambulatory or have other functional or cognitive disabilities. **Placing a hotel at the base of Sierra Sunrise Terrace, closest to Bruce Road, and adding in hotel guests and staff will bottleneck and further delay the evacuation process, creating a dangerous situation.**

The older population of Chico is growing, and California Park is the only Chico neighborhood where planning for older adults has been in the forefront. With concerns about safety relative to increased traffic at all times, proximity to the wildfire corridor, and the amount of time and number of vehicles needed to fully evacuate vulnerable older adults in a disaster, as well as environmental concerns relative to the proximity to a local waterway, this project is quite simply inappropriate for this location. **I therefore urge that the Use Permit application for this project be denied.**

Sincerely,



Diane Puckett, 5 Burney Drive, Chico, CA 95928



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: California Park hotel project; comments for Planning Commission consideration  
**Date:** Friday, February 25, 2022 10:16:33 AM

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**From:** B. T. Chapman <[btc05usn@gmail.com](mailto:btc05usn@gmail.com)>  
**Sent:** Friday, February 25, 2022 10:10 AM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
**Subject:** California Park hotel project; comments for Planning Commission consideration

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To: City of Chico Planning Commission  
From: B T Chapman

I provide the following:

1. The public deserves to hear the developer's business case that makes the project work and describes why the proposed location was chosen Vs other options in the area, especially Miriam Park. What is the prime motivating factor to place a 4 story hotel in a retiree and residential district?
2. With 589 hotel rooms (including the Hampton Inn hotel in the Chico Market Place Mall), all within less than a mile of California Park, and other short-stay options, what is the case for a hotel in CalPark?
3. Recognize that to make the project fiscally doable at 117 rooms, a 4 story structure (totally out of character with surrounding buildings) is necessary due to the small lot size. The new Hampton Hotel nearby at Kohls is doable at 3 stories because the lot is larger and the building can be built out Vs built up.
4. How about the negative impact on nearby home values? There will be a definite impact.
5. Referring to the Notice of Exception document - bottom of page 20 and top of page 21. "the project is in an urbanized area east of Rt 99 and where much commercial and high density residential has been developed". I cannot identify "much" commercial development near CalPark and the majority of residential is 2 story.
6. What other sites for a hotel did the developer research and why was each one rejected over the CalPark site?
7. Parking is inadequate. 117 rooms and 121 parking spaces. In their initial presentation they identified employing 25-30 associates. In a full occupancy condition where will they park?

8. The VMT Analysis portion of the Environmental Analysis for the project states that " the site was selected to be a local-serving hotel for the large underserved area East of highway 99 and the limited supply of hotel rooms on the east side of Chico." 589 rooms now available within 1 mile of CalPark is far from a limited supply. How was "limited supply" defined?

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: NO HOTEL California Park  
**Date:** Thursday, February 24, 2022 10:15:18 AM

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**From:** Nancy Bailey <bailey2me@gmail.com>  
**Sent:** Thursday, February 24, 2022 9:56 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** NO HOTEL California Park

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Dear Mr. Ambo:

**Please do not approve the special use permit for a hotel on Sierra Sunrise Terrace by Highway 32 and Bruce.**

We were burned out in Paradise and it took nearly two years to return to Butte County and find housing at the Sierra Sunrise Apartments (for seniors).

We truly enjoy the peaceful, beautiful, safe neighborhood and walk the lakes, taking in nature--turtles, birds, and an occasional otter. We have even seen otters in the big lake by the Sierra Sunrise Terrace road.

We have stopped driving Sierra Sunrise Terrace because of the increased traffic, especially when needing to turn left on Bruce. The Native Oak neighborhoods on the other side of 32 are quickly growing and will increase the traffic greatly. I just read the city approved an Arco Minimart.

**A hotel will just change the landscape and environment with unfamiliar people and additional traffic. This is a residential area with senior and medical facilities. Please keep the zoning and building as it is. Do not ruin California Park by allowing this hotel to be built.**

Thank you for your time and consideration.

To: Bruce Ambo, Chico City Principal Planner  
From: Dr. Dan Toy and Dr. Lauren Wright, 12 Catalina Point Road, 530.312.0938  
Re: California Park Hotel  
Date: 2/24/2022

Dear Mr. Ambo:

We have been members of the California Park community since we built our home at 12 Catalina Point Road in 1990. Our decision to make California Park our home was based on the tranquility of the setting, the lakes, the paths, and the residential nature of the community. We still value these characteristics and can't think of another location we would rather be in Chico.

We came to California in 1989 from Penn State University to become faculty members in the College of Business at CSU Chico. Our decision to make Chico and Cal Park our home was a good one from both a professional and lifestyle perspective.

Several years ago, there was a meeting in Cal Park to allow members of the community to discuss the new hotel plans with representatives from the hotel chain and city planners. The meeting turned out to be standing room only. Many concerned Cal Parkers showed up to express their concerns about the new hotel. These concerns have been clearly articulated in the latest consideration of the hotel: zoning, traffic, fire safety, size of hotel vs. surrounding structures, environmental impact, trespassing, etc. The emotions ran high during the meeting, and it was clear that our residential committee had ample reasons for rejecting the idea of the new hotel.

As we reflected on the meeting our thinking went like this: How could the hotel chain and the Chico planning department even consider the hotel when the Cal Park community was so against it? Given the other alternatives for hotel location, would it even be ethical for the company or the development department to select one that had so many good reasons to be rejected? Shouldn't citizen stakeholders' concerns come before corporate interests?

We were sure that the issue of the Cal Park Hotel was over and that the company and the Development Department would find a more appropriate location for the hotel. Clearly, we were wrong. So, given the ample space for a hotel in Chico (think Meriam Park for example) why would a location that is so against the wishes of the community be considered yet again. Most likely this is a money issue. Money for the hotel, money for the landowner, and for some reason a "win" for the Chico Planning Division.

The cost/benefit calculation for this project is simple. The cost is the disenfranchisement of the Cal Park community, and the benefit goes to the company, the landowner, and maybe the Development Department. Few people benefit, many people lose. We are not sure why this is such a difficulty decision. Please do not allow the hotel to be built in Cal Park!

Sincerely,

Dan Toy and Lauren Wright

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#); [Kelly Murphy](#)  
**Subject:** FW: cal park hotel  
**Date:** Thursday, February 24, 2022 8:34:53 AM

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**From:** linda wilk <[barrywilk@comcast.net](mailto:barrywilk@comcast.net)>  
**Sent:** Wednesday, February 23, 2022 8:27 PM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
**Subject:** cal park hotel

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This development MUST NOT take place. As a resident of Cal Park, I am extremely concerned with the current growth of apartments and housing on HWY 32. I am sure you are aware of all of the arguments against this hotel plan. I URGE you to make your decisions based on the needs and desires of the citizens of Chico and NOT on the BIG MONEY, and power brokers. I have lived in this area for the past 50 years. It is sad and distressing to see what has happened to our beautiful little town.....PLEASE STOP !!!!!

Sent from [Mail](#) for Windows



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: No Hotel California Park  
**Date:** Wednesday, February 23, 2022 1:43:35 PM

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**From:** Sue Baber <[schbchico@gmail.com](mailto:schbchico@gmail.com)>  
**Sent:** Wednesday, February 23, 2022 12:21 PM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
**Subject:** No Hotel California Park

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Bruce:

I am a 50 year resident of Chico and live in California Park near the Sierra Sunrise complex on Shearwater Court.

There are huge concerns about having a 4 story hotel in our area.

Most importantly, the design, size and location of the proposed hotel are not at all compatible with our surrounding community. Besides being an eyesore with the bright nightly lights and size, it is certainly not a fit for our community.

Other concerns include traffic issues and that the property is not zoned for this type of commercial building.

As our City Planner Principal Planner, please hear our concerns and thoughts by strongly rejecting this proposal to have a hotel built in California Park.

Susan Baber  
11 Shearwater Court  
Chico, CA 95928

**From:** [Bruce Ambo](#)  
**To:** [Becki](#)  
**Cc:** [Nicole Acain](#)  
**Subject:** RE: Proposed hotel on Sierra Sunrise issue  
**Date:** Wednesday, February 23, 2022 8:59:56 AM

---

Hello,

Thank you for sharing your concerns with us. This project has been deemed to be eligible for a Categorical Exemption (i.e., Notice of Exemption) pursuant to Section 15332 (Infill Development Projects) of the California Environmental Quality Act (CEQA). A "courtesy review" of the Categorical Exemption is being provided due to the public interest in the project. CEQA does not require any responses to comments or questions other than those of the Planning Commission when the project is being reviewed. No decision on the project or the environmental documentation will be made until the project is considered by the Planning Commission for review in a public hearing (yet to be scheduled). All comments and questions received (over 100+ thus far) on the project will be included in the record of communication that goes to the Planning Commission. Staff anticipates addressing the majority of the questions on the project in the Staff Report that is provided to the Planning Commission at that time.

Bruce Ambo, AICP  
Principal Planner  
City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927

<http://www.ci.chico.ca.us/>

-----Original Message-----

From: Becki <[rebeck99@comcast.net](mailto:rebeck99@comcast.net)>  
Sent: Tuesday, February 22, 2022 9:46 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Cc: [rebeck99@comcast.net](mailto:rebeck99@comcast.net)  
Subject: Proposed hotel on Sierra Sunrise issue

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I am asking the city to deny the proposed hotel construction on Sierra Sunrise Terrace. I live in the Village Green subdivision. I moved there after losing my home in the Campfire. I live in a 55 year old limit community. I do not want the increased traffic on that street. It is already difficult to navigate safely on Sierra Sunrise Terrace where there is mostly elderly drivers. Turning south onto Bruce Road is tricky. I have seen accidents occur there. Increasing traffic on this road is not responsible. This along with the fact this hotel would not fit into our existing architecture, there has been no full environmental impact review, and the cost our HOA will incur in the need to add protocols to our area, are enough for you to stop the hotel from being built at the proposed location.

Please do not build a hotel on Sierra Sunrise Terrace.

Thank you,  
Rebecca Nelson

1361 Botticelli Bend  
Village Green Subdivion.

Sent from my iPad

**From:** [Bruce Ambo](#)  
**To:** [Phillip Hemenway](#)  
**Cc:** [Nicole Acain](#)  
**Subject:** RE: Proposed California Park Hotel  
**Date:** Wednesday, February 23, 2022 9:29:07 AM  
**Attachments:** [image001.png](#)

---

Hello,

*Thank you for sharing your concerns with us. This project has been deemed to be eligible for a Categorical Exemption (i.e., Notice of Exemption) pursuant to Section 15332 (Infill Development Projects) of the California Environmental Quality Act (CEQA). A "courtesy review" of the Categorical Exemption is being provided due to the public interest in the project. CEQA does not require any responses to comments or questions other than those of the Planning Commission when the project is being reviewed. No decision on the project or the environmental documentation will be made until the project is considered by the Planning Commission for review in a public hearing (yet to be scheduled). All comments and questions received (over 100+ thus far) on the project will be included in the record of communication that goes to the Planning Commission. Staff anticipates addressing the majority of the questions on the project in the Staff Report that is provided to the Planning Commission at that time.*

**Bruce Ambo, AICP**

Principal Planner  
City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927



<http://www.ci.chico.ca.us/>

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**From:** Phillip Hemenway <hemenha@gmail.com>  
**Sent:** Wednesday, February 23, 2022 9:18 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Proposed California Park Hotel

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You will have read the six objections to the construction of a hotel at the proposed site below the lake at Cal Park, so you know that they are all thoughtful, rational points. Each one will have a profound impact on the lives of the people who live in the area, but all of them together will create major negative effects on the quality of life for the residents. One can only hope that your office will use due diligence and come to see that a hotel at that site is just not a good fit for good reason.

Thank you for your consideration.

Sincerely,

Phillip Hemenway  
285 Pinyon Hills Dr  
Chico, CA 95928



**From:** [Bruce Ambo](#)  
**To:** [Joan Goodreau](#)  
**Cc:** [Nicole Acain](#)  
**Subject:** RE: Opposition to Proposed Hotel Project  
**Date:** Wednesday, February 23, 2022 9:30:34 AM  
**Attachments:** [image001.png](#)

---

Hello,

*Thank you for sharing your concerns with us. This project has been deemed to be eligible for a Categorical Exemption (i.e., Notice of Exemption) pursuant to Section 15332 (Infill Development Projects) of the California Environmental Quality Act (CEQA). A "courtesy review" of the Categorical Exemption is being provided due to the public interest in the project. CEQA does not require any responses to comments or questions other than those of the Planning Commission when the project is being reviewed. No decision on the project or the environmental documentation will be made until the project is considered by the Planning Commission for review in a public hearing (yet to be scheduled). All comments and questions received (over 100+ thus far) on the project will be included in the record of communication that goes to the Planning Commission. Staff anticipates addressing the majority of the questions on the project in the Staff Report that is provided to the Planning Commission at that time.*

**Bruce Ambo, AICP**

Principal Planner  
City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927



<http://www.ci.chico.ca.us/>

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**From:** Joan Goodreau <jmgoodreau@yahoo.com>  
**Sent:** Wednesday, February 23, 2022 9:28 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Opposition to Proposed Hotel Project

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Bruce Ambo, Chico City Principal Planner

I fear for my neighborhood. Its serenity and community spirit are under attack. I oppose the proposed Hotel Project for the following reasons.

The many seniors who live in this area are at severe risk and in danger during evacuations in emergencies.

Our already over-crowded roads cannot support the increased traffic of a hotel.

The size of the hotel and its large occupancy will be intrusive in our neighborhood and an eyesore.

It will ruin the surrounding natural environment that is intended to preserve the plants and wildlife around here.

The homeowners will have to spend more on clean-up and patrol services to try to protect us from the impacts of hotel patrons, visitors, and staff trespassing onto our private lakes and walkways.

Our neighborhood should not be destroyed by an invasion of monied interests. We must preserve the spirit and legacy of Cal Park.

Thank you for your attention to this crucial issue,  
Joan Goodreau, Retired Teacher and Program Specialist  
12 Lakeshore Terrace, Chico 95928

**From:** [Bruce Ambo](#)  
**To:** [Jeffrey Fox](#)  
**Cc:** [Nicole Acain](#)  
**Subject:** RE: ProposedHotel at California Park  
**Date:** Wednesday, February 23, 2022 10:21:46 AM

---

Hello,

Thank you for sharing your concerns with us. This project has been deemed to be eligible for a Categorical Exemption (i.e., Notice of Exemption) pursuant to Section 15332 (Infill Development Projects) of the California Environmental Quality Act (CEQA). A "courtesy review" of the Categorical Exemption is being provided due to the public interest in the project. CEQA does not require any responses to comments or questions other than those of the Planning Commission when the project is being reviewed. No decision on the project or the environmental documentation will be made until the project is considered by the Planning Commission for review in a public hearing (yet to be scheduled). All comments and questions received (over 100+ thus far) on the project will be included in the record of communication that goes to the Planning Commission. Staff anticipates addressing the majority of the questions on the project in the Staff Report that is provided to the Planning Commission at that time.

Bruce Ambo, AICP  
Principal Planner  
City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927

<http://www.ci.chico.ca.us/>

-----Original Message-----

From: Jeffrey Fox <fox1524free@gmail.com>  
Sent: Wednesday, February 23, 2022 10:16 AM  
To: Bruce Ambo <bruce.ambo@Chicoca.gov>  
Subject: ProposedHotel at California Park

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I am writing due to my being opposed to the building of the hotel in California Park vicinity.

1. The are is not zoned for this type of building
2. There are already traffic concerns that will only get worse if this Hotel goes through
3. We are the largest senior community in the area and this will add significant increase in safety concerns for vulnerable seniors. EG: increase traffic in an emergency evacuation situation, increase risk for personal safety with an increase in a transit community.
4. It simply won't look good and a highly potential eyesore on the area.
5. There has not been a full environment impact study
6. It will cost those of us in the immediate neighborhood, significant increase in cost for added patrol services. Many many of our residents are on fixed incomes.

Jeffrey Fox

jeffreyfox5519@yahoo.com

**From:** [Bruce Ambo](#)  
**To:** [Hesh Kaplan](#)  
**Cc:** [Nicole Acain](#)  
**Subject:** RE: Yes on Hotel  
**Date:** Wednesday, February 23, 2022 8:50:47 AM

---

Hello,

Thank you for taking the time to share your thoughts with us in support of the project. This project has been deemed to be eligible for a Categorical Exemption (i.e., Notice of Exemption) pursuant to Section 15332 (Infill Development Projects) of the California Environmental Quality Act (CEQA). A “courtesy review” of the Categorical Exemption is being provided due to the public interest in the project. CEQA does not require any responses to comments or questions other than those of the Planning Commission when the project is being reviewed. No decision on the project or the environmental documentation will be made until the project is considered by the Planning Commission for review in a public hearing (yet to be scheduled). All comments and questions received (over 100+ thus far) on the project will be included in the record of communication that goes to the Planning Commission. Staff anticipates addressing the majority of the questions on the project in the Staff Report that is provided to the Planning Commission at that time.

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City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927

<http://www.ci.chico.ca.us/>

-----Original Message-----

From: Hesh Kaplan <[heshelak@gmail.com](mailto:heshelak@gmail.com)>  
Sent: Wednesday, February 23, 2022 8:08 AM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Yes on Hotel

.  
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I have been bombarded with ‘No on Hotel’ signs and literature. The opposition seems to be well organized and well funded. Who are these people?

I, for one, would love to have a hotel on the corner allowing my visiting children and grandchildren a convenient place to stay when visiting.

I am not alone; most of the elderly folks in our apartments that I talk to also support the addition of a hotel in our neighborhood. But, alas, we are all old, not organized and can’t afford the slick advertising the opponents do.

Hesh



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Cal Park Hotel  
**Date:** Wednesday, February 23, 2022 11:22:22 AM

---

**From:** Bernadette Black <[broseblack@gmail.com](mailto:broseblack@gmail.com)>  
**Sent:** Wednesday, February 23, 2022 11:13 AM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
**Subject:** Cal Park Hotel

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Hello,  
We would like to voice our opposition to the proposed hotel near California Park.  
Thank you,  
The Blacks  
Cal Park residents

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: California Park Hotel  
**Date:** Wednesday, February 23, 2022 9:17:10 AM

---

**From:** EDWARD STEIGER <edjoan1@att.net>  
**Sent:** Tuesday, February 22, 2022 3:07 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** California Park Hotel

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First of all the location is not zoned for commercial development and that should be enough. This development does not benefit California Park residents, it only provides more noise and congestion. Hotels be long in close proximity to Hwy.99 where they least impact residential neighborhoods. There is already a commercial project being built at Bruce Rd. & E. 20<sup>th</sup> that will create additional congestion. I think enough is enough.  
Ed Steiger

Sent from [Mail](#) for Windows

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: California Park Hotel  
**Date:** Wednesday, February 23, 2022 9:17:10 AM

---

**From:** EDWARD STEIGER <edjoan1@att.net>  
**Sent:** Tuesday, February 22, 2022 3:07 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** California Park Hotel

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Ed Steiger

Sent from [Mail](#) for Windows

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: No Hotel  
**Date:** Tuesday, February 22, 2022 12:39:05 PM

---

-----Original Message-----

From: Rose Fostine <[rose.fostine@gmail.com](mailto:rose.fostine@gmail.com)>  
Sent: Tuesday, February 22, 2022 11:57 AM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: No Hotel

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I'm writing to express my opposition to the hotel in CAL Park. This is a residential community, not commercial. Both the use and size of this hotel will negatively impact our community. Traffic is obviously a serious concern. This would be an eyesore with lighting, multi-story structure in a beautiful desirable area of Chico. Those of us who paid a premium to live in Cal Park and pay dues to our HOA will undoubtedly feel an impact with increased cost of security, damages to our private lakes and walking paths and our wildlife that live here. Please do NOT approve this rezoning which would allow this unnecessary and potentially dangerous monstrosity. There are better locations in Chico for a hotel in a more commercialized area. There is already a new hotel currently under construction by Kohls, why do we need another?  
Thank you for your consideration.

Rose Fostine

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Hotel in California Park  
**Date:** Tuesday, February 22, 2022 9:59:23 AM

---

-----Original Message-----

From: Patricia Moyer <[pmoyer@rocketmail.com](mailto:pmoyer@rocketmail.com)>  
Sent: Tuesday, February 22, 2022 9:34 AM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Hotel in California Park

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I am a resident of Sierra Lakeside and would like to voice my opposition to the proposed hotel in our area. We are a community of senior citizens and having a large hotel in our neighborhood would adversely affect our quality of life. Traffic would increase as well as guests from the hotel using our private lake and would negatively affect the sense of safety that we now have. I hope you will listen to the community that will be most adversely affected and not approve this project. Thank you for your consideration.

Sent from my iPhone



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Sierra Sunrise Village  
**Date:** Tuesday, February 22, 2022 10:02:47 AM

---

**From:** James Smith <fresnojimmy@yahoo.com>  
**Sent:** Tuesday, February 22, 2022 8:02 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Sierra Sunrise Village

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Mr. Ambo,  
I am writing to you as a resident of Sierra Sunrise Village, requesting information regarding the construction of a new hotel at the site at Hwy 32 and Bruce Rd, Chico, CA 95928.

Specifically, I am inquiring about a statement submitted as part of NorthStar Engineering report on traffic and usage that reads as follows:

*"It is anticipated that little, if any, project traffic will travel east on Sierra Sunrise Terrace into the residential neighborhoods. There is little desire for hotel visitors to wander through residential backstreets considering the close proximity to the Bruce Road / Sierra Sunrise Terrace intersection."*

It is my belief that the subcontracted surveyor for NorthStar, Lauren Chilson, has no qualification to make that determination, submitted no accompanying data for the statement, and draws more questions about the study than answers.

Can you please inform me if further studies are being done to identify the social impact of the new hotel on surrounding senior communities? How was it determined that it meets the needs of our senior community? Is the planning commission relying on Mr. Chilson's and NorthStar's statements for construction of this hotel?

Thank you,

James Smith  
5 Valley Lake Commons  
Chico, CA 95928

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acaín](#)  
**Subject:** FW: Towne Place/Cal Park Hotel Opposition letters  
**Date:** Tuesday, February 22, 2022 9:59:11 AM  
**Attachments:** [Marriott letter I.doc](#)  
[Marriott letter II.doc](#)

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**From:** Sandy Makau <ssmakau@yahoo.com>  
**Sent:** Tuesday, February 22, 2022 9:27 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Towne Place/Cal Park Hotel Opposition letters

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February 22, 2022  
Bruce Ambo, Chico City Principal Planner  
Attachments below: Marriott letter I (2020)  
Marriott letter II (2022)

Dear Mr. Ambo,  
My Name is Sandra S. Makau, I live at 2603 Lakewest Dr. in California Park. Below are two attachments, one is a letter I wrote on June 2020 to the previous planner, Dexter O'Connell, in opposition to the proposed Towne Place hotel, and the other is a copy of this letter for your records.

Much has changed in the last two years in the area surrounding the proposed hotel project. There are at least two new hotels built nearby as well as numerous apartment complexes with over 100 units. These apartments add more cars that impact traffic, and the new hotels, Holiday Inn Express and Hampton Inn, are within 2 miles and have amenities to serve the needs for hotel beds in our area, without to putting another hotel in our neighborhood.

I hope the Planning Commission considers these additional hotel beds, and the negative safety, and traffic issues that the Towne Suites would bring to our neighborhood as you debate if this project is a good fit for this location..

Regardless of the zoning this hotel is an inappropriate location for this residential neighborhood location.

Our home is on the California Park lake, right across the lake from the proposed hotel. **The hotel is a 24-hour business that will have four stories of 24-hour lighting, coming from the parking lot, and attached to all sides of the building. This lighting will shine into my home and be**

**disruptive to my family, and our neighbors. Lights will be shining into our homes at all hours, dawn, dusk and all throughout the night.**

Having this hotel in our neighborhood is a safety issue for residents, many of them are senior citizens. There will be 24 hour business traffic, strangers driving in and out and walking in the neighborhood and around the lake, at all hours of the day. No amount of security can prevent trespassing, and insure the safety of these residents from the hotels constant flux of occupants. The fact that it will be a long-term residence hotel is not predicable, and hotel guests do not commit to the neighborhood in a way that makes this hotel a fit for this neighborhood.

There are currently businesses around the lake, but they are only two story, and do not operate on a 24 hour basis.

There are no amenities like restaurants or shopping for hotel guests to take advantage of near the proposed hotel site. This makes for even more traffic in and out of the parking lot and onto Sierra Sunrise Rd and Bruce Rd., which makes a dangerous traffic flow, and prevents residents from walking safely near their homes in their neighborhood.

There are currently two hotels, Holiday Inn Express, 20th St. and Hampton Inn, Springfield Dr., in our area within 2 miles, that are near amenities, that can better serve the needs of Chico's visitor population without putting this hotel in an isolated residential neighborhood. Putting a stop light at Sierra Sunrise Rd onto Bruce Rd is not a solution. Currently there are cars backed up past Sierra Sunrise Rd, on Bruce Rd., waiting to cross or turn right onto Hwy 32. A second stoplight would be an impractical solution. That is not the answer to the traffic congestion in that area.

**The practical solution is to not add to the traffic and fire evacuation problems that this proposed hotel will add to our neighborhood.**

Hwy 32 is a vulnerable fire corridor from Forest Ranch, Bidwell Park and across the canyon from Skyway. During a fire evacuation there would be a bottle neck from the traffic flowing on to Hwy 32. We learned a lot about this areas fire vulnerability during the Campfire.

Sierra Sunrise Terrace, Senior Citizen Community, located on Sierra Sunrise Rd, next to the proposed hotel has special fire evacuation and ambulance needs because they are a vulnerable community. Putting in a hotel is in conflict with the safety and fire evacuation requirements of this senior community.

It is shocking that the City of Chico is considering putting a >100-unit hotel on this plot of land that is so close to the creek. The run off into the natural waterways from a high impact building will have an unavoidable impact on the environment. I thought structures were not allowed to be built close to a creek!

Whether we are homeowners, renters or apartment dwellers we love our neighborhood, and want our safety, traffic and esthetics to reflect our sense of community in an appropriate way. We do not want a four story, 24- hour hotel business that impacts negatively on so many parts of our lives: fire safety, security and traffic, environment.

The City of Chico has successfully approved and developed areas in town that are convenient and integrated with other business; these areas are where we can shop, go to restaurants and have hotel visitors stay.

I support these businesses and want them to thrive, but in the proper locations, not in a neighborhood, or near a senior citizen communities, or on a private poorly accessible road.

Please consider the Towne Place Suites as an inappropriate business to put in the California Park, Sierra Sunrise neighborhood.

Thank you for your time,  
Sandra Sortwell Makau

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Marriott at California Park  
**Date:** Tuesday, February 22, 2022 9:18:34 AM

---

**From:** Lyn Jacobsen <mandl1974@sbcglobal.net>  
**Sent:** Monday, February 21, 2022 2:57 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Marriott at California Park

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Hi Mr. Ambo. My name is Mike Jacobsen, and I reside at 1358 Padova Place, in California Park. I would like to let you know that I am not in favor of Marriott coming into California Park. My reasoning is:

- 1) The main entrance for seniors of the Sierra Sunrise entrance. The number of cars entering that road will cause a lot of accidents with the seniors fighting hotel traffic. I believe the extra traffic would necessitate another traffic light on Bruce Rd, which would give us 3 lights in a 1/4 mile stretch.
- 2) If one has been done, I would like to see the environmental impact report. Has one been done, and is it available to the general public?
- 3) One of the biggest concerns for me personally is safety. Right now, I feel very comfortable allowing my wife to walk around the lake unaccompanied. With a 4 story hotel in front of the dam, I would not feel comfortable letting her walk around that side of the lake, and I'm not sure if I would feel comfortable with her walking around the other side of the lake, either. I've spent many nights of my life in hotels, and even the nicest hotels do not always have the greatest clientele.
- 4) Lastly, from my point of view, I would not feel comfortable allowing people who have no monetary connection with California Park being allowed to have access to the lake across the street. They will not feel the need to carry their trash to the nearest receptacle, which might be 3-4 hundred yards away, and that will eventually cost us homeowners to hire someone just to patrol the lake and pick up trash. I also believe much of that same trash will end up in the lake and cause irreparable damage to the fish/animals in the lake.

Thank you for allowing me to express my feelings. Again, please do not approve the Marriott Towne Place Suites coming into California Park.

Sincerely,

Mike Jacobsen



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Hotel in California Park  
**Date:** Tuesday, February 22, 2022 9:59:23 AM

---

-----Original Message-----

From: Patricia Moyer <[pmoyer@rocketmail.com](mailto:pmoyer@rocketmail.com)>  
Sent: Tuesday, February 22, 2022 9:34 AM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
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I am a resident of Sierra Lakeside and would like to voice my opposition to the proposed hotel in our area. We are a community of senior citizens and having a large hotel in our neighborhood would adversely affect our quality of life. Traffic would increase as well as guests from the hotel using our private lake and would negatively affect the sense of safety that we now have. I hope you will listen to the community that will be most adversely affected and not approve this project. Thank you for your consideration.

Sent from my iPhone

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Sierra Sunrise Village  
**Date:** Tuesday, February 22, 2022 10:02:47 AM

---

**From:** James Smith <fresnojimmy@yahoo.com>  
**Sent:** Tuesday, February 22, 2022 8:02 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Sierra Sunrise Village

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Specifically, I am inquiring about a statement submitted as part of NorthStar Engineering report on traffic and usage that reads as follows:

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Thank you,

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5 Valley Lake Commons  
Chico, CA 95928

February 22, 2022

Bruce Ambo, Chico City Principal Planner

Attachments below: Marriott letter I (2020)

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I hope the Planning Commission considers these additional hotel beds and the safety, and traffic issues that the Towne Suites would bring to our neighborhood as you debate if this project is a good fit for this location

Regardless of the zoning this hotel is inappropriate for this residential neighborhood location.

Our home is on the California Park lake, right across the lake from the proposed hotel. **The hotel is a 24-hour business that will have four stories of 24-hour lighting, coming from the parking lot, and attached to all sides of the building. This lighting will be disruptive to my family, and our neighbors. Lights will be shining into our homes at all hours, dawn, dusk and all throughout the night.**

Having this hotel in our neighborhood is a safety issue for residents, many of them are senior citizens. There will be 24-hour business traffic, strangers driving in and out and walking in the neighborhood and around the lake, at all hours of the day. No amount of security can prevent trespassing, and insure the safety of these residents from the hotels constant flux of occupants. The fact that it will be a long-term residence hotel is not predicable, and hotel guests do not commit to the neighborhood in a way that makes this hotel a fit for this neighborhood.

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I support these businesses and want them to thrive, but in the proper locations, not in a neighborhood, or near a senior citizen community, or on a private poorly accessible road. Please consider the Towne Place Suites as an inappropriate business to put in the California Park, Sierra Sunrise neighborhood.

Thank you for your time,  
Sandra Sortwell Makau

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Marriott at California Park  
**Date:** Tuesday, February 22, 2022 9:18:34 AM

---

**From:** Lyn Jacobsen <mandl1974@sbcglobal.net>  
**Sent:** Monday, February 21, 2022 2:57 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Marriott at California Park

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Hi Mr. Ambo. My name is Mike Jacobsen, and I reside at 1358 Padova Place, in California Park. I would like to let you know that I am not in favor of Marriott coming into California Park. My reasoning is:

- 1) The main entrance for seniors of the Sierra Sunrise entrance. The number of cars entering that road will cause a lot of accidents with the seniors fighting hotel traffic. I believe the extra traffic would necessitate another traffic light on Bruce Rd, which would give us 3 lights in a 1/4 mile stretch.
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- 3) One of the biggest concerns for me personally is safety. Right now, I feel very comfortable allowing my wife to walk around the lake unaccompanied. With a 4 story hotel in front of the dam, I would not feel comfortable letting her walk around that side of the lake, and I'm not sure if I would feel comfortable with her walking around the other side of the lake, either. I've spent many nights of my life in hotels, and even the nicest hotels do not always have the greatest clientele.
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Thank you for allowing me to express my feelings. Again, please do not approve the Marriott Towne Place Suites coming into California Park.

Sincerely,

Mike Jacobsen



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: No Hotel California Park  
**Date:** Tuesday, February 22, 2022 9:18:08 AM

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**From:** Kathryn Fritz <kandbfritz@sbcglobal.net>  
**Sent:** Monday, February 21, 2022 1:59 PM  
**To:** Bruce Ambo <bruce.ambo@Chico.ca.gov>  
**Subject:** No Hotel California Park

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Please listen to the neighbors voices in stating that we do not want a hotel in California Park. This piece of land was not meant for a hotel and should reflect the area and the local residents around this area.

With all the building around our area, the traffic around our senior citizens, our school in the area it would exacerbate problems in our area. We have built two new hotels, 1 by Winco and the other by Kohl's. Do we really need one more hotel in this area. Why not redo one of the hotels in our downtown?

Please keep our neighborhood a neighborhood.

Katy Fritz

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Marriott's Towne Place Suites hotel project  
**Date:** Tuesday, February 22, 2022 9:19:47 AM

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**From:** dan ehman <ehman3d@att.net>  
**Sent:** Monday, February 21, 2022 7:03 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Marriott's Towne Place Suites hotel project

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TO: Bruce Ambo, Chico City Principal Planner

I am extremely opposed to the proposed hotel project - Marriott's Towne Place Suites in California Park for all the reasons below.

There are plenty of viable and better suited locations for a project of this nature.

1. The property is not zoned for this type of commercial building and will require a special 'use permit' from the City of Chico. The land was intended to be developed to support the needs of the local residents, and clearly not for this type of commercial development.
2. Traffic concerns are already an issue in this area, but a hotel would exacerbate these problems on a daily/nightly basis and increase the transient population in Cal Park exponentially.
3. Cal Park houses the largest Senior community in Chico. Seniors and all residents in our community, including Canyon Oaks, are already concerned about their safety and ability to evacuate, if necessary, in a timely and safe manner. Remember the Camp Fire!
4. The location and enormous size of the proposed 4-story hotel are not compatible with the surrounding community, making it an eyesore with its nightly business lights and generally obtrusive nature.
5. The project has not gone through a full environmental impact review. There would certainly be impacts to the surrounding natural environment and the adjacent riparian area,
6. The homeowners of Californian Park Association, which includes Sierra Sunrise Village, Sierra Lakeside, and Village Green will be required to spend substantially increased dollars to provide significantly more hours of patrol services to protect against the inevitable detrimental impacts from hotel patrons, visitors, and staff trespassing onto our private lakes and walkways.

Thank you

Dan Ehman  
4 Kestrel Ct.  
Chico, Ca. 95928  
530-513-8812

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Cal Park Hotel  
**Date:** Monday, February 21, 2022 12:58:38 PM

---

-----Original Message-----

From: Stephanie Hardie <[smhardiechico@gmail.com](mailto:smhardiechico@gmail.com)>  
Sent: Saturday, February 19, 2022 4:52 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Cal Park Hotel

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I am definitely opposed to a 4 story hotel on Bruce Road. It will bring a lot more traffic to the area, in addition to the new houses and apartments being planned for in the future in the area. Also, it will complicate being able to evacuate the area in an emergency.

Stephaney Hardie  
2674 Fairfield Commons

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Wrong place for a 4-story Marriott hotel - NO Hotel  
**Date:** Monday, February 21, 2022 12:56:14 PM

---

**From:** [buttecreek@comcast.net](mailto:buttecreek@comcast.net) <[buttecreek@comcast.net](mailto:buttecreek@comcast.net)>  
**Sent:** Monday, February 21, 2022 12:44 PM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
**Subject:** FW: Wrong place for a 4-story Marriott hotel - NO Hotel

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Dear Mr. Ambo,

My wife and I have been living in Chico since 1984 and have lived in California Park since 1996. It makes no sense putting this type of commercial building/hotel in a residential area and near a 55+ year old living community.

There are better areas for this type of development.

Please stand up to the pressures and do what is right. Do not give a "special use permit" for this hotel.

These are valid and honest concerns.

1. The property is **not zoned** for this type of commercial building and will require a special 'use permit' from the City of Chico. The land was intended to be developed to support the needs of the local residents, and clearly not for this type of commercial development.
2. **Traffic concerns** are already an issue in this area, but a hotel would exacerbate these problems on a daily/nightly basis and increase the transient population in Cal Park exponentially.
3. Cal Park houses the largest Senior community in Chico. Seniors and all residents in our community, including Canyon Oaks, are already concerned about their safety and ability to evacuate, if necessary, in a timely and safe manner. **Remember the Camp Fire!**
4. The location and enormous size of the proposed 4-story hotel are **not compatible** with the surrounding community, making it an eyesore with its nightly business lights and generally obtrusive nature.
5. The project has **not** gone through a full environmental impact review. There would certainly be impacts to the surrounding natural environment and the adjacent riparian area,
6. The homeowners of Californian Park Association, which includes Sierra Sunrise Village, Sierra Lakeside, and Village Green will be required to spend substantially increased dollars to provide significantly more hours of patrol services to protect against the inevitable detrimental impacts from hotel patrons, visitors, and staff **trespassing** onto our private lakes and walkways.

Sincerely,

Dave & Cari Schmucker



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Hotel proposal  
**Date:** Monday, February 21, 2022 12:58:07 PM

---

**From:** Kathy Gochenour <kathygoch@gmail.com>  
**Sent:** Monday, February 21, 2022 11:53 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Hotel proposal

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Bruce,

I am writing to you in regards to the proposed hotel that will be situated on the corner of Bruce Rd. And Hwy 32.

The neighbors and I are very concerned with having any hotel situated in this area. It was never planned to be a commercial property and I know you have been given the many reasons by many of the neighbors in this area, so I won't repeat them to you. I just want you to take a hard look at this proposal and hope that you can see our view over the money that Chico could make with having this hotel in this spot. There are so many other areas of Chico where this hotel would be better for the community and for the guests that stay in the hotel. The residents in California Park and especially the large senior community are extremely concerned.

We would love for you to address all the concerns we have and what you plan to do to alleviate our concerns if this hotel ends up getting the special use permit to build on this lot.

Thank you for your time, Kathy and Bill Gochenour

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Hotel  
**Date:** Monday, February 21, 2022 12:58:52 PM

---

**From:** Ron Serrano <rxrn5902@gmail.com>  
**Sent:** Saturday, February 19, 2022 12:58 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Hotel

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To: Bruce Ambo  
From: Carol Serrano  
1374 Botticelli Bend, Chico 95928

Re: Proposed hotel development at The 32 and  
Bruce Rd

I have the following concerns:

1. Safety...any increase in traffic in that area looms large even without an evacuation in the event of a fire emergency. As a Camp Fire survivor I know perils of decreased visibility due to smoke and frightened drivers. Also the lake area can be attractive to poorly supervised children which a hotel will foster
2. Environmental impact...there are Herons and other birds in the wetland near that corner, as well as turtles and othe life. Disturbing the drainage of the are (especially during construction) will damage the area. I have witnessed what collateral damage occurs with excavation to the surrounding area while eager developers make their profit.
3. Real Estate speculation is no different from any other investment. There is no entitlement to profit , especially on the backs of current residents in their primary or only home.

4. Aesthetics...light pollution in an increasingly disturbing problem. A multi-story hotel will alter the serenity and view for many residents who likely can not relocate or duplicate what they currently depend on for there quality of life.

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Hotel  
**Date:** Monday, February 21, 2022 12:58:52 PM

---

**From:** Ron Serrano <rxrn5902@gmail.com>  
**Sent:** Saturday, February 19, 2022 12:58 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Hotel

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To: Bruce Ambo  
From: Carol Serrano  
1374 Botticelli Bend, Chico 95928

Re: Proposed hotel development at The 32 and  
Bruce Rd

I have the following concerns:

1. Safety...any increase in traffic in that area looms large even without an evacuation in the event of a fire emergency. As a Camp Fire survivor I know perils of decreased visibility due to smoke and frightened drivers. Also the lake area can be attractive to poorly supervised children which a hotel will foster
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3. Real Estate speculation is no different from any other investment. There is no entitlement to profit , especially on the backs of current residents in their primary or only home.

4. Aesthetics...light pollution in an increasingly disturbing problem. A multi-story hotel will alter the serenity and view for many residents who likely can not relocate or duplicate what they currently depend on for there quality of life.



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Hotel proposal  
**Date:** Monday, February 21, 2022 12:58:07 PM

---

**From:** Kathy Gochenour <kathygoch@gmail.com>  
**Sent:** Monday, February 21, 2022 11:53 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Hotel proposal

**ATTENTION:** This message originated from outside **City of Chico**. Please exercise judgment before opening attachments, clicking on links, or replying. Please report any suspicious emails with the Phishing Alert Button in Outlook or forward the email to [phishing@chicoca.gov](mailto:phishing@chicoca.gov)

Bruce,

I am writing to you in regards to the proposed hotel that will be situated on the corner of Bruce Rd. And Hwy 32.

The neighbors and I are very concerned with having any hotel situated in this area. It was never planned to be a commercial property and I know you have been given the many reasons by many of the neighbors in this area, so I won't repeat them to you. I just want you to take a hard look at this proposal and hope that you can see our view over the money that Chico could make with having this hotel in this spot. There are so many other areas of Chico where this hotel would be better for the community and for the guests that stay in the hotel. The residents in California Park and especially the large senior community are extremely concerned.

We would love for you to address all the concerns we have and what you plan to do to alleviate our concerns if this hotel ends up getting the special use permit to build on this lot.

Thank you for your time, Kathy and Bill Gochenour

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Wrong place for a 4-story Marriott hotel - NO Hotel  
**Date:** Monday, February 21, 2022 12:56:14 PM

---

**From:** buttecreek@comcast.net <buttecreek@comcast.net>  
**Sent:** Monday, February 21, 2022 12:44 PM  
**To:** Bruce Ambo <bruce.ambo@Chico.ca.gov>  
**Subject:** FW: Wrong place for a 4-story Marriott hotel - NO Hotel

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Dear Mr. Ambo,

My wife and I have been living in Chico since 1984 and have lived in California Park since 1996. It makes no sense putting this type of commercial building/hotel in a residential area and near a 55+ year old living community.

There are better areas for this type of development.

Please stand up to the pressures and do what is right. Do not give a "special use permit" for this hotel.

These are valid and honest concerns.

1. The property is **not zoned** for this type of commercial building and will require a special 'use permit' from the City of Chico. The land was intended to be developed to support the needs of the local residents, and clearly not for this type of commercial development.
2. **Traffic concerns** are already an issue in this area, but a hotel would exacerbate these problems on a daily/nightly basis and increase the transient population in Cal Park exponentially.
3. Cal Park houses the largest Senior community in Chico. Seniors and all residents in our community, including Canyon Oaks, are already concerned about their safety and ability to evacuate, if necessary, in a timely and safe manner. **Remember the Camp Fire!**
4. The location and enormous size of the proposed 4-story hotel are **not compatible** with the surrounding community, making it an eyesore with its nightly business lights and generally obtrusive nature.
5. The project has **not** gone through a full environmental impact review. There would certainly be impacts to the surrounding natural environment and the adjacent riparian area,
6. The homeowners of Californian Park Association, which includes Sierra Sunrise Village, Sierra Lakeside, and Village Green will be required to spend substantially increased dollars to provide significantly more hours of patrol services to protect against the inevitable detrimental impacts from hotel patrons, visitors, and staff **trespassing** onto our private lakes and walkways.

Sincerely,

Dave & Cari Schmucker

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Marriott's Towne Place Suites hotel project  
**Date:** Tuesday, February 22, 2022 9:19:47 AM

---

**From:** dan ehman <ehman3d@att.net>  
**Sent:** Monday, February 21, 2022 7:03 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Marriott's Towne Place Suites hotel project

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TO: Bruce Ambo, Chico City Principal Planner

I am extremely opposed to the proposed hotel project - Marriott's Towne Place Suites in California Park for all the reasons below.

There are plenty of viable and better suited locations for a project of this nature.

1. The property is not zoned for this type of commercial building and will require a special 'use permit' from the City of Chico. The land was intended to be developed to support the needs of the local residents, and clearly not for this type of commercial development.
2. Traffic concerns are already an issue in this area, but a hotel would exacerbate these problems on a daily/nightly basis and increase the transient population in Cal Park exponentially.
3. Cal Park houses the largest Senior community in Chico. Seniors and all residents in our community, including Canyon Oaks, are already concerned about their safety and ability to evacuate, if necessary, in a timely and safe manner. Remember the Camp Fire!
4. The location and enormous size of the proposed 4-story hotel are not compatible with the surrounding community, making it an eyesore with its nightly business lights and generally obtrusive nature.
5. The project has not gone through a full environmental impact review. There would certainly be impacts to the surrounding natural environment and the adjacent riparian area,
6. The homeowners of Californian Park Association, which includes Sierra Sunrise Village, Sierra Lakeside, and Village Green will be required to spend substantially increased dollars to provide significantly more hours of patrol services to protect against the inevitable detrimental impacts from hotel patrons, visitors, and staff trespassing onto our private lakes and walkways.

Thank you

Dan Ehman  
4 Kestrel Ct.  
Chico, Ca. 95928  
530-513-8812

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Cal Park Hotel  
**Date:** Monday, February 21, 2022 12:58:38 PM

---

-----Original Message-----

From: Stephanie Hardie <[smhardiechico@gmail.com](mailto:smhardiechico@gmail.com)>  
Sent: Saturday, February 19, 2022 4:52 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Cal Park Hotel

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I am definitely opposed to a 4 story hotel on Bruce Road. It will bring a lot more traffic to the area, in addition to the new houses and apartments being planned for in the future in the area. Also, it will complicate being able to evacuate the area in an emergency.

Stephaney Hardie  
2674 Fairfield Commons



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: No Hotel  
**Date:** Friday, February 18, 2022 11:10:41 AM

---

-----Original Message-----

From: Laura Moretti <[animalsvoice@me.com](mailto:animalsvoice@me.com)>  
Sent: Friday, February 18, 2022 10:06 AM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Cc: Mike Wolcott <[mwolcott@chicoer.com](mailto:mwolcott@chicoer.com)>  
Subject: No Hotel

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To Bruce Ambo, Chico City Principal Planner:

I live in California Park near where the proposed hotel is considered to be built. I admit that I love having a hotel nearby to put guests up when they visit, but after seriously weighing the pros and cons, I wish to add my voice to the NO HOTEL IN CALIFORNIA PARK campaign.

Ever since the 3-story assisted living facility was built across the street 3 years ago, traffic has already increased considerably in our neighborhood. This hotel is only going to add yet more traffic, as well as more people, noise, and strangers to our neighborhood. Therefore, I vote a strong NO HOTEL IN CALIFORNIA PARK!!

Thank you.

Laura Moretti  
1 Ginger Lane  
Chico, CA 95928

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acaín](#)  
**Subject:** FW: Marriott Towne Place Suites/Curious and Opposed  
**Date:** Friday, February 18, 2022 4:08:08 PM

---

**From:** Laurie Benson <bensonlaurie59@gmail.com>  
**Sent:** Friday, February 18, 2022 3:44 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Marriott Towne Place Suites/Curious and Opposed

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To: Bruce Ambo, Chico City Principal Planner  
From: Elizabeth Benson and Laurie Benson  
#28 Sierra Lakeside Lane  
Chico. Ca, 95928

Greetings Mr. Ambo-

2/18/22

The curious portion of my heading is in regards to how, of all the open lots in Chico zoned for commercial use, you all came up with this strip of land as being deemed appropriate for a 4-story hotel. We have lived in Chico since 1970 and have witnessed the tremendous growth of hotels, motels and chain restaurants and shopping off I99 and around the new mall. Although not a magnet for our family, the parking and multiple lanes for traffic definitely seem to be planned out to make more sense for shopping in that area, as well as convenient for travelling and being a hotel guest.

So my curiosity is piqued by the Marriott Towne Place Stes plan, Highway 32 is one lane in each direction at that juncture, the established residential area behind it is a senior/retirement community. There are no restaurants or any entertainment within walking distance, so traffic would increase to unimaginable levels at all hours. It seems to me that building this type of hotel would be better suited to your new City Hall area, where you are already contending with road and commercial planning and building. A hotel equidistant to the Skyway and Hwy 32 might be a better location, and gets a large, noisy, brightly lit eyesore of a hotel away from a quiet neighborhood and over to a location more suited to development of "like " buildings.

I have read your Environmental Analysis report as well and have been following this idea of yours through the pandemic and before. So in the long run, this is an opposition letter from one Sierra Lakeside address. In our eyes, your planning has fallen way short.

Elizabeth Benson  
Laurie Benson

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: No Hotel  
**Date:** Friday, February 18, 2022 2:07:06 PM

---

-----Original Message-----

From: Colleen <esterjunebug@yahoo.com>  
Sent: Friday, February 18, 2022 1:49 PM  
To: Bruce Ambo <bruce.ambo@Chicoca.gov>  
Subject: No Hotel

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I would like to oppose this hotel going up in Cal Park. I think there are many other locations around Chico that would be more appropriate and accepted. This is a peaceful little community that is not conducive to hotel traffic.  
Sincerely submitted, Colleen Feeney

"Take delight in The Lord, and He will give you your heart's desires." Psalm 37:4

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: California Park Hotel  
**Date:** Thursday, February 17, 2022 8:39:26 AM  
**Attachments:** [image001.png](#)

---

Be add this to the list of correspondence received.

Thanks

**Bruce Ambo, AICP**

Principal Planner  
City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927



<http://www.ci.chico.ca.us/>

---

**From:** Rob Ayers <kaibacha@gmail.com>  
**Sent:** Thursday, February 17, 2022 8:08 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** California Park Hotel

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I support it.

I am the homeowner of 1323 Yosemite Dr.

It seems like the perfect place for a hotel given the growth and improvements happening in the area.

Thank you.

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Hotel in Cal Park  
**Date:** Thursday, February 17, 2022 8:56:38 AM

---

fyi

-----Original Message-----

From: Tonya Munoz <[munozfive@yahoo.com](mailto:munozfive@yahoo.com)>  
Sent: Wednesday, February 16, 2022 8:22 PM  
To: Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
Subject: Hotel in Cal Park

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Sir,

I am writing to express my concern about the hotel proposed to be built at the entrance to our senior community of California Park. I agree with my fellow neighbors that it is not a good idea. I am very worried about our quiet neighborhood becoming unsafe for all of us, especially our senior neighbors. They will be subjected to the bright lights, noise and hotel guests wandering through their private streets. The amount of traffic in that narrow street will be a nightmare. We have senior housing, assisted living, a memory care facility and a rehabilitation hospital in this small area. Emergency vehicles need to be able to get through quickly and safely, not to mention the chaos of an evacuation.

We pay our HOA dues to protect and care for our lakes and ponds and for our patrol officers, who are only here during the day. Hotel guests will be drawn to our beautiful grounds to walk around. We already have people coming through our streets in the early morning hours looking for things to steal. Some of the guests may see our cars and houses as easy targets. We will be forced to pay increased security fees for the extra patrol we will need. The parents of children in our neighborhood may not feel safe letting them playing outside unsupervised and us adults who like to walk in the evenings or early mornings may not feel safe to do so.

I feel strongly that this hotel is inviting a whole host of issues that does not belong in any neighborhood.

Thank you for your time.  
Tonya Munoz  
California Park Resident

Sent from my iPad



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: No Hotel  
**Date:** Friday, February 18, 2022 11:10:41 AM

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Sent: Friday, February 18, 2022 10:06 AM  
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Cc: Mike Wolcott <[mwolcott@chicoer.com](mailto:mwolcott@chicoer.com)>  
Subject: No Hotel

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To Bruce Ambo, Chico City Principal Planner:

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**Date:** Friday, February 18, 2022 4:08:08 PM

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**Sent:** Friday, February 18, 2022 3:44 PM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>  
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Elizabeth Benson  
Laurie Benson

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**Subject:** FW: No Hotel  
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Sincerely submitted, Colleen Feeney

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**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: NO HOTEL IN CALIFORNIA PARK!  
**Date:** Thursday, February 17, 2022 8:57:26 AM

---

**From:** Lynne Elhardt <bratpeep@aol.com>  
**Sent:** Thursday, February 17, 2022 8:04 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** NO HOTEL IN CALIFORNIA PARK!

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Dear Mr. Ambo. I am a homeowner in the senior community of Sierra Lakeside where my elderly mother in law lives. I cannot overstate how much I am against the hotel being considered for the southeast corner of Bruce and Hwy 32. The negative impacts of that area far outweigh the positive. Lack of employee parking, unmanaged, transient access to the private lands of California Park and it's surrounding lakes and trails, stranger danger risk to seniors, lack of water concerns, traffic, and the negative impact on the land are just a few of the reasons this is no place for a four story, extended, stay hotel. Our community is already growing at an unhealthy and unsustainable rate.

Sincerely,

Lynne Elhardt  
1 Casa Del Lago

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Hotel in Cal Park  
**Date:** Thursday, February 17, 2022 8:56:38 AM

---

fyi

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Sent: Wednesday, February 16, 2022 8:22 PM  
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Subject: Hotel in Cal Park

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Sir,

I am writing to express my concern about the hotel proposed to be built at the entrance to our senior community of California Park. I agree with my fellow neighbors that it is not a good idea. I am very worried about our quiet neighborhood becoming unsafe for all of us, especially our senior neighbors. They will be subjected to the bright lights, noise and hotel guests wandering through their private streets. The amount of traffic in that narrow street will be a nightmare. We have senior housing, assisted living, a memory care facility and a rehabilitation hospital in this small area. Emergency vehicles need to be able to get through quickly and safely, not to mention the chaos of an evacuation.

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I feel strongly that this hotel is inviting a whole host of issues that does not belong in any neighborhood.

Thank you for your time.  
Tonya Munoz  
California Park Resident

Sent from my iPad



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**To:** [Nicole Acain](#)  
**Subject:** FW: California Park Hotel  
**Date:** Thursday, February 17, 2022 8:39:26 AM  
**Attachments:** [image001.png](#)

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Be add this to the list of correspondence received.

Thanks

**Bruce Ambo, AICP**

Principal Planner  
City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927



<http://www.ci.chico.ca.us/>

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**Sent:** Thursday, February 17, 2022 8:08 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** California Park Hotel

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I support it.

I am the homeowner of 1323 Yosemite Dr.

It seems like the perfect place for a hotel given the growth and improvements happening in the area.

Thank you.

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: NO HOTEL IN CALIFORNIA PARK!  
**Date:** Thursday, February 17, 2022 8:57:26 AM

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Sincerely,

Lynne Elhardt  
1 Casa Del Lago

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Concerns- Hotel in California Park  
**Date:** Thursday, February 17, 2022 8:59:46 AM

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-----Original Message-----

From: Kelly Young <kellbo22@gmail.com>  
Sent: Wednesday, February 16, 2022 6:39 PM  
To: Bruce Ambo <bruce.ambo@Chicoca.gov>  
Subject: Concerns- Hotel in California Park

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Hello,

I have been a homeowner in California park for over 10 years. As much as I love to see our beautiful neighborhood update and grow, I am totally against a 3 story hotel being in my neighborhood! To be honest, I don't even want a one story hotel. One big Part of cal parks beauty is our views! not only would this COMPLETELY ruin our views, it will over crowd our streets, and parks and allow NON CAL-PARK residence use of OUR many parks, walking paths, playgrounds etc, along with ALLOWING PUBLIC ACCESS TO A PRIVATE STREET!!! I, along with ALL other cal park residents pay my \$435 HOA fee to access those things! And I also pay that fee to insure a SAFE, CLEAN and QUIET neighborhood. Any type of hotel would clearly Violate ALL of those things, along with lots more. No hotel should be allowed inside of a residential HOA ever, period! It's not safe for our families, children and it's not something anyone who owns a home and has built a life here for over 10 (some 30+) years wants! I vote no , 1000 times! Please let me know how to sign any petition or anything else. Thank you -

Young

Cal Park home owner -Kestrel court

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: NO HOTEL IN CALIFORNIA PARK  
**Date:** Thursday, February 17, 2022 10:35:23 AM

---

**From:** John Page <executive3131@gmail.com>  
**Sent:** Thursday, February 17, 2022 10:24 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** NO HOTEL IN CALIFORNIA PARK

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Hello,

I am writing to express my deep disappointment and concern that the city council would even consider a special use permit to put a hotel in the middle of a large residential area. I didn't buy a home on the edge of town to stare at a giant hotel so you and the board can profit off it. The only possible reason for you to consider this is profit for yourselves. It will degrade our community, the property value of one of your nicest neighborhoods will immediately drop, and most importantly it does not solve any issue currently facing our community. Average Hotel occupancy in chico is under 50% yearly and that number is driven primarily from graduation once a year. If you so desperately want a hotel stick it in the middle of Ken Grossmans giant development and let it fail there like all of those empty buildings.

Thank you,  
John

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: NO HOTEL  
**Date:** Thursday, February 17, 2022 11:12:02 AM

---

-----Original Message-----

From: Janet Donohue <j-donohue@att.net>  
Sent: Thursday, February 17, 2022 10:52 AM  
To: Bruce Ambo <bruce.ambo@Chicoca.gov>  
Subject: NO HOTEL

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Hi, Bruce:

If you are taking votes on those who object to the placement of a hotel in the area of Bruce Road and Highway 32, please add my objection vote to your list.

I moved into Village Green about two years ago and just before my move, I heard that a woman had been killed in a car accident at Yosemite and Hwy. 32. I am a frequent user of that intersection on trips into town and have been surprised at the absence of cars coming out of the homes built on the south side of Hwy. 32 and Yosemite Rd. However, when I saw the building of eight apartment buildings on that southside of Hwy. 32, I was definitely concerned at the amount of traffic that those eight apartment buildings, three stories high with four apartments on each floor were going to create as those residents attempt to access not only Bruce Rd., but also Hwy. 32.

Then came the construction of stop lights on Yosemite & Hwy. 32, so I am definitely against any further housing developments anywhere near the Bruce Road and Hwy. 32.

Sincerely, Janet Donohue



**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: PLEASE PLEASE PLEASE  
**Date:** Thursday, February 17, 2022 12:57:45 PM

---

**From:** fperata@aol.com <fperata@aol.com>  
**Sent:** Wednesday, February 16, 2022 5:22 PM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** PLEASE PLEASE PLEASE

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Don't OK that hotel for Calif Park. I just don't understand how anyone can think that this is a good location for a hotel! Bruce Road is congested as it is. This will only make it worse. And a gas station??? Aren't there enough hotels/motels in and around Chico?

It's my understanding that there are dozens (or more) homes for rent through AirBnB or whatever it's called. I'm surprised the hotel business isn't kaput by now anyway.

*Frances Perata  
(530) 893-5231  
Lakewest in Cal Park*

**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Proposed Hotel in California Park  
**Date:** Thursday, February 17, 2022 10:47:46 AM

---

**From:** Don Brasel <donb.lee@gmail.com>  
**Sent:** Thursday, February 17, 2022 10:38 AM  
**To:** Bruce Ambo <bruce.ambo@Chicoca.gov>  
**Subject:** Proposed Hotel in California Park

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Dear Sir:

I am a resident in Cal Park and oppose the building of a hotel in this area for the following reasons:

1. The property is not zoned for this type of commercial building and will require a special 'use permit' from the City of Chico. The land was intended to be developed to support the needs of the local residents, and clearly not for this type of commercial development.
2. Traffic concerns are already an issue in this area, but a hotel would exacerbate these problems on a daily/nightly basis and increase the transient population in Cal Park exponentially.
3. Cal Park houses the largest Senior community in Chico. Seniors and all residents in our community, including Canyon Oaks, are already concerned about their safety and ability to evacuate, if necessary, in a timely and safe manner. Remember the Camp Fire!

Sincerely, Don and Lee Brasel  
2623 Amanecida Commons,

**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>

**Cc:** Gary Taylor <[Gary@Hignell.com](mailto:Gary@Hignell.com)>

**Subject:** Towne Place Suites

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Hi Bruce,

Thanks again for the call today, we really appreciate you taking the time to explain to us where we are in the timeline of this project.

We believe that the representatives from our associations would benefit from hearing from you as well. We would like to schedule a time that you could meet with them next week. We would be happy to host here at our office or we could come to you if you have a meeting space that would accommodate a group of 5-10 people. Do you have any times next that we could propose to our group?

Jennifer Treff,

Certified Community Association Manager, CCAM-PM.AA.CI®

The Hignell Companies

1750 Humboldt Road

Chico, CA 95928

(530)894-0404

Fax: (530)894-6984

Corporate BRE #317250 Cont. Lic. #143778

[Jennifer@hignell.com](mailto:Jennifer@hignell.com)



*Creating Caring Communities  
Since 1948*



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**From:** Bruce Ambo [<mailto:bruce.ambo@Chicoca.gov>]  
**Sent:** Friday, February 11, 2022 3:10 PM  
**To:** Jennifer Treff <[Jennifer@Hignell.com](mailto:Jennifer@Hignell.com)>  
**Cc:** Gary Taylor <[Gary@Hignell.com](mailto:Gary@Hignell.com)>; Jeff Carter <[jeff@jccarterlaw.com](mailto:jeff@jccarterlaw.com)>; Kelly Murphy <[Kelly.murphy@Chicoca.gov](mailto:Kelly.murphy@Chicoca.gov)>  
**Subject:** RE: Towne Place Suites

Hi Jennifer/Gary,

I am confused about what the purpose of this meeting is, since I thought we covered the status of the project and next steps in our phone call? I do not think it will be productive for your group (5 – 10 people?) to direct their dislike or frustration with the proposed project to City staff, when those thoughts and ideas need to be expressed in a public hear to the Planning Commission (i.e., decision-maker). The opinions of the HOA and neighborhood are well-documented from past emails, letters, and neighborhood meetings where the developer shared their plans. I encourage any comments on the environmental document or the project to be submitted in writing to me via email to me, and I will likely not be able to respond to impromptu inquiries immediately. Any and all past, present and future correspondence on this project will be included in the staff report that goes to the Planning Commission. Full consideration of the land use (i.e., Use Permit) and environmental documentation for the proposed hotel will be at a subsequent date at the Planning Commission in April or May.

Best Regards

**Bruce Ambo, AICP**  
Principal Planner  
City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927



<http://www.ci.chico.ca.us/>

**From:** Jennifer Treff <[Jennifer@Hignell.com](mailto:Jennifer@Hignell.com)>  
**Sent:** Friday, February 11, 2022 2:26 PM

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530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927



<http://www.ci.chico.ca.us/>

**From:** Gary Taylor <[Gary@Hignell.com](mailto:Gary@Hignell.com)>  
**Sent:** Friday, February 11, 2022 4:41 PM  
**To:** Bruce Ambo <[bruce.ambo@Chicoca.gov](mailto:bruce.ambo@Chicoca.gov)>; Jennifer Treff <[jennifer@Hignell.com](mailto:jennifer@Hignell.com)>  
**Cc:** Jeff Carter <[jeff@jccarterlaw.com](mailto:jeff@jccarterlaw.com)>; Kelly Murphy <[Kelly.murphy@Chicoca.gov](mailto:Kelly.murphy@Chicoca.gov)>  
**Subject:** RE: Towne Place Suites

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Bruce,

Our thought was that the parties involved could hear the information directly from you so that all could get a clear understanding of the process (rather than us trying to give information second hand). In no way were we thinking it would be a complaint session. In addition, if there were any questions, you could field them for even greater clarity of the process. Thank you for your consideration.

Regards,

Gary Taylor, CCAM-PM<sup>®</sup>  
Property Manager  
The Hignell Companies  
1750 Humboldt Road  
Chico, CA 95928  
530-894-0404 ext.105 phone  
888--304-4674 ext. 105 phone  
530-894-6984 fax  
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**From:** [Bruce Ambo](#)  
**To:** [Nicole Acain](#)  
**Subject:** FW: Towne Place Suites  
**Date:** Monday, February 14, 2022 1:10:32 PM  
**Attachments:** [image001.png](#)  
[image002.png](#)  
[image003.png](#)  
[image004.png](#)

---

Fyi

**Bruce Ambo, AICP**

Principal Planner  
City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927



<http://www.ci.chico.ca.us/>

**From:** Bruce Ambo  
**Sent:** Monday, February 14, 2022 8:33 AM  
**To:** Gary Taylor <Gary@Hignell.com>; Jennifer Treff <Jennifer@Hignell.com>  
**Cc:** Jeff Carter <jeff@jccarterlaw.com>; Kelly Murphy <Kelly.murphy@Chicoca.gov>; Brendan Vieg <brendan.vieg@Chicoca.gov>  
**Subject:** RE: Towne Place Suites

Hi Gary.

If the ground rules can be set that this won't turn into a "complaint session" and just to inform folks about the planning process, we would be happy to meet with you. Let me circle back with Kelly Murphy (planner handling the Use Permit and Architectural Review) and see what dates and times might work best and follow up with you.

Best Regards

**Bruce Ambo, AICP**

Principal Planner  
City of Chico, Community Development

**Nicole Acain**

---

**From:** Julie Ricci <info@chicoca.gov>  
**Sent:** Saturday, January 29, 2022 10:14 AM  
**To:** Zoning  
**Subject:** Form submission from: Contact us

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01/29/2022 - 10:13am City of Chico »

**WEBFORM SUBMISSION**

Submitted by anonymous user:  
[2601:200:4501:5c40:28a6:9c48:1fcc:3169]

Your name:  
Julie Ricci

Your e-mail:  
JulieRicci@msn.com

Message:  
Dear Planning Commission,  
I hope you reject the idea of having a hotel or motel off Sierra Sunrise Terrace or any corner of Bruce Road and Hwy. 32. Traffic is already becoming congested in the area. If we were to have another catastrophe such as the Camp Fire, there would be no way to safely evacuate our senior housing, much less the rest of the residents from Forest Ranch to Hwy.99. There is no reason other than monetary reasons for a hotel to be put in this vicinity. It would not serve the residents of the area.

**CONTEXT INFORMATION**

Profile contacted:  
Planning Division

**From:** [Sandy Mayer](#)  
**To:** [Nicole Acain](#)  
**Subject:** Re: Towne Place Suites Hotel Proposal  
**Date:** Friday, January 28, 2022 5:11:12 PM  
**Attachments:** [image001.png](#)

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I submitted questions and concerns last year to the Planning Dept. .... no response!

Sent from my iPhone

On Jan 28, 2022, at 3:41 PM, Nicole Acain <[nicole.acain@chicoca.gov](mailto:nicole.acain@chicoca.gov)> wrote:

**Subject: Towne Place Suites Hotel Proposal**

You are receiving this email because you expressed interest in the hotel project (Towne Place Suites) proposed south of Sierra Sunrise Terrace and northeast of the intersection of Bruce Road and Highway 32. The proposed Towne Place Suites hotel project (UP 19-25 & AR 19-22) includes the construction of a 4-story, 112-room hotel on a 4.09 acre site (APN: 018-230-001). The project site is designated Commercial Mixed Use on the General Plan Land Use Diagram and zoned CC (Community Commercial). Pursuant to Chico Municipal Code Section 19.44.020, Table 4-6 (*Allowed Uses and Permit Requirements for Commercial Zoning Districts*), a Use Permit is required to establish a hotel in the CC zoning district. At this time, the applicant is only requesting approval of a Use Permit from the Planning Commission for the hotel use in the CC zoning district, although detailed architectural and landscaping plans have been provided. Should the Use Permit for the hotel use be approved by the Planning Commission, a subsequent hearing on the architectural and landscape design of the project will be held by the City's Architectural Review and Historic Preservation Board. Any decision by the Planning Commission on the Use Permit for the project is appealable to the City Council, as is any subsequent decision of the Architectural Review and Historic Preservation Board.

This project is Categorical Exempt from further environmental review pursuant to the California Environmental Quality Act (CEQA) Guidelines Section 15332 (Infill Development Projects). The proposed project is: 1) consistent with the applicable General Plan designation and all applicable General Plan policies, as well as the applicable zoning district and associated development standards and regulations; 2) proposed within city limits on a site of no more than five acres substantially surrounded by urban uses; 3) proposed on a site that has no value as habitat for endangered, rare or threatened species; 4) determined to not result in any significant effects relating to

traffic, noise, air quality, or water quality; and 5) located on a site that can be adequately served by all required utilities and public services. **The Notice of Exemption for Towne Place Suites – Environmental Analysis is provided at the link below (Environmental Analysis).**

Due to the public interest in this project, there will be a 30-day courtesy public review of the Environmental Analysis from January 28, 2022, through February 28, 2022. Following the public review period, staff anticipates scheduling a public hearing before the Planning Commission in March or April 2022 to consider the Use Permit request. Questions regarding this project may be directed to Principal Planner Bruce Ambo at (530) 879-6801 or [bruce.ambo@chicoca.gov](mailto:bruce.ambo@chicoca.gov).

To view all documents on our webpage, please follow the link here:

<https://chico.ca.us/post/towne-place-suites-hotel-proposed-bruce-road-hwy-32-and-sierra-sunrise-terrace>

Thank you,

*Nicole Acain*

Administrative Assistant  
City of Chico, CDD I Planning  
411 Main Street, 2<sup>nd</sup> Floor  
P.O Box 3420  
Chico, CA 95927-3420  
(530) 879-6800 / [nicole.acain@chicoca.gov](mailto:nicole.acain@chicoca.gov)



**From:** [Laurie J Aaron](#)  
**To:** [Nicole Acain](#)  
**Subject:** Re: Towne Place Suites Hotel Proposal  
**Date:** Friday, January 28, 2022 4:03:02 PM  
**Attachments:** [image001.png](#)

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This is crazy that the builder is exempt from an environmental review.

How in the world is it possible that this project would NOT affect traffic and parking?

And the Creek is home to many bird species, as well as other animals.

Laurie Aaron

On Fri, Jan 28, 2022 at 3:41 PM Nicole Acain <[nicole.acain@chicoca.gov](mailto:nicole.acain@chicoca.gov)> wrote:

**Subject: Towne Place Suites Hotel Proposal**

You are receiving this email because you expressed interest in the hotel project (Towne Place Suites) proposed south of Sierra Sunrise Terrace and northeast of the intersection of Bruce Road and Highway 32. The proposed Towne Place Suites hotel project (UP 19-25 & AR 19-22) includes the construction of a 4-story, 112-room hotel on a 4.09 acre site (APN: 018-230-001). The project site is designated Commercial Mixed Use on the General Plan Land Use Diagram and zoned CC (Community Commercial). Pursuant to Chico Municipal Code Section 19.44.020, Table 4-6 (*Allowed Uses and Permit Requirements for Commercial Zoning Districts*), a Use Permit is required to establish a hotel in the CC zoning district. At this time, the applicant is only requesting approval of a Use Permit from the Planning Commission for the hotel use in the CC zoning district, although detailed architectural and landscaping plans have been provided. Should the Use Permit for the hotel use be approved by the Planning Commission, a subsequent hearing on the architectural and landscape design of the project will be held by the City's Architectural Review and Historic Preservation Board. Any decision by the Planning Commission on the Use Permit for the project is appealable to the City Council, as is any subsequent decision of the Architectural Review and Historic Preservation Board.

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site that has no value as habitat for endangered, rare or threatened species; 4) determined to not result in any significant effects relating to traffic, noise, air quality, or water quality; and 5) located on a site that can be adequately served by all required utilities and public services. **The Notice of Exemption for Towne Place Suites – Environmental Analysis is provided at the link below (Environmental Analysis).**

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To view all documents on our webpage, please follow the link here:  
<https://chico.ca.us/post/towne-place-suites-hotel-proposed-bruce-road-hwy-32-and-sierra-sunrise-terrace>

Thank you,

*Nicole Acain*

Administrative Assistant

City of Chico, CDD I Planning

411 Main Street, 2<sup>nd</sup> Floor

P.O Box 3420

Chico, CA 95927-3420

(530) 879-6800 / [nicole.acain@chicoca.gov](mailto:nicole.acain@chicoca.gov)



Thanks for the notice, Please make sure to leave enough room for the homeless and their tents. Leave about 1/2 acre for them to throw their garbage & needles. Be sure to make a fresh water stream for them to shit & piss in. Cater to their every need as much as possible.  
Thank

## NOTICE FOR PRE-APPLICATION NEIGHBORHOOD MEETING

January 11, 2022

A pre-application meeting will be held on **Tuesday, January 25, 2022 at 5:30 p.m.**  
VIA Zoom, due to current COVID-19 Restrictions.

<https://us02web.zoom.us/j/88201044714?pwd=MGFiTnVnQmFjR3hPNTc3bWdYNURPdz09>

**Meeting ID: 882 0104 4714**

**Passcode: 955930**

One tap mobile +16699006833,,88201044714# US (San Jose) 12532158782,,88201044714#  
US+(Tacoma)

**Location:** The proposed project will be located in the southeastern portion of the City of Chico, Butte County within an urban residential neighborhood on the vacant unimproved parcel on the eastern side of Bar Triangle Street, just south of Robailey Drive. Assessor's Parcel Number 002-190-025-000. It is surrounded on the north, east, and west by single-family homes and 2-story apartment buildings, to the south is an undeveloped lot

**Proposed Project Description:** The applicant for this unaddressed parcel is in the process of submitting an application for Architectural Review for a 70-unit affordable housing project. The proposed development is a two- and three-story affordable, multi-family apartment community that is comprised of 70 dwelling units, consisting of one, two, and three-bedroom stacked flats on approximately 3.3 acres. The project is comprised of five residential buildings. There are 18 one-bedrooms, 34 two-bedrooms, and 18 three-bedrooms. Our target population will be families and residents of Chico and the surrounding areas that are income qualified. Also included on site is a community building with managerial/leasing offices, multi-purpose lounge, kitchenette, and outdoor gathering spaces. The proposed Architectural Review submittal is attached.

This meeting is intended to provide an opportunity for neighbors to meet with the project proponents and to review and comment on the site plan in advance of the public meeting. A City staff member will be present to answer questions about the development process.

For further information regarding this project, please contact the applicant's representative:

James Stevens, NorthStar  
111 Mission Ranch Blvd. Ste. 100  
Chico, CA 95926  
(530) 893-1600  
jstevens@northstareng.com

For further information regarding Chico's development process and regulations, please contact:

City of Chico Community Development  
Department  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
(530) 879-6800 zoning@chicoca.gov

## Nicole Acain

---

**From:** Tally Sturm <ttreyster@gmail.com>  
**Sent:** Monday, November 30, 2020 5:45 PM  
**To:** Timothy Donohue  
**Cc:** Bruce Ambo; jcpearce2015@gmail.com; Brendan Vieg; Kelly Murphy; Sandy Goulart; Jeffrey Lambkin; Vic & Sandy Makau  
**Subject:** Re: Towne Place Suites Application Status Update -- AR 19-22 and UP 19-25

**ATTENTION:** This message originated from outside **City of Chico**. Please exercise judgment before opening attachments, clicking on links, or replying.

This response from city seems again to be resistant to any of our input, though inferring they are open. It is not clear to me how state law can crown city planning, however, nothing should surprise me in CA! I will certainly resend all my letters to the new crew if that will help.

Thanks again for your insightful letter, Tim.

Tally

Sent from my iPad

On Nov 30, 2020, at 5:12 PM, Timothy Donohue <timinchico@gmail.com> wrote:

Bruce,  
Thank you for the courteous contact. I was unaware that Dexter was not the lead planner.  
>>I have copied a few other residents who have great interest in this project.<<

If I may briefly touch on a couple of points.

I accept the fact that you will be transparent and  
*"thoughtfully engage the community and do our best in every way to ensure that you and your neighbors have ample opportunities to review this proposal and provide your comments, suggestions, criticisms and concerns."*

Nothing less is expected and you should already have a large file of protests communications from residents back to 2019. If this is not the case, then we are off to a bad start. The good people of Sierra Sunrise and California Park have made considerable effort to communicate with City officials and to include them in our past well attended pre-covid community meetings.

Please do not assume that anything is normal and any adverse decision will not play well if the residents feel covid restrictions played in favor of the applicant. It would not be unreasonable for the City to propose a possible delay with the

**Nicole Acain**

---

**From:** Timothy Donohue <timinchico@gmail.com>  
**Sent:** Monday, November 30, 2020 5:12 PM  
**To:** Bruce Ambo  
**Cc:** jcpearce2015@gmail.com; Brendan Vieg; Kelly Murphy; Sandy Goulart; Jeffrey Lambkin; Vic & Sandy Makau; Tally Sturm  
**Subject:** Re: Towne Place Suites Application Status Update -- AR 19-22 and UP 19-25

**ATTENTION: This message originated from outside City of Chico. Please exercise judgment before opening attachments, clicking on links, or replying.**

Bruce,  
Thank you for the courteous contact. I was unaware that Dexter was not the lead planner.  
>>I have copied a few other residents who have great interest in this project.<<

If I may briefly touch on a couple of points.

I accept the fact that you will be transparent and  
*"thoughtfully engage the community and do our best in every way to ensure that you and your neighbors have ample opportunities to review this proposal and provide your comments, suggestions, criticisms and concerns."*

Nothing less is expected and you should already have a large file of protests communications from residents back to 2019. If this is not the case, then we are off to a bad start. The good people of Sierra Sunrise and California Park have made considerable effort to communicate with City officials and to include them in our past well attended pre-covid community meetings.

Please do not assume that anything is normal and any adverse decision will not play well if the residents feel covid restrictions played in favor of the applicant. It would not be unreasonable for the City to propose a possible delay with the applicant who could voluntarily hold their final submissions as a sign of respect for the special times.

I also notice as I peruse the CEQA that it has a specific exemption to zoning changes which seems to relate to a special exemption they are requesting-which in itself resides under the General Plan. I would argue that CEQA may not apply in this specific case.

***C. The Permit Streamlining Act does not apply to the following:  
Legislative land use decisions, such as amendments to the zoning ordinance or general plan. Land Waste Management v. Contra Costa County, 222 Cal. App. 3d 950, 271 Cal. Rptr. 909 (1990); Landi v. Monterey County, 139 Cal. App. 3d 934, 189 Cal. Rptr. 55 (1983).***

Thank you for your professional time and be assured that there is broad support from all demographics against this project.  
Also be assured the same people support and value the growth we see at Meriam Park and South of Deer Creek HWY 32. The issue is not about growth but about dropping a non-conforming commercial hotel in the middle of a Senior Planned Community.  
And so much more- I am sure you will hear about

**Very Respectfully,**

*Tim Donohue  
Chico Resident*

++++  
++++

On Mon, Nov 30, 2020 at 3:44 PM Bruce Ambo <[bruce.ambo@chicoca.gov](mailto:bruce.ambo@chicoca.gov)> wrote:

Hello Mr. Donohue and Ms. Pearce,

I am Bruce Ambo, Principal Planner, and I along with Kelly Murphy, Planner, will be the planners assigned to manage the Townplace Suites hotel proposal at the northwestern corner property bounded by Highway 32/Bruce Road/Sierra Sunrise Terrace. As you may know, the previous planner, Dexter O’Connell, is on extended leave and Kelly and I are now handling the project and will be the contacts from hereon. In addition to the brief update that Dexter provided below, I will add that we are only in the beginning stages in analyzing the project. For example, the project is still incomplete for processing and has yet to pay their full fees for the staff to begin preparation of the initial environmental assessment.

While you have requested that we delay processing of this project due to the pandemic and difficulty associated with public engagement in a safe online format during the pandemic, it is also important to remind you that this is not a City project and we are obligated under State law to process this application in accordance with the timeframes set forth in the California Permit Streamlining Act and the California Environmental Quality Act (CEQA). The City has a mandatory obligation to process this application in accordance with these State laws and cannot intentionally delay or postpone the processing of the plans for the proposed project.

You have cited the AICP code of ethics below which I ascribe to in all of my professional endeavors, and as an AICP member I assure you that when and if this project is ready for public review we will thoughtfully engage the community and do our best in every way to ensure that you and your neighbors have ample opportunities to review this proposal and provide your comments, suggestions, criticisms and concerns on the project in a meaningful, easily accessible, and productive hearing format. Once we have completed the initial draft of the environmental assessment, we will send out a notice of the release of the environment document for public review and comment, as well as provide notice of all hearings and neighborhood meetings on the project that we as City staff are aware of.

Please feel free to contact Kelly (879-6535 [kelly.murphy@chicoca.gov](mailto:kelly.murphy@chicoca.gov)) or me with any questions on the status of the proposed project.

Sincerely,

**From:** [Dexter O'Connell](#)  
**To:** [Bruce Ambo](#)  
**Subject:** FW: Towne Place Suites Application Status Update -- AR 19-22 and UP 19-25  
**Date:** Monday, November 30, 2020 7:54:30 PM  
**Attachments:** [image001.png](#)

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Per your request, sorry for the delay.

Dexter

Dexter N. O'Connell  
Associate Planner



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**From:** Timothy Donohue <timinchico@gmail.com>  
**Sent:** Saturday, November 14, 2020 9:59 AM  
**To:** Dexter O'Connell <dexter.o'connell@Chicoca.gov>  
**Subject:** Re: Towne Place Suites Application Status Update -- AR 19-22 and UP 19-25

**ATTENTION:** This message originated from outside **City of Chico**. Please exercise judgment before opening attachments, clicking on links, or replying.

Dexter,

Thank you for the update on Towne Place Suites at Sierra Sunrise.

I would like to point out the **irony in your reply**. Just as you and other city employees are working remotely from home, we too are impacted by covid restrictions. The residents of Sierra Sunrise all fall into the highest risk group and will not be available for public meetings on this important issue. Just this week we are trending toward a higher level of restriction from the state.

I'm sure you are aware that last winter we had one of the biggest public meetings ever seen in Chico to oppose this project. It is unlikely we can replicate this type of participation with covid restrictions in place. I would appeal to you and the city planners to immediately recommend this **project be put on hold for one year until the covid restrictions are not in force**. To approve or promote this development during this pandemic would be tone deaf to the community and dismissive to a very large group of people unable to make their case in public. I also believe **under the current circumstances it would be imprudent and unethical. I make reference to the code upheld by the American Institute of Certified Planners** and many other municipal groups who set high standards of fairness in public agency development. See below.



I would also like to remind everyone that our local board of supervisors chose to put a green line west of town in 1982. This boundary protects the prime agricultural property from urban development.

I would like to propose that we now consider a **grey line boundary** that protects the Sierra Sunrise Retirement Community from the sprawl of commercial hotels and other unsuitable development that does not complement the area. We all know there is plenty of land in proximity that is perfectly suited for this type of development.

Very Respectfully,

Tim Donohue  
Chico, CA

+++++

Ethics in Planning  
A: Principles to Which We Aspire

1. Our Overall Responsibility to the Public

Our primary obligation is to serve the public interest and we, therefore, owe our allegiance to a conscientiously attained concept of the public interest that is formulated through continuous and open debate. We shall achieve high standards of professional integrity, proficiency, and knowledge. To comply with our obligation to the public, we aspire to the following principles:

- a) We shall always be conscious of the rights of others.
- b) We shall have special concern for the long-range consequences of present actions.
- c) We shall pay special attention to the interrelatedness of decisions.
- d) We shall provide timely, adequate, clear, and accurate information on planning issues to all affected persons and to governmental decision makers.
- e) We shall give people the opportunity to have a meaningful impact on the development of plans and programs that may affect them. Participation should be broad enough to include those who lack formal organization or influence.**
- f) We shall seek social justice by working to expand choice and opportunity for all persons, recognizing a special responsibility to plan for the needs of the disadvantaged and to promote racial and economic integration. We shall urge the alteration of policies, institutions, and decisions that oppose such needs.
- g) We shall promote excellence of design and endeavor to conserve and preserve the integrity and heritage of the natural and built environment.
- h) We shall deal fairly with all participants in the planning process. Those of us who are public officials or employees shall also deal evenhandedly with all

## planning process participants.

On Fri, Nov 13, 2020 at 7:31 PM Dexter O'Connell <[dexter.o'connell@chicoca.gov](mailto:dexter.o'connell@chicoca.gov)> wrote:

Hello!

I know it's been quite some time since you last heard from me about the proposed Towne Place Suites hotel on Sierra Sunrise Terrace. In the spirit of transparency with which I've striven to conduct this project for quite some time, I write now to inform you that the applicant has submitted the required traffic study based on the Vehicle Miles Travelled metric I've previously mentioned. The City will now thoroughly review the study to determine whether it meets our requirements. If it does, the application will be deemed complete, and review will begin to determine the next steps in the project.

I will reiterate that a complete application does not mean that City Staff have made a recommendation of approval or denial on the proposed project, nor that the decision of any of the city's Boards or Commissions (with whom the power to approve, modify, or deny such projects lays) has been preordained. Instead, it means that the project is in the position to begin the thorough review process that all projects proposed in the City of Chico are subject to. I will advise you as to any changes in the project's status.

As always, I am available for your questions. I do regret to inform you, however, that I am currently working fully remotely, and so I will be unavailable by phone. Please do not hesitate to reach out to me by email.

Thanks,  
Dexter

Dexter N. O'Connell  
Associate Planner



--  
*Tim Donohue*

Re: Marriott Proposed Hotel, California Park  
6/12/20

Dear Dexter O'Connell,

Thank you for your time, I am a resident of California Park, 2603 Lakewest Dr., where the Marriott four story hotel is being proposed.

I want to weigh in with my opinion that this is an inappropriate location for this hotel, a few reasons listed below:

### 1. Traffic

The proposed hotel location, on the corner of Bruce Rd. and Hwy 32, would add to the traffic problems, in this already overburdened area. Because of the surrounding roads and their capacity, and quality and inadequacy to handle more cars/traffic flow, this hotel would create more congestion and a burden on our infrastructure.

Future traffic problems are also a concern and should be researched now, not down the road when it's too late. There will be additional traffic generated from the Merriam Park Development, Skyline Luxury Apartments, that are being built on the corner of California Park Dr, and Bruce Rd, the future Gas Station, on corner of Hwy 32, as well as currently Marsh Jr High. These are all traffic concerns and a bottleneck for this area.

Current residents already have a problem getting onto Bruce Rd from Sierra Sunrise Rd. Access to and from Sierra Sunrise Rd is already dangerous.

Because there are no services near the hotel like restaurants etc., there will be even more traffic as hotel guests come and go to obtain services.

### 2. Parking

Sierra Sunrise Rd is a partial private road that is in a planned Senior Citizen community, Sierra Sunrise Terraces. Putting the hotel at this location would be incompatible with the neighborhood and safety concerns of those residents.

There will never be, in reality, enough parking spaces for hotel guests (112 rooms), visitors, employees, and necessary handicap parking spaces to prevent overflow parking on to the street.

There will be parking problems. From the hotel parking lot there will be overflow parking, (race cars, boats, 2 cars per room, visitors) on Sierra Sunrise Rd, whether intentional on the part of Marriott or not- this will be a constant problem.

This overflow parking issue, and road blockage would interfere with Emergency Medical Services, ambulances and firetrucks accessing the facilities in the senior community. Currently these vehicles drive to Sierra Sunrise Terrace and their facilities daily!

## 2. Security

California Park paths and lakes are private property; they are patrolled by a Security Company. Homeowners pay for the security patrol. But this hotel is beyond the scope of our security patrol capabilities and we should not have to pay for the hotel trespassers on to our lake.

Hotel security can't offer enough security, or have enough liability, for the hotel guests who go off Marriott property, and trespass onto California Park property, jeopardizing the safety and privacy of the citizens in this neighborhood.

California Park Lake is a private lake, there are homes that are on the lake and have exposed walk-ways in their backyards. Hotel guests would cross the street to walk onto private property, in the California Park neighborhood.

It would be impossible to prevent the home owners from being vulnerable to hotel guests walking on private property; strangers who are trespassing from the hotel. Especially since Marriott is touting this a "residents inn" where guests are staying long term, and will be curious and want to walk around the area they are staying.

There would be too many opportunities for hotel guests to trespass and block traffic with overflow parking.

## 4. Environment

The environmental impact of this hotel with its 112 rooms, and its proximity to the creek, and the run off it will produce, which will go directly into the Waterways of California, is incompatible with the state of California and any thoughts of sustainability. California Park's lakes and ponds are constantly monitored by Fish and Game to make sure we comply with chemical and run off regulations.

This hotel is not an appropriate spot for a multi-use hotel. I am surprised that such a huge environmental burden/footprint, is being proposed on such a small parcel - so near the creek.

This hotel will be too close to the creek and is not an appropriate place for this size residency.

## 5. 24-hour Lighting and Signage Pollution

This hotel will be right in the view sight of many homes, with **signage** and extreme **24-hour business lighting**. This signage and light pollution are not conducive to a neighborhood environment. This is our home.

This hotel would be an eye sore for many California Park residents

We think that Marriott's advertising campaign will use pictures of the lake to make this location more desirable, maybe that is why they picked this site. This will not be allowed. Our neighborhood is not a destination. It is our home.

With so many environmental, safety, privacy, traffic and neighborhood issues I would like the Planning Commission to look extremely hard at not approving this hotel for this location.

Thank you for your time.

Sandra S. Makau

## Dexter O'Connell

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**From:** Sandy Makau <ssmakau@yahoo.com>  
**Sent:** Friday, June 12, 2020 11:04 AM  
**To:** Dexter O'Connell  
**Subject:** Proposed California Park Hotel  
**Attachments:** Marriott letter.doc

**ATTENTION:** This message originated from outside **City of Chico**. Please exercise judgment before opening attachments, clicking on links, or replying.

Mr. O'Connell,  
I wanted to weigh in on the NO HOTEL issue. My email below lines out my opinions. I also have sent you an attachment of this same letter if you want to download it. Thanks you for directing it to where it needs to go.  
Thanks so much for your time on this issue.  
Sandy Makau

Re: Marriott Proposed Hotel, California Park  
6/12/20  
Dear Dexter O'Connell,

Thank you for your time, I am a resident of California Park, 2603 Lakewest Dr., where the Marriott four story hotel is being proposed.

I want to weigh in with my opinion that this is an inappropriate location for this hotel, a few reasons listed below:

### 1. Traffic

The proposed hotel location, on the corner of Bruce Rd. and Hwy 32, would add to the traffic problems, in this already overburdened area. Because of the surrounding roads and their capacity, and quality and inadequacy to handle more cars/traffic flow, this hotel would create more congestion and a burden on our infrastructure,(stop lights, more lanes needed).

Future traffic problems and population growth in this area, are also concerns and should be researched now, not down the road when it's too late. There will be additional traffic generated from the Merriam Park Development, Skyline Luxury Apartments, that are being built, on the corner of California Park Dr, and Bruce Rd, the future Gas Station, on corner of Hwy 32, as well as currently Marsh Jr High. These are all traffic concerns and a bottleneck for this area.

Current residents already have a problem getting onto Bruce Rd from Sierra Sunrise Rd. Access to and from Sierra Sunrise Rd is already dangerous.



Because there are no services near the hotel like restaurants etc., there will be even more traffic as hotel guests come and go to obtain services. This is a neighborhood not a business district.

## 2. Parking

Sierra Sunrise Rd is a partial private road that is in a planned Senior Citizen community, Sierra Sunrise Terraces. Putting the hotel at this location would be incompatible with the neighborhood and safety concerns of those residents.

There will never be, in reality, enough parking spaces for hotel guests (112 rooms), visitors, employees, and necessary handicap parking spaces to prevent overflow parking on to the street.

There will be ongoing parking problems. From the hotel parking lot there will be overflow parking, (race cars, boats, 2 cars per room, visitors) on Sierra Sunrise Rd, whether intentional on the part of Marriott or not- this will be a constant problem .

This overflow parking issue, and road blockage would interfere with Emergency Medical Services, ambulances and firetrucks accessing the facilities in the senior community. Currently these vehicles drive to Sierra Sunrise Terrace and their facilities daily!

## 3. Security

California Park paths and lakes are private property; they are patrolled by a Security Company. Homeowners pay for the security patrol. But the size of this hotel is beyond the scope of our security patrol capabilities and we should not have to pay for the hotel trespassers on to our lake.

Hotel security can't offer enough security, or have enough liability, for the hotel guests who go off Marriott property, and trespass onto California Park property, jeopardizing the safety and privacy of the citizens in this neighborhood.

California Park Lake is a private lake, there are homes that are on the lake and have exposed walk-ways in their backyards. Hotel guests would cross the street to walk onto private property, in the California Park neighborhood.

It would be impossible to prevent the home owners from being vulnerable to hotel guests walking on private property, or walking around our lake; these are strangers who are trespassing from the hotel. Especially since Marriott is touting this a "residents inn" where guests are staying long term, and will be curious and want to walk around the area they are staying.

There will be too many opportunities for hotel guests to trespass and block traffic with overflow parking.

## 4. Environment

The environmental impact of this hotel with its 112 rooms, and its proximity to the creek, and the run off it will produce, which will go directly into the Waterways of California, is incompatible with the state of California and any thoughts of sustainability. California Park's lakes and ponds are constantly monitored by Fish and Game to make sure we comply with chemical and run off regulations.

This hotel is not an appropriate spot for a multi-use hotel. I am surprised that such a huge environmental burden/footprint, is being proposed on such a small parcel - so near the creek.

This hotel will be too close to the creek and is not an appropriate place for this size residency.

#### 5. 24-hour Lighting and Signage Pollution

This hotel will be right in the view sight of many homes, with **signage** and extreme **24-hour business lighting**. This signage and light pollution are not conducive to a neighborhood environment. This is our home.

This hotel would be an eye sore for many California Park residents.  
The bright lights of this 4 story, 24 hour business is not appropriate for our neighborhood.

We think that Marriott's advertising campaign will use pictures of the lake to make this location more desirable, maybe that is why they picked this site. This will not be allowed. Our neighborhood is not a destination. It is our home.

With so many environmental, safety, privacy, traffic and neighborhood issues I would like the Planning Commission to look extremely hard at not approving this hotel for this location.

Thank you for your time.

Sandra S. Makau  
2603 Lakewest Dr.  
Chico, 95928

Planning Dept.  
411 Main Street  
Chico CA 95928

June 8, 2020

Really? A large hotel at the corner of Bruce Road and Sierra Sunrise??? As if there isn't enough traffic already on Bruce Road!

Another traffic signal there?

I don't know how anyone could have considered this a good idea. There are lots of spaces in and around Chico for this type of business.

Please please please reconsider!

A handwritten signature in black ink, appearing to read 'F Perata', with a stylized, cursive script.

Frances Perata  
2651 Lakewest Dr  
Chico 95928



**Lorraine Kurtela  
2750 Sierra Sunrise Terrace, #216  
Chico, Ca 95928**

**RECEIVED**

MAR 14 2022

CITY OF CHICO  
PLANNING SERVICES

**March 18, 2020**

**Janet Pearce  
7 Catalina Point  
Chico, CA.**

**Dear Janet:**

**As you can see, I live at the Terraces. We are Independent residents living here. Next door is the memory care facility, and around the corner is the Assisted Living facility. In all, there are approximately 250 residents**

**I attended the "NO HOTEL" meeting on March 2<sup>nd</sup>. As retirees, we saved our money to retire in a nice quiet location and facility. The traffic that this hotel will create will cause so much congestion. Congestion leads to SAFETY issues, especially in cases of emergencies and natural disaster evacuations. One such example of natural disaster evacuation occurred at the time of the Camp Fire in November 2018. All residents from the Westmont facilities were evacuated. Had a hotel been on the proposed site, the evacuation procedures would have been severely hampered.**

**Sierra Sunrise is the main road traveled for emergency vehicles getting to and from the Westmont Terraces facilities. Time is of the essence in getting patients to and from the hospital. Exiting from the hotel on to Bruce Road will be a nightmare for all Chico residents, as well as slow down emergency vehicles.**

**We have a beautiful lake on our side of the road. People staying at the hotel will no doubt take a walk around this PRIVATE property. We keep it clean and maintain its beauty through homeowner fees. We do not want hotel guests roaming on the private property, which will diminish the quiet solitude we expect and pay for.**

**Most Chico hotels are located in business districts, not in residential areas. This is another reason to relocate this proposed project to another location.**

**Environmental issues were also brought up at this meeting, as were water drainage concerns.**

**For the simple reasons stated, it is a concern and a bad idea for the City of Chico to allow a hotel on the current proposed site:**

**Lorraine Kurtela  
President Resident Council**

|                  |                   |
|------------------|-------------------|
| Kenneth Hyman    | Barbara Carney    |
| Beverly Dee      | Patty Loomer      |
| Josephine Adelfa | Patricia Collins  |
| Klaus & Ted      | Georgina Summers  |
| Catherine Sony   | Betty J. Callen   |
| Chas L. Pearl    | Edaine Kuhl-Brock |
| Carla E. Miles   | Ken Brock         |
| Evelyn D. Junker | Barbara Sutton    |
| Norene Solder    | Berry Drexler     |
| Kay Bowers       | Margaret Audberry |
| Freda Thorksson  | Jamie L. Nelson   |
| Loraine Kustan   | Hilda Blument     |
| Joyce Matthews   | Judith A. Clark   |
| Leila Anne Pina  | Barbara Mc Bride  |
| Hilda Luoma      | Wardene Moratti   |
| Kay Witt         | Jane Jarule       |
| Arthur B. Jr.    | Arlene Colman     |
| Liz King         | Ryan Matogunas    |
| Louise Cummins   | Wile West         |
| Marilyn Hall     | Fredy Soares      |
| Pete Feder       | Orin - Kay Parks  |
| Aloha J. Long    |                   |
| Jan Hauke        |                   |

Louise Kurita

Mary Mc Ling

Faye Edwards

Bru Maniscalco BRU MANISCALCO

Faye Brown

Margaret H. Haqueve

Betty Leblant

Janet Rose  
Jesse Rose

Norma DeGrado

Daniel DeGrado

Russell

Dickie Haand

Dickie

Bonnie M. Brown

Meg De Rose

Delyssa Weingarten

Sandra Brimmer

Chva Lee

Betty Reed

K. Ann

Patricia McDonald

Jane Sutta

Megan Crowston



Daisy Goodsell

Fanny D. Yaska

Maurice Glade

Rose Lunak

Irene Barrera

~~SAH~~

Evangelina Magallon

Judy

Nope Weintz

Anne Loomer

Jan Mafij

Dorothy Alden

Hannelore Kleimann

Bess Dempsey

M. W. [unclear]

Bonnie Evans

Bern [unclear]

Virginia Yaska

## Dexter O'Connell

---

**From:** Diane Robel <disco60zena@gmail.com>  
**Sent:** Thursday, March 12, 2020 11:07 AM  
**To:** Bruce Ambo; Mike Sawley; Dexter O'Connell  
**Subject:** Marriott Residence Hotel

**ATTENTION:** This message originated from outside **City of Chico**. Please exercise judgment before opening attachments, clicking on links, or replying.

We are urging you to deny the request for a CUP to Marriott. The location is not appropriate for a hotel, nor does it do anything to help or support the surrounding residential communities.

The impact on over-burdened Bruce Road and Hwy 32 would make traffic worse. If Marriott deems Chico to be a good demographic fit, they should have chosen a spot in or closer to the downtown area, where restaurants and stores could benefit from their guests.

Please deny the CUP and prevent further progress on this project.

Diane and Jess Robel  
2370 Farmington Avenue  
Chico

March 10, 2020

City of Chico Community Development Department  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
[zoning@chicoca.gov](mailto:zoning@chicoca.gov)

RE: Evacuating Senior Residents in Sierra Sunrise Village

Attention: City Planners

In the letters sent to the City Planners we have raised concerns over the Use Permit Request – APN 018-230-001 for a 4-story 112 room hotel known as the Townplace Suites by Marriott, given its proximity to Chico’s largest Senior Citizen Community. We have not wavered in our opposition towards the construction of a 4-story 112 room hotel on a private street intended to service the 800+/- seniors who live in this community.

We continue to believe that a 112-room hotel on Sierra Sunrise Terrace is incompatible with a senior residential community. We’ve documented the neighborhood demographics, our traffic safety issues, and the overall need to protect our Senior Citizens living in Sierra Sunrise Village - especially during a life-threatening disaster. (Ref: CITY OF CHICO CAMP FIRE TRAFFIC IMPACT report and California State Auditor’s report of December 2019, entitled, California Is Not Adequately Prepared to Protect Its Most Vulnerable Residents from Natural Disasters).

It’s a massive undertaking to evacuate residents who have transportation disadvantages, and especially those with disabilities.

For example the residents living at The Terraces, an independent senior living facility with 118 rooms, at Compass Rose a 28-unit memory care unit, and at The Inn a 99-room assisted living unit, were all notified of the fire in Paradise and were told if you have someone you’ve been wanting to visit or someone who could pick you up, you should make those arrangements now. Otherwise, go pack 1 bag in preparation of a possible evacuation.

That morning, a call from Paradise came into The Terraces Director’s office asking for evacuation help with 2 facilities. Brad Azevedo, Director of The Terraces, without hesitation, sent their two facility buses equipped with lifts to Paradise. While there, they stopped to evacuate yet another facility, still not knowing the extent of the fire. They later found Skyway blocked and that people had abandoned their vehicles and were fleeing on foot; not an option for these passengers. Other drivers might have fled to save their own lives but not these two heroes. They put their buses in drive and hit the gas plowing through every obstacle. Along the way they opened the bus doors and told people in the streets to get in. Through the grace of God everyone made it down safely.

The Director of those 3 Westmount Living properties chose to evacuate when the Cal Fire put out the evacuation warning to residents north of Bruce Road east of Hwy 32. Two previously reserved charter buses arrived at 8:00 o'clock pm and residents were then directed or assisted to buses based on their disability. Those with wheelchairs were evacuated in the 2 facility buses equipped with lifts. The others were transported on the charter buses, including dogs and crated cats.

Medical records and medication for the Compass Rose and The Inn residents were loaded into the back of the Director's pick-up truck. The bus bays were designated for wheelchairs and walkers, etc. so a large U-Haul truck was used for suitcases, mattresses, and bedding as the residents were being transported to another facility where residents supply their own furniture.

We don't know when or where the next natural disaster will happen so we should take every opportunity to re-evaluate all evacuation plans, routes, and infrastructure in order to ensure that Chico is adequately prepared to protect its most vulnerable residents from natural disasters.

So again, we ask the City Planners to consider the **safety** and **security** of the Sierra Sunrise Village Seniors and their need to protect their private street, bridge, common property, lake and overall tranquility of their neighborhood and urge that you deny the Conditional Use Permit for this project.

Sincerely,

Valerie Priola

Valerie Priola  
7 Valley Lake commons  
Chico, California 95928

March 10, 2020

City of Chico Community Development Department  
c/o Bruce Ambo, Chico City Principal Planner  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
[bruce.ambo@chico.gov](mailto:bruce.ambo@chico.gov)

RE: Evacuating Senior Residents in Sierra Sunrise Village

Attention: City Planners

In the letters sent to the City Planners we have raised concerns over the Use Permit Request – APN 018-230-001 for a 4-story 112 room hotel known as the TownPlace Suites by Marriott, given its proximity to Chico's largest Senior Citizen Community. We have not wavered in our opposition towards the construction of a 4-story 112 room hotel on a private street intended to service the 800+/- seniors who live in this community.

We continue to believe that a 112-room hotel on Sierra Sunrise Terrace is incompatible with a senior residential community. We've documented the neighborhood demographics, our traffic safety issues, and the overall need to protect our Senior Citizens living in Sierra Sunrise Village - especially during a life-threatening disaster. (Ref: CITY OF CHICO CAMP FIRE TRAFFIC IMPACT report and California State Auditor's report of December 2019, entitled, California Is Not Adequately Prepared to Protect Its Most Vulnerable Residents from Natural Disasters).

It's a massive undertaking to evacuate residents who have transportation disadvantages, and especially those with disabilities.

For example, the residents living at The Terraces, an independent senior living facility with 118 rooms, at Compass Rose a 28-unit memory care unit, and at The Inn a 99-room assisted living unit, were all notified of the fire in Paradise and were told if you have someone you've been wanting to visit or someone who could pick you up, you should make those arrangements now. Otherwise, go pack 1 bag in preparation of a possible evacuation.

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Other drivers might have fled to save their own lives but not these two heroes. They put their buses in drive and hit the gas plowing through every obstacle. Along the way they opened the bus doors and told people in the streets to get in. Through the grace of God everyone made it down safely.

The Director of those 3 Westmount Living properties chose to evacuate when the Cal Fire put out the evacuation warning to residents north of Bruce Road east of Hwy 32. Two previously reserved charter buses arrived at 8:00 o'clock pm and residents were then directed or assisted to buses based on their disability. Those with wheelchairs were evacuated in the 2 facility buses equipped with lifts. The others were transported on the charter buses, including dogs and crated cats.

Medical records and medication for the Compass Rose and The Inn residents were loaded into the back of the Director's pick-up truck. The bus bays were designated for wheelchairs and walkers, etc. so a large U-Haul truck was used for suitcases, mattresses, and bedding as the residents were being transported to another facility where residents supply their own furniture.

We don't know when or where the next natural disaster will happen so we should take every opportunity to re-evaluate all evacuation plans, routes, and infrastructure in order to ensure that Chico is adequately prepared to protect its most vulnerable residents from natural disasters.

So again, we ask the City Planners to consider the **safety** and **security** of the Sierra Sunrise Village Seniors and their need to protect their private street, bridge, common property, lake and overall tranquility of their neighborhood and urge that you deny the Conditional Use Permit for this project.

Sincerely,

A handwritten signature in black ink that reads "Valerie Priola". The signature is written in a cursive style with a large, looped initial "V".

Valerie Priola  
Sierra Sunrise Village, President



## Dexter O'Connell

---

**From:** Bruce Ambo  
**Sent:** Monday, March 9, 2020 9:29 AM  
**To:** Victor Estrada  
**Cc:** Dexter O'Connell  
**Subject:** RE: Hotel California Park to be built on corner of Bruce Rd and 32

Hello,

Thank you for your comments. I have copied Dexter O'Connell, Associate Planner, who is handling the review of this project and keeping track of the comments and correspondence on this project. The project applications (Use Permit and Architectural Review) remain incomplete at this time. The applicant team has indicated that they are close to a resubmittal that will include minor modifications to the site plan as request by Planning staff, and a traffic study as requested by Traffic Engineering.

Upon resubmittal, City staff has 30 days to review the application for completeness. This will include Traffic Engineering concurring/accepting the traffic study and providing comments/conditions/recommendations based on the results. If further information is needed, staff will make a request to the applicant team and the project will remain incomplete. Upon a complete submittal, the application is deemed complete. Planning staff will then move forward with the environmental review of the project. As the lead agency, staff is responsible for determining the appropriate level of CEQA review. Given the variety of issues that have been raised, including from attorneys from the various homeowners association, this process could take up to three months to complete, and additional to circulate documents for public review and comment.

Upon completion of the environmental review, staff will begin scheduling the project for public hearings. Pursuant to the City Municipal Code, the Architectural Review and Historic Preservation Board will first review the site design and architecture of the proposal and make a recommendation to the Planning Commission. The Planning Commission will make a final decision regarding the use permit, site design and architecture, and the CEQA determination. Planning staff will send notices regarding meeting dates/times to landowners/occupants within 500 feet of the project site. As of today, approximately 40 people have declared themselves as interested parties, and they would also be notified of meeting dates. The project would only go before the City Council upon an appeal of the Planning Commission's decision.

At this time, there are no specific actions being taken by the City on the proposed project. The community will be notified when meetings are scheduled.

Regards

Bruce Ambo, AICP  
Principal Planner  
City of Chico, Community Development  
530/879-6801  
411 Main Street, Chico, CA 95928  
PO Box 3420, Chico, CA 95927

<http://www.ci.chico.ca.us/>

-----Original Message-----

From: Victor Estrada <pvestrada98@aol.com>

Sent: Monday, March 9, 2020 9:10 AM  
To: Bruce Ambo <bruce.ambo@Chicoca.gov>  
Subject: Hotel California Park to be built on corner of Bruce Rd and 32

ATTENTION: This message originated from outside City of Chico. Please exercise judgment before opening attachments, clicking on links, or replying.

Hi Bruce:

My name is Patricia Estrada. I reside in the Nob Hill/Husa Ranch Division. I am writing you to inform you that I am not encouraged about the possibility of the Hotel that may be built on the corner of Bruce Road and 32.

My reasons for this are several:

1. I do not feel that this property is zoned for this type of commercial building.
2. I am already seeing a huge increase in traffic on Bruce Road extending to Road 32.  
I feel that this Hotel will bring more of that congestion to the area. That corner of 32 and Bruce Road is dangerous because people traveling on 32 travel at speeds upward of 55.
3. When the Camp Fire happened in 2018, the evacuation of facilities was difficult. I feel adding another 4 story complex to this issue is not a viable effort of goodwill or intelligence.
4. The closeness of the lake and the creek behind the proposed building site also is of concern to me.

I sincerely hope that these things will be considered when the planning dept reviews the proposed permits for this building. There are other concerns that other people that live in this area. I know that you will be hearing from them also. This area is not the best choice for a hotel.

Thank you for allowing me to present my input and reading my concerns. Have a nice day.

Patricia Estrada  
Retired Registered Nurse  
Chico, Calif.

## Dexter O'Connell

---

**From:** Sandy Mayer <seemay52@gmail.com>  
**Sent:** Saturday, March 7, 2020 12:49 PM  
**To:** Dexter O'Connell  
**Subject:** NO HOTEL

ATTENTION: This message originated from outside City of Chico. Please exercise judgment before opening attachments, clicking on links, or replying.

Dear Mr. O'Connell,

I am writing to express my opposition to the Marriott Hotel which has been proposed, at the corner of Bruce Rd. and Hwy 32. I am shocked! The property is zoned Community/Commercial and the access road to this proposed Hotel is the only path current seniors living in this area use. This area has not even felt the impact of the traffic which will be horrendous when the luxury apartments are complete and full of residents (8th and Bruce Rd). Money talks, yes it does, but this is no place for a 4 story, 5 color, block style Hotel!!! (what is with all these ugly buildings anyway? That are being built all over CHICO?) Please consider our senior population, don't approve this location. Let's approve a Hotel is a more suitable location. I live in Nob Hill, and travel Bruce Rd. every day. It's already a nightmare! Thank you for consideration in advance. Sincerely, Sandy Mayer

Sent from my iPhone

Sent from my iPhone

Sent from my iPhone

## Dexter O'Connell

---

**From:** Tally Sturm <ttreyster@gmail.com>  
**Sent:** Wednesday, March 4, 2020 8:50 PM  
**To:** Dexter O'Connell  
**Subject:** No Hotel Californi Park

ATTENTION: This message originated from outside City of Chico. Please exercise judgment before opening attachments, clicking on links, or replying.

Dear Mr. O'Connell,

Though you are new to the hotel project proposal, not certain if you are also new to Chico, but as an old time resident I am submitting this letter which was sent to the Chico City Council. It is for your perusal and understanding. As you know the proposed hotel site will solely be accessible by a private road in a senior citizen private, residential community. Sierra Sunrise Terrace exits onto a busy four lane street, Bruce Road.

Dear City Council:

In your mad scramble to build more and more housing in Chico, it appears you have lost sight of the very reason people found it desirable to live here. Simply increasing the population without regard to easy community access by foot rather than car, creates the type of city from which all the people have fled!

Specific point in question is on Highway 32, Bruce Road and California Park. Put the "Village" back in (or near at this point) Cal Park by offering walkable, useful businesses such as produce market, bakery, coffee/wine bar, or an independent restaurant. Do your homework and you will discover that the earlier, enlightened concept has been obliterated by large buildings housing types of enterprises which are not conducive to the original plan of California Park and are not useful to residents who pay for the lakes and paths. You had a chance to rectify the mistake on one of the last pieces of property, yet chose to build even more jammed housing for hundreds on the busy corner of California Park Drive and Manzanita.

Now you are pondering what to do with acreage on the edge of California Park and you are considering MORE housing and a ridiculous four story hotel which will just increase traffic further and offer nothing to existing residents but cement, frustration, anxiety and accidents. Confirming a glaring 24 hour Arco was an erroneous choice yet brought money to your coffers which appears to be the sole reason for most of your decisions.

Please be wise and consider the beauty, environment and existing residents before dashing to build another San Jose, San Francisco or Los Angeles. The population will flee to greener pastures as they have been doing, and leave Chico a wasteland of asphalt and high rises.

Sincerely,

Tally Sturm  
2859 Pennyroyal Drive  
Chico, CA 95928

(530) 798-6144

## Dexter O'Connell

---

**From:** Bob Kromer <bobkro99@gmail.com>  
**Sent:** Wednesday, March 4, 2020 11:23 PM  
**To:** Dexter O'Connell  
**Subject:** Cal Park Hotel

ATTENTION: This message originated from outside City of Chico. Please exercise judgment before opening attachments, clicking on links, or replying.

Welcome to the project. You said you welcome input so here goes.

A 4 story hotel on a private street located in very close proximity to multiple Senior Living facilities and a Memory Care facility is the wrong location.

Traffic in the surrounding area of Bruce Rd, Hyw 32 and Sierra Sunset is already congested before this hotel is built not to mention the 120 soon to be completed apartments at Bruce and California Park Blvd. Add in all the extra traffic on Bruce from Marion Park and the housing along the Eaton Extension it just doesn't make sense and creates issues of traffic/pedestrian/bicycle safety.

The original community plan calls for the parcel in question to be developed for services compatible with a residential area and Senior Community. The Chico Municipal Code mentions the appropriateness of development/location as well as the enhancement of community values. This location doesn't fit these well thought out goals.

Some 250 neighbors attended the Informational Mtg on Monday night with 99% in opposition to this hotel location. 112 rooms plus a staff of ~30 and a small meeting facility of 12-15 people requires more than 117 parking spaces. Street parking is not a viable option due to congestion and emergency vehicle access (happens a lot on this street).

This hotel built somewhere else like Marion Park or near the Mall or near downtown where services exist like restaurants, bars and shops makes so much sense. Not having these services near by will just increase daily vehicle trips/congestion.

If this hotel is built it should be limited to 2-3 stories and be a maximum of 70 rooms with 90-95 parking places. All of the other neighborhood office buildings, apartments and Enloe Prompt Care are 2-3 stories.

I was distressed to learn the development company has already purchased the parcel before the zoning/use permit/variances/traffic study/environmental, CEQA study/planning approvals have been completed. Why would they buy it without placing multiple contingencies on the purchase pending all of these necessary approvals?

I object to the fact that the grading permit approved 2 years ago has "morphed" into the current project proposal.

Please add me to your "interested neighbor" email list for all future meetings regarding this project.

Thank You,  
Bob Kromer  
592-9669



RECEIVED

FEB 27 2020

CITY OF CHICO  
PLANNING SERVICES

## NO HOTEL

I would like to share with you some anecdotal stories from a few short / long term residents at the Marriott Hotel on the corner of Bruce road and Sun Rise Terrance lane.

- 1- Checking in resident, left trunk open. Stole all my stuff. Was up for the CSU graduation. Looking back some suspicious looking guys fishing by the water fall, come to think of it they had no fishing gear. I made a report to the CPD and have filed a claim against Marriott. Waste of time.
- 2- Am in the military, stationed overseas and my daughter goes to CSU. Came up for a 2 month stay to visit her. Knew Chico had great biking in BP so I brought my bike. It's a high-speed Trek and locked it up in the secure hotel bike rack and it was gone within 2 days. It was locked up very well

but they busted into the bike storage area. I reported it to the CPD for whatever that is worth. I have filed a claim with my insurance company and against Marriott.

- 3- Up to a CSU graduation and when going out to the event, the hotel staff said to make a right and go through the retirement community. My wife and I got part way up and were going downhill but at the corner of Amanacida Common and Sierra Sunrise Terrace Road there were at least 3 emergency vehicles at the retirement center. A very congested corner with cars parked all over that corner. There must have been a large emergency and they were blocking the road for at least 30 minutes. But it is to be expected as it is an adult retirement community with a Rehab Center, and an Alzheimer Facility. We missed a major part of the graduation ceremony and did not see our granddaughter graduate. Very

irritating. I am making financial demands on Marriott for missing the graduation event.

- 4- I am up for a CSU conference on Construction Engineering and brought my bike to ride in the wonderful BP bike paths. Upon arrival I needed to stretch my legs so I began a 30-minute walk outside the hotel. Not much safe, good walking at all left or right outside the hotel. Looking across I did see an elevated cinder pathway and stairs that led to a lovely lake and long cinder path. It was a terrific walk and right outside my door at the hotel! Perfect. I even noticed folks boating on the lake. Maybe I can rent a boat or take a swim.
- 5- Up to CSU for an international criminology conference and brought my bike to use in BP. I did notice a nice loose cinder walking / biking path directly outside the hotel across the street, very convenient. The

path went around a lovely lake which connected to another lake and path. Saw all sorts of water fowl and several beautiful otters. The next evening, I got my bike out and had a great ride around both lakes. On the way back, I hit some loose cinder on a curve and went down hard and fell down the embankment to the edge of the lake. Am badly banged up with a broken collar bone and mild concussion. I have contacted my attorney about filing a lawsuit with the owner of the lake and path.

And I could go on and on with these hypothetical cases. But they will come to pass if the hotel is built and we the retirees, on a fixed income, will have to pay for all changes made to the the intrusive guests at the Marriott hotel. This will come to pass. And long term guests will have young children and they will become a pain in the ass, to boot.

*T. Wilson*

Terrance Wilson DVM  
2679 Fairfield Commons  
Chico CA 95928

February 24, 2020

City of Chico Community Development Department  
P.O. Box 1420  
Chico, CA 95927

NorthStar  
111 Mission Ranch Blvd., Ste. 100  
Chico, CA 95926

RECEIVED

MAR 06 2020

CITY OF CHICO  
PLANNING SERVICES

RE: Use Permit Request – APS 018-230-001  
Marriott Hotel /California Park area

The Marriott Hotel is a fine product with an excellent reputation and will be a welcome addition to Chico and its future. But, the egress constraints on this site should cause wise heads to look elsewhere to bring Marriott to Chico.

The current uncontrolled access onto Bruce Road from Sierra Terraces Rd. is manageable because the area served, is primarily a Retirement Community. The residents do not generally utilize the intersection during heavy traffic periods. This noted, the egress from the neighborhood is already difficult and egress at busier traffic times already frequently requires a right turn onto Bruce, with a U-turn executed at the traffic light at Bruce Rd/Lakeside West.

The Marriott project at this location requires intensive vehicular access for both guests and staff. The 4 pm check-in time of a Hotel operation will aggravate and very possibly cause failure to these two key arterial roads (Bruce & Hwy32) during commute times.

Unfortunately, solutions to solve access constraints -- traffic light or roundabout -- are not available at this site for several reasons.

- (1) CDOT limits access permits onto Hwy 32.
- (2) A traffic light at the intersection of Sierra Terraces and Bruce Road would heavily impact the flow on the arterial roadways (Hwy 32 and Bruce Rd) during commuter times. A light so close will likely cause backups onto both Arterial roadways during heavy traffic periods with possible failure.
- (3) A round-is precluded due to intersections proximity to California Park Lake's earthen dam and the riparian area.

  
Michelle Roy  
21 Sierra Lakeside Ln.  
[shellieroy@icloud.com](mailto:shellieroy@icloud.com)

cc: Sierra Lakeside HOA



February 23, 2020

City of Chico Community Development Department  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
[zoning@chicoca.gov](mailto:zoning@chicoca.gov)

RE: Traffic on Bruce Road at Sierra Sunrise Terrace

Attention: City Planners

In the letters sent to the City Planners we have raised concerns over the Use Permit Request – APN 018-230-001 for a 4-story 112 room hotel known as the Townplace Suites by Marriott, given its proximity to Chico's largest Senior Citizen neighborhood. We have been vehemently opposed the construction of a 4-story 112 room hotel on a private street intended to service the 800+/- seniors who live in this community.

We continue to believe that adding a 112-room hotel on Sierra Sunrise Terrace would exacerbate the existing traffic problems especially at the intersection of Bruce Road and Sierra Sunrise Terrace. We've seen an increase of traffic and aggressive behavior not limited to speed on Bruce Road since the Camp Fire. According to the CITY OF CHICO CAMP FIRE TRAFFIC IMPACT report ([http://www.chico.ca.us/building\\_development\\_services/traffic/documents/ME\\_MOCampFireTrafficImpacts.pdf](http://www.chico.ca.us/building_development_services/traffic/documents/ME_MOCampFireTrafficImpacts.pdf)) the traffic along Bruce Road at Lakewest Drive has increased 35%. If measured today we might also see an additional increase coming south from Eaton and Manzanita Avenue corridor. We've had two very serious north bound traffic collision on Bruce Road at Sierra Sunrise Terrace; one was a rollover on November 4, 2019 (City of Chico Rpt. # 19008125/1911050013) and the other, on November 29, 2019 (City of Chico Rpt. # 19008657), was a two car collision which left one car straddled on the center divider and the other vehicle perched on the sidewalk. Overall valley accidents, according to the same Camp Fire Report, are up 49.8% since the Camp Fire. The subjective idea that adding a signal at Bruce Road and Sierra Sunrise Terrace solves this safety issue is ill conceived!

The property on the adjacent north west corner owned by 32<sup>nd</sup> & Bruce Partners, LLC has advertised approx. 20.28 acres of approved med-high density apartment and commercial land. The 11.5-acres of residential land is zoned R3 and R4 which is divided equally between R3 and R4. R3 zoning allows for 14-22 units per acre. R4 zoning allows for 20-70 unit per acre. The zoning allows for 2 or 3 story apartments and 40,000 SF of retail/commercial. An access road will have to be constructed between the two (residential/commercial) projects. The access will be from Bruce Road and will be signalized, with city reimbursing 75% of the cost. That means the Bruce Road intersection at Sierra Sunrise might need to accommodate approx. **506** apartments (not residents), **353** senior residences from Sierra Sunrise Village and **112+** from the hotel, if approved. That's a lot of people for one signal in a location already considered to be the neck of a bottleneck!

The signal at Sierra Sunrise Terrace will only add to the traffic and undesirable traffic behavior on Bruce Road because adding a signal there would disrupt the normal traffic flow of might otherwise be considered thorough-fare. Sierra Sunrise Terrace is only approximately 285 feet off Hwy 32. The speed limit on Bruce Road is 45 mph. A vehicle going 45 mph travels 300 feet in 5 seconds and requires 132 feet to safely stop. Vehicles using that short stretch of Bruce Road must contend with the sheer number of vehicles, abrupt lane changes, and a mix of decelerating and accelerating traffic speeds. Vehicles entering the intersection from the east off Sierra Sunrise Terrace are confronted with the same traffic assessment but for 4 lanes. Risk factors are higher leaving Sierra Sunrise Terrace than coming in because the drivers 4 lane assessment can change dramatically in 5 seconds!

So again, we asked the City Planners to consider the **safety** and **security** of the Sierra Sunrise Village Seniors and their need to protect their private street, bridge, common property, lake, lake views and overall tranquility of their neighborhood when considering the Use Permit for this project.

Sincerely,

Valerie Priola

Valerie Priola  
7 Valley Lake Commons  
Chico, California 95928

February 23, 2020

City of Chico Community Development Department  
c/o Bruce Ambo, Chico City Principal Planner  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
[bruce.ambo@chico.gov](mailto:bruce.ambo@chico.gov)

RE: Traffic on Bruce Road at Sierra Sunrise Terrace

Attention: City Planners

In the letters sent to the City Planners we have raised concerns over the Use Permit Request – APN 018-230-001 for a 4-story 112 room hotel known as the TownPlace Suites by Marriott, given its proximity to Chico's largest Senior Citizen neighborhood. We have vehemently opposed the construction of a 4-story 112 room hotel on a private street intended to service the 800+/- seniors who live in this community.

We continue to believe that adding a 112-room hotel on Sierra Sunrise Terrace would exacerbate the existing traffic problems especially at the intersection of Bruce Road and Sierra Sunrise Terrace. We've seen an increase of traffic and aggressive behavior not limited to speed on Bruce Road since the Camp Fire. According to the CITY OF CHICO CAMP FIRE TRAFFIC IMPACT report ([http://www.chico.ca.us/building\\_development\\_services/traffic/documents/MEM\\_OCampFireTrafficImpacts.pdf](http://www.chico.ca.us/building_development_services/traffic/documents/MEM_OCampFireTrafficImpacts.pdf)) the traffic along Bruce Road at Lakewest Drive has increased 35%. If measured today we might also see an additional increase coming south from Eaton and Manzanita Avenue corridor. We've had two very serious northbound traffic collisions on Bruce Road at Sierra Sunrise Terrace; one was a rollover on November 4, 2019 (City of Chico Rpt. # 19008125/1911050013) and the other, on November 29, 2019 (City of Chico Rpt. # 19008657), was a two car collision which left one car straddled on the center divider and the other vehicle perched on the sidewalk. Overall valley accidents, according to the same Camp Fire Report, are up 49.8% since the Camp Fire. The subjective idea that

adding a signal at Bruce Road and Sierra Sunrise Terrace solves this safety issue is ill conceived!

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So again, we ask the City Planners to consider the **safety** and **security** of the Sierra Sunrise Village Seniors and their need to protect their private street, bridge, common property, lake, lake views and overall tranquility of their neighborhood when considering the Use Permit for this project.

Sincerely,



Valerie Priola

Sierra Sunrise Village, President

Valerie Priola  
7 Valley Lake Commons  
Chico, California 95928

February 18, 2020

City of Chico  
c/o Bruce Ambo, Chico City Principal Planner  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
[bruce.ambo@chico.gov](mailto:bruce.ambo@chico.gov)

RE: Hotel Parking (Use Permit APN 018-230-001)

Attention: City Planners

We are writing to oppose the parking plan currently being presented for the proposed Town Place Suites by Marriott. We believe that the allotted plan for on-site parking spaces is less than adequate to accommodate a hotel of this size. The parking plan doesn't even meet the minimum code for Butte County (Table 24-93-1 On-site parking requirement). The current code requires hotels to provide 1 parking space per room plus 1 space per 300 sq. ft. of office space. The current parking plan has accommodations for 112 spaces, which includes restricted parking for handicapped persons, charging stations, clean air, and bicycles. It fails to provide for office space, staffing, service vehicles or expected hotel clientele who drive vehicles that don't fit into the standard vehicle parking space.

We ask the City Planners to consider the deficient parking allotments for this hotel and the fact that the hotel would be built on a private street servicing Chico's largest Senior Community when considering the Use Permit for this project.

Sincerely,



Valerie Priola  
Sierra Sunrise Village, President

RECEIVED

FEB 27 2020

CITY OF CHICO  
PLANNING SERVICES

February 17, 2020.  
SHANNON COSTA  
ASSOCIATE PLANNER  
COMMUNITY DEVELOPMENT-PLANNING DIVISION  
411 MAIN STREET, 2<sup>ND</sup> FLOOR  
PO BOX 3420  
CHICO CA 95927

CITY PLANNER-MARRIOTT HOTEL

I live at 2679 Fairfield Commons, Chico 95928

CRIME will increase in my area. Parked cars will increase in an already overcrowded car parking neighborhood.

EMERGENCY VEHICLES common on my street, very common; ON MY STREET of 25 houses: 1 MOBILE SCOOTER, 3 WHEEL CHAIR, 5 WALKERS, SEVERAL CANES, MANY SLOW WALKERS, pace makers galore. Quietly I call it the cripple street ! THIS IS AN ADULT COMMUNITY.NO SIDE WALKS on my street, WE WALK IN THE STREET.

HOTEL GUESTS WILL USE CINDER PATHS AROUND LAKE FOR WALKING AND BIKING.



HOTEL GUESTS WILL USE LAKE FOR BOATING ETC, maybe even swimming despite any signage to the contrary!

RELIABLE SOCIAL SCIENCE STUDIES SHOW CRIME INCREASES WHEN A HOTEL MOVES INTO A RESIDENTIAL / SENIOR CITIZEN NEIGHBORHOOD SUCH AS OURS, check it out.

PARKING PROBLEMS, WE HAVE PLENTY OF THAT ON VERY CONGESTED AMANECIDA COMMON AND SIERRA SUNRISE RD INTERSECT WHERE THE WORKERS AT THE RETIREMENT CENTER PARK ALL OVER THE PLACE.

SERVICE VEHICLES WILL USE OUR PRIVATE ROAD! THE INTERSECTION OF SERRIA SUNRISE TERRANCE AND BRUCE RD IS NOW A BRUTAL NIGHTMARE WHEN TURNING LEFT, HOTEL WILL MAKE IT MUCH WORSE

EMERGENCY EVACUATION NOW DIFFICULT.

A HOTEL BUILT IN THIS AREA WILL MAKE IT MUCH WORSE

NO ADDED BENEFIT TO US, HOWEVER, SIGNIFICANT DISADVANTAGES FOR US.

KIDS AT THE HOTEL WILL CAUSE US A PROBLEM.ITS A 55+ COMMUNITY.

BAD / LOUSY / CRAPPY / MISERABLE / SHABBY /  
UNHAPPY / CHEERLESS / GLUM / DISMAL /  
WRETCHED / WOEFUL / CONTEMPTABLE /  
CHEERLESS / LOW / DEJECTED / ROTTEN / IDEA. DO  
YOU AND MARRIOTT GET THE MESSAGE?  
GO SOME WHERE ELSE

*T Wilson*

TERRANCE M WILSON DVM  
2679 Fairfield Commons  
Chico, CA 95928

Valerie Priola  
7 Valley Lake Commons  
Chico, California 95928

February 17, 2020

City of Chico Community Development Department  
c/o Bruce Ambo, Chico City Principal Planner  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
[bruce.ambo@chico.gov](mailto:bruce.ambo@chico.gov)

RE: East Foothills Evacuation Planning

Attention: City Planners

In the letter I sent to the City Planners dated February 6, 2020, I raised concerns over the Use Permit Request – APN 018-230-001 for a 4-story 112 room hotel known as the TownPlace Suites by Marriott, given its proximity to Chico's largest Senior Citizen neighborhood. Our opposition to locating the proposed hotel on a private street intended to service the 800+/- seniors who live in this community has not changed.

We have asked the City Planners to consider the safety and security of the Sierra Sunrise Village Seniors and their need to protect their private street, bridge, common property, lake, lake views and overall tranquility of their neighborhood when considering the Use Permit for this project.

Our residents are not opposed to another hotel in Chico. We just believe it should be built in a location better suited to its target clientele, where infrastructure and services already, or will soon, exist to support it.

What has changed, however, is our recent awareness of the City of Chico's plans to focus on mid-high-density housing on Bruce Road between Skyway and Hwy 32, and other potential undefined properties and real estate developments currently being advertised. I estimate that in the next 10 years the population along Bruce Road will exceed 7,000 to 8,000 residents. When you add in all the planned

commercial development into this 2-mile stretch of road it will struggle with enormous traffic congestion and increased public safety risks.

The State Auditor's Report (Ref: Auditor of the State of California Report 2019-103, dated 2019) concluded that Butte County is one of the 3 counties found not to be prepared to protect its residents from a natural disaster. The Auditor's Report focused on the most vulnerable populations but it also highlights the risk that all east side residents have in the event of a natural disaster. Clearly, we know we need a better plan to prepare for a natural disaster, but so does the City of Chico, Assembly member James Gallagher (R-3<sup>rd</sup> District) having said as much in a recent interview with KNCR.

Planned improvements to Bruce Road from Skyway to Hwy 32 and Hwy 32 from Bruce to Yosemite will help traffic flow but they are inadequate in effectively dealing with a natural disaster. The majority of the current and future residents along the Bruce corridor are, or will be, dependent on Bruce Road to maneuver to a thoroughfare that moves traffic East to West: 8<sup>th</sup> Street, Hwy 32, Humboldt Avenue, East 20<sup>th</sup> Street or Skyway; each of which are potential bottlenecks in themselves. Other roads that move traffic north and south such as Yosemite Drive, El Monte Avenue, Forest Avenue or Notre Dame Blvd. end up at the same bottlenecks - Hwy 32, East 20<sup>th</sup> Street, or Skyway. Those who manage to get on Hwy 32, East 20<sup>th</sup> or Skyway won't fare very well because they all funnel to Hwy 99 which is only a four lane Hwy and it can't even accommodate commuter traffic, much less the gridlocking traffic associated with a disaster!

A traffic study with a focus on the preparation for a disaster is needed NOW, not after we've built out Chico's 10 Year Plan. There won't be many options left if we wait until 2030. Continuing to build without a comprehensive Disaster and Evacuation Plan is unconscionable. A building moratorium should be immediately instituted until this is done!

Sincerely,



Valerie Priola

Sierra Sunrise Village, President

February 17, 2020

City of Chico Community Development Department  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
[zoning@chicoca.gov](mailto:zoning@chicoca.gov)

RE: East Foothills Evacuation Planning

Attention: City Planners

In the letter I sent to the City Planners dated February 6, 2020, I raised concerns over the Use Permit Request – APN 018-230-001 for a 4-story 112 room hotel known as the Townplace Suites by Marriott, given its proximity to Chico’s largest Senior Citizen neighborhood. Our opposition is to locating the proposed hotel on a private street intended to service the 800+/- seniors who live in this community has not changed.

We have asked the City Planners to consider the safety and security of the Sierra Sunrise Village Seniors and their need to protect their private street, bridge, common property, lake, lake views and overall tranquility of their neighborhood when considering the Use Permit for this project.

Our residents are not opposed to another hotel in Chico. We just believe it should be built in a location better suited to its target clientele, where infrastructure and services already, or will soon, exist to support it.

What has changed, however, is our recent awareness of the City of Chico’s plans to focus on mid-high-density housing on Bruce Road between Skyway and Hwy 32, and other potential undefined properties and real estate developments currently being advertised. I estimate that in the next 10 years the population along Bruce Road will exceed 7,000 to 8,000 residents. When you add in all the planned commercial development into this 2 mile stretch of road it will struggle with enormous traffic congestion and increased public safety risks.

The State Auditor's Report (Ref: Auditor of the State of California Report 2019-103, dated 2019) concluded that Butte County is one of the 3 counties found not to be prepared to protect its residents from a natural disaster. The Auditor's Report focused on the most vulnerable populations but it also highlights the risk that all east side residents have in the event of a natural disaster. Clearly, we know we need a better plan to prepare for a natural disaster, but so does the City of Chico, Assembly member James Gallagher (R-3<sup>rd</sup> District) having said as much in a recent interview with KNCR.

Planned improvements to Bruce Road from Skyway to Hwy 32 and Hwy 32 from Bruce to Yosemite will help traffic flow but they are inadequate in effectively dealing with a natural disaster. The majority of the current and future residents along the Bruce corridor are, or will be, dependent on Bruce Road to maneuver to a thoroughfare that moves traffic East to West: 8<sup>th</sup> Street, Hwy 32, Humboldt Avenue, East 20<sup>th</sup> Street or Skyway; each of which are potential bottlenecks in themselves. Other roads that move traffic north and south such as Yosemite Drive, El Monte Avenue, Forest Avenue or Notre Dame Blvd. end up at the same bottlenecks - Hwy 32, East 20<sup>th</sup> Street, or Skyway. Those who manage to get on Hwy 32, East 20<sup>th</sup> or Skyway won't fair very well because they all funnel to Hwy 99 which is only a four lane Hwy and it can't even accommodate commuter traffic, much less the gridlocking traffic associated with a disaster!

A traffic study with a focus on the preparation for a disaster is needed NOW, not after we've built out Chico's 10 Year Plan. There won't be many options left if we wait until 2030. Continuing to build without a comprehensive Disaster and Evacuation Plan is unconscionable. A building moratorium should be immediately instituted until this is done!

Sincerely,

Valerie Priola





City of Chico

**From:** Jeff Carter <[jeff@jicarterlaw.com](mailto:jeff@jicarterlaw.com)>  
**Sent:** Wednesday, February 12, 2020 9:37 AM  
**To:** Shannon Costa <[shannon.costa@Chicoca.gov](mailto:shannon.costa@Chicoca.gov)>  
**Subject:** RE: TownePlace Suites by Marriott Project

**ATTENTION: This message originated from outside City of Chico. Please exercise judgment before opening attachments, clicking on links, or replying.**

Shannon,

I was hoping to touch bases on the status of the application and what is planned by way of CEQA compliance for the project. I am concerned that given the reference in the UP and ARB files to the "stand alone" grading permit file, there may be some intent to rely on the IS/MND done in connection therewith. I'm sure you would agree that that is not appropriate. Is a new IS being considered in connection with the pending application? If not, I would like to discuss such with you.

Jeff

**Carter Law Office**  
329 Flume Street  
Chico, CA 95928  
T 530.342.6196  
F 530.342.6195

\*\*\*\*\*  
**CONFIDENTIALITY NOTICE:** This email, together with any attachment is confidential, intended for only the recipient(s) named above and may contain information that is privileged, attorney work product or exempt from disclosure under applicable law. If you have received this email in error, or are not the named recipient(s), you are hereby notified that any use, dissemination, distribution or copying of this email, or any attachments, is strictly prohibited. Please immediately notify the sender and delete this email and any attachments from your computer. You should not retain, copy, or use this email or any of its attachments for any purpose, nor should you disclose all or any part of the contents to any person. Although this e-mail and any attachments are believed to be free of any virus or other defect that may affect any computer system into which it is received or opened, it is the responsibility of the recipient to ensure that it is virus free and no responsibility is accepted by the sender for any loss or damage arising in any way from its use  
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**From:** Shannon Costa <[shannon.costa@Chicoca.gov](mailto:shannon.costa@Chicoca.gov)>  
**Sent:** Tuesday, February 11, 2020 4:32 PM  
**To:** Jeff Carter <[jeff@jicarterlaw.com](mailto:jeff@jicarterlaw.com)>  
**Subject:** RE: TownePlace Suites by Marriott Project

Hi Jeff

There is unfortunately not much more to talk about. A neighborhood meeting has been scheduled for March 2, but there has otherwise not been any change to the status of the project.



Thank you,  
*Shannon Costa*  
Associate Planner  
(530) 879-6807  
City of Chico- Community Development Department



City of Chico

**From:** Jeff Carter <[jeff@jccarterlaw.com](mailto:jeff@jccarterlaw.com)>  
**Sent:** Tuesday, February 11, 2020 3:40 PM  
**To:** Shannon Costa <[shannon.costa@Chicoca.gov](mailto:shannon.costa@Chicoca.gov)>  
**Subject:** TownePlace Suites by Marriott Project

**ATTENTION:** This message originated from outside **City of Chico**. Please exercise judgment before opening attachments, clicking on links, or replying.

Shannon,

Would you have any time for a brief meeting tomorrow regarding the TownePlace Suites project status? If so, when are you available? Thanks.

Jeff

**Carter Law Office**  
329 Flume Street  
Chico, CA 95928  
T 530.342.6196  
F 530.342.6195

\*\*\*\*\*

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\*\*\*\*\*

**Shannon Costa**

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**From:** Jeff Carter <jeff@jccarterlaw.com>  
**Sent:** Wednesday, February 12, 2020 10:10 AM  
**To:** Shannon Costa  
**Subject:** RE: TownePlace Suites by Marriott Project

**ATTENTION:** This message originated from outside **City of Chico**. Please exercise judgment before opening attachments, clicking on links, or replying.

The IS was done expressly and strictly for the grading of the site. The MND was based thereon. Doesn't this proposed project require its own and a new IS? How can the IS/MND for the grading permit be amended when an IS is required on this new project to determine whether a MND will suffice or whether an EIR will be necessary? I am concerned that any reliance on the previous IS/MND may run afoul of CEQA by, among other things, facilitating the chopping up of a big project in smaller, bite-sized pieces. Has City staff determined that this project, based on the previous IS, will require only a MND?

Jeff

**Carter Law Office**  
329 Flume Street  
Chico, CA 95928  
T 530.342.6196  
F 530.342.6195

\*\*\*\*\*

**CONFIDENTIALITY NOTICE:** This email, together with any attachment is confidential, intended for only the recipient(s) named above and may contain information that is privileged, attorney work product or exempt from disclosure under applicable law. If you have received this email in error, or are not the named recipient(s), you are hereby notified that any use, dissemination, distribution or copying of this email, or any attachments, is strictly prohibited. Please immediately notify the sender and delete this email and any attachments from your computer. You should not retain, copy, or use this email or any of its attachments for any purpose, nor should you disclose all or any part of the contents to any person. Although this e-mail and any attachments are believed to be free of any virus or other defect that may affect any computer system into which it is received or opened, it is the responsibility of the recipient to ensure that it is virus free and no responsibility is accepted by the sender for any loss or damage arising in any way from its use

\*\*\*\*\*

**From:** Shannon Costa <shannon.costa@Chicoca.gov>  
**Sent:** Wednesday, February 12, 2020 9:55 AM  
**To:** Jeff Carter <jeff@jccarterlaw.com>  
**Subject:** RE: TownePlace Suites by Marriott Project

The IS/MND will be amended to consider the full scope of the project. That analysis has not yet begun.

Thank you,  
*Shannon Costa*

Associate Planner  
(530) 879-6807  
City of Chico- Community Development Department

Valerie Priola  
7 Valley Lake Commons  
Chico, California 95928

February 6, 2020

City of Chico Community Development Department  
c/o Bruce Ambo, Chico City Principal Planner  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
[bruce.ambo@chico.gov](mailto:bruce.ambo@chico.gov)

RE: Proposed Townplace Suites Hotel for APN 018-230-001

The preface for this letter was the November 21, 2019 notice of a land owner's intent to build a 112 room 4-story hotel on Sierra Sunrise Terrace. The proposed site for the hotel known as Townplace Suites Hotel would be located on the lot at the north east corner of Bruce Road and Hwy 32. The hotel would share a private street with Chico's Largest Senior Citizen neighborhood known as Sierra Sunrise Village. Sierra Sunrise Terrace is privately owned and maintained by the Sierra Sunrise Village Homeowners Association. Traffic from the hotel would place undue burden on the HOA to maintain both the road and bridge. **We oppose the hotel in this location and request that the City of Chico Planners consider all the ramifications before approving the use permit for this project!**

Sierra Sunrise Terrace street is less than a half-mile-long +/- and services 626 residences or approx. 1,000 people. This senior community has 163 homes, 190 independent living apartments, 84 residents in 2 memory care facilities, 99 in assisted living apartments and 90 beds in the California Park Rehabilitation Hospital. Emergency responders are a common site within the development and must have unimpeded ingress and egress. Sierra Sunrise Terrace is a private two-lane road intended to service the senior community. In the event of an evacuation this street is our life line especially for the 353 residents that have transportation disadvantages.

There are already ingress and egress safety issues on Sierra Sunrise Terrace especially at the corner of Sierra Sunrise Terrace at Bruce Road. Adding a 112-room hotel with insufficient parking for hotel guests and no parking for guests with oversized commercial or recreational vehicles, staff and service providers would exacerbate the existing problems. Adding a signal at the corner would make turning on and off Bruce Road safer but would not solve the overall chaos the hotel would create. And, would still leave Chico's most vulnerable citizens at risk during a natural disaster.

On Dec 5, 2019 KCRA reported that Butte County is one of the 3 counties found in the Auditor's report (Ref: Auditor of the State of California Report 2019-103, dated 2019) not to be prepared to protect its residents from a natural disaster. The Auditor's report focused on the most vulnerable populations—such as seniors, people with disabilities, residents with chronic illness, people lacking English proficiency and people with transportation disadvantages. During an interview with Assembly member James Gallagher (R-3<sup>rd</sup> District) said, "I appreciate the audits focus on how we can do better to address the elderly and those with functional needs in the event of emergencies". **We are looking for City of Chico Planners to take this opportunity to stand up and protect the Sierra Sunrise Village Senior Citizens by denying the hotel's request for a Use Permit!**

Over 1,300 residents have signed a petition supporting the effort to "Stop the Proposed Hotel" and others have expressed concerns:

"This development is a nightmare. When at full occupancy, Cal Park will experience increased security costs, many trespassers onto common areas, significant parking issues, and added management expenses. All this with not one dime to support our overhead. I am a multifamily owner with property on the extreme other end of Cal Park with no real direct stake in this, but my commitment to the Cal Park community as a whole compels me to contribute and encourage every single member to do so. STOP THIS TRAIN WRECK before it occurs!" -Bill Sheridan

"This project is not suited geographically, sustainably, or harmoniously for the proposed location. It needs better infrastructure in place for its and neighbors' safety and would be much better suited for Merriam Park or other more spacious Chico location. Cramming a multi-story building on a small lot with little surrounding parking, limited accessibility and in the center of a neighborhood for senior citizens is not smart city planning." – Susan Shortino

"The hotel would be an eye sore to our beautiful community of California Park! I never bought a home on the lake so I could have a hotel to look at. I bought it on the lake so I could take my boat out and enjoy the beautiful sunsets that appear every night on our lake!! If the hotel wants to be on a lake, they can build their own lake! I have lived on this lake for 26 years, and I don't want to stare at a four-story hotel when I come out! Somehow it takes away the ambience, if you know what I mean!!!" -Gregg Scott

"I live 2 blocks away; we do not have the infrastructure to handle any MORE TRAFFIC AND CONGESTION! What a joke the Chico planning commission is." -Kathleen Lynch

"This hotel should be built in an area that's better suited to its target client, where infrastructure and services exist to support it." -Vicky Bridges

Other planned improvements like widening Bruce from Skyway to Hwy 32 and widening Hwy 32 from Bruce to Yosemite will help traffic flow but they too are not enough when dealing with a natural disaster!

Bruce Road is the only road on the east side of Hwy 99 that runs north and south and doesn't currently meet the community needs: it's a bottleneck! The planned improvements to Bruce Road may be short lived: Jerry Olney, of KRCR reported on July 17<sup>th</sup> 2019, that the two mile stretch of Bruce road from Highway 32 to the Skyway and the bridge along the Little Chico Creek on the same route could be widened sooner than hoped. He reported that Assemblyman James Gallagher (R-3<sup>rd</sup> District) has applied for \$15 million in state funding to fast track development along Bruce Rd and City Manager Mark Orme said, "that if the city gets the funding, it will shave off several years of time from the completion of the project so construction could begin sooner".

It's conceivable, even with improvements, that Bruce Road may have already exceeded the capacity to serve the current east side residents and all the planned medium-high density residential and commercial development needs when dealing with a natural disaster.

Since the November 8<sup>th</sup> 2018 CampFire, everyone's focus has shifted to safety (Ref: Auditor of the State of California Report 2019-103, dated 2019). The stories the Paradise Survivor's tell are heartbreaking. One resident who lived in the planned senior community known as The Plantations said, "96 homes were destroyed and tragically 3 of the 67 seniors who died from the CampFire lived there". She also said, "she got to safety because she left before her area received an evacuation warning". She was unable to follow evac instructions because her exit route was already burning, Skyway was a parking lot so she ended up following other cars down the eastbound lanes; it was like I was driving blindfolded. She is dealing with losing everything but the memory of the evacuation haunts her. **Don't let this happen in Chico!**

**The California Park, Sierra Sunrise Village and Sierra Lakeside Board of Directors stand with our senior citizens...hear our cry and reject the use permit for the proposed hotel!**

Sincerely,



Valerie Priola  
Sierra Sunrise Village, President



Sierra Sunrise Village HOA  
February 6, 2020

City of Chico Community Development Department  
411 Main Street  
P.O. Box 3420  
Chico, CA 95927  
[zoning@chicoca.gov](mailto:zoning@chicoca.gov)

RE: Proposed Townplace Suites Hotel for APN 018-230-001

Attention: Shannon Costa

The preface for this letter was the November 21, 2019 notice of a land owners intent to build a 112 room 4-story hotel on Sierra Sunrise Terrace. The proposed site for the hotel known as Townplace Suites Hotel would be located on the lot at north east corner of Bruce Road and Hwy 32. The hotel would share a private street with Chico's Largest Senior Citizen neighborhood known as Sierra Sunrise Village. Sierra Sunrise Terrace is privately owned and maintained by the Sierra Sunrise Village Homeowners Association. Traffic from the hotel would place undue burden on the HOA to maintain both the road and bridge. **We oppose the hotel in this location and request that the City of Chico Planners consider all the ramifications before approving the use permit for this project!**

Sierra Sunrise Terrace street is less than a half-mile-long +/- and services 626 residences or approx. 1,000 people. This senior community has 163 homes, 190 independent living apartments, 84 residents in 2 memory care facilities, 99 in assisted living apartments and 90 beds in the California Park Rehabilitation Hospital. Emergency responders are a common site within the development and must have unimpeded ingress and egress. Sierra Sunrise Terrace is private two-lane road intended to service the senior community. In the event of an evacuation this street is our life line especially for the 353 residents that have transportation disadvantages.

There are already ingress and egress safety issues on Sierra Sunrise Terrace especially at the corner of Sierra Sunrise Terrace at Bruce Road. Adding a 112-room hotel with insufficient parking for hotel guest's and no parking for guest's with oversized commercial or recreational vehicles, staff and service providers would exacerbates the existing problems. Adding a signal at the corner would make turning on and off Bruce Road safer but would not solve the overall chaos the hotel would create. And, would still leave the Chico's most vulnerable citizens at risk during a natural disaster.

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to protect its residents from a natural disaster. The Auditor's report focused on the most vulnerable populations—such as seniors, people with disabilities, residents with chronic illness, people lacking English proficiency and people with transportation disadvantages. During an interview with Assembly member James Gallagher (R-3<sup>rd</sup> District) said, "I appreciate the audits focus on how we can do better to address the elderly and those with functional needs in the event of emergencies". **We are looking for City of Chico Planners to take this opportunity to stand up and protect the Sierra Sunrise Village Senior Citizens by denying the hotels request for a Use Permit!**

Over 1,300 residents have signed a petition supporting the effort to "Stop the Proposed Hotel" and others have expressed concerns:

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**The California Park, Sierra Sunrise Village and Sierra Lakeside Board of Director's stand with our senior citizens...hear our cry and reject the use permit for the proposed hotel!**

Sincerely,

Valerie Priola  
President of the Sierra Sunrise Village HOA

February 5, 2020

Shannon Costa,  
Associate Planner  
Community Development Department - Planning Division  
411 Main Street, 2nd Floor  
P.O. Box 3420  
Chico, CA 95927

Re: Use Permit APN 018-230-001 - Proposed 4-story 112-unit Hotel on Sierra Sunrise Terrace

Dear Ms. Costa:

We are writing to vehemently oppose this hotel construction, in its entirety, at the currently proposed location on the corner of Sierra Sunrise Terrace and Bruce Road. We strongly urge that the Chico planning commission summarily deny the use permit application for this project.

Sierra Sunrise Village was established in the late 1980's and is a textbook example of a multi-use service retirement and rehabilitation community. This is the precise type of development that the City of Chico endorses in their General Plan 2030:

*"Mixed use is the vertical or horizontal integration of residential, commercial, office, community or civic uses within the same development. Integrating these uses can create desirable places for people to live, work, shop, and play. Mixed-use development supports the City's goal of a compact urban form and its accompanying benefits."*

This area was designed with slow speed private roads in the interests of mature residents. There was never any projection of multi-story commercial buildings that would vastly impact traffic flow and complicate already difficult ingress and egress with Bruce Road. Sierra Sunrise Terrace is a *private* two-lane road, with limited access, designed in the interest of primarily mature drivers and populated with senior pedestrians with canes, walkers, and wheelchairs. This street is already congested with traffic, and all without the benefit of a traffic light at the intersection of Sierra Sunrise Terrace and Bruce Road.

This hotel would have limited access since it could not be accessed from either CA Highway 32 or Bruce Road. Continuous commercial usage of our private road,

Sierra Sunrise Terrace, created by the hotel, would place undo wear and additional maintenance requirements on the road and the bridge over the spillway.

Community Commercial zoning districts that are primarily residential in design are intended to implement the Commercial Mixed-Use land use designation in the city's General Plan. The plan for California Park was never envisioned to accommodate a large hotel; hence the requirement for such a proposal to qualify for a use permit.

In accordance with Title 19, Land Use and Development Regulations, Chapter 19.24.040 – Decision and Findings, we believe the required findings can NOT be met by this project.

A hotel is incompatible with a predominantly senior residential community. The neighborhoods adjacent to the proposed hotel comprise the largest concentration of seniors in the city of Chico, including independent senior single-family dwellings, senior condominiums, senior apartments, assisted living, memory care, skilled nursing care, and a rehabilitation hospital. From a health, safety and general welfare standpoint, this location houses one of the most vulnerable populations in the city.

This is the wrong location for this project for reasons of access noted above, and serious health and safety concerns as will be addressed below.

Perhaps the strongest reason to reject this hotel project is its potential impact on life-threatening disaster scenarios.

As detailed in the California State Auditor's report of December 2019, entitled, **California Is Not Adequately Prepared to Protect Its Most Vulnerable Residents From Natural Disasters**, Butte County *specifically* is NOT adequately prepared to protect its most vulnerable residents from natural disasters. Quoting from the cover letter to the report, "*Butte County does not have an up-to-date plan for alerting and warning their residents about the danger from natural disasters, conducting evacuations, or sheltering evacuees...Inadequate preparation likely hindered the counties' responses to the 2018 Camp Fire...*"

In an emergency evacuation, the proposed hotel would create nightmarish conditions for Sierra Sunrise Village residents, a significant number of whom would require assistance in evacuating.

Furthermore, in an emergency this proposed hotel would severely hamper access for first responders and emergency vehicles. It is almost impossible to imagine the catastrophe that would result from the addition of a 4-story, 112-room hotel, with their guests, employees, contractors, and all manner of vehicles attempting to evacuate simultaneously.

Firefighting helicopters will no longer have their approach path to the Cal Park lake obstacle free. A four-story building in close proximity might be safety hazard, thus ending this valuable wildfire resource in the future.

The proposed hotel does not provide for adequate parking within the property boundaries. The current plan for parking is insufficient to provide for guests, hotel employees and parking for RV's, vehicles with trailers, freight trucks, etc. Sierra Sunrise Village HOA would NOT grant approval for parking on Sierra Sunrise Terrace for this hotel project.

Sierra Sunrise Terrace is a lakeside community. The three HOA boards that govern this larger community – California Park, Sierra Sunrise Village, and Sierra Lakeside – are concerned with protecting our common areas, lakes, and the security of those living in this area.

The Community Design element of Chico's General Plan stipulates the following policies:

\*CD-2.4 (Context Sensitive Foothill Development) – Protect viewsheds from foothill development, through the careful location and design of roads, buildings, lighting, landscaping, and other infrastructure.

\*CD-2.4.1 (Protection of Foothill Viewshed) – Design and blend foothill development with the surrounding landscape and topography to diminish its visual prominence from the valley floor.

\*CD-2.4.2 (Foothill Light Levels) – Design low light levels in foothill settings to optimize views of dark skies and minimize light pollution.

\*PPFS-2.1.4 (Assess Potential Impacts to Creeks) – Through the development and environmental review process, including consultation with state and federal agencies and non-profit organizations, ensure that natural areas and habitat located in and adjacent to the City's creeks are protected and enhanced.

Adding a 4-story 112-unit hotel at California Park would negatively impact our general scenic vistas and lake views, as well as increasing light pollution, pollution of natural water resources, and noise pollution. In addition, this operation could



adversely affect local flora and wildlife, including flight patterns of Snow Geese and Canada Geese who live on and around the lakes.

We might also surmise that one of the rationales for choosing this location is its proximity to the California Park lakes. It is important to state emphatically that access to the lakes and common areas is limited ONLY to those individuals residing WITHIN California Park. Hotel guests, employees, etc. would NOT be granted permission to enter this area. To do so would constitute trespassing. Protecting our private common areas will require tens of thousands of dollars in fencing and security measures that the hotel will not pay for.

According to the General Plan there are over 319 acres of vacant land designated for various types of commercial use in Chico. This means that there are numerous alternative sites for this project on Bruce Road, Meriam Park, or the Skyway areas that would not split or degrade an established senior residential community. These locations would also have much less impact on an already overburdened traffic bottleneck on Highway 32. Merriam Park already has two traffic lights to assist with merging traffic.

While we do not object to the construction of another hotel in Chico, *this* project has been proposed for the wrong location and does NOT support the current zoning and the urban/rural interface of our predominantly senior residential community. A hotel would not benefit the Sierra Sunrise Village community by adding any compatible supportive services to our neighborhood.

Lastly, an active petition opposing this hotel project has already secured over 1,100 signatures, and more will be forthcoming. Within the larger community there is widespread opposition to the hotel proposed for this location. We are prepared to engage all actionable and legal means necessary to halt the building of this hotel in California Park.

Please do not allow our most vulnerable seniors to be forgotten, or worse, to be needlessly placed in harm's way. Please reject this ill-conceived project.

Thank you for your consideration of these concerns in your deliberative process.

Sincerely,

Rev. Jeffrey P. Lambkin

## Shannon Costa

---

**From:** dcpolen@comcast.net  
**Sent:** Wednesday, December 18, 2019 4:33 PM  
**To:** Shannon Costa  
**Subject:** hotel calpark

ATTENTION: This message originated from outside City of Chico. Please exercise judgment before opening attachments, clicking on links, or replying.

We are very concerned and against any possibility of a hotel being built in the California Park area off of Bruce Road. We feel that a hotel on that property, adjacent to Bruce Road will cause a danger to drivers using Bruce Road. Bruce Road is one of only four main routes north and south through Chico, including highway 99. With the impact of the new residents from the Camp Fire, Bruce Road now has severe traffic issues, with backups at the traffic lights of highway 32 and Bruce Road. The impact of a four story hotel with 100's of guest using Bruce road at the intersection of Hwy 32 and Bruce Road as their only entrance and exit will be accidents waiting to happen.

By adding more traffic to an already high traffic area the hotel would impact evacuation of the area if the dam at California Park should flood. As more home are built in the upper California Park area on both the north and south sides of highway 32 more traffic will be added to the Bruce Road corridor causing a bottle neck and danger to those traveling on that corridor.

This is a bad use of this land and should for the good of the city of Chico be stopped now.

Thank you for helping us stop this development.

**12-16-2019**

**To: Nicole Ledford  
NorthStar Eng**


**From: Ron and Carol Serrano**

**Re: Proposed development, Townplace Suites**

**We wish to express our concerns about the proposed development at the corner of Hwy 32 and Bruce Rd.**

- Parking: Number of spaces for guests, employees and overflow is inadequate**
- Traffic: No stoplight at Sierra Sunrise and Bruce Rd, an already a dangerous intersection**
- Drainage: The land in question is a natural overflow for creek water**
- Access: access is on a private road, but should be planned for a public Highway**
- Fire Danger: the development would cause further congestion in a fire emergency**
- Access to Calif. Park HOA common areas (lake, trails etc) must be protected**
- Bridge: Integrity of the bridge on Sierra Sunrise must be protected**
- The developer needs to appear before the concerned citizens at the next Meeting.**
- The project provides no additional benefit to the residents of Calif. Park and should not be located in a residential area.**

**Thank you for your consideration,**

  
**Ron and Carol Serrano  
1374 Botticelli Bend  
Chico, Ca 95928**

F Y I

December 3, 2019

The October 25, 2004 letter from the City of Chico is an indication of the historical reluctance to accept dedication of any portion of Sierra Sunrise Terrace.

If the City would accept dedication from Bruce Road to the "bulb" (all commercial area) it would be very helpful.

Greg Steel  
342-3191



DEPARTMENT  
OF PUBLIC WORKS  
ENGINEERING

411 Main Street 2nd Floor  
P.O. Box 3470  
Chico, CA 95927

(530) 895-4887  
Fax (530) 895-4899  
<http://www.ci.chico.ca.us>

|                   |            |         |               |            |   |
|-------------------|------------|---------|---------------|------------|---|
| Post-It® Fax Note | 7671       | Date    | 10-25-04      | # of pages | 2 |
| To                | GREG STEEL | From    | WOUTER HAAR   |            |   |
| Co./Dept.         |            | Co.     | CITY OF CHICO |            |   |
| Phone #           |            | Phone # |               |            |   |
| Fax #             | 342-3191   | Fax #   |               |            |   |

October 25, 2004

Mr. Greg Steel  
603 Parkwood Drive  
Chico, CA 95928

Re: Sierra Sunrise Terrace Acquisition

I am writing to confirm the details of my message of Tuesday, October 12, 2004 and our conversation of Friday, October 22, 2004 in which we discussed the acquisition of a portion of Sierra Sunrise Terrace.

The homeowners' association needs to provide the following information in order for the City to initiate consideration of acquiring the portion of Sierra Sunrise Terrace from Bruce Road to approximately the southerly prolongation of the easterly property line of lot 58:

1. A current condition inspection of the bridge crossing Dead Horse Slough.
2. Plats and descriptions conveying:
  - (a) A storm drain easement, ten feet in width from DI #35 @ sta. 5+00 Rt. To Dead Horse Slough.
  - (b) A sanitary sewer easement ten feet in width from SSMH #12 @ sta. 1+76.51 to Bruce Road.
  - (c) The entire area proposed for acquisition to the City in fee.
3. Video tape record of a current condition inspection of all sanitary sewer and storm drain pipes within the area of acquisition.
4. All the street lights within the area of acquisition must be on service point(s) separate from the remaining street lights on Sierra Sunrise Terrace.
5. Engineers calculation confirming that the existing structural section of Sierra Sunrise Terrace is adequate for the anticipated traffic.  
Note: The existing structural section of Lakewest Drive from Bruce Road to Idyllwild Circle is thicker and has an equivalent T.I.
6. "As Built" drawings of all facilities being acquired by the City.
7. Complete supporting documentation for all plats and descriptions, current title report, deeds, signature authority for signatures, etc.

I have enclosed sheet 14 of 23 of Sierra Sunrise Village Phase 2 to help in clarifying the above descriptions. Please contact me at 895-4884 if you have any additional questions.



John Vonderhaar  
Senior Civil Engineer

JV:sh

Enc.

cc: DPW  
ADPW-E  
ADPW-O&M  
Crystal Root, Hignell Incorporated





## Shannon Costa

---

**From:** Janet Pearce <jcpearce2015@gmail.com>  
**Sent:** Monday, December 2, 2019 5:49 PM  
**To:** Shannon Costa; nledford@northstareng.com  
**Subject:** Re: Proposed hotel

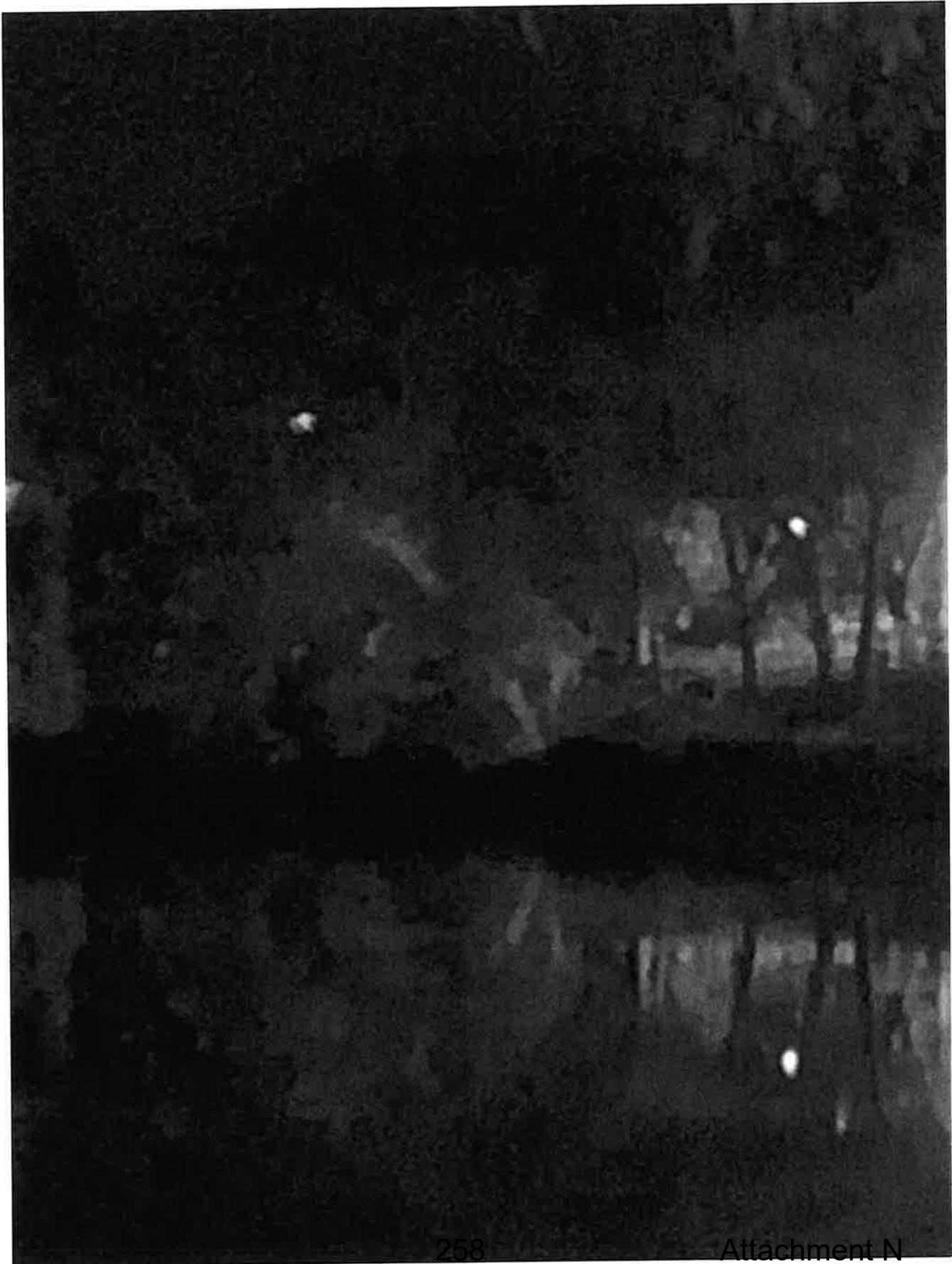
**ATTENTION:** This message originated from outside **City of Chico**. Please exercise judgment before opening attachments, clicking on links, or replying.

Hi Shannon and Nicole,

This is where the placement of the hotel would be. Right in this opening. We have no way to block it from our view as that is the spillway and then the street that would lead into the proposed site. People that live on the lake face that direction so a lighted hotel view would definitely be a downgrade to them all. Most importantly reflection on the water and the cars of an additional 112 people pulling into our Senior living area will devastate this treasure of Chico. On top of that, we are on private property which requires people to pay HOA fees to walk around the lake etc. We would have to have full time patrol for all of our areas which would be a huge blow to our budget. Visitors of the hotel would be asked to leave the premises. It would be a disaster for our Association. Increased traffic would surely slow the response time for emergency response vehicles to get to our Sr care facilities quickly on Sierra Sunrise Terrace where there is no traffic light at this time. It seems this hotel would be much better suited in Merium Park next to the VA clinic where people could have all the amenities they need. Care, food and lodging without having to drive and cross all our already over congested intersections. I'll remind you what hey 32 looked like during the Campfire evacuations. Dangerous. Current lighting problem on the lake. The first is an acceptable level, the last two are light pollution. Water magnifies the light. You can see that in the sunset photos as

well.











Shannon and Nicole,  
Will you please add me to the interested parties list.  
Janet Pearce  
California Park Association  
7 Catalina Point Rd  
Chico, CA 95928  
530-518-2636  
Jcpearce2015@gmail.com

Shannon,  
Thank you for your kind help today.J

On Dec 2, 2019, at 10:37 AM, Shannon Costa <shannon.costa@chicoca.gov> wrote:

Our GIS system generates a notice for every tenant and property owner with 500 feet of the site. I encourage all groups/HOAs to provide their information for the interested parties list.

The meeting has been moved to 2750 Sierra Sunrise Terrace (still 5:30 pm) - the Shasta Room at the Terraces Lodge. Northstar will post this change on the project site, but please let anyone know who you may have contact with.

Thank you,  
*Shannon Costa*  
Associate Planner  
(530) 879-6807  
City of Chico- Community Development Department

*\*\*Reminder: City Hall will be closed over the Christmas holiday. Closure dates are December 24, 2019 through January 1, 2020. Please plan accordingly.\*\**

<image002.jpg>

**From:** Janet Pearce <jcpearce2015@gmail.com>  
**Sent:** Monday, December 2, 2019 9:47 AM  
**To:** Shannon Costa <shannon.costa@Chicoca.gov>  
**Subject:** Re: Proposed hotel

**ATTENTION:** This message originated from outside **City of Chico**. Please exercise judgment before opening attachments, clicking on links, or replying.

Hi Shannon,  
Thank you so much for your quick response. I really appreciate your clarity. California Park Association is within the 500 ft guideline. Are Associations not notified, just homeowners?  
I will contact Northstar about this meeting.  
Thank you for your help

Janet Pearce

On Dec 2, 2019, at 8:53 AM, Shannon Costa <[shannon.costa@chicoca.gov](mailto:shannon.costa@chicoca.gov)> wrote:

Hello Janet-

Thank you for your email. I first want to clarify that this pre-application neighborhood meeting is the responsibility of the applicant. A pre-application neighborhood meeting is required as part of the application completeness process for all projects going before the Planning Commission or Council (PC in this case). The application cannot be deemed complete for processing until this meeting takes place (so no, it is definitely not a "done deal"). The purpose of the meeting is for the applicant to listen to neighbor concerns, answer questions, and inform the neighbors of their intent. The City's role in this meeting is to attend, listen, and inform people of the City's process the application will go through, but not to lead, control or direct the meeting.

I have not received a physical notice about this meeting (although I was verbally made aware of this meeting date) so I was not aware that the meeting would be held outside. In response to your email I have requested to Northstar that the meeting be moved to an indoor location, or that some kind of shelter be provided (I too do not want to stand in the rain!). If there is any contact information on the notice for the applicant or their representative (Northstar Engineering) I suggest you also reach out them with your concerns.

The City is required to notify everyone within 500 feet of the project site of any public hearings. If you are not within that 500-foot radius, I would be happy to add you to the "interested parties" list, and you would be notified.

Thank you,

*Shannon Costa*

Associate Planner

(530) 879-6807

City of Chico- Community Development Department

*\*\*Reminder: City Hall will be closed over the Christmas holiday. Closure dates are December 24, 2019 through January 1, 2020. Please plan accordingly.\*\**

<image002.jpg>

**From:** Janet Pearce <[jcpearce2015@gmail.com](mailto:jcpearce2015@gmail.com)>

**Sent:** Sunday, December 1, 2019 4:13 PM

**To:** Zoning <[zoning@Chicoca.gov](mailto:zoning@Chicoca.gov)>

**Cc:** Mark Orme <[mark.orme@Chicoca.gov](mailto:mark.orme@Chicoca.gov)>

**Subject:** Proposed hotel

**ATTENTION:** This message originated from outside **City of Chico**. Please exercise judgment before opening attachments, clicking on links, or replying.

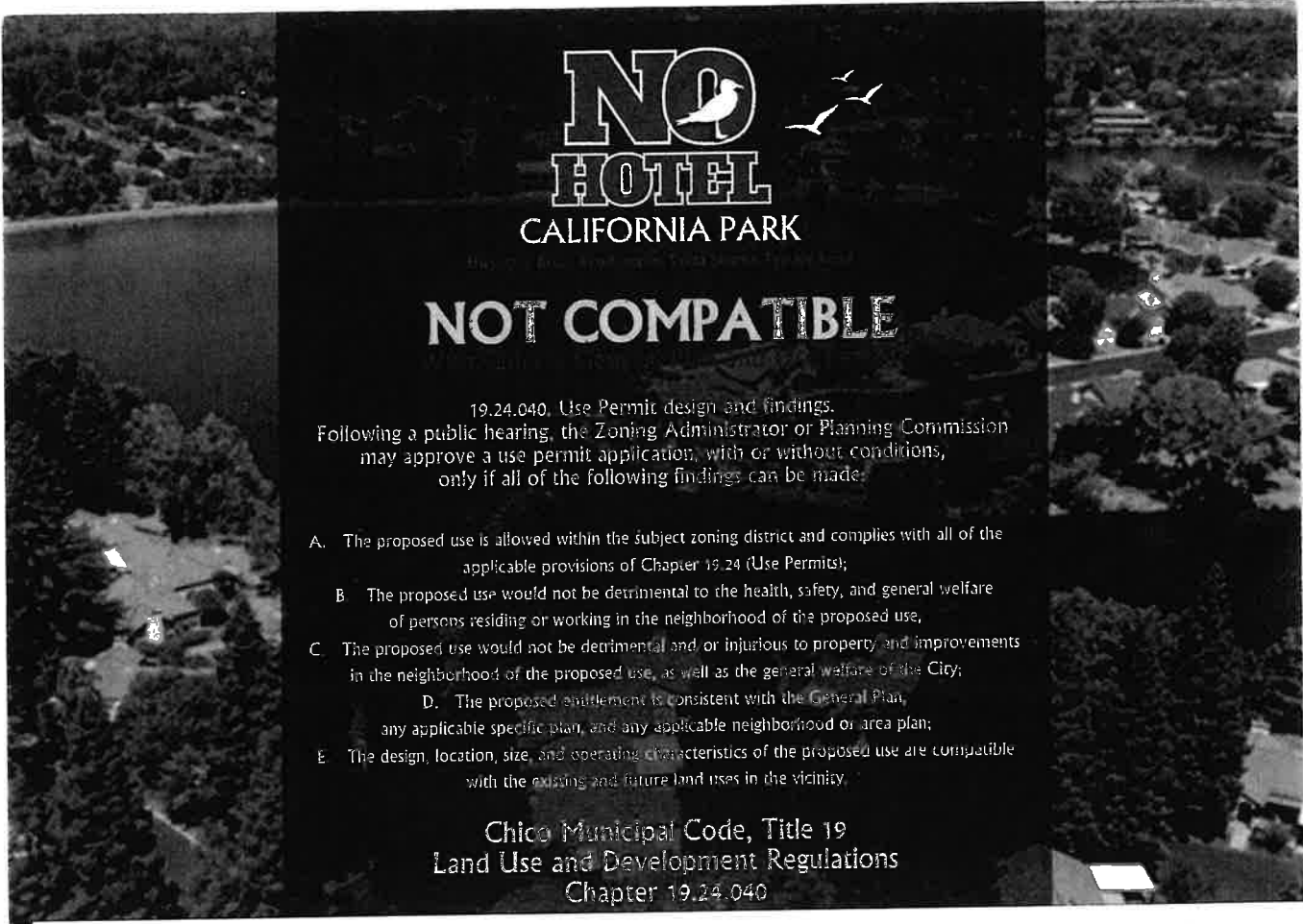
**Subject: Proposed hotel**

To whom it may concern,

I just heard of the meeting for the proposed hotel on the corner of highway 32 and Bruce Road. My name is Janet Pearce and I am the President of the California Park Association which is the parent HOA of Sierra Sunrise and Sierra Sunrise Terrace. We were not notified of the meeting or the proposed project. When you last scheduled a meeting for the 24 hr mini market, you also didn't notify us. What was said in the meeting that evening was, you only had to notify people within 500 ft (which there weren't any) and we were 900ft. It would have been a great courtesy to let those residents mostly affected, speak to the project. The only and closest thing to the proposed site. Even if nothing changed. That is what this City of Chico is about. Kind, caring and courteous people doing things to help each other out. I'm sure it was a done deal and this one probably is as well. But people should get to speak and hopefully some compromises could be made. The people most affected by the hotels presence are Senior citizens living around the lake. The lights and traffic level will surely affect the noise level in our neighborhoods as well as confusion on the streets. Noise and lights are magnified on water. The back of this hotel will face the lake homes that are on a large body of water. Our spillway leaves no way to plant a barrier from the proposed building. The lighting and noise will have an affect on the natural habitats that surround our lakes here on California Park Lake. As a resident, I personally feel this is not the appropriate place for a 4 story hotel. We pay plenty of money in patrol to keep non members off our property. Placing a hotel with rooms that face a lake and look into people's homes will only cost us more in patrol to keep our neighborhoods free of people wishing to explore it. But what deeply disturbs me is that you planned a meeting in a Senior community, after dark, in the rain and outside. That is just not right and seems very unkind and unsafe for those who wish to attend. Although some residents want to report this to the news, I thought it best to contact you directly. Do you really want a large group of seniors standing in the dark, cold, rain? Is this process really that skewed? Please consider changing your location and date to somewhere, such as the pavilions, where people can be safe, warm and comfortable. And please let me know why as the parent HOA of Sierra Sunrise we weren't notified? We do lots of business with Northstar. They do know who is over who as far as the Association's go I'm sure or could easily find out. They know our property management well. I will be contacting them next. Please let me know if you are willing to have a proper meeting to let these tax paying residents of Chico speak to this project. We are a VERY large community of taxpayers who deserve that right. That's all I am asking for.

Thank you,  
Janet Pearce

As of the date of the distribution of the staff report there were 95 postcards submitted in opposition to the project, in addition to the numerous emails and letters. Hard copy originals of the postcards will be brought to the meeting and available for review.



**NO HOTEL CALIFORNIA PARK**

**NOT COMPATIBLE**

19.24.040. Use Permit design and findings.  
Following a public hearing, the Zoning Administrator or Planning Commission may approve a use permit application, with or without conditions, only if all of the following findings can be made:

- A. The proposed use is allowed within the subject zoning district and complies with all of the applicable provisions of Chapter 19.24 (Use Permits);
- B. The proposed use would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed use;
- C. The proposed use would not be detrimental and/or injurious to property and improvements in the neighborhood of the proposed use, as well as the general welfare of the City;
- D. The proposed entitlement is consistent with the General Plan, any applicable specific plan, and any applicable neighborhood or area plan;
- E. The design, location, size, and operating characteristics of the proposed use are compatible with the existing and future land uses in the vicinity.

Chico Municipal Code, Title 19  
Land Use and Development Regulations  
Chapter 19.24.040

**Bruce Ambo**, Chico City Principal Planner  
Community Development Dept., Planning Division  
411 Main Street, Second Floor  
Chico, CA 95928



I am objecting to the hotel being built in the Calif. Park area. This will be detrimental to the senior community that lives here.

1. Design, location + size ★
2. Increase in traffic
3. Evacuation a night mare (if necessary.)

*Patricia Corney*

