



City Council Agenda Report

Meeting Date: 11/20/2018

TO: **Honorable City Council**
FROM: **Mark Orme, City Manager**
RE: **Review, Discussion, and Direction regarding Tax Feasibility Voter Survey**

REPORT IN BRIEF:

At its 6/19/2018 meeting, the City Council directed City staff to present information related to public infrastructure bond financing, specifically streets, to the Finance Committee. At its 9/26/2018 meeting, City staff Finance Committee heard the item and voted unanimously to recommend the City Council consider engaging a professional consultant to perform a tax feasibility voter survey to determine what voters would prioritize to fund with any such additional tax revenue.

Recommendation: The Finance Committee recommend engaging a professional consulting firm to conduct a tax feasibility voter survey of City residents to determine the viability of passing a tax measure and to determine what they would prioritize to fund with such additional tax revenue.

FISCAL IMPACT:

The City was able to work with two experienced California firms frequently used to conduct such assessments. They provided quotations under \$25,000 (See Attachment 1). At the meeting of October 16, 2018, the City Council approved a budget appropriation for \$25,000 towards this item.

BACKGROUND:

At its 6/19/2018 meeting, the City Council directed City staff to present information related to public infrastructure bond financing, specifically streets, to the Finance Committee. At its 9/26/2018 meeting, the Finance Committee heard the item and voted unanimously to recommend the City Council consider engaging a professional consultant to perform a tax feasibility voter survey to determine what voters would prioritize to fund with any such additional tax revenue.

California cities have a variety of options to increase revenues for services and capital projects, which range from general and restricted (special) tax increases to bonded indebtedness. While general tax increases are often used for operational expenditures, bond measures are generally used and restricted to non-operational, capital projects where the bond payments match the long life of the capital asset constructed. It is important to match the appropriate revenue tool to the type of funded activity or project. Using bond financing to reconstruct, service, and maintain high quality streets is the type of use for which the debt financing is well suited.

Many cities utilize professional experts to determine the feasibility of a tax measure and to determine the type of activities desired by the public. While cities already know the items desiring additional funding in many cases, others look to the survey as providing objective, statistical information to inform the City's decision-making on the viability of a tax measure.

DISCUSSION¹:

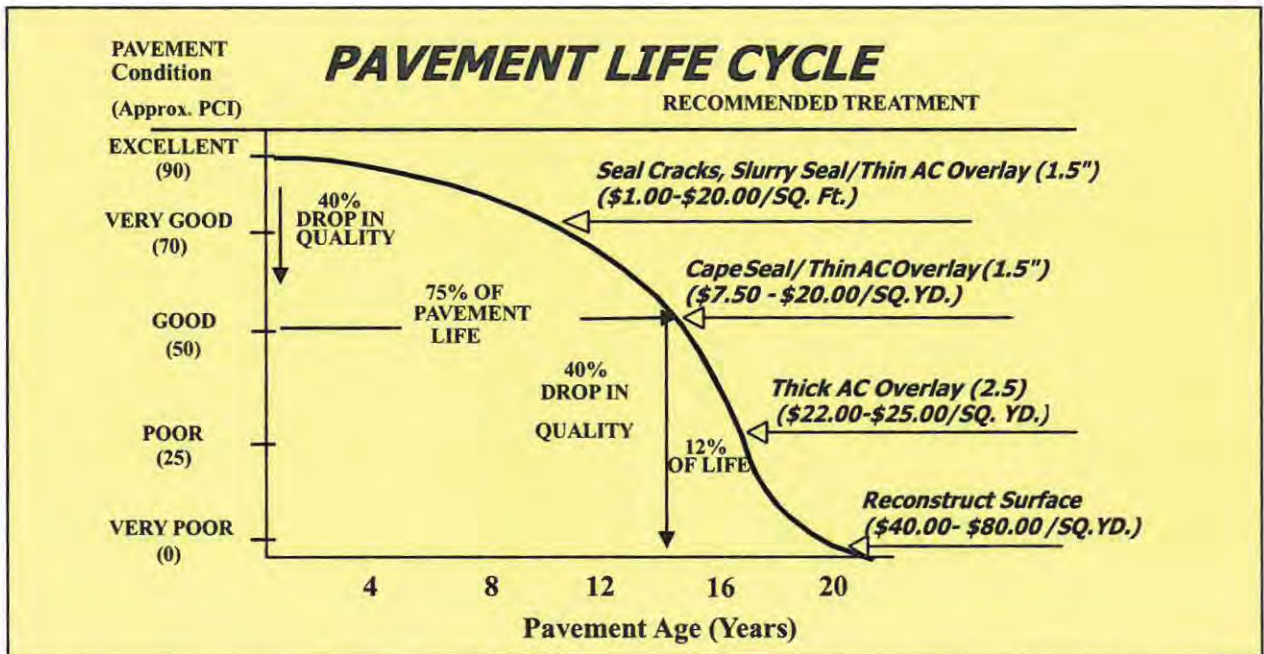
Cities exist to provide public services and to maintain/operate/utilize various assets, i.e. something the City owns that has value. The largest category of assets in Chico include roadways, bridges, and drainage (street related assets). These assets directly support residents and visitors’ ability to live, work, and play in Chico. In June 2017, the City reported having these assets represent about \$366 million of the \$426 million in governmental assets². While annual operations are the most significant expenditures over time, streets represent the greatest total infrastructure investment that provides sustained value which have the tendency to degrade and depreciate.

City has significant unfunded needs for street rehabilitation and preventative maintenance

In 2015, the City performed an update to its street condition assessment and identified a \$125 million unfunded need for rehabilitation and preventative maintenance. Attachment 2 provides the complete street condition assessment. The assessment estimated that if streets were left untreated during the period of 2016-2020, the pavement condition index (PCI) of the streets would drop from 60 in 2016 down to 50 (A new street would equal a PCI of 100). In 2018, the Public Works Director provided an updated cost estimate for rehabilitation and preventative maintenance for arterial, collector, and residential/local streets which indicated the total unmet need was about \$188 million. This cost also did not include storm drain, sidewalk, park strip, Americans with Disability Act (ADA) updates, some of which become necessary when performing significant street work.

Exhibit 1 shows the Pavement Life Cycle as presented in the 2015 Pavement Management Program Update performed by Harris & Associates. The costs associated with rehabilitating streets grows significantly after streets fall below a pavement condition index of 50.

Exhibit 1



¹ Substantial information is included or adapted from the California Debt Issuance Primer published by the California Debt and Investment Advisory Commission (CDIAC 06-04).

² As reported in the City of Chico 2016-17 CAFR – Governmental Activities, prior to depreciation

The current PCI is reduced annually based on this deterioration curve. Maintenance activities increase the PCI value as they are applied to the street, thus, regular maintenance has the effect of increasing street life expectancy and reducing the long-term ownership costs.

City Chico Street Maintenance Program

Chico Public Works has moved to a zone approach to maintaining streets breaking the City into eight zones (7 plus Bidwell Park). City resources are directed into one zone annually; however, emergency repair and grant-funded activity still occur citywide. Attachment 2 provides a map of the 8 City of Chico Zones. Annually, the City of Chico spends approximately \$547,939 on street maintenance labor and materials by in-house maintenance worker staff.

Expenditures for larger scale reconstruction and rehabilitation have come mostly through gas tax and waste hauler franchise revenues. In 2018, the City Council approved the new franchise agreements and restricted the additional revenue from the franchise fee for reconstruction and rehabilitation activities. This is estimated to bring another \$800,000 to \$1 million in additional resources to major capital projects. One example of major capital was the recent Cohasset Road project which improved the segment from East Avenue to Eaton Road. For that project, the length of the 4-lane road way was about 1 mile and the total project cost came to approximately \$1.5 million.

The City's current ability to fund \$7 million addition annually to address the need does not exist without a drastic change in resource allocations. Given the City's general fund investment of about 75% of total funds for public safety, any significant change in resource allocations will have a negative effect on public safety delivery. Attachment 4 provides a matrix of select revenue sources, their legal authority, and a description of each revenue source used by typical cities such as Chico – Sales and Property Tax constituting most general City revenues.

The inability to adequately fund roads has been persistent for over 10 years. Given the rate of road degradation annually, years of inadequate investment not only results in lower quality roadways, but exponentially increases the tax payer cost of restoring roads to an adequate condition – up to 14 times the cost of regular maintenance.

Financing Street Reconstruction and Rehabilitation

The significant cost associated with reconstruction and rehabilitation limits the City's ability to use current annual resources to make a meaningful impact. As a result, a new revenue source and debt secured by such revenue source become one of the few viable options to consider.

Cities have some ability to generate new revenues; however, most of the mechanisms available require a vote of the public. Generally, sales tax and property tax increases have been used by other cities to finance new capital projects and roadway improvements. Bonded indebtedness is utilized and secured against these new revenue streams to finance projects over their life expectancy which matches the debt payments to the public benefiting from such tax increases.

There are significant policy considerations in the type of new revenue and debt mechanism utilized. Property tax increases place the burden solely on Chico property owners, and to some degree, renters of Chico property, while sales tax increases spread some of the burden to non-Chico residents who shop in Chico. Additionally, the purpose of any revenue tax increase, for a specific purpose or unrestricted, also determines if a public vote is required at the two-thirds or majority level. This also is a policy consideration.

For consideration, the additional benefits of funding public infrastructure, not only includes the quality of life improvements with well maintained infrastructure, but also economic and job creation incentives. Because most of the work to reconstruct roadways would require large-scale capital projects, public bidding procedures would ensure privately led contractors would bid on the work. The construction industry has historically been a significant indicator to the economic health and vitality of the economic, due to the trades workforce opportunity that is provided through such a funding mechanism. This provides sustained construction works, which requires significant private sector jobs in the trades, such as laborers, operating engineers, electricians and carpenters. In addition, minimal City staff needs are required to meet this demand to produce projects and other in-house maintenance activities, therefore minimizing long-term pension liabilities.

Further, the City may strategically time significant construction activity for downturns in the economy which may provide a much-needed boost at a time construction activity would be at lows. This would allow the opportunity to retain construction jobs that would otherwise leave the community and not come back. In accomplishing this, there are several financing measures available to the City. **Exhibit 2** provides some pros and cons of different funding mechanisms.

Exhibit 2 Funding Mechanisms, Benefits and Drawbacks³

| Financing Measures | Description | Pros | Cons |
|-----------------------------|---|---|--|
| Property Tax – value-based | Tax on parcels based on per \$100,000 value | <p>Lowers burden on lower valued property</p> <p>Has more stability for the purposes of bond indebtedness than a transaction tax</p> | <p>Provides disparate impact for potentially similar services – higher valued properties taxed more</p> <p>Depending on use, tax burden placed only on property owners and to some extent, renters where transient individuals benefit</p> |
| Property Tax – parcel based | Tax on parcels | <p>Provides equal financial impact on property owners</p> <p>Has more stability for the purposes of bond indebtedness</p> | <p>May require a higher contribution per parcel for equal amount of value-based tax</p> <p>Impact equity can vary dramatically based on beneficiary of funded projects</p> |
| Sales Tax – unrestricted | Increment with no use limitation | <p>Tax liability is shared with non-Chico property owners who purchase taxable items in Chico</p> <p>Beneficial use of funds for any general purpose</p> | <p>Variability in revenue source would lower the percentage of revenue available for bond indebtedness or require higher sales tax rate</p> <p>Increases overall cost of taxable items</p> |
| Sales Tax - restricted | Increment with restriction | <p>Tax liability is shared with non-Chico property owners who purchase taxable items in Chico</p> <p>Use of tax can be restricted to certain purposes</p> | <p>Variability in revenue source would lower the percentage of revenue available for bond indebtedness or require higher sales tax rate</p> <p>Increases overall cost of taxable items</p> |

³ This is estimated based on \$8,449,685,363 as the net assessed valuation for property and an assessment based on property value. The estimated annual per parcel is based on a 30-year repayment and does not include any calculation for interest rate of return.

| | | | |
|----------------------|--|--|---|
| Property Assessments | Levy assessed on properties for specific purpose | Flexible – can target levy to properties deriving a specific benefit Place burden of capital or operational costs on owners who benefit | Smaller number of parcels results in higher property assessments which may impact long-term property values Service differentials may result due to some areas having special assessments and others not having them |
|----------------------|--|--|---|

The revenue sources listed have additional pros and cons that are not listed. Consequently, it is important for policy makers to consider the appropriate revenue source, its benefit as a source for further debt financing, and the willingness of those impacted to self-assess themselves to accomplish the ultimate goal involving the revenue.

Bond Financing

Cities utilize bond indebtedness to generate resources to pay for a significant capital project that tend to have a long-life expectancy. Some examples of this financing mechanism have supported streets, sidewalks, affordable housing, public safety facilities and other types of capital projects. Others included funding for enterprises that generate revenue to pay for those bonds while most of the ones listed do not.

To issue a bond is to borrow money. A bond is simply the evidence of the debt, in the same way that a promissory note is evidence of the obligation to repay an ordinary loan. The issuance of bonds in connection with borrowing results in the creation of securities evidencing the loan that can be bought and sold, i.e. “traded.” Some of the debt obligations require a nexus of the benefit as is required in both Assessment and Community Facility (Mello-Roos) Bonds, while others, such as General Obligations and sales tax bonds, make no such requirement. Revenue bonds, such as the Downtown Parking revenue bond the City previously undertook, generate funds to build some type of revenue or enterprise asset which will generate revenue to satisfy the revenue bond.

Types of Bond Financing Available

Assessment Bonds

Assessment bonds are repaid from taxes collected from those who benefit from the project. An assessment is any levy or charge imposed upon real property by a local agency for a special benefit conferred upon the real property from a public improvement.

Assessment bonds are issued upon the security of the assessments and are payable from either (a) scheduled installments of assessments, collected either by a direct billing to the property owner or by posting to the secured property tax roll of the county in which the real property is located or (b) proceeds of prepayments of assessments made by property owners to discharge the lien of the unpaid assessment on a specific parcel. Typically, assessment bonds are used for projects within a specific development or group of neighborhoods to finance such improvement that has a direct, special benefit to those properties being assessed.

General Obligation Bonds

General obligation (GO) bonds are bonds secured either by a pledge of the full faith and credit of the issuer and/or by a promise to levy taxes in an unlimited amount as necessary to pay debt service. GO bonds are typically payable only from ad valorem property taxes (taxes based on the value of property “*x per \$100,000 assessed valuation*”), which are to be levied in an amount enough to pay interest and

principal on the bonds maturing in each year. GO bonds are typically the least expensive debt available to a government and require two-thirds voter approval for levying taxes.

Revenue Bonds

Revenue bonds are long-term debt instruments retired by specific dedicated revenues, often revenues generated by a project funded out of bond proceeds. Revenue bonds are designed to be self-supporting through user fees or other special earmarked receipts; the general taxing powers of the jurisdiction are not pledged. The debt created through the issuance of revenue bonds is to be repaid by the earnings from the operations of a revenue-producing enterprise (an enterprise revenue bond), from special taxes (a special revenue bonds), or from contract leases or rental agreements (a lease revenue bond).

Mello-Roos Bonds

The Mello-Roos Community Facilities Act of 1982 authorizes a public entity to form a Community Facilities District (a “CFD” or “district”), otherwise known as a Mello-Roos district. Once formed, the district can finance facilities and provide services. Upon approval by a two-thirds vote of the registered voters or landowners within the district, the district may issue bonds secured by the levy of special taxes. The special taxes are not assessments, and there is no requirement that the special tax be apportioned based on benefit to property. A special tax levied by a district is not an ad valorem property tax; however, the lien of the special taxes has the same priority as property taxes.

Sales Tax Bonds

Sales tax revenue bonds are bonds that are payable from and secured by revenues received by the issuer from the imposition of a sales and use tax, or a transaction and use tax, on retail transactions within the issuer’s boundaries. While sales tax revenue bonds may be used to finance projects that are similar in many respect to the projects funded by public enterprise revenue bonds (sewer, water, electric plants or other self-supporting enterprises), sales tax revenue bonds are useful for financing projects that will not generate revenues for some time or will not generate revenues sufficient to cover the costs of the project, such as mass transit facilities. For cities, authorized projects include the acquisition, installation, construction, or improvement of public works or improvements, and the acquisition of lands and easements.

Evaluation of Impacts on Chico Residents and Properties

Public Works provided a summary analysis of the total need for street reconstruction for arterial, collector, and residential/local streets. The summary includes the cost of dealing with just the roadway itself as well as taking a complete streets approach, which is recommended, to account for all the public infrastructure having a nexus with the roads. The complete streets approach includes the capital infrastructure from private property line to private property line – such as, sidewalks, park strips, street lights, storm drains, ADA requirements, and roads. The adopted 2030 General Plan also identifies complete streets as the City of Chico’s expectation for design standards of our public rights-of-way. **Exhibit 3** provides a summary of the totals for the three functional street classes based on roads only and complete streets. Additionally, the exhibit provides the estimated total contribution required for an average parcel and annual contribution based on the required amount.

Exhibit 3 Estimated Total Financing Required by Functional Class and Property Value⁴

| Class | Roads Only | Complete Streets | Est. Per \$100,000 AV Total Contribution | Est. Annual per \$100,000 AV |
|---------------|----------------------|----------------------|--|------------------------------|
| Arterial | \$38,945,314 | \$73,625,118 | \$871 | \$29 |
| Collector | \$51,331,402 | \$86,312,616 | \$1,021 | \$34 |
| Residential | \$97,737,025 | \$167,097,267 | 1,978 | \$66 |
| TOTALS | \$188,013,741 | \$327,035,001 | \$3,870 | \$129 |

Exhibit 4 provides the same breakdown by functional class but with the assessments being by parcel and not by property valuation. Current data shows 26,756 parcels listed in County records for the City of Chico that indicate taxable assessed valuation.

Exhibit 4 Estimated Total Financing Required by Functional Class and Parcel Numbers⁵

| Class | Roads Only | Complete Streets | Est. Per Parcel Total Contribution | Est. Annual per Parcel |
|---------------|----------------------|----------------------|------------------------------------|------------------------|
| Arterial | \$38,945,314 | \$73,625,118 | \$2,752 | \$92 |
| Collector | \$51,331,402 | \$86,312,616 | \$3,226 | \$108 |
| Residential | \$97,737,025 | \$167,097,267 | \$6,245 | \$208 |
| TOTALS | \$188,013,741 | \$327,035,001 | \$12,223 | \$407 |

While these values are reasonable, a more detailed analysis by professionals should be performed prior to any formal decisions regarding the revenue required or the debt financing approach.

Public Works staff also indicate investment in roadway improvements is also a cost-effective measure to reduce overall operating and maintenance costs of personal vehicles. The Federal Highway Administration estimates that for every dollar spent on road, highway and bridge improvements, returns approximately \$5.20 in the form of lower vehicle maintenance costs, lower road and bridge maintenance costs, and reduced emissions as a result of improved traffic flow.

Process for Initiating Bond Financing

The City recently refunded its Successor Agency Debt. The process involved bringing in Financial Advisors that were selected by a competitive process. From there, the Financial Advisors assisted staff to go through the various steps required to conduct a debt financing, or in our case, a refunding of existing debt. Additionally, the City has also created Community Facilities Districts (CFDs) which require tax levies on properties deriving a specific benefit; however, the City has not issued debt financed by CFDs.

Any future bond financing will follow very similar steps as the refunding generally. However, most debt issuances of the large nature required also necessitate voter approval. **Exhibit 5** highlights the approval requirements for a variety of financing mechanisms.

⁴ This is estimated based on \$8,449,685,363 as the net assessed valuation for property and an assessment based on property value. The estimated annual per parcel is based on a 30-year repayment and does not include any calculation for interest rate of return.

⁵ This is estimated based on 26,756 parcels. The estimated annual per parcel is based on a 30-year repayment and does not include any calculation for interest rate of return.

Exhibit 5 Summary of Approvals for Financing Mechanisms

| Financing Mechanism | Governing Body Approval | Voter Approval |
|--|---|--|
| City or county “general” taxes (revenues used for unrestricted purposes) | If consolidated with a regularly scheduled election of members of the Council <ul style="list-style-type: none"> • 2/3 for transactions & use taxes • Other taxes: 2/3 for general law cities; majority for charter cities. If not consolidated, unanimous declaration of “emergency” required. | Majority |
| City or county “special” taxes (revenues used for specific purposes) | Majority (2/3 for transactions & use taxes) | 2/3 |
| General obligation bonds | Majority | 2/3 |
| Other debt | Majority | None |
| Property assessments | Majority | Majority of affected property owners. Votes weighted by assessment liability |
| Property – related fees | Majority | 2/3 of voters or majority of affected property owners ⁶ |
| Fees – all other | Majority | None |

Survey of Other Tax Revenue Measures in California (Examples of Successful Bonds)

Since 2001 and through the November 2016 election, over 3,500 local revenue measures have been placed before local voters concerning school, city, county or special district taxes or bonds. Over a quarter of these measures concerned city or county general purpose taxes requiring majority voter approval; about a third were 55 percent approval school bonds; and the rest were parcel tax or special tax measures requiring two-thirds supermajority approval. **Exhibit 6** highlights this information.

Exhibit 6 Local Revenue Measures Since 2001 through November 2016⁷

| Measure | Total | Pass | Passing Rate |
|------------------------------------|--------------|-------------|---------------------|
| City Majority Vote | 832 | 612 | 74% |
| County Majority Vote | 94 | 53 | 56% |
| Special District Fee Majority Vote | 3 | 2 | 67% |
| City (2/3 Vote) | 373 | 191 | 51% |
| County (2/3 Vote) | 138 | 60 | 43% |
| Special District (2/3 vote) | 424 | 196 | 46% |
| School Parcel Tax (2/3 vote) | 351 | 228 | 65% |
| School Bond (2/3 vote) | 50 | 17 | 34% |
| School Bond (55% vote) | 1213 | 1026 | 85% |
| TOTAL | 3478 | 2385 | 69% |

Attachment 5 provides summaries from the elections of 2016-present. This includes pass/fail rates, types of purpose for the funding, and is the most detailed compilation of data for these types of elections. While we could use case examples, each case is different, and a higher-level summary is more informative at this stage.

⁶ No vote required for gas, electric, water, sewer, refuse, or developer fees.

⁷ Data from Michael Coleman, California City Finance, <http://www.californiacityfinance.com/>

Next Steps to Proceed

After discussion at the Finance Committee, the next step is to perform a statistical public survey poll to ascertain the appetite for various tax measures and the degree of willingness to fund certain activities. This survey poll should provide objective information to inform any decision of the City Council. Given the cost, it is prudent to proceed with a poll testing broad financing mechanisms (bonds and revenue measures) as well as to identify the strength of different activities to fund through these financing mechanisms.

Based on the results of the survey, the City Council should deliberate whether to proceed, and if so, the types of activities and financing mechanisms to utilize. City staff can evaluate more specifically the need and impact of any decision depending on the decision made by the City Council. From there, the City Council would be presented with the appropriate documentation for approval, and the measure will be placed on the ballot.

Current Case: City of Oceanside

Currently, the City of Oceanside is seeking a revenue measure in November 2018, and they utilized one of the two polling firms identified through our research. **Attachment 6** provides the completed Revenue Measure Feasibility Study, and **Attachment 7** provides the Oceanside City Council's agenda item implementing the decision for their tax measure. This example is provided to see the information obtained from the public and the translation into an actual measure. As of November 13, 2018, it appeared the Oceanside measure was holding above 50%, the required threshold for passing, and was at 54.7%.

CONCLUSION:

The Finance Committee unanimously recommended engaging a professional consultant to perform a tax feasibility voter survey. City staff support obtaining objective information to inform future decision-making. Performing such a survey does not obligate the City to proceed with a tax measure.

Prepared by:



Chris Constantin,
Assistant City Manager

Recommended and Approved by:



Mark Orme,
City Manager

DISTRIBUTION:

City Clerk

ATTACHMENTS:

- Attachment 1 – Voter Opinion Survey prepared by True North Research
- Attachment 2 – Public Works Zones for Street Maintenance
- Attachment 3 – Street Condition Report
- Attachment 4 – City Revenue Matrix
- Attachment 5 – Summary of Past Elections
- Attachment 6 – Oceanside Revenue Measure Feasibility Study
- Attachment 7 – Oceanside City Council Agenda Item – Tax Measure



ATTACH 1

**Proposal for the City of Chico
Voter Polling Services**

Prepared for:
The City of Chico

Submitted by:



EMC Research, Inc.

October 3, 2018

Contact:

Emily Goodman | Senior Vice President | Emily@EMCresearch.com
436 14th Street, Suite 820 | Oakland, California 94612
(510) 844-0680

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October 3, 2018

Chris Constantin
Assistant City Manager
City of Chico
411 Main Street
Chico, CA 95927

Dear Mr. Constantin:

Thank you for this opportunity to submit our proposal to conduct public opinion research for the City of Chico. EMC Research, Inc., (EMC) is a full-service opinion research and strategic consulting firm serving a broad range of public, private, non-profit, and institutional clients. Founded in 1989, EMC has been involved in thousands of public opinion studies, ranging from public policy strategy polls to extensive market share and customer/resident satisfaction surveys.

EMC Research has assisted numerous cities, towns, public agencies, and other public clients with research to determine community satisfaction and opinions on local and regional issues. In particular, we have strong experience in research designed to develop local revenue measures. We conducted research on behalf of the Chico Unified School District in anticipation of their successful November 2016 bond measure, and have done research for the Chico Area Recreation and Park District to assess community priorities and explore preliminary feasibility of a potential future revenue measure.

Members of our team have guided more than 350 revenue measures in California, raising over \$40 billion at the local level. We know firsthand what it takes to be successful in a project like this one. Our collective career experience includes numerous successful tax measures in difficult economic and political environments since the start of the recession. We will use our experience and insights throughout the project to identify challenges, develop effective strategies, and position the City for success.

We look forward to the opportunity to work with you on this important project. Should you have any questions about our proposal, please do not hesitate to contact me directly (510-550-8932; emily@emcresearch.com).

Sincerely,

A handwritten signature in black ink, appearing to read "Emily Goodman", written in a cursive style.

Emily Goodman
Senior Vice President, EMC Research

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Firm Profile

Firm Overview

EMC Research is a full-service market and opinion research firm serving a broad range of clients, including public and private corporations, state and local governments, nonprofit organizations, public agencies, and political campaigns. EMC Research is a team of over 50 professionals with decades of research experience that we apply to every project. Our firm has offices in Columbus, OH; Irving, TX; Oakland, CA; Orlando, FL; Portland, OR; Seattle, WA; and Washington, D.C.; and we have conducted research in all 50 states and several foreign countries. EMC is a member of the Insights Association (formerly the Marketing Research Association) and the American Association for Public Opinion Research, the two leading industry groups for the market research and public opinion industry, and is the United States representative to IRIS, the world's largest international network of market research providers.

EMC has built its reputation as an innovative firm, and is one of the premier firms in California on public policy issues. Our expertise in research is based on academic training and real-life application. We tailor methodologies to achieve your objectives, making data applicable to your strategic goals and providing ongoing consulting to apply actionable recommendations.

Our research capabilities include:

- Telephone Surveys
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- Predictive Analytics
- Microtargeting
- Electorate Profiling
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- Multi-lingual Research
- Advertising Effectiveness Tracking
- Customer Satisfaction Research
- Market Segmentation
- Market Positioning Studies
- Consumer Behavior Studies
- Conjoint and Maxdiff Analysis
- Multivariate Analysis

When you hire EMC Research, you bring to the table a committed team of research and consulting professionals who will listen to your needs, design the best combination of research methods, collect reliable and accurate data, and synthesize research results into a product that is easy to understand and apply to further your strategic goals and initiatives. EMC Research stands out from other market and opinion research firms in several key ways:

- **We make complex research understandable.** We provide research as a service, not a product. Our goal is to provide you with actionable research-based recommendations that help you achieve your goals.
- **We know what works.** We combine years of practical experience with an in-depth academic understanding of the science of research. We've conducted thousands of research projects using every imaginable research methodology. This ensures that the methodology and analysis employed on each and every project is scientifically sound and practical.

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- **We start at the beginning.** Every step of the research process is important because each step builds on the last. But the first step, framing and focus, is critical to designing a research program that meets your needs. We start each project by working with you to clearly and completely define your overall goals and how (and if) those goals can best be served by research. We will tell you if research is the wrong answer.
- **We take a multidisciplinary approach.** Our principals and staff come from a wide variety of academic and professional backgrounds, and we conduct research on a broad range of issues and topics. We often find creative solutions by applying the lessons learned in a different type of research.
- **We know what it's like.** We're a small business so we know and appreciate the real life constraints that all of our clients face, such as limited budgets and tight timelines. Our experience allows us to recommend the best research program to meet your budget and timeline while getting you the information you need.
- **We're there when you need us.** We are committed to your success, and that's why we provide substantial and ongoing consulting for as long as you use our research. And we do it at no extra cost.
- **We're a good company.** You'd be hard pressed to find a company that treats its employees better. Why does this matter to you? Because employees who are treated well care more about what they do and it shows in every aspect of their work. And they stay around longer, which means they bring added experience to every project.

Public Clients

EMC Research has nearly 30 years of experience conducting research on behalf of public agencies and governments throughout Northern California on local issue and revenue measures. We have years of tracking data on voter opinions on a variety of issues and have a deep understanding of voter reactions to increasing taxes. We are well-versed in the differences between counties, cities, ethnicities and other demographic sub-groups that make-up our diverse region and are well aware of the timelines and processes brought about by a public environment. Our work is respected and familiar to many local public figures and has been used in the development, timing, wording and strategy for assessment districts, general obligation bonds, parcel taxes and sales taxes throughout the region. We are pleased to have been a part of the millions of dollars of improvements to the environment, parks and open space, schools, public safety, transportation, and health care brought about by these measures.

Our experience also includes navigating public contracting processes, presenting politically sensitive opinion research in a public setting, dealing with media inquiries, and meeting open records requirements. Many of our local and state government clients continue to return to EMC Research year after year to help them keep in touch with the needs and priorities of residents or customers.

Some of our recent city and local municipality revenue measure projects/clients include:

| | |
|--|--|
| Alameda County Transit..... | Public Transit Tax |
| Alameda County Congestion Management Agency | Transportation Sales Tax, Vehicle Fee |
| Alameda County..... | Sales Tax |
| Alameda Unified School District | Parcel Tax |
| Cabrillo Unified School District | Parcel Tax and Bond |
| Cambrian School District..... | Parcel Tax and Bond |
| Chico Unified School District..... | Bond |
| City of Oakland..... | Parcel Tax |
| Contra Costa Community College District..... | Bond |
| Contra Costa Transportation Authority | Transportation Sales Tax |
| Cupertino School District | Parcel Tax and Bond |
| Davis Unified School District..... | Parcel Tax |
| City of Foster City | Levee Bond Measure |
| Los Altos School District..... | Parcel Tax and Bond |
| Los Gatos-Saratoga High School District..... | Parcel Tax and Bond |
| Menlo Park City School District..... | Parcel Tax and Bond |
| MidPeninsula Regional Open Space District | Open Space Bond |
| Mill Valley School District | Bond, Parcel Tax |
| Milpitas Unified School District..... | Parcel Tax and Bond |
| Mt. Diablo Unified School District..... | Bond, Parcel Tax |
| Moreland School District | Parcel Tax and Bond |
| Peninsula Open Space Trust | Open Space Bond |
| Peralta Community College District | Parcel Tax |
| San Francisco Bay Restoration Authority..... | Bay Restoration Parcel Tax |
| San Francisco County Transportation Authority..... | Transportation Sales Tax and Vehicle Fee |
| San Francisco..... | Transportation and Housing Bond |
| San Jose Unified School District..... | Parcel Tax |
| Santa Clara County..... | General Sales Tax and Bond |
| Santa Clara County Parks and Recreation Department | Charter Renewal Measure |
| Santa Clara Valley Water District..... | Flood Protection Parcel Tax |
| Santa Clara Valley Open Space Authority | Open Space Parcel Tax |
| San Ramon Valley Unified School District..... | Parcel Tax |
| Saratoga School District | Parcel Tax |
| Solano County Transportation Agency | Transportation Sales Tax |
| Sonoma County Water Agency | Parcel Tax |
| Stanislaus County..... | Marijuana Regulation and Taxation |
| Union School District..... | Parcel Tax and Bond |
| West Valley-Mission Community College District..... | Bond |



Firm Experience

Local Government Experience

EMC Research is in its third decade conducting opinion research for a wide variety of public, private, not for profit, and quasi-governmental clients. EMC has assisted public entities with gathering feedback from residents to guide strategic decisions, acknowledge citizen priorities, enhance communication with the community, determine the feasibility of potential ballot measures, and gauge residents' satisfaction with quality of life issues, governance, and city services.

Case Studies

The following case studies highlight some of the research we've conducted for local municipalities, as well as research that is similar in scope to what we are recommending in this proposal.

Chico Unified School District – Bond Measure Feasibility

In the fall of 2015, Chico Unified School District hired EMC to conduct a feasibility study regarding a potential bond measure to fund facility improvements at the District's schools. Chico USD used the survey to explore two potential bond amounts as well as the best timing for the measure. Our research found that voters were supportive of a \$152M bond measure, and we recommended that the District move forward with placing a measure on the November 2016 ballot. EMC went on to assist the privately funded campaign effort, and the measure ultimately passed with 71% support.



City of Oakland – Revenue Measure Feasibility

The City of Oakland has turned to us time and time again over the past 15 years for public opinion research services. With a wealth of experience working with elected officials, presenting survey results in public meetings, and crafting public releases of survey data, EMC is able to work with diverse elected officials, staff, and other stakeholders, using the research process to help build consensus and get everyone pointed in the same direction.



Most recently, the City of Oakland worked with EMC Research to assess voter priorities regarding Oakland infrastructure improvements. Two surveys showed that an infrastructure bond for the City to fund categories like affordable housing, street/infrastructure, and public safety was well-supported by the public. The City used the results of the research to help to prepare for Measure KK, which appeared on the November 2016 ballot and was passed by voters with 82% approval.

We also provided research for the City's successful renewal of the parcel tax and parking charge fee to fund public safety and violent prevention programs, 2014's Measure Z.

In addition to the work described above, EMC has worked with the City of Oakland numerous times over the past fifteen years, on research leading up to the Measure DD waterfront bond, and the reestablishment of the Oakland Hills fire prevention assessment.

City of Walnut Creek, CA – Community Priorities Survey

EMC was hired by the City of Walnut Creek for a set of surveys to understand community sentiment about living in Walnut Creek, measure community awareness of the city's fiscal challenges, capture community priorities and values around city services, and gauge attitudes about revenue measures and other potential solutions to the city's fiscal challenges. One of the challenges of this work was that the city is faced with very difficult budgeting decisions, and they wanted very granular data on how the public would approach the tough choices they are being faced with. The type of analysis they were interested in would have been simple in an online survey, but because the city wanted the reliability of a telephone survey, it had to be adapted for administration over the telephone. For the initial survey under this work, EMC developed a method where we asked a number of short forced choice priority questions about ten different budget categories, which we combined to enable a full pairwise comparison of the ten budget items without undue respondent burden or confusion. This was followed by two additional surveys to more explicitly test the feasibility of placing a city revenue measure on a future ballot. All three surveys were designed with explicit input from a subcommittee of the City Council and key staff members including the Chief of Police, a step that took additional time compared with working only with staff, but certainly helped with the public presentation of this complex study design and survey results.

*Alameda County Transportation Commission – Revenue Planning Surveys*

EMC has worked with Alameda County Transportation Commission (Alameda CTC) (formerly, the Alameda Congestion Management Agency and the Alameda County Transportation Improvement Authority) for more than 15 years. Projects have included:



- Transportation sales tax measures in 2000, 2012, and 2014
- A Vehicle Registration Fee measure in 2010 (Measure F)
- A longitudinal study about participation and advertising penetration for Bike to Work Day
- A paratransit rider survey
- A survey of contractors about their outreach process
- Research on HOV and express lanes.

With a 22-member board that includes representatives from the 14 cities in the county, the entire Board of Supervisors, and two major transit providers (BART and AC Transit), one of the critical pieces of the work we do is ensuring we are addressing the needs of every stakeholder, as well as key staff and community groups. In our work for Alameda CTC, we have presented research plans and results to the full Board, as well as Board subcommittees, technical committees, citizen advisory committees, and staff groups. We work hard to use the research to help the agency build the internal consensus they need, to ensure they are presenting a united front when pursuing a course of action, whether it's making decisions about expenditure allocations, or choosing an election for a ballot measure.

Much of our work for Alameda CTC has focused on planning for their transportation sales tax measures. We worked with the agency in 2000 on their successful Measure B, which got 81.5% of the vote. Leading up to the 2012 election, EMC worked with Alameda CTC in planning Measure B1. Research showed the election was likely to be very close, and EMC worked with the agency to develop ballot language and a

communication strategy designed to optimize support. Unfortunately, Measure B1 missed reaching the two-thirds threshold by just a few hundred votes, finishing with 66.53% support. Following that election, we conducted a survey and focus groups to better understand what the measure did not pass, and we worked closely with the agency again to develop Measure BB, which passed in November of 2014 with nearly 70% support.

References

EMC Research is happy to provide additional references or case studies as requested.

Chico Unified School District, CA – Bond Measure Feasibility

Contact Name: Kevin Bultema | **Title:** Assistant Superintendent | **Address:** 1163 East Seventh Street, Chico, CA 95928 | **Phone:** (530) 891-3000 | **Email:** kbultema@chicousd.org

City of Oakland, CA – Revenue Measure Feasibility

Contact Name: Christine Daniel | **Title:** Assistant City Administrator | **Address:** 1 Frank H. Ogawa Plaza Oakland, CA, 94612 | **Phone:** (510) 238-6906 | **Email:** cdaniel@oaklandnet.com

Alameda County Transportation Commission – Revenue Planning Surveys

Contact Name: Art Dao | **Title:** Executive Director | **Address:** 1111 Broadway, Suite 800 Oakland, CA, 94612 | **Phone:** (510) 208-7402 | **Email:** adao@alamedactc.org

City of Walnut Creek, CA – Community Priorities Survey

Contact Name: Ken Nordhoff | **Title:** Principal, HdL Companies (Former City Manager) | **Address:** 1340 Valley Vista Dr., #200, Diamond Bar, CA 91765 | **Phone:** (909) 861-4335 | **Email:** knordhoff@hdlcompanies.com

Approach, Scope and Timelines

Demonstrated Understanding and Scope of Services

The purpose of this public opinion research project is to provide statistically valid quantitative data regarding public attitudes and opinions of City of Chico voters towards city services as well as to gauge support for a potential revenue measure to be placed on a future ballot.

EMC's work will provide the City with answers to questions needed to make important strategic decisions such as:

- How should a revenue measure be structured in order to most likely meet with success?
- What is the appropriate timing for a measure?
- What are the types of projects that are most important to those who will vote?
- What are the themes and messages that will assist in reaching a successful outcome?
- Would a measure be vulnerable to opposition?
- What are suggested areas for public education and community outreach?

For this survey, EMC will work with the City of Chico team and other key stakeholders to design a survey instrument that is scientifically sound. By employing an inclusive research design process, we can enhance confidence in the survey results and help build consensus in making important strategic decisions based on the research results.

Research Design Approach and Scope of Work

For this project, we recommend a mixed-mode telephone and online study among 400 voters in the City of Chico. We believe this approach will yield scientifically sound and actionable data, and provide a sufficient sample size at a competitive price. The telephone survey portion will be conducted on landlines and cell phones. We have the capacity to offer the survey in Spanish in addition to English, but given the makeup of the electorate, we do not think this is statistically necessary. The overall sample will be demographically and geographically representative of voters in the City. Given the scope of information needed, and the revenue measures the City wishes to test, we estimate an average interview length of 15 minutes will be required.

By employing a mixed-mode/hybrid methodology where we will conduct interviews both by phone and online, we will maximize sample size and survey reliability within a reasonable budget. As the database of available voter emails becomes increasingly robust, we have been utilizing this approach in other communities and have had great success. We have found that the two modes complement each other well; we are able to obtain data from a broad cross-section of registered voters because those who may not answer their phone may take the survey online, and those who do not respond to the email survey invitation may take the survey over the phone.

To conduct the online portion of the survey, EMC would pull a list of likely voters with email addresses from a sample vendor that maintains a robust voter file with email addresses and supplements the email list with commercial lists. We will send an email invitation to respondents that includes a web link to take the survey. We take several steps to ensure that our online voter survey respondents are

demographically representative of likely voter universes through targeting reminders via email to harder to reach populations.

For this project, EMC will complete the following:

- Develop final research design in consultation with the client;
- Design a survey questionnaire in consultation with the client;
 - *Deliverable: final poll questionnaire*
- Collect survey data through interviews conducted by telephone from a central telephone bank, supervised by an on-duty supervisor;
- Adapt the questionnaire for online application; program and host the survey using professional survey software; manage the deployment of survey invitations via email; and collect survey responses online;
- Monitor data collection daily to ensure the process is going according to plan, and adjust strategies as-needed to ensure a representative sample of the City's likely voter universe;
- Tabulate, code, clean, and weight the survey data from both modes;
 - *Deliverable: topline poll results*
- Perform in-depth analysis of the data;
- Produce cross-tabulations of voter responses based on key demographic information, such as but not limited to geography, ethnicity, age, homeownership status, etc.;
 - *Deliverable: crosstabs*
- Prepare a report of results with charts, analysis, and recommendations;
 - *Deliverable: detailed report, including graphic presentation of key findings, analysis and recommendations, provided electronically for distribution*
- Present results and analysis to key stakeholders;
- Be available for consultation on findings and strategy.

Timeline

EMC Research is available to begin work on this project immediately upon acceptance of our proposal. Outlined below is a possible timeline for the voter survey. Once the project gets underway, EMC Research will develop a specific timeline with exact dates for key deliverables. This specific timeline would take into consideration the specific needs of the client, including stakeholder meetings, election timelines, and other considerations.

In addition, EMC Research will take into account outside factors that can influence research timing, such as national holidays, school breaks, and other events.

| | |
|-------------------|---|
| Week 1: | Project kick-off meeting; review any past research and other background materials; identify key project goals and objectives; begin drafting questionnaire. |
| Weeks 2-3: | Provide draft questionnaire; conference call(s) or in-person meetings as necessary to review and edit the questionnaire; submit revised drafts as needed. |
| Week 3: | Final sign-off on questionnaire. |
| Week 3-4: | Purchase sample of likely voters; conduct live telephone interviews (anticipated fielding window of 2 weeks). |
| Week 5: | Provide topline survey results and draft PowerPoint presentation of results; meeting or teleconference to discuss. |
| Timing TBD | Detailed presentation of results, analysis, and specific strategic recommendations to City of Chico City staff and key stakeholders. |



EMC Research Assignments

We ensure each client gets attention from every member of our project team to handle this research project in an expedient manner. The following is the EMC Research team that would be assigned to work on behalf of the City of Chico:

- **Emily Goodman, Senior Vice President.** Emily will be available for calls and/or meetings with the City to discuss the project approach and timeline, provide input on drafting the questionnaire, be available for calls and/or meetings with the City to discuss questionnaire revisions, develop key findings and recommendations after data collection is complete, and be available to meet with the City to discuss and present survey results.
- **Sianna Ziegler, Senior Analyst.** Sianna will help manage the research project and will work closely with Emily in the supervision of all aspects of the project, including the development of the survey, sample design, fielding, and the preparation of all deliverables and reports. She will be available for all calls and meetings needed throughout the course of the project, and for the presentation of results as needed.
- **Research Analyst.** EMC will assign a research analyst to assist with questionnaire edits, data collection, management, and analysis, and creation of deliverables. The research analyst will be supervised by senior management and all deliverables created by the research analyst will be thoroughly reviewed and checked for accuracy.

Emily Goodman, Senior Vice President

Emily Goodman joined EMC in 2012 after working at Hart Research Associates in Washington, D.C. for six years.

Emily brings her lifetime of political experience and her Texas charm to every project, treating each client as unique and important. She is hands-on, making sure that her polling isn't simply following a formula, but addresses the specific challenge at hand.

Pulled into politics by her mom who served as a councilmember and mayor, Emily learned to walk precincts, get out the vote, and debate local issues from an early age. She has turned that into a career of polling at all electoral levels. Her expertise includes Presidential, U.S. Senate, and gubernatorial races, but she especially loves local candidate and issue races where she can be deeply involved and see the impact of change.

Emily has her B.A. in Political Science from Texas A&M University and her M.A. in Political Communication from Johns Hopkins University. Her thesis was about electing women to office, something that continues to be a passion. Emily loves living in California with its year-round access to outdoor activities, but continues to be passionate about her Dallas Cowboys.

Some of Emily's recent clients include the City of Oakland, the City of Citrus Heights, the City of San Francisco, and other municipalities in California. Emily has successfully designed and fielded the research instruments that helped these cities pass revenue measures that currently fund essential municipal services, from the filling of potholes to early childhood education.



Sianna Ziegler, Senior Analyst

Sianna Ziegler joined EMC Research as a research analyst in 2015 after spending three years at the Mellman Group in Washington, D.C. She has experience in public opinion research at all levels, from local school and parks districts to national polls, and has worked with a wide variety of clients including candidates, public agencies, unions, and environmental groups.

Sianna has a BA in Psychology from Reed College and an MS in Social Psychology from the University of Washington. As a graduate student, she focused her studies on stereotyping and prejudice, conducting research on the impact of race attitudes on voting behavior in the 2012 Presidential election as well as contributors to women's underrepresentation in computer science and engineering. She enjoys putting her training in statistics and research design into practice on behalf of clients.

Sianna spends her spare time exploring Puget Sound and cheering on the Seattle Reign with her husband.

Costs

The cost to conduct the mixed-mode survey of 400 likely voters in the City of Chico would be **\$22,850**.

In addition to all aspects of conducting the survey, the proposed cost include reasonable consulting and presentation of results. This is our start to finish, all-inclusive cost for all services and deliverables outlined in this proposal. We understand that you are receiving bids from other firms. Should your final decision come down to cost, we would love an opportunity to price match competitive bids.

Thank you for considering EMC Research to conduct this important study of voters in the City of Chico!

VOTER OPINION SURVEY
SCOPE OF SERVICES & AGREEMENT

PREPARED FOR THE
CITY OF CHICO



SEPTEMBER 28, 2018



1592 N COAST HIGHWAY 101
ENCINITAS CA 92024
760.632.9900 WWW.TN-RESEARCH.COM

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COVER LETTER

True North Research (True North) is pleased to present this proposal to provide voter opinion research and revenue measure consulting services to the City of Chico (City). Based on recent discussions with Chris Constantin (Assistant City Manager) as well as our experience conducting hundreds of similar studies for public agencies throughout California, True North understands that the City is interested in using the survey to produce an *unbiased, statistically reliable* evaluation of voters' interest in supporting a local tax measure to provide funding for infrastructure and city services, as well as identify how to structure a measure so that it is consistent with the electorate's priorities and sensitivities. In short, the goal is to assess the feasibility of placing a local tax measure on an upcoming ballot and—if feasible—determine how best to package a measure for success.

Assuming the survey results are promising and the City chooses to move forward with a measure, True North is also prepared to assist the City and the City's communications consultant with preparing the revenue measure, as well as developing and implementing a successful public education campaign.

CHALLENGES To successfully meet the challenges of the proposed project, the City will need a consultant that has a diverse set of skills and experience. First and foremost, the consultant should possess a highly sophisticated understanding of survey research methodology to ensure that the resulting data are valid and reliable measures of voters' opinions, and that the results are representative of the individuals who may ultimately decide the fate of the revenue measure. This expertise should extend to all aspects of conducting surveys including sampling design, questionnaire design, data collection, data processing, weighting, statistical analysis, and presentation. In addition to statistical know-how, this study further requires a consultant that has ample experience working with public agencies translating positive survey results into effective public information campaigns and successful revenue measures. And, of course, the consultant must have the *availability* and a *commitment* to work closely with the City throughout the pre-electoral cycle.

WHAT SETS TRUE NORTH APART? The True North team has the expertise, experience, resources, and dedication needed to successfully overcome the challenges of the proposed study. Research is our passion—and it shows in the energy, expertise, and dedication we bring to all aspects of the research process and our client relationships. Although we provide more details in the body of this proposal, we thought it would be helpful to highlight at the outset what distinguishes True North from other research firms.

Highest Success Rate in California for Revenue Measures Creating revenue measures that are ultimately approved by the necessary percentage of voters is difficult, especially in the State of California. Successful measures require insightful research and sound, strategic advice. One testimony to the accuracy of our research and the reliability of our strategic advice is that we have the highest *verifiable* success rate in the State of California for revenue measures since 2008 (95%), and a 100% success rate in 2016 (45 wins/45 measures on the ballot in 2016). To date, we have helped raise over \$29 billion in voter-approved bonds and taxes. We will use our experience and insights throughout the project to identify challenges, develop effective strategies, and position the City's measure for success.

How We Manage Your Study Most research firms (and all of the larger firms) rely heavily on delegation to manage their work flow. Although they may send the Principals to the interview, the unfortunate reality is that once they 'win' a project the actual research tasks—design, data processing, data analysis, report writing, presentation building—are promptly delegated to junior-level staff.

At True North, we follow a very different model for managing your project. We believe the best approach to conducting your study is to have the person in our organization with the *most* expertise and experience roll-up his sleeves and actually do the work. At True North, who you see is who you get. True North's President (Dr. McLarney) will personally manage more than 75% of the research tasks for the proposed survey.

Value-Added Services We go the extra mile for our clients by providing value-added research, project management, and modeling services that other firms do not. In addition to the standard deliverables (survey report & crosstabulations), we provide additional services including advanced statistical modeling (regression analysis) and targeting analyses. These value-added services will ensure that the City receives the most *accurate* and *insightful* research, and are very helpful in developing effective strategies for building and sustaining community support for a tax measure.

The True North team is excited to partner with the City of Chico on this important project. We think that you will find our qualifications, our attention to client service, and our interest in this study to be exceptional. Should you have any questions about this proposal, please do not hesitate to contact me by email at mclarney@tn-research.com or by phone at 760.632.9900. I will serve as the City's point of contact and Project Manager throughout the entire project.

Sincerely,



Timothy McLarney, Ph.D.
President
True North Research
1592 N. Coast Highway 101, Encinitas CA 92024
p: 760.632.9900
mclarney@tn-research.com

QUALIFICATIONS & EXPERIENCE

TRUE NORTH OVERVIEW True North is a full-service survey research firm that is dedicated to providing cities and other public agencies with a clear understanding of the opinions, perceptions, priorities and concerns of their residents and voters. Through designing and implementing scientific surveys, focus groups and one-on-one interviews, as well as expert interpretation of the findings, True North helps its clients to move with confidence when making strategic decisions in a variety of areas—such as planning, policy evaluation, passing revenue measures, and developing effective public information campaigns. To date, the Principals at True North have designed and conducted over 1,000 research studies for public agencies, more than 350 of which were tax measure feasibility studies.

True North Research was co-founded in 2002 as a two-person survey consulting firm by Dr. McLarney and Mr. Sarles and is an S-Corporation in good standing with the State of California. Although we have had many opportunities to expand our business during the past 17 years, we have chosen to maintain True North as a two-person firm. Dr. McLarney and Mr. Sarles still manage every aspect of every True North project. We are an extremely efficient, collaborative team of two—and we have chosen to keep it that way so that our clients continue to receive exceptionally high quality customer service and research insights. Dr. McLarney and Mr. Sarles are committed to working with the City of Chico for the duration of this project—there will be no staff turnover or changes.

True North provides all services related to a survey project (questionnaire design, sampling, data collection, data processing, weighting, descriptive analysis, advanced statistical analysis, reporting, and presentation), and we work collaboratively with our clients and their consultant teams to translate the survey results into effective communications strategies and successful measures.

PERSONAL VS. FIRM-LEVEL EXPERIENCE Before discussing our experience in more detail, we feel it is important to note the difference between *personal* experience and *firm-level* experience. It is very common for research firms to provide a long list of projects in their proposals in an effort to demonstrate that they are both well-qualified and highly experienced in conducting surveys of this type. The reality, however, is that this firm-level experience is often a game of smoke-and-mirrors. What they *don't* tell you is that their list of projects was managed by dozens of different project managers over many years, many of whom no longer work for the firm and only one of whom will be assigned to your project (along with junior-level staff).



The bottom line is that it doesn't matter how many projects or clients a firm has worked with over the years. Nor does the experience of the Principals or others at the firm matter if they aren't going to be the ones rolling-up their sleeves to work on your study. What matters is the experience, education, and expertise of the specific individuals who will be working directly on your project and handling each of the key research tasks.



For this reason, we take a different approach. The experience discussed throughout this proposal—and the research projects referenced below—are the *personal* experience of the individuals who will be working on *all* aspects of the survey for the City of Chico: Dr. McLarney and Mr.

Sarles at True North. We encourage the City to take this difference into account when reviewing our proposal.

REVENUE MEASURE EXPERIENCE Creating revenue measures that are ultimately approved by the necessary percentage of voters is difficult, especially in the State of California. Successful measures require insightful research and sound, strategic advice. One testimony to the accuracy of our research and the reliability of our strategic advice is that we have the highest *verifiable* success rate in the State of California for revenue measures since 2008 (95%), and a 100% success rate in 2016 (45 wins/45 measures on the ballot in 2016). To date, we have conducted over 350 revenue measure feasibility studies and helped raise over \$29 billion in voter-approved taxes, including more than \$5 billion in the 2016 election cycle alone. Our experience includes all types of revenue measures including general and special sales taxes, parcel taxes, bonds, transient occupancy taxes, utility users taxes, benefit assessments, and property-related fees.

CITY SURVEY EXPERIENCE True North is the State's leader in designing community and voter opinion survey research studies for California cities. Below is a sampling of the California cities for which Dr. McLarney and Mr. Sarles have personally designed and conducted surveys.

| | | | |
|----------------|----------------------|------------------------|------------------|
| Agoura Hills | Encinitas | Long Beach | San Carlos |
| Alhambra | Folsom | Los Angeles | San Clemente |
| Aliso Viejo | Fontana | Los Gatos | San Diego |
| Anaheim | Fremont | Madera | San Gabriel |
| Apple Valley | Gilroy | Malibu | San Jose |
| Atherton | Glendale | Manhattan Beach | San Luis Obispo |
| Azusa | Glendora | Menifee | San Marcos |
| Banning | Goleta | Mission Viejo | San Marino |
| Barstow | Grand Terrace | Moorpark | Santa Barbara |
| Beverly Hills | Half Moon Bay | Murrieta | Santa Clarita |
| Brea | Hesperia | Norwalk | Santa Monica |
| Burbank | Huntington Beach | Oceanside | South Lake Tahoe |
| Campbell | Indian Wells | Oxnard | South Pasadena |
| Cerritos | Irvine | Palmdale | Temecula |
| Chino | Kerman | Petaluma | Thousand Oaks |
| Chula Vista | La Canada-Flintridge | Pico Rivera | Truckee |
| Claremont | La Palma | Placentia | Upland |
| Clayton | Laguna Beach | Port Hueneme | Ventura |
| Costa Mesa | Laguna Niguel | Rancho Cucamonga | Watsonville |
| Cudahy | Lake Elsinore | Rancho Palos Verdes | West Hollywood |
| Cypress | Lake Forest | Rancho Santa Margarita | Westlake Village |
| Dana Point | La Mesa | Redlands | Whittier |
| Del Mar | Lathrop | Redondo Beach | Yorba Linda |
| Diamond Bar | La Verne | Riverside | Yucca Valley |
| East Palo Alto | Lemon Grove | Rocklin | |
| El Cajon | Lompoc | San Anselmo | |

REFERENCES

We encourage the City to contact our references and ask about the quality of work performed by True North.

City of Chula Vista True North assisted the City of Chula Vista with feasibility research and strategic advice that led to the successful passage of a local sales tax measure on the November 2016 ballot, and the passage of a *second* local sales tax on the June 2018 ballot. We also assisted the City with research and strategic consulting related to a cannabis measure to appear on the November 2018 ballot. Contact: Gary Halbert, City Manager. 276 Fourth Avenue, Chula Vista CA, 91910; 619.691.5002.

City of Del Mar True North assisted the City of Del Mar with feasibility research and strategic advice that led to the successful passage of a general sales tax measure on the November 2016 ballot. Contact: Scott Huth, City Manager. 1050 Camino del Mar, Del Mar CA, 92014; 858.755.9313.

City of La Palma True North provided research and strategic advice to the City of La Palma that led to the successful passage of a general tax measure on the November 2016 ballot. Contact: Laurie Murray, City Manager. 7822 Walker St., La Palma CA 90623. 714.690.3338. Reference for True North only.

City of Madera True North provided the City of Madera with research and strategic consulting that led to the successful passage of a general tax on the November 2016 ballot. Contact: David Tooley, City Administrator. 205 West Fourth Street, Madera CA 93637; 559.661.5400.

Town of Truckee True North provided research and strategic advice that led to two successful special tax increases for the Town of Truckee (2014 & 2008) dedicated to roads and trails. Contact: Tony Lashbrook, Town Manager. 10183 Truckee Airport Rd, Truckee CA 96161; 530.582.7700.

City of Oceanside True North recently assisted the City of Oceanside in exploring the feasibility of a general sales tax measure to fund city services and infrastructure. The measure will appear on the November 2018 ballot. Contact: Deanna Lorson, Assistant City Manager. 300 North Coast Highway, Oceanside CA 92054. 760.495.3055.

City of Manhattan Beach True North is currently assisting the City of Manhattan Beach in exploring the feasibility of a transient occupancy tax measure to fund general city services. True North also previously assisted the City in establishing benefit assessment districts to successfully fund utility undergrounding projects. Contact: Bruce Moe, City Manager. 1400 Highland Ave, Manhattan Beach, CA 90266; 310.802.5053.

City of Placentia Survey research, measure preparation and strategic advice led to a successful utility users tax measure in November 2008. True North also recently worked with the City to explore the feasibility of a November 2018 sales tax increase. Contact: Damien Arrula, City Administrator. 401 E. Chapman Ave., Placentia CA 92870; 714.993.8117.

PROJECT MANAGEMENT & RESUMES



Most research firms manage survey projects through division of labor and a *lot* of delegation. Although the Principals may appear for the interview and for client meetings, their involvement beyond that point is generally negligible. The Project Manager's role is to be the point of contact and help design the study, but even they only handle about 20% of the workload. The unfortunate reality is that most of the important research tasks—data processing, data analysis, report writing, presentation building—are delegated to junior-level staff.

The division of labor and delegation model is popular among research firms because it is scalable and built for *quantity*. Any firm that has just a few Principals, yet boasts multiple offices and several hundred studies every year, is certainly relying heavily on junior staff to get the work done. However, it isn't a model that is consistent with high quality research, careful attention to a client's needs, or insightful strategic advice.



We do things **differently** at True North. In fact, one of the main reasons we founded True North 17 years ago was because we recognized that there is a much better model for managing a research project—have the person with the *most* experience and expertise in the firm roll up his/her sleeves and actually do the work. At True North, who you see is who you get. Dr. McLarney will not only serve as the Project Manager and the day-to-day contact for the City, he will *personally* perform about 75% of the research tasks on the survey project including *all* design, analysis, report writing, and presentations. We have purposely kept True North as a single office, two-person consulting firm because of our commitment to this project management model and the many benefits it provides to our clients.

FIGURE 1 PROJECT ORGANIZATION & TASKS BY TEAM MEMBER

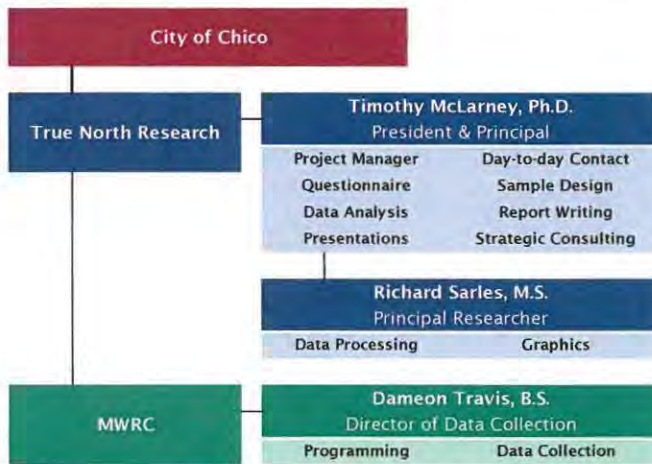


Figure 1 presents the organizational chart for the proposed study, as well as the tasks to be performed by each member of our team. True North Research will serve as prime contractor at the direction of the City of Chico. Dr. Timothy McLarney (President of True North) will be the Project Manager for the study and will perform all key design, analysis, and reporting tasks. Richard Sarles (Principal Researcher at True North) will assist with data processing and graphics production for the report.

The telephone interviews will be conducted by the same high quality, professional data collection firm that True North has teamed with to complete more than 800 voter and community surveys—MWRC. Dameon Travis (Director at MWRC) will manage the day-to-day interviewing schedule and quality control procedures in close consultation with Mr. Sarles and Dr. McLarney.

RESUMES AND ROLES For the City's reference, the following paragraphs provide additional information about Dr. McLarney's, Mr. Sarles', and Mr. Travis' respective roles on this project, as well as their education and research backgrounds.

Project Manager

Timothy McLarney, Ph.D., will serve as the Project Manager for the proposed survey and the City's point of contact on all matters related to the study. Dr. McLarney will lead all design, analysis, reporting and presentation tasks. Approximately 75% of the research tasks will be performed directly by Dr. McLarney.

Dr. McLarney has more than 20 years of experience in survey methodology, statistics, and public opinion analysis. As President of True North, Dr. McLarney is responsible for the design, management and analysis of True North's qualitative and quantitative research projects, including those that address community needs assessments, revenue measure feasibility, public policy and strategic planning issues. Dr. McLarney's work to date has provided California cities, counties, school districts, colleges, universities, special jurisdictions, transportation planning agencies, councils of government, and not-for-profits with research to address their often complex marketing, planning and/or performance measurement needs. During his career, Dr. McLarney has occupied a lead role in over 1,000 research studies, more than 350 of which had research objectives similar to those of the City of Chico.

Dr. McLarney is a nationally recognized expert in survey research methodology, sampling theory, weighting and the use of statistical methods to generalize survey results. His research has been recognized at numerous national and state conferences, has been published in academic journals, and has earned him honors including the title of Visiting Scholar at the Institute of Governmental Studies at UC Berkeley. He has also served as an independent expert witness in survey research methodology for California legal cases.

Dr. McLarney earned a Ph.D. and M.A. in Government from Cornell University with an emphasis in survey methodology, sampling theory and public opinion, as well as a Bachelor's degree in Politics from the University of California, Santa Cruz.

Principal Researcher

Richard Sarles, M.S., will assist Dr. McLarney with managing data collection, data processing, and graphics production for the report. Mr. Sarles will manage approximately 20% of the research tasks. As Principal Researcher and co-founder of True North Research, Mr. Sarles applies his expertise in statistics, sampling, weighting and analysis to a wide variety of research studies. A veteran of over 1,000 research studies, public and private clients throughout the country rely on Mr. Sarles' research to help them meet their performance and planning goals.

Prior to co-founding True North Research, Mr. Sarles was employed as the Associate Research Director at a west coast research firm. One testimony to Mr. Sarles' expertise is he authored a primer on survey research methodology and customer satisfaction studies for the California Library Association that was published to all institutional members in the state. Mr. Sarles received his M.S. in Experimental Psychology from Illinois State University with magna cum laude honors. He also received his B.S. in Psychology from Illinois State University, graduating with summa cum laude honors.

Data Collection Manager **Dameon Travis, B.A.**, is the Director and Project Manager of MWRC and will serve as the Data Collection Manager for this study. As he has for more than 400 of True North's survey projects, Mr. Travis will manage the data collection process and interviewing team and will report directly to Dr. McLarney and Mr. Sarles. Mr. Travis has extensive experience in managing all aspects of telephone interviewing, including sampling designs, CATI stations and programming, interviewer training, daily reporting procedures, and quality control processes. In the past five years alone, Mr. Travis has managed more than 500 studies such as the one proposed here. With a B.A. in Communications and Economics and more than eight years experience at MWRC working closely with Dr. McLarney and Mr. Sarles on similar studies, Mr. Travis will be an invaluable asset to the project.

SCOPE OF WORK

Trade Secret Notification: *This section of the proposal contains information and a description of services and project approaches and procedures that are trade secrets as defined by CA Civil Code Section 3426 - 3426.11. This proposal contains a description of a unique process, approach and procedures that are known only to True North Research and which are not known or used by other competitors. This section of our proposal should not be duplicated, distributed or shown to parties other than the individuals at the City of Chico who are involved with the Project—in whole or in part—without the expressed written consent of True North Research, Inc.*

INTRODUCTION TO OUR SURVEY APPROACH True North has developed a sophisticated research methodology for identifying whether voters will support a revenue measure and, if so, how best to package the measure for success. The key elements of our work plan and methodology are described in the following sections. **Although our research design is based on methods that are proven industry standards, our attention to detail and both the extent and manner in which we analyze the data are anything but standard.** In addition to the types of deliverables that all research firms provide (i.e., a report and crosstabulations), we go the extra mile by using multiple regression analysis to provide our clients with a much deeper and more reliable understanding of the factors that shape voters' opinions about a measure (see *Advanced Voter Behavior Analysis* on page 14). Through our proprietary *Targeting Tool*, we are also able to put this information at our clients' finger-tips in a way that helps them make sound, strategic decisions throughout the pre-electoral and electoral cycles (see *Targeting Tool* on page 15). True North is unique in providing these value-added services based on *advanced* statistical techniques.



COMMUNICATION Before discussing the 'nuts-and-bolts' of the research design, we'd like to first briefly discuss True North's approach to communicating effectively with the City on this study. At the outset of this study, True North recommends having a kick-off meeting with all individuals who will be actively involved in the study (or will be an important stakeholder) in attendance. The kick-off meeting is a great forum for talking about past research conducted and the City's needs, expectations, and concerns for the upcoming survey and deliverables. The kick-off meeting will also provide an opportunity for Dr. McLarney to clarify any questions about the proposed methodology, ask questions of City staff, and identify changes to the methodology (as proposed) that may better serve the City's interests. In short, the purpose of the kick-off meeting is to make sure that everyone is in agreement about the details of the study, identify areas that need further clarification, share materials, and establish a more detailed work plan for the project.

After the kick-off meeting, Dr. McLarney will keep in regular contact with the Project Manager identified by the City through telephone, email, and in-person meetings. Dr. McLarney will serve as the point of the contact for the City on all matters related to this project and will provide frequent updates (typically at least once per week) on the status of the project. These updates are generally made either by telephone or email, depending on the City's preference and the urgency of the topic.

SAMPLING METHOD In revenue measure research, the first step is to identify the population of individuals who will vote on the measure, then survey a representative sub-sample of these individuals. The appropriate set of individuals to be included in the sample thus varies depending on a number of factors. The expected election date, for example, is one factor that has a large influence on voter turnout and the profile of participating voters.

Although there are 45,385 registered voters who reside within the boundaries of the City of Chico, approximately 36,562 are expected to participate in the *high-turnout* November 2020 election. The projected turnout for a special election in 2019 is substantially lower (13,392). Moreover, as shown in Table 1, the demographic and political profile of the City's electorate changes somewhat across election environments. As turnout shrinks, the ratio of Republicans to Democrats increases, as does the voting power of Dual Republican households, older voters (50+), long-time residents, and home owners.

TABLE 1 PROFILE OF CHICO VOTERS: ALL VOTERS, LIKELY NOVEMBER '20, & LIKELY SPECIAL ELECTION '19

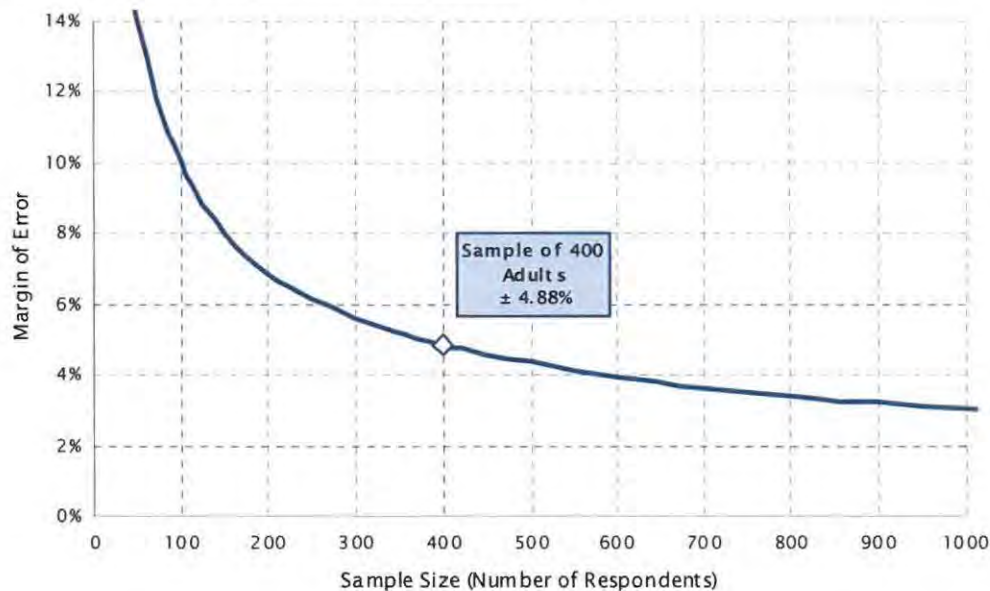
| Subgroup | All Registered Voters | | Likely November '20 | | Likely Special '19 | |
|-----------------------------|-----------------------|-----|---------------------|-----|--------------------|-----|
| | # | % | # | % | # | % |
| Total | 45,385 | | 36,562 | | 13,392 | |
| Party | | | | | | |
| Democrat | 19557 | 43% | 16170 | 44% | 6065 | 45% |
| Republican | 12333 | 27% | 10940 | 30% | 5105 | 38% |
| No Party | 10562 | 23% | 7299 | 20% | 1658 | 12% |
| Other | 2933 | 6% | 2153 | 6% | 564 | 4% |
| Household Party Type | | | | | | |
| Single Democrat | 12637 | 28% | 9919 | 27% | 3194 | 24% |
| Dual Democrat | 4363 | 10% | 3978 | 11% | 1984 | 15% |
| Single Republican | 5561 | 12% | 4568 | 12% | 1831 | 14% |
| Dual Republican | 4532 | 10% | 4298 | 12% | 2395 | 18% |
| Other | 10693 | 24% | 7218 | 20% | 1563 | 12% |
| Dem/Rep | 1636 | 4% | 1501 | 4% | 687 | 5% |
| Mixed | 5963 | 13% | 5080 | 14% | 1738 | 13% |
| Age | | | | | | |
| 18-29 | 11764 | 26% | 7014 | 19% | 344 | 3% |
| 30-39 | 7585 | 17% | 5925 | 16% | 961 | 7% |
| 40-49 | 6236 | 14% | 5389 | 15% | 1376 | 10% |
| 50-64 | 9565 | 21% | 8673 | 24% | 3822 | 29% |
| 65+ | 10099 | 22% | 9561 | 26% | 6889 | 51% |
| Not coded | 136 | 0% | 0 | 0% | 0 | 0% |
| Registration Date | | | | | | |
| Before 2004 | 8199 | 18% | 8002 | 22% | 4930 | 37% |
| 2004-2009 | 5736 | 13% | 5440 | 15% | 2883 | 22% |
| 2010-2015 | 7755 | 17% | 6675 | 18% | 2333 | 17% |
| 2016-2018 | 23695 | 52% | 16445 | 45% | 3246 | 24% |
| Ethnicity (surname) | | | | | | |
| Hispanic | 5263 | 12% | 3688 | 10% | 588 | 4% |
| Asian | 447 | 1% | 340 | 1% | 94 | 1% |
| Home Ownership | | | | | | |
| Owner | 22117 | 49% | 20270 | 55% | 9882 | 74% |
| Renter | 23,268 | 51% | 16,292 | 45% | 3,510 | 26% |
| Gender | | | | | | |
| Male | 19417 | 43% | 15475 | 42% | 5673 | 42% |
| Female | 21124 | 47% | 17641 | 48% | 6906 | 52% |
| Note Coded | 4844 | 11% | 3446 | 9% | 813 | 6% |

Given the City's potential interest in a 2019 or 2020 measure, we recommend selecting a sample of voters who are likely to participate in the high turnout November 2020 election on the natural or in response to a campaign's get-out-the-vote efforts. This strategy will also allow us to isolate the smaller number of likely special election 2019 voters nested within the larger November 2020 universe and thereby estimate how support for a potential measure may vary between the special election 2019 and November 2020 electorates.

Once the appropriate universe of voters is selected, the universe will be *stratified* by demographic characteristics that are known to influence voting behavior—such as partisan affiliation, household party composition, age, gender and geographic precincts. Voters will then be randomly selected into *clusters* within the appropriate strata. This sampling method ensures that the composition of the sample accurately reflects the composition of the electorate that is expected to participate in the election of interest.

SAMPLE SIZE By using a stratified and clustered random sampling design, True North will ensure that the sample is representative of likely voters in the City. The results of the sample can then be used to estimate the opinions of *all* likely voters in the City who are expected to participate in the election of interest. Because not all likely voters will participate in the survey, however, the results will have what is known as a statistical margin of error due to sampling. The margin of error refers to the difference between what is found from the sample of likely voters and what would have been found had *all* likely voters in the universe been surveyed.

FIGURE 2 MAXIMUM MARGIN OF ERROR DUE TO SAMPLING FOR CHICO



This is where sample size becomes important. The larger the sample, all other things being equal, the smaller the margin of error and the more confidence one can have that the survey results accurately reflect all adults in the City. To provide the City with a sense for how the statistical margin of error is related to sample size, Figure 2 plots the relationship between sample size and the *maximum*¹ statistical margin of error for the proposed survey. As illustrated in the figure, beyond a sample size of 400 the statistical margins of error begin to flatten-out such that

incremental units of additional sample result in smaller and smaller reductions to the statistical margins of error. For this reason, True North generally recommends a sample of 400 residents as it best balances the need for reliability with the costs associated with data collection. A sample of 400 completed interviews will have a maximum margin of error at the 95% confidence level of 4.88% (plus or minus).

Although 400 is the *minimum* guaranteed sample size for the study, it is also likely that we will deliver a larger sample size. One of the advantages of using two recruiting strategies and offering respondents the option to participate online as well as by telephone (see *Mixed-Mode Recruiting and Data Collection* on page 13) is that if the response to the email invitations is greater than our conservative estimate, the resulting sample size will be larger than 400 completed interviews. The additional completed surveys will be provided to the City at no additional cost.

QUESTIONNAIRE Creating revenue measures that are ultimately approved by the necessary percentage of voters is difficult, especially in the State of California. Successful measures require careful packaging—and it is in this area that the research performed by True North will be quite valuable. True North has developed a proven framework for the design of revenue measure survey instruments that captures the pieces of information needed for estimating the feasibility of a measure and, if feasible, determining how best to package the measure for success. The instrument will identify the issues that voters feel are most important, how providing additional funding for city projects and services ranks next to other important issues, baseline (natural) support for a measure, the tax rate that voters will support, the specific projects and improvements that voters are most interested in funding with measure proceeds, the information items that shape voter support for a measure, as well as how voter support for a measure may change once voters are exposed to the type of discussion and debate they will undoubtedly encounter during the election cycle. Collectively, this information will allow the team to determine if the City should move forward with a tax measure and, if so, how best to structure the measure and the steps that should be taken to maximize the measure's chances of success including a voter outreach plan.

True North will lead the questionnaire design and revision process in close consultation with City staff. Throughout the design process, we are careful to develop a questionnaire that covers the topics of interest and avoids the many possible sources of systematic measurement error, including position-order effects, wording effects, response-category effects, scaling effects and priming.

PRE-TEST & PROGRAMMING Once a refined draft of the instrument meets the approval of the City, it will be pre-tested internally and with approximately 20 voter households to ensure its integrity prior to finalization. Once any final changes are made in consultation with the City, the final survey will be CATI and web programmed for efficient telephone and online data collection. Based on our experience, a 15 minute survey instrument will capture the necessary information for this project.

1. The margin of error will vary depending on the distribution of responses to the question. The maximum margin of error occurs when the results are evenly split (50-50) between two response options.

MIXED-MODE RECRUITING AND DATA COLLECTION Quality telephone interviewing will be an important element of the proposed survey. Using a 'bargain' phone facility is rarely a bargain, as the integrity of the data and the accuracy of the results depends on the knowledge, experience and professionalism of the interviewers and supervisors. In the end, it is critical that the research team and the City can trust the data that were collected. For this reason, True North partners with MWRC—a firm that has worked with Dr. McLarney and Mr. Sarles on more than 800 survey projects in the past and has a proven track-record of professional interviewers, quality data collection, and competitive pricing. Telephone interviewing for the survey will be conducted in the evenings (5:30PM to 9:00PM PST) during weekdays, and 10:00AM to 6:00PM on weekends.

Although telephone interviewing has (and continues to be) the single most reliable method for conducting revenue measure surveys, it is also that case that a growing percentage of voters are unreachable by telephone due to factors such as call-screening, abandoned land lines, and/or not listing a valid phone number when registering to vote. Reaching these voters requires a different strategy, which is why we recommend that in *addition* to recruiting participation in the survey and conducting interviews by telephone, we also utilize an additional recruiting method (email) and allow voters to participate in the survey online through a secure, password-protected survey site designed and hosted by True North. When used *appropriately*, a mixed-method approach will deliver higher participation rates, minimize response bias, and ultimately deliver more reliable results than using telephone-only based data collection methodologies.



True North works closely with MWRC to manage quality control procedures during data collection, which will include a training session with interviewers and supervisors to ensure that telephone interviewers understand the purpose of all of the questions in the survey, thoroughly testing the CATI and web programs prior to fielding, active and remote supervisor monitoring of interviews, quality control checks on a random selection of interviews to identify any discrepancies that may occur (and need adjusting), and daily monitoring of the sample characteristics to ensure that data collection is adhering to the sampling plan.

Why not use text messages to recruit participation in the survey? Although appealing in concept, we strongly advise against soliciting participation in the survey using text messages due to legal considerations and the financial liability it can create for the City. The Federal Communications Commission (FCC) created the Telephone Consumer Protection Act (TCPA) to regulate telemarketing calls and the use of automatic telephone dialing systems. Recent updates to the TCPA and court cases make it clear that the use of autodialed text messages requires prior *express written consent* from recipients, a higher standard than has been required in the past. The fines for violating TCPA regulations can be extremely expensive—up to \$18,936 per violation (i.e, per text).

DATA PROCESSING Once the interviewing is complete, True North will process the data. This consists of checking the data for errors or inconsistencies, coding and recoding responses, and preparing frequency analyses and cross-tabulations. If strategic oversampling is used during the sampling stage, True North will adjust for the oversampling via a statistical procedure known as *weighting* to ensure representative results.

TOPLINE SUMMARY Once the data processing is complete, the team will analyze the results and prepare an initial topline summary, which consists of the percentage responses for each question asked in the survey. Although the topline summary represents the 'first cut' at the data and presents the results only for respondents as a whole, the summary will often allow the team to answer the essential questions for the study, such as does the measure have a reasonable chance to succeed?

SURVEY REPORT The survey report prepared by True North for the City will include a thorough question-by-question analysis of the survey findings (including overall results, and by voter subgroups), an executive summary of the key findings and conclusions, a detailed discussion of the methodology used to complete the study, a tabulation of all calls made during the study, a copy of the topline report, and a complete set of crosstabulations for reference that display the results broken down by various subgroups of voters. True North believes that the quality of the final report and presentation reflects directly upon the quality of the study as a whole as well as True North Research, so Dr. McLarney and Mr. Sarles take great care in developing a report which is thorough (200+ pages cover to cover)², insightful, free of mistakes, and presents the information in an eye-catching, full-color display.

True North's reports are the most thorough and insightful in the industry. Our reports are designed to meet the needs of the most sophisticated audiences and critics, while at the same time are accessible to individuals who may not have the interest or the familiarity with the survey to wade through the typical 'academic style' report. We also don't just report the results—we put the findings into context and explain their meaning based upon related questions in the survey, as well as our experience working on hundreds of tax measures in the State.

ADVANCED VOTER BEHAVIOR ANALYSIS One of the value-added services that True North provides that other firms do not is to use advanced statistical techniques (e.g. multiple regression analysis) for understanding what factors drive voters' behaviors and opinions with respect to a measure. Human behavior is complex and shaped by a multitude of factors—so to understand it (and thereby gain insights that will help in packaging the measure and the campaign for success) we need to use tools that are capable of handling that complexity. Regression analysis is one such tool that True North uses to understand the *independent* impact of each possible factor in shaping voters' opinions while simultaneously controlling for the possible influence of other factors.

Although other firms may mention that they provide "models", the reality is that these models are nothing more than basic descriptive statistics in one, two or maybe three dimensions. They are not using advanced statistical techniques capable of handling 10+ dimensions (e.g., age, gender, partisanship, children in home, geography, etc.) and isolating the independent impact that each has on voter support for a measure. True North is unique in providing these more advanced statistical analyses and doing it within our standard cost structure—no additional charges apply.

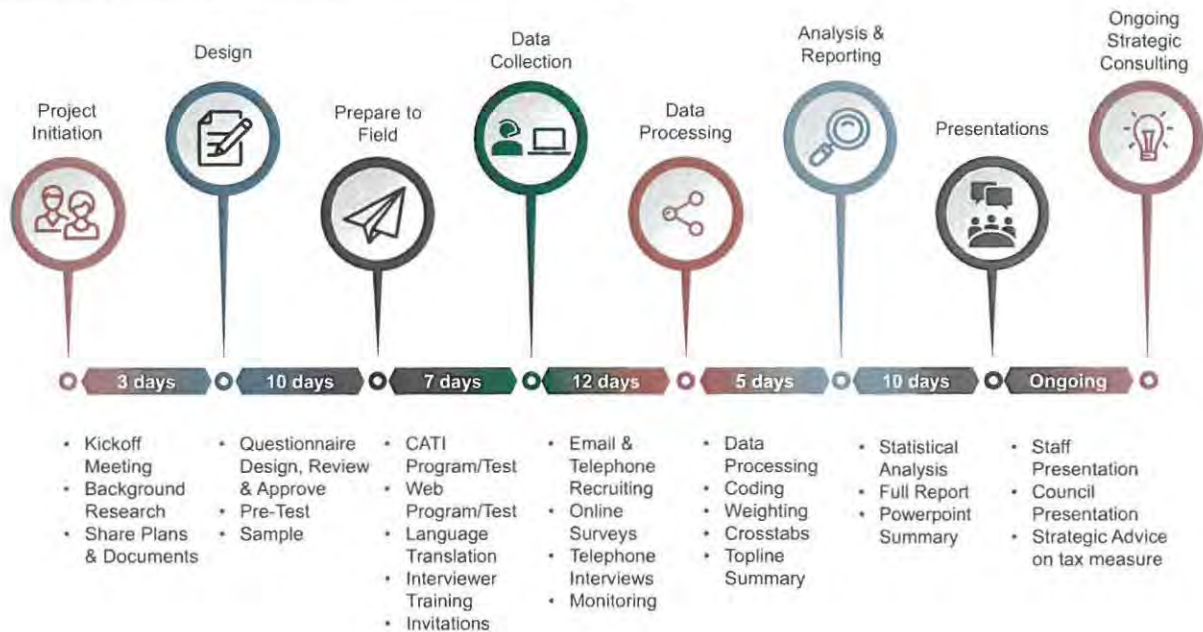
2. For those readers who don't have the time or the interest to review the entire report, we present a concise executive summary of the key findings and conclusions.

TARGETING TOOL In addition to providing more reliable, detailed information about the factors that shape voters' opinions, through our proprietary *Targeting Tool* we are able to put this information at our clients' finger-tips in a way that helps them make sound, strategic decisions throughout the pre-electoral and electoral cycles. The *Targeting Tool* is a user-friendly searchable database that we develop that allows our clients to search, sort and rank more than 100 defined subgroups of voters according to their responses to key survey questions. Unlike crosstabulations that are static and time-consuming to use, the *Tool* makes it quick and easy to answer questions that are often critical to a successful measure—such as which groups are initially most responsive, but are clearly tax-rate sensitive? Which groups are most responsive to positive messages, and which messages are the most compelling for each group? Which groups contain the most swing voters? Like the modeling services described above, True North is the only firm to provide the *Targeting Tool* or anything like it—and we provide it within our standard cost structure.

PRESENTATION AND CONSULTING True North will prepare and deliver a PowerPoint presentation of the study results, conclusions and recommendations to the City at a council meeting or other venue of the City's choosing. We will also be happy to work closely with the City to determine how best to package the measure for success.

TIME LINE True North will work with the City of Chico to establish a project schedule that meets the City's needs. Below is a proposed time line for the project, showing the number of days that we recommend devoting to each stage, as well as the key research tasks that are completed in each stage. We have the flexibility to adjust this time line if the City desires.

FIGURE 3 PROJECT SCHEDULE



PROPOSED COST

The following table presents True North's fixed-fee cost estimate to design and conduct the survey and scope of work described in this proposal. The costs shown are *inclusive* of all fees and expenses—there will be no additional charges—and are valid for 180 days from the date of the proposal. If selected as the City's research consultant for this project, our payment terms are simple: we request full payment at the *completion* of the study. No interim or up-front payments are needed.

TABLE 2 FIXED-FEE COST ESTIMATE FOR SURVEY

| Task | Cost |
|------------------------------------|-----------------|
| <i>Core Services</i> | |
| Random Sample | \$1,525 |
| CATI & Web Programming | \$1,400 |
| Data Collection | \$8,700 |
| Data Processing/Weighting | \$2,400 |
| Design, Analysis & Reporting | \$8,500 |
| Project Management/Travel | \$1,840 |
| Misc. Expenses | \$600 |
| <i>Value Added Services</i> | |
| Targeting Tool | Inc |
| Statistical Modeling | Inc |
| Advise on measure package | Inc |
| TOTAL | \$24,965 |

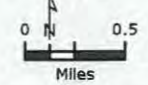
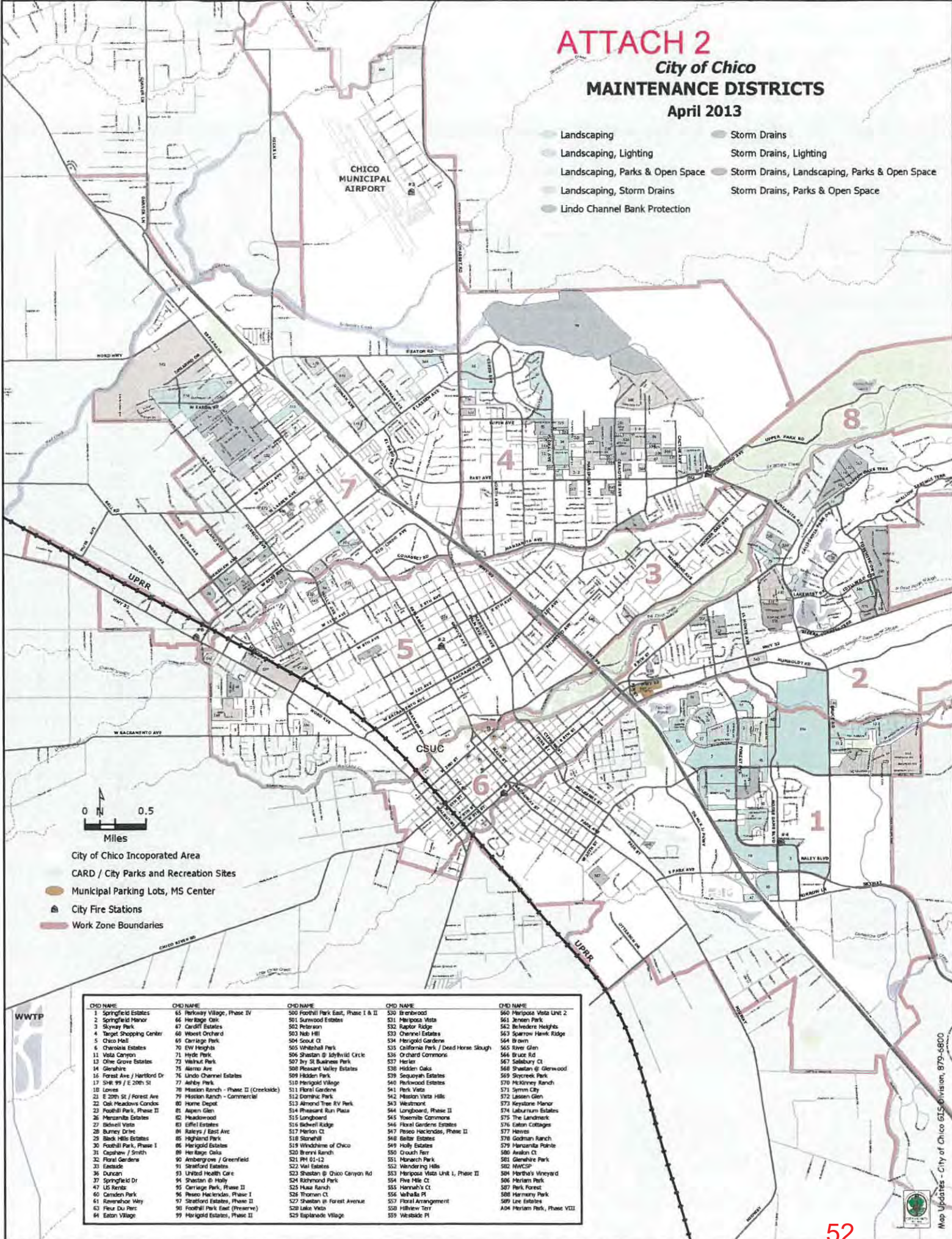
ATTACH 2

City of Chico

MAINTENANCE DISTRICTS

April 2013

- Landscaping
- Landscaping, Lighting
- Landscaping, Parks & Open Space
- Landscaping, Storm Drains
- Lindo Channel Bank Protection
- Storm Drains
- Storm Drains, Lighting
- Storm Drains, Landscaping, Parks & Open Space
- Storm Drains, Parks & Open Space



- City of Chico Incorporated Area
- CARD / City Parks and Recreation Sites
- Municipal Parking Lots, MS Center
- City Fire Stations
- Work Zone Boundaries

| DISTRICT NAME | DISTRICT NAME | DISTRICT NAME | DISTRICT NAME | DISTRICT NAME |
|-----------------------------|--|--------------------------------------|---|-----------------------------|
| 1 Springfield Estates | 65 Parkway Village, Phase IV | 300 Football Park East, Phase I & II | 530 Birchwood | 650 Mariposa Vista Unit 2 |
| 2 Springfield Manor | 66 Herbage Oak | 501 Sunwood Estates | 531 Mariposa Vista | 661 Jensen Park |
| 3 Skyway Park | 67 Cardiff Estates | 502 Peterson | 532 Raptor Ridge | 662 Belvedere Heights |
| 4 Target Shopping Center | 68 Woodloch Orchard | 503 Hob Hill | 533 Cherril Estates | 663 Sparrow Hawk Ridge |
| 5 Chico Mall | 69 Carriage Park | 504 Scout Ct | 534 Heritage Gardens | 664 Brown |
| 6 Charolais Estates | 70 EW Heights | 505 Whitehall Park | 535 California Park / Dead Horse Slough | 665 River Glen |
| 11 Vista Canyon | 71 Hyde Park | 506 Shastan @ Ivywild Circle | 536 Orchard Commons | 666 Bruce Rd |
| 12 Olive Grove Estates | 72 Washburn Park | 507 Ivy St Business Park | 537 Harbor | 667 Salisbury Ct |
| 14 Glenshire | 75 Alamo Arc | 508 Pleasant Valley Estates | 538 Hidden Oaks | 668 Shastan @ Glenwood |
| 15 Forest Ave / Hartford Dr | 76 Lindo Channel Estates | 509 Hidden Park | 539 Sequoyia Estates | 669 Skyview Park |
| 17 5th St / E 20th St | 77 Jubilee Park | 510 Heritage Village | 540 Redwood Estates | 670 McKinney Ranch |
| 18 Loma | 78 Mission Ranch - Phase II (Creskade) | 511 Floral Gardens | 541 Park Vista | 671 Symm City |
| 23 E 20th St / Forest Ave | 79 Mission Ranch - Commercial | 512 Dornick Park | 542 Mission Vista Hills | 672 Lassen Glen |
| 22 Oak Meadows Condos | 80 Home Depot | 513 Almond Tree RV Park | 543 Westmont | 673 Keystone Manor |
| 23 Football Park, Phase II | 81 Aspen Glen | 514 Pleasant Run Phase | 544 Longboard, Phase II | 674 Laburnum Estates |
| 26 Marzanita Estates | 82 Meadowood | 515 Longboard | 545 Yosemite Commons | 675 The Landmark |
| 27 Bidwell Vista | 83 Effie Estates | 516 Bidwell Ridge | 546 Floral Gardens Estates | 676 Eaton Cottages |
| 28 Burney Drive | 84 Avelys / East Arc | 517 Marlon Ct | 547 Paseo Macdonalds, Phase II | 677 Haven |
| 29 Black Hills Estates | 85 Highland Park | 518 Stonehill | 548 Estler Estates | 678 Godman Ranch |
| 30 Football Park, Phase I | 86 Heritage Estates | 519 Windchime of Chico | 549 Holly Estates | 679 Marzanita Pointe |
| 31 Capshaw / Smith | 88 Her Range Oaks | 520 Brenni Ranch | 550 Grouch Farm | 680 Avalon Ct |
| 32 Floral Gardens | 89 Ambergrove / Greenfield | 521 PH 01-12 | 551 Marancho Park | 681 Glenshire Park |
| 33 Eastside | 91 Stratford Estates | 522 Mail Estates | 552 Wandering Hills | 682 HWZSP |
| 36 Duncan | 93 United Health Care | 523 Shastan @ Chico Canyon Rd | 553 Mariposa Vista Unit 1, Phase II | 683 Martha's Vineyard |
| 37 Springfield Dr | 94 Shastan @ Holly | 524 Richmond Park | 554 Pine Hill Ct | 684 Marlan Park |
| 47 US Bank | 95 Carriage Park, Phase II | 525 Haas Ranch | 555 Harrah's Ct | 687 Park Forest |
| 60 Candan Park | 96 Paseo Macdonalds, Phase I | 526 Thomas Ct | 556 Valhalla Pl | 688 Harmony Park |
| 61 Ravenwood Winy | 97 Stratford Estates, Phase II | 527 Shastan @ Forest Avenue | 557 Floral Arrangement | 689 Lee Estates |
| 63 Flew Du Parc | 98 Football Park East (Preserve) | 528 Lake Vista | 558 Hillcrest Tor | 690 Herlan Park, Phase VIII |
| 84 Eaton Village | 99 Heritage Estates, Phase II | 529 Espanade Village | 559 Valhalla Pl | |



City Council Agenda Report

Meeting Date: April 19, 2016

TO: City Council

FROM: Public Works Director – Engineering, Brendan Ottoboni, 879-6901

RE: PAVEMENT MANAGEMENT PROGRAM (PMP) ASSESSMENT UPDATE

INTRODUCTION:

In August of 2015, City Staff began working with consultant, Harris & Associates, to update the City of Chico's Pavement Management Program (PMP). The PMP provides a management tool to inventory street pavement, assess pavement condition, record historical maintenance, forecast budget needs, and view impacts of funding on City-wide pavement condition over time. The PMP also includes a software based tool for analyzing pavement conditions and recommending rehabilitation strategies based on funding levels, with a focus on providing cost effective recommendations that enhance the overall system Pavement Condition Index (PCI). The software based PMP will be instrumental for Public Works (both Engineering and Operations & Maintenance) to more efficiently manage, track and readily communicate pavement conditions on all City streets. Finally, the PMP will serve as the guiding document in the development of an annual roadway maintenance program to best maintain our roadways to current standards within existing budget constraints.

Recommendation: The Public Works Director-Engineering recommends the City Council adopt the City of Chico's 2015 Pavement Management Program Update.

FISCAL IMPACT:

This project was approved by City Council through the budget process under Capital Project #50057 – Pavement Management Program Assessment. The City entered into a contract with Harris & Associates, a specially trained and qualified firm with expertise in pavement assessment and management programs that performs work for the State of California and various other governmental agencies. The contract for Harris & Associates to complete the program was \$105,550. Additionally, staff time totaling approximately \$60,000 has been expended to ensure that the information and setup of software is performed accurately.

Based on the recommendations in the report, in order to increase the PCI by five (5) points and thereby achieve the State average condition by 2020, an average of \$9.8M is recommended to be budgeted towards roadway maintenance. This would require an additional estimated \$9.0M annually to be budgeted towards roadway maintenance. There is no current funding specified to make up this additional expense.

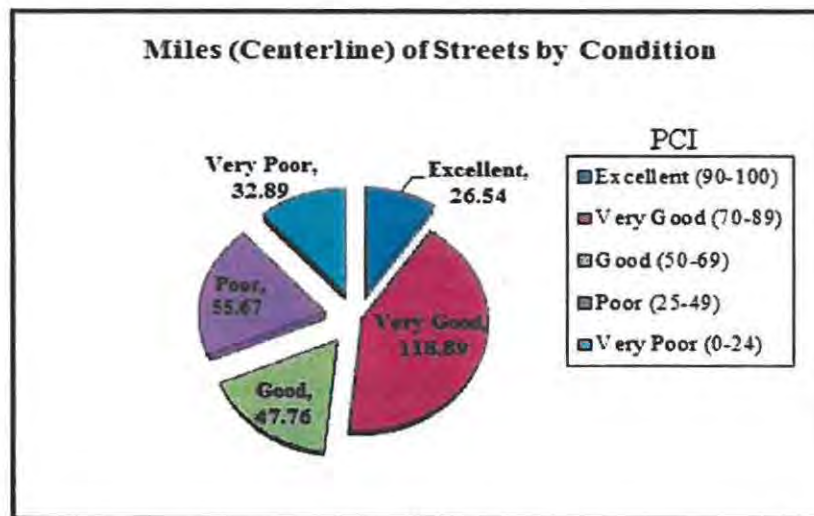
DISCUSSION:

The City of Chico's roadway asset consists of 282 Centerline miles, with a total of 564 Lane Miles of roadway. The City's roadway network, as a physical infrastructure asset, has a total replacement value of approximately \$310,000,000. Given this investment and benefit to the citizens and businesses in the community, it is important that the City properly maintains this asset as it only gets increasingly more expensive to protect the longer the needs go without being addressed. The current overall PCI for roads in the City of Chico is a 61. The table below shows the breakdown by classification:

| Classification | 2015 PCI* |
|---------------------|-----------|
| Arterial | 69 |
| Minor Arterial | 44 |
| Rural Interstate | 0 |
| Collector | 60 |
| Local | 76 |
| Residential/Local | 61 |
| TOTAL SYSTEM | 61 |

*Algorithm as developed by the Army Corps of Engineers

The State of California, through the League of California Cities, has recently completed a State-wide pavement condition survey and determined the State average PCI to be **66**. The City's overall PCI of 61 is considered in the mid-range of 'good'; however, this is slightly skewed as a result of the Local roadways being in the 'Very Good' range at a PCI of 76. This is mostly due to the fact that roads recently built as part of newer subdivisions are accepted by the City in excellent condition. The chart below provides the breakdown of PCI ranges that are used to describe what each range of conditions means, with the number of centerline roadway miles in each category:



The maintenance strategy to best extend the life of pavement, depends on the category in which it exists, and applying the best and most cost effective treatments. The table below describes the condition categories, their equivalent PCI range, and typical prescribed maintenance treatments:

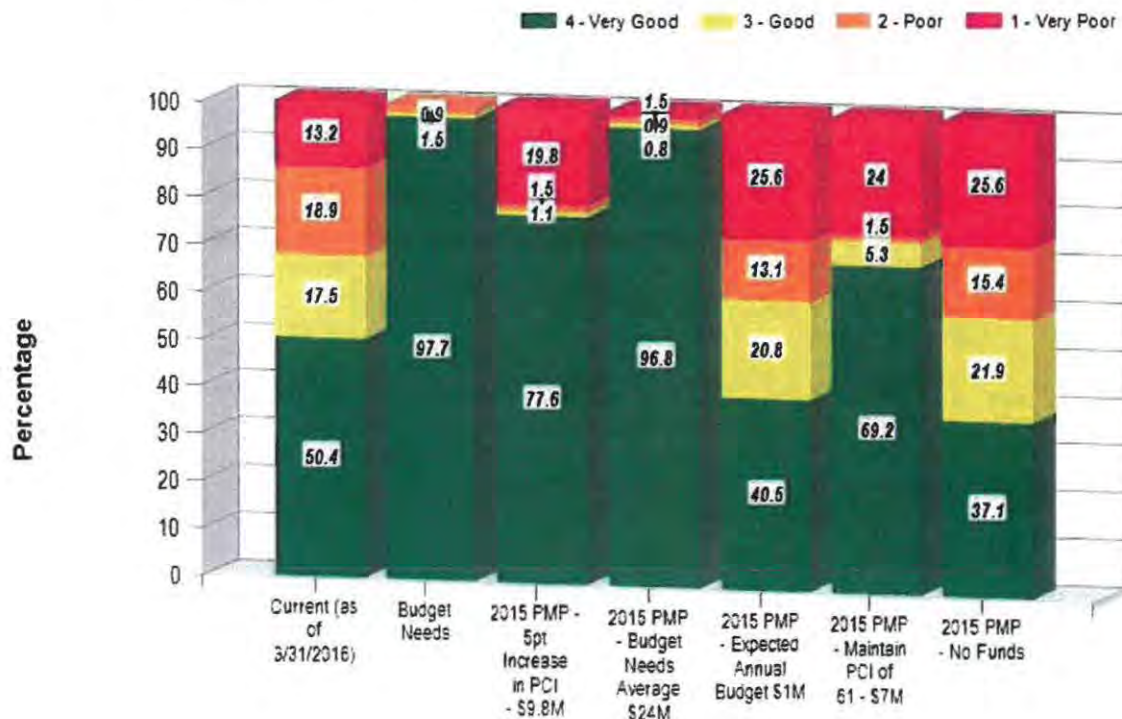
| Chico's Maintenance Treatments | | |
|--------------------------------|-----------|-----------------------------------|
| Condition | PCI Range | Typical Maintenance Treatment |
| Excellent | 90-100 | Do Nothing. |
| Very Good | 70-89 | Seal Cracks/Slurry Seal, |
| Good | 50-69 | Cape Seal/ Thin AC Overlay (1.5") |
| Poor | 25-49 | Thick AC Overlay (2.5") |
| Very Poor | 0-24 | Reconstruct Structure |

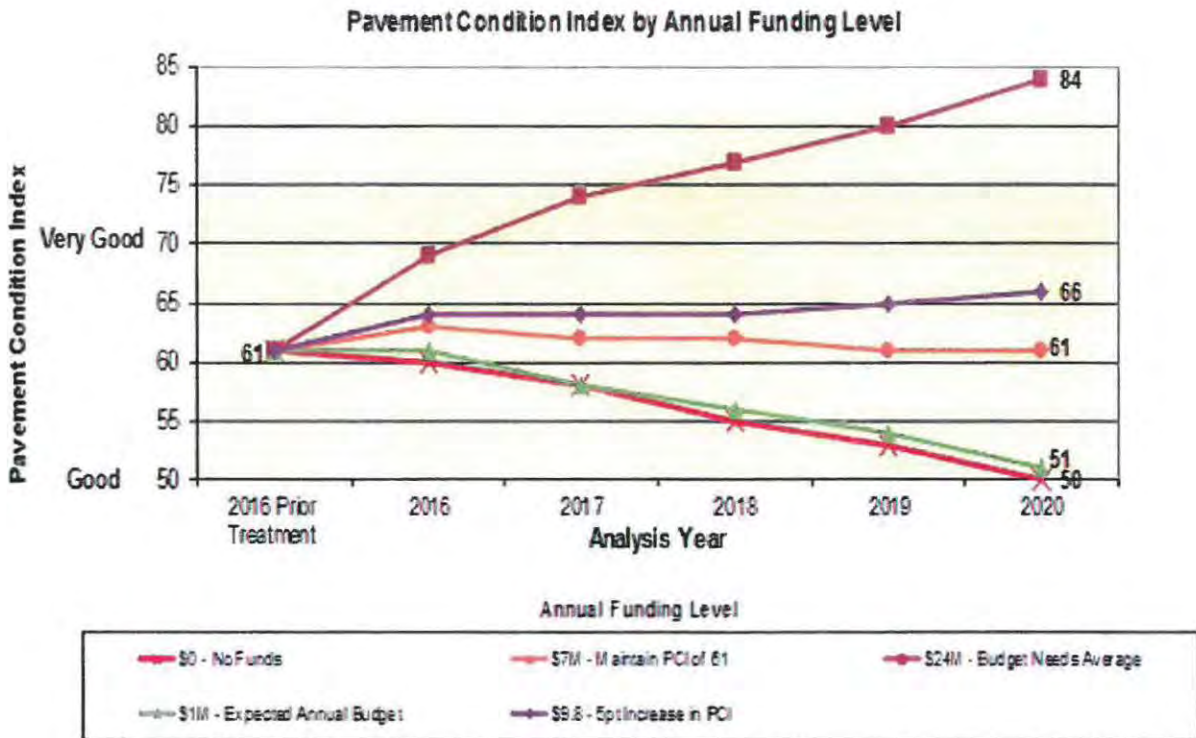
Roadway maintenance is not typically performed in a 'worst-first' mentality because it does not best manage the long-term life of the pavement network as a whole. The most expensive type of treatment is a full rehabilitation; therefore, preventative maintenance is necessary to keep roadways from reaching that rehabilitation stage (PCI below 50). Ideally, the City should utilize preventative maintenance as the primary roadway maintenance type, however, this is yet to be achieved. In order to reach that level of asset management, it is estimated that the City of Chico would need to budget a total of approximately \$120 MILLION for FY's 2016-2020. With the known funding that essentially consists of Gas Tax funds, currently, staff is only projecting a budgeted amount of approximately \$5 MILLION for FY's 2016-2020. Obviously this is a very large discrepancy and based on these facts, roadway conditions will continue to deteriorate to a condition that is more costly than the currently projected \$120 Million.

Through this Pavement Management Program Update, there are different budget scenarios that have been considered and what those five (5) year projections look like in each scenario. Below is a list of the different options considered, followed by a bar graph depicting five (5) year projections based on each scenario:

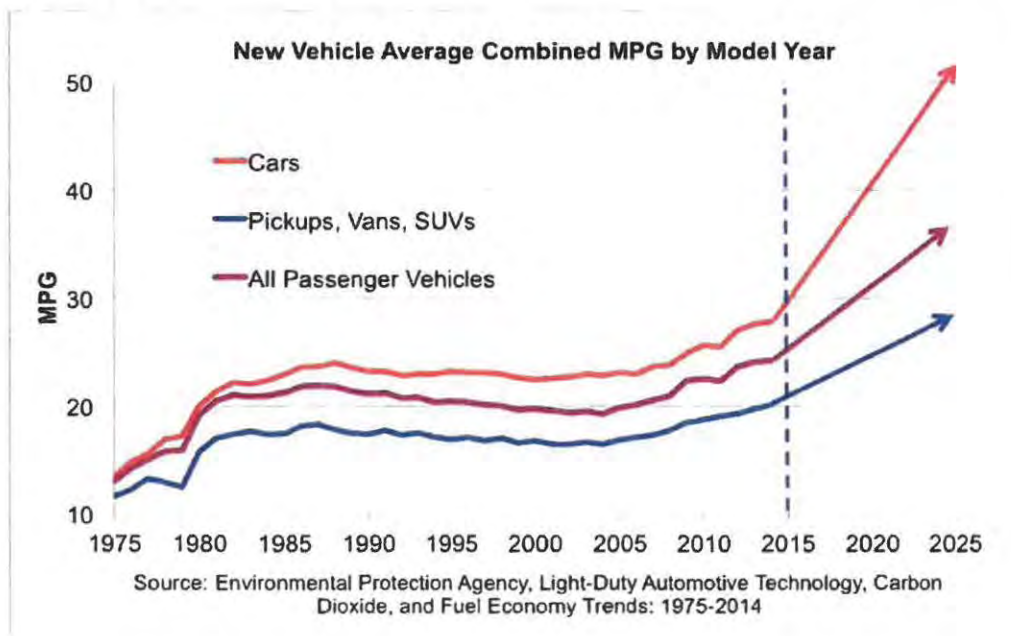
1. \$0 - No Funds (Do Nothing)
 - a. Results in a decrease in PCI from a 61 to a 50 in 2020
2. \$1M - Expected Annual Budget (Approximate current amount spent on Roadway maintenance)
 - a. Results in a decrease in PCI from 61 to a 52 in 2020
3. \$7M - Maintain a PCI of 61 (Assuming the number of centerline miles of roadway do not increase)
 - a. Results in maintaining a PCI of 61
4. \$9.8M - Five Point Increase in PCI
 - a. Results in an increase in PCI from 61 to 66 in 2020
5. \$24M - Budget Needs Average to reach a Preventative Maintenance state
 - a. Results in an increase in PCI from 61 to 84

Pavement Condition Changes under Budget Scenarios as of 2020

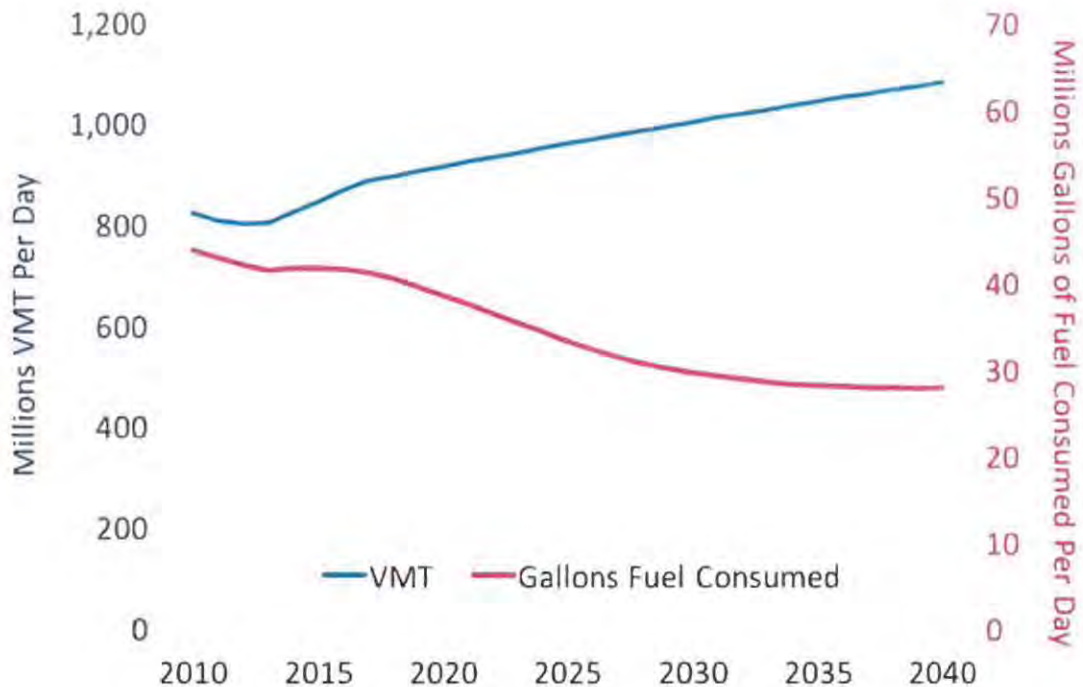




With the current funding source for roadway maintenance consisting of Gas Tax Revenues, there are simply not enough funds to maintain this asset of the City. Gas Tax Revenues continue to decrease as a result of more fuel efficient vehicles, electrical vehicles, etc. The California Transportation Commission provides information on the trends of this funding source. The chart below provides visual details showing the increase in average Miles Per Gallon (MPG) of vehicles.



As a result of more fuel efficient vehicles, Gas Tax Revenues continue to decrease all while data indicates that roadway usage and demand is on the increase. The trends locally are following this same pattern as the amount of funds received have declined in general over the last several years. Roadways deteriorate based on the loading of the pavement (usage), weathering, etc. Usage is measured in total Vehicle Miles Traveled (VMT) to determine trends in usage of the roadway networks. Below is a chart showing this trend of an increase in VMT, decrease in the number of gallons of fuel consumed as a result of more fuel efficient vehicles, and resultant decrease in Gas Tax Revenues provided to the City to maintain roads.



Source: California Transportation Commission (CTC)

It should be noted that the City uses Gas Tax Revenues as the funding source for bridge maintenance, as well as street sweeping, storm drain cleaning and pothole repairs. Currently, however, the City does not have enough Gas Tax Revenues to do any maintenance on the bridges in the City. When an emergency situation occurs on a bridge, funds that were intended for roadway maintenance have to be redirected to handle that emergency situation as a result of continued deferred maintenance. Further, with development of subdivisions continuing to boom, the costs associated with the roadway network will only increase, making the City fall further behind in maintenance. This continues to be a State-wide issue and is being discussed at the State legislature level, looking for solutions to this issue. In the absence of a statewide solution, many cities and counties in California have implemented local funding sources to fund transportation projects specifically in acknowledgement of this State funding discrepancy.

Public Works staff continues to receive complaints each and every day from residents in regards to conditions of the roads, as well as concerns from the development community, as they hear from those looking to

purchase new homes, that the roadways are a major concern. Staff will do its best to continue to get the biggest 'bang for our buck', but with the current funding allocations to roadways, the Community will continue to see the conditions of roadways decline.

Accompanying this staff report is the full and complete 2015 Pavement Management Program Update Report. This includes the data collected at each roadway segment, as well as the complete analysis of our roadway network.

Reviewed By:



Brendan Ottoboni, Public Works Director-Engineering

Approved By:



Mark Orme, City Manager

DISTRIBUTION:

City Clerk (3)

Attachments:

Attachment 'A' – Final PMP Update



Harris & Associates

**Program Managers
Construction Managers
Civil Engineers**

March 31, 2016

Mr. Brendan Ottobani
City of Chico
411 Main Street
Chico, CA 95927

Subject: Final Report - Pavement Management Program Update

Dear Mr. Ottobani:

As part of the development of the Pavement Management Program for Chico, Harris & Associates hereby submits the Final Pavement Management Report.

The information contained in this report presents the findings from a pavement condition survey of the City's street network. The City is also provided with the information that was used to develop the recommended improvement program. The report covers the following categories:

- Executive Summary of 2015 PMP Update
- Pavement Condition Index for all streets
- Budget Analysis and Recommended Work Program based on various budgets
- Backup Data

It has been a pleasure working with you on this project. Do not hesitate to contact me at (925) 827-4900 ext. 1219 or vpulijjal@harris-assoc.com if you have any further questions.

Sincerely,

Harris & Associates

Vijay Pulijjal, P.E.
Senior Project Manager

The preparation of this report has been financed in part by grants from the Federal Highway Administration, U.S. Department of Transportation. The contents of this report do not necessarily reflect the official views or policy of the U.S. Department of Transportation.

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- I. Introduction
- II. Methodology
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 - B. Average PCI by Annual Funding Chart
 - C. Deferred Maintenance Cost Trend by Annual Funding Chart
 - D. Budget Scenario Cost and Network Summaries
 - E. Annual Work Program –\$1M
- V. Backup Data
 - A. Section Description Inventory Report (Included on DVD)
 - B. Maintenance Treatment Decision Trees
 - C. Pictures of Street Inspection (Pictures included on DVD)

EXECUTIVE SUMMARY

In the August of 2015, Harris & Associates created a Pavement Management Program (PMP) for City of Chico. Pavement condition evaluations were performed on all streets (approximately 282 centerline miles). The PMP provides a management tool to inventory street pavement, assess pavement condition, record historical maintenance, forecast budget needs, and view impacts of funding on City-wide pavement condition over time.

The PMP is also a software-based tool for analyzing pavement conditions and recommending rehabilitation strategies based on funding levels. The software focuses on providing cost effective recommendations that enhance the overall system Pavement Condition Index (PCI). In general, asphalt pavement deteriorates over time by both traffic loading and weathering. The Metropolitan Transportation Commission (MTC) software recommends that about 6% of the budget be put to preventive maintenance treatments such as crack sealing, slurry seals, or thin overlays. The remaining budget is programmed for more expensive asphalt overlays and reconstruction. Why is preventive maintenance important? Preventive maintenance treatments sustain a street’s PCI at a high level and at relatively low cost. Preventive maintenance treatments can be applied to many streets (large pavement area) with a positive effect of raising the system PCI for a fraction of the cost to asphalt overlay one street (small pavement area).

The City currently uses the Metropolitan Transportation Commission’s (MTC) Pavement Management System StreetSaver® online version. The City uses the software to help make cost-effective decisions related to the road network, maximizing the City’s return on investment from available maintenance and rehabilitation funds; generating a prioritized plan; and identifying specific areas in need of maintenance and rehabilitation.

◆ **Pavement mileage & replacement value**

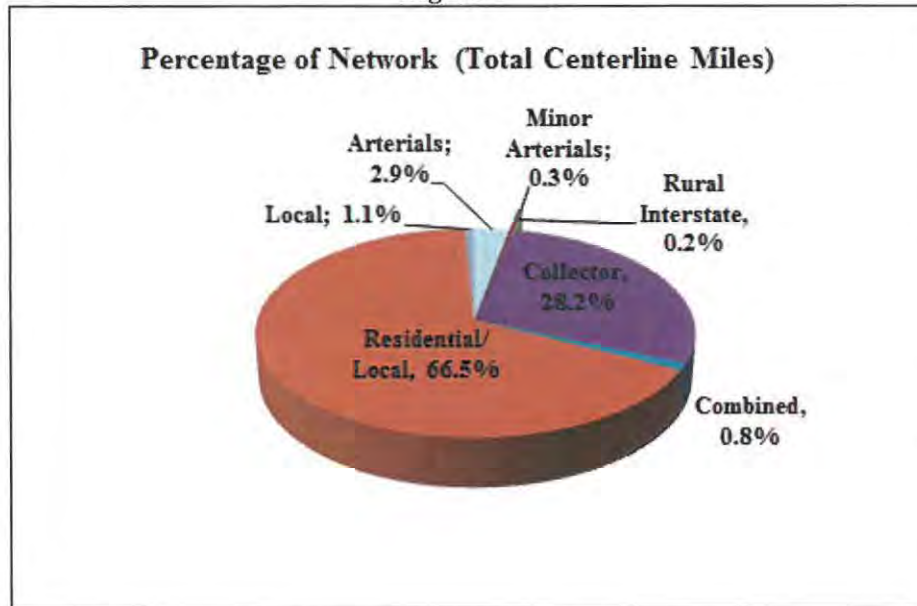
The City has approximately 282 miles of paved streets, divided into 1830 pavement management segments. Following is the breakdown of Chico’s street pavement mileage grouped by functional class:

Table 1

| Chico’s Streets By Functional Class | | | |
|--|-----------------------|--------------------------------|-------------------------|
| Classification | Total Sections | Total Center Line Miles | Total Lane Miles |
| Arterials | 38 | 8.26 | 16.52 |
| Minor Arterials | 3 | 0.97 | 1.93 |
| Rural Interstate | 1 | 0.46 | 0.92 |
| Collector | 394 | 79.33 | 158.67 |
| Local | 20 | 3.13 | 6.26 |
| Residential/Local | 1363 | 187.45 | 375.66 |
| Combined | 11 | 2.17 | 4.33 |
| Totals | 1830 | 281.77 | 564.29 |

It is important to consider the overall investment the City has in its pavements. The unit cost for a very poor condition category (consisting of moderate base failure repair, removal of existing surface, and pavement overlay to reconstruction) is from \$40.00 - \$80.00 per square yard. The cost to reconstruct all streets (Full replacement of the pavement, base, and structure of the streets) is over \$310 million.

Figure 1



City of Chico

Network Replacement Cost

Printed: 03/31/2016

| Functional Class | Surface Type | Lane Miles | Unit Cost/ Square Foot | Pavement Area/ Square Feet | Cost To Replace (in thousands) |
|----------------------|--------------|--------------|---------------------------|-------------------------------|-----------------------------------|
| Arterial | AC | 16.5 | \$8.89 | 1,820,072 | \$16,178 |
| Collector | AC | 161.4 | \$8.89 | 15,234,045 | \$135,412 |
| Local (7) | AC | 6.3 | \$4.44 | 540,875 | \$2,404 |
| Minor Arterial (4) | AC | 1.9 | \$8.89 | 219,660 | \$1,953 |
| Residential/Local | AC | 374.7 | \$4.44 | 34,703,329 | \$154,235 |
| | PCC | 2.5 | \$0.81 | 194,010 | \$157 |
| Rural Interstate (1) | AC | 0.9 | \$8.89 | 63,310 | \$563 |
| Grand Total: | | 564.3 | | 52,775,301 | \$310,902 |

◆ **Condition of Chico’s Street Asphalt Pavement**

The PCI is an overall measure of the condition of the road surface based on a scale of zero (0) (failed) to one hundred (100) (excellent). The chart below relates PCI ranges to general pavement condition definitions.

| <u>PCI RANGE</u> | <u>PCI Ranges</u> | <u>CONDITION</u> |
|------------------|-------------------|------------------|
| 90 - 100 | | Excellent |
| 70 - 89 | | Very Good |
| 50 - 69 | | Good |
| 25 - 49 | | Poor |
| 0 - 24 | | Very Poor/Failed |

The City’s average Pavement Condition Index (PCI) is 61 on a 100-point scale, with 100 being a new street. PCIs for the City’s pavement network are based on a visual distress rating system‡. The overall condition of Chico’s street pavement is in the range of MTC’s designation “Good”. The 2003 MTC State of Repair report states, “Approximately 75 percent of a pavement’s serviceable life has been expended by the time its PCI rating falls to 60.” Chico’s average PCI condition value by street classification is as follows:

Table 2

| Classification | 2015 PCI* |
|-----------------------|------------------|
| Arterial | 69 |
| Minor Arterial | 44 |
| Rural Interstate | 0 |
| Collector | 60 |
| Local | 76 |
| Residential/Local | 61 |
| TOTAL SYSTEM | 61 |

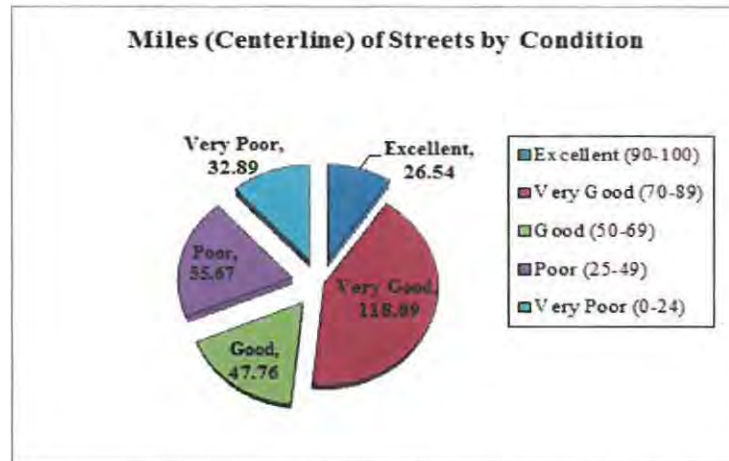
The City’s pavement condition has increased by two PCI points since the entire network was last inspected in 2007. In 2007, the average network PCI was 59 and the current overall network PCI is 61. The two point increase in the network PCI is due to the addition of 55 centerline miles of streets to the network, most of which were newly constructed roads and/or subdivisions, maintenance & rehabilitation work done and existing street segment limit modifications since 2007. The weighted PCI for the 55 miles of newly added streets is 68. This weighted PCI will certainly create a increase in the City’s overall PCI from 10 years ago.

‡Note: PCI weighted by area.

*Calculated by an algorithm developed by the Army Corps of Engineers.

The following figure 2 shows the City’s total pavement mileage by condition.

Figure 2



The maintenance strategy described in the table below is based on PCI scores and the corresponding condition category. Streets with PCI scores over 90 are considered to be in excellent condition and require no treatment. Streets with scores from 70 to 89 are considered “Very Good”, but may require cracks to be sealed or slurry seal. Streets with scores from 50 to 69 are considered “Good”, but may require a cape seal or thin AC overlay (1.5”). Streets with scores from 25 to 49 are considered “Poor” and generally require a thick AC overlay (2.5”). Streets with scores 24 and below are “Very Poor” and are in need of a surface reconstruction (AC).

Table 3 describes the condition categories, their equivalent PCI range, and typical prescribed maintenance treatments:

| Chico’s Maintenance Treatments | | |
|---------------------------------------|------------------|--------------------------------------|
| Condition | PCI Range | Typical Maintenance Treatment |
| Excellent | 90-100 | Do Nothing. |
| Very Good | 70-89 | Seal Cracks/Slurry Seal, |
| Good | 50-69 | Cape Seal/ Thin AC Overlay (1.5”) |
| Poor | 25-49 | Thick AC Overlay (2.5”) |
| Very Poor | 0-24 | Reconstruct Structure |

*Table 3 created by Harris & Associates based on feedback from the City.

In the present condition, about 27 miles in the “Excellent” category, 119 miles in the “Very Good” category, about 48 miles in the “Good” category, about 56 miles in the “Poor” category, and about 33 miles in the “Very Poor” category.

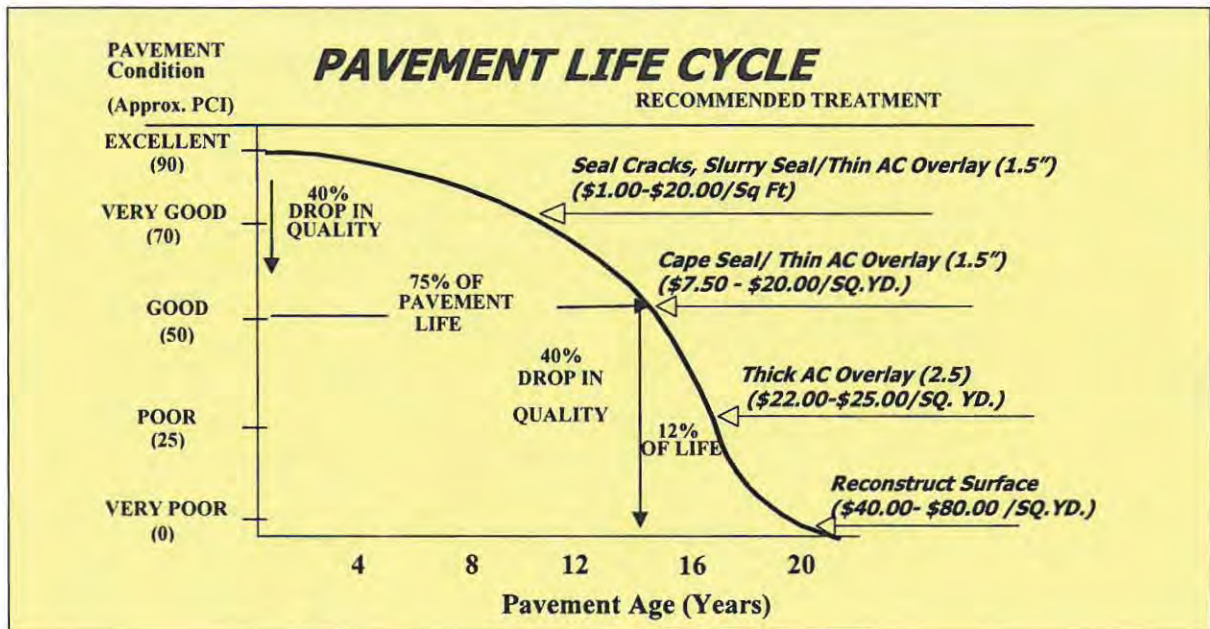
◆ **Budget Analysis**

Following the treatment strategy described in the table below and an inflation rate of 3%, the MTC PMP software generates a Budget Needs analysis. The Budget Needs analysis projects the total budget needed to bring the City’s pavement system to a condition where most pavement sections require only minor preventive maintenance (i.e., PCI = 70 or higher). The following chart illustrates the cost effectiveness of keeping the pavement condition index above 70 for a typical street.

Table 4

| Chico’s Budget Needs Analysis | | | | | | |
|-------------------------------|-------|-------|-------|-------|-------|--------|
| Year | 2016 | 2017 | 2018 | 2019 | 2020 | Totals |
| PCI Treated | 69 | 74 | 77 | 81 | 84 | N/A |
| PCI Untreated | 60 | 58 | 55 | 53 | 50 | N/A |
| Preventative Maintenance | 0.974 | 1.38 | 1.42 | 1.43 | 1.50 | 6.72 |
| Rehab (\$M) | 22.55 | 22.53 | 22.55 | 22.54 | 22.49 | 112.67 |
| Total Needs (\$M) | 23.52 | 23.91 | 23.97 | 23.97 | 23.99 | 119.39 |

Figure 4



The current PCI is reduced annually based on this deterioration curve. Maintenance activities increase the PCI value as they are applied to the segment. The overall program is dynamic in that each strategy consists of a cyclic series of actions that simulates the pavement's anticipated life cycle. As shown in the above picture, a typical pavement section will deteriorate approximately

40% in the first 75% of its lifespan. However, that same pavement section, if untreated, will experience another 40% reduction in overall quality in only the next 12% of lifespan, effectively deteriorating an equivalent amount in only one-sixth (1/6) the time. As a result of this continued deterioration, the quantity and cost of the maintenance activities needed to rehabilitate the pavement will increase in both scope and costs. In other words, it is not simply “pay today or pay tomorrow”, but rather a “pay today or pay more tomorrow” proposition. Overall pavement maintenance cost is reduced by the timely application of crack sealing, slurry seals and pavement overlays before the subgrade fails and requires a total pavement reconstruction.

To reach that level of minor preventive maintenance* in five (5) years, the Budget Needs analysis determined a total need of approximately \$120 million for the years 2016-2020. See section IV-A for the Needs - Projected PCI/Cost Summary.

The Budget Needs Average is defined as the cumulative budget needs over the course of the analysis period (\$120 million) divided by the number of years in the analysis period (5 years). For this study, the Budget Needs Average is \$24 million per year. After the Budget Needs have been calculated, Budget Scenarios are run to determine the funding levels required to maintain and/or improve the current PCI level and generate a list of street maintenance for the next five (5) years. The software analyzes each pavement section and picks specific maintenance to maximize the improvement of the entire pavement system. Maintenance treatments are allocated to as many streets as the annual budget will allow. The budget scenarios tested were calculated utilizing a 6% fixed preventative-maintenance-split, 3% interest, and 3% inflation values.

For Chico, the following five annual budget scenarios were generated with 6% of the annual budget applied towards preventative maintenance:

1. \$0 – No Funds (Do Nothing)
2. \$1M – Expected Annual Budget
3. \$7M - Maintain PCI of 61
4. \$9.8M - Five Point Increase in PCI
5. \$24M – Budget Needs Average

The MTC PMP software recommends spending 6% of the budget toward preventive maintenance because it is the optimum level according to the specific conditions of the City’s system. This means that 6% of the annual budget is spent on crack seal, slurry seal, and thin overlays while the remainder of the budget is spent on thick overlays and reconstruction. These budgets do not account for stopgap maintenance repairs, such as emergency pothole repair.

◆ **Budget Analysis Results**

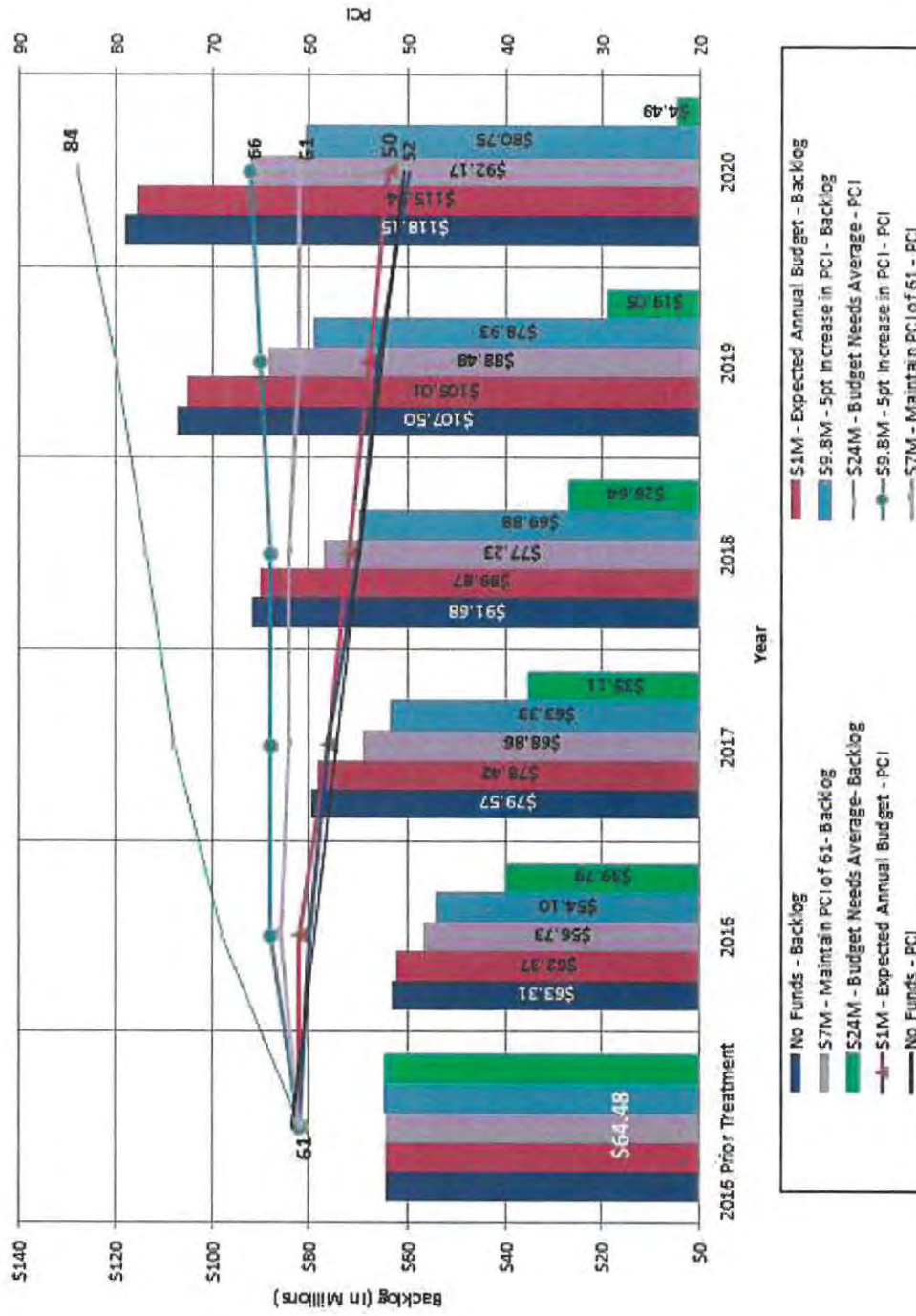
After the MTC PMP software analyzes the pavement system according to the specified annual budget over a period of five (5) years, trends are evident in the PCI and Deferred Maintenance backlog (the amount of necessary reconstruction and overlays not performed each year due to budget constraints). An increase in deferred maintenance shows that necessary rehabilitation is not being performed. The total deferred maintenance in 2016 before any suggested maintenance is around \$5 million. The following figure shows the impacts of the City’s overall PCI and backlog for the 5 generated scenarios:

Deferred maintenance consists of pavement maintenance that is needed, but cannot be performed due to lack of funding. Shrinking budget has forced many agencies to defer much needed street maintenance. By deferring maintenance, not only does the frequency of resident complaints about the condition of the network increase, but the cost to repair these streets rises as well. It is cost effective to keep pavement about a certain PCI because the cost to maintain the high PCI is less, than to bring a road segment with a low PCI to a high PCI.

** Preventive maintenance is a schedule of planned maintenance actions aimed at the prevention of failure of streets. These actions are designed to detect, preclude, or mitigate degradation of a streets segment. The goal of a preventive maintenance approach is to minimize degradation and thus sustain or extend the useful life of the street.*

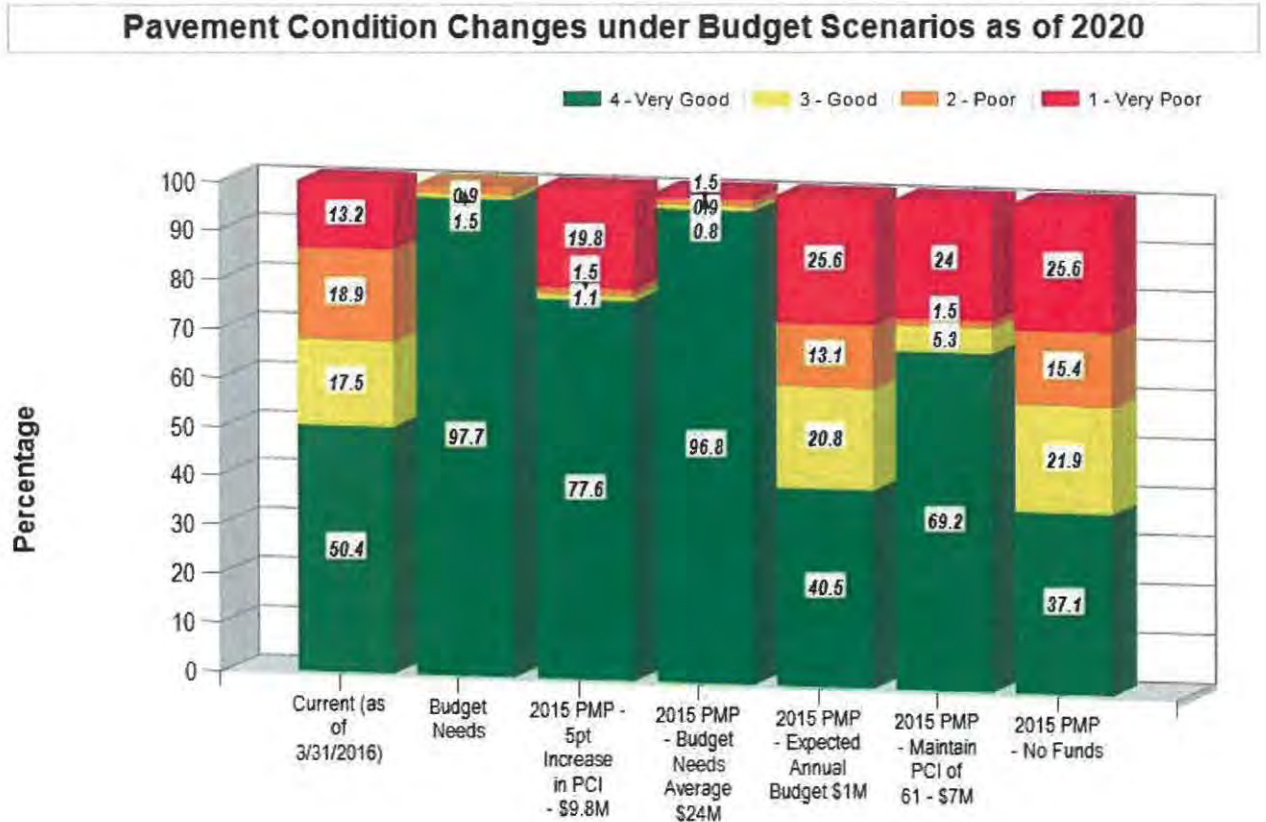
Executive Summary

Figure 5



The Figure 6 and 7 contrasts the pavement changes under the analyzed budget scenarios. The percentage of the roads in the “Poor” and “Very Poor” condition categories must be monitored as these represent the greatest liability to the City in regards to expense and serviceability off this really costly public asset.

Figure 6



The following PCI values reflect the average PCI and deferred maintenance after suggested treatments are applied.

- \$0 No Funds.
PCI Trend: Decreases from 61 PCI in 2016 to 50 PCI in 2020.
Deferred Maintenance Trend: Increases from \$64.4 million in 2016 to \$118.2 million in 2020.
- \$1M Expected Annual Budget
PCI Trend: Decreases from a 61 PCI in 2016 to a 52 PCI in 2020.
Deferred Maintenance Trend: Increases from \$64.4 million in 2016 to \$115.5 million in 2020.

- \$7M Maintain PCI of 61
PCI Trend: From a 61 PCI in 2016 to a 61 PCI in 2020.
Deferred Maintenance Trend: Increases from \$64.4 million in 2016 to \$92.2 million in 2020.
- \$9.8M Five Point Increase in PCI
PCI Trend: Increases from a 61 PCI in 2016 to a 66 PCI in 2020.
Deferred Maintenance Trend: Increases from \$64.4 million in 2016 to \$80.7 million in 2020.
- \$24M - Budget Needs Average
PCI Trend: Increases from a 61 PCI in 2016 to 84 PCI in 2020.
Deferred Maintenance Trend: Decreases from \$64.4 million in 2016 to \$4.5M in 2020.

Scenario charts (Figures 7 and 8) showing the impact of the five budgets on street condition and deferred maintenance backlog over five (5) year period is shown on the following pages and in Sections IV-B and IV-C. The Cost Summary Reports, which provide information on pavement funding distribution by pavement condition, and the Network Condition Summary Reports, which project pavement condition trends, can be found in Section IV-D.

◆ **Recommendations**

Harris & Associates recommends the Agency raise their annual budget to a minimum of \$7.5 million. Spending this budget will begin an increasing trend in overall pavement condition while slowing the growth of the deferred maintenance backlog. At this budget level, the overall PCI will increase from a 61 PCI in 2016 to a 62 after treatments are applied in 2020.

The City should utilize cost effective treatments where appropriate, such as slurry seals and crack seal and continue to evaluate emerging cost effective techniques like rubberized chip seals, thin-bonded wearing courses and rubberized overlays. Maintenance and rehabilitation performed annually must also be recorded in the MTC PMP software. The software allows the City to track the performance of past treatment strategies to determine their effectiveness.

Harris & Associates commends the City for its active participation in the pavement management program and also recommends that the City continue to maintain its pavement management program to be eligible for grants and state gas tax funding. All arterials and collector routes should be re-inspected every two years and all residential streets every five years. The costs for the re-inspection should be included in the annual pavement management budget.

The City should also perform annual database updates that include:

- Review and update decision trees (maintenance activities and unit costs);
- Consider establishing districts within City limits for maintenance planning and utilizing the StreetSaver user defined fields within the section description;
- Update the road network when streets are annexed or newly constructed; and
- Enter in Maintenance and Rehabilitation activities.

Chico's overall street system is currently in the range of MTC's "Good" condition category. To help maintain and improve the current condition, certain projects have been recommended within the context of this program. Annual work programs for the expected annual budgets can be found in Section IV-E. The report provides detailed listings of suggested maintenance projects for Chico based on the overall PMP suggested needs funding and base annual budgets. The report also provides a first step in identifying segments to be repaired under Chico's annual work programs.

The City should update the City's Finance Department with any changes or adjustments that have been made to the City's road network and subsystems (roads assets and pavement subsystems that have been acquired through annexation, deletion, etc.) for GASB-34 compliance.

The City should continue utilizing the StreetSaver Online GIS module or in-house GIS to better manage roads within City limits. The GIS technology is very useful to spatially view tabular reports that are derived from the pavement management system, such as scenarios, identification of maintenance and rehabilitation, planning, maintenance and rehabilitation history, pavement condition index, etc. The tool is very useful for exporting information out to current GIS and AutoCAD projects.

Although these project listings are recommendations generated by the PMP, they are for planning purposes only and are not intended to replace sound engineering judgment. Final project recommendations should be weighed against the actual approach the City wishes to utilize in scheduling the workloads for contracting purposes. Once a street segment is identified for repair, a closer site inspection and detailed project repair scope is required.

Figure 7

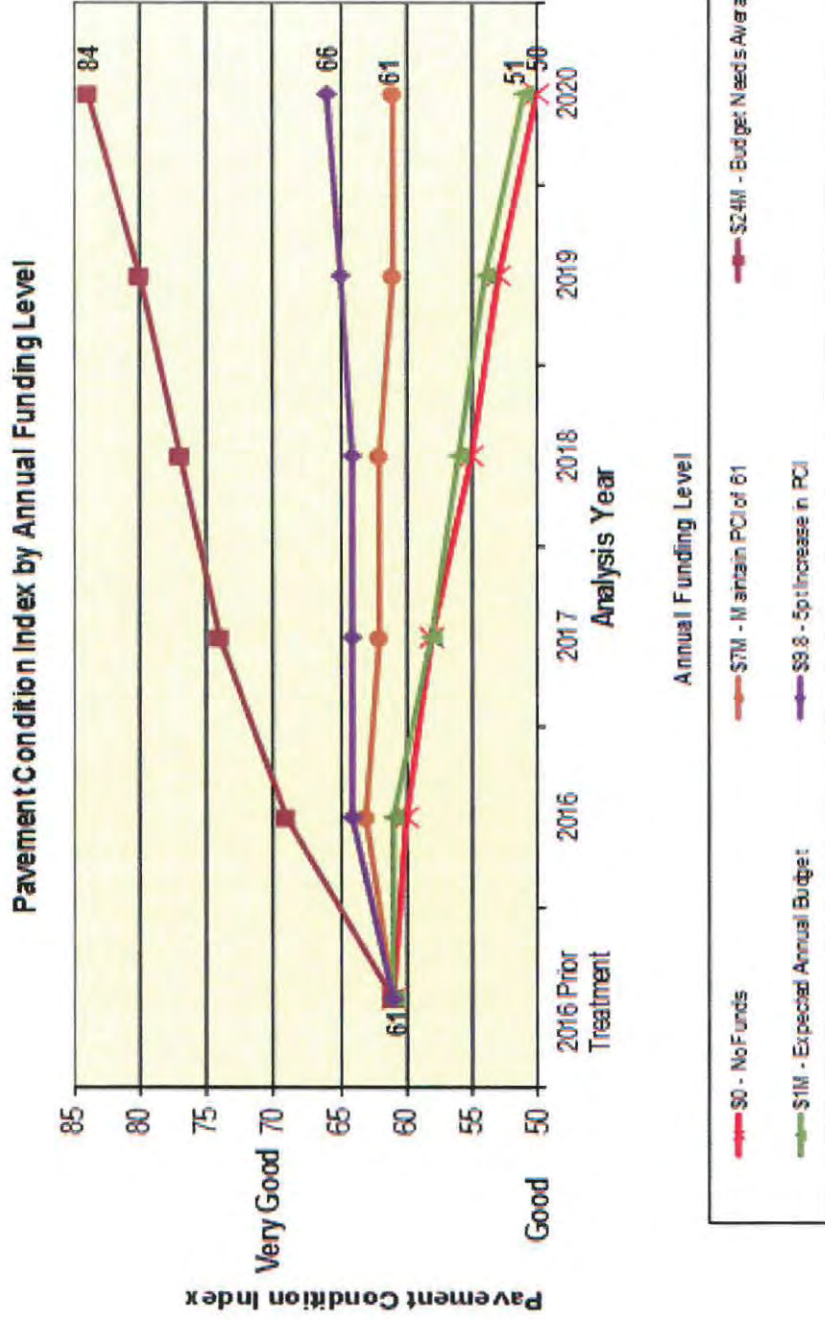
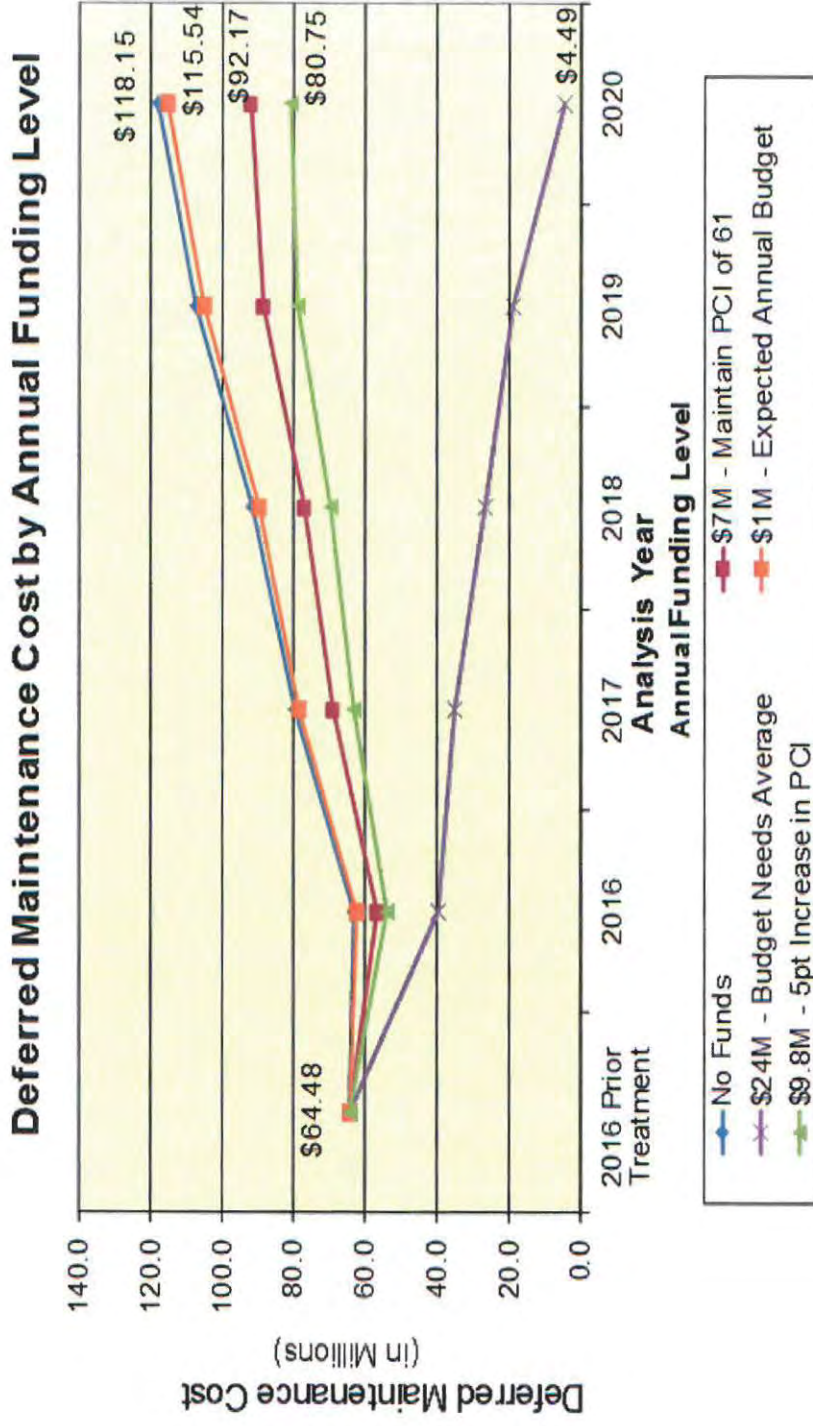


Figure 8



SECTION I INTRODUCTION

NEED FOR A PAVEMENT MANAGEMENT PROGRAM

Pavement Management Program assists the City by providing current inspection data used to evaluate current pavement condition. This helps to maintain a City-defined desirable level of pavement performance while optimizing the expenditure of limited fiscal resources. A PMP system is also often required to obtain state and federal funds.

Specifically, the program provides administrators and maintenance personnel with:

- A current inventory of all public roadways
- The current pavement condition for all public roadways
- A project listing of all pavement needing maintenance, rehabilitation, or replacement
- The most cost effective level of maintenance or repair appropriate at the time of the inspection
- A forecast of budget needs for maintenance, rehabilitation, or replacement of deficient pavement sections for a five (5) year Capital Improvement Program, at various alternative overall condition levels

LIMITS OF STUDY

It must be recognized that this report is limited to the existing pavement repairs. It does not include existing deficiencies for right-of-way concrete sidewalks, curb & gutter, drainage, trees, bus pads and non-structural improvements such as decorative crosswalks, medians, lighting and street furniture. Costs for these right-of-way repairs and improvements throughout the City would easily exceed the deferred maintenance costs (the cost of crucial maintenance work not performed in a specific year) identified in this report and can be identified and estimated separately in future reports.

The following recommendations generated by the Pavement Management Program are for planning purposes only. The resulting general recommendations are not intended to replace sound engineering judgment, which should dictate specific needs for an individual project. Maintenance and rehabilitation projects should be based on a combination of the system's recommendations weighed against the City's preferences, budget constraints, and other contributing factors. In addition, further refinements may be warranted from an engineering staff review of the pavement condition. For example, a particular pavement section may require treatment earlier (or later) than the rest of the roads in its localized area.

THE PAVEMENT SYSTEM

The entire pavement system within Chico is composed of approximately 282 miles of paved surfaces and is divided into 1830 pavement management segments. To assist in planning maintenance needs, the City's streets were grouped by functional class (arterial, collector, and residential). The table below shows the City's pavement mileage by functional class.

Table 5

| Chico's Streets By Functional Class | | | |
|--|-----------------------|--------------------------------|-------------------------|
| Classification | Total Sections | Total Center Line Miles | Total Lane Miles |
| Arterials | 38 | 8.26 | 16.52 |
| Minor Arterials | 3 | 0.97 | 1.93 |
| Rural Interstate | 1 | 0.46 | 0.92 |
| Collector | 394 | 79.33 | 158.67 |
| Local | 20 | 3.13 | 6.26 |
| Residential/Local | 1363 | 187.45 | 375.66 |
| Combined | 11 | 2.17 | 4.33 |
| Totals | 1830 | 281.77 | 564.29 |

The entire pavement system has a current reconstruction value of over \$310 million. The current system reconstruction value (consisting of moderate base failure repair, removal of existing surface, and pavement overlay) by functional class is as follows:

Table 6

System Reconstruction Value

City of Chico

Network Replacement Cost

Printed: 03/31/2016

| Functional Class | Surface Type | Lane Miles | Unit Cost/ Square Foot | Pavement Area/ Square Feet | Cost To Replace (in thousands) |
|-------------------------|---------------------|-------------------|-------------------------------|-----------------------------------|---------------------------------------|
| Arterial | AC | 16.5 | \$8.89 | 1,820,072 | \$16,178 |
| Collector | AC | 181.4 | \$8.89 | 16,234,045 | \$135,412 |
| Local (7) | AC | 6.3 | \$4.44 | 540,875 | \$2,404 |
| Minor Arterial (4) | AC | 1.9 | \$8.89 | 219,860 | \$1,953 |
| Residential/Local | AC | 374.7 | \$4.44 | 34,703,329 | \$154,235 |
| | PCC | 2.5 | \$0.81 | 194,010 | \$157 |
| Rural Interstate (1) | AC | 0.9 | \$8.89 | 63,310 | \$563 |
| Grand Total: | | 564.3 | | 52,775,301 | \$310,902 |

Table 6 provides network replacement cost sorted by Functional Class first and then by Surface Type. For each combination of Functional Class and Surface Type, the number of Lane Miles, Unit Cost (sq. ft.), Pavement Area (sq. ft.), and the Cost to Replace (in thousands) is reported.

The replacement cost is based on the Category V ("Very Poor") treatment cost provided in the Decision Tree for each functional class and surface type combination. Grand Totals are provided at the bottom of applicable columns.

CURRENT PAVEMENT CONDITION

A visual survey of approximately 282 miles of the City’s streets was conducted to assess the existing surface condition of each individual pavement segment. Upon completion of this survey, a Pavement Condition Index (PCI) was calculated for each segment to reflect the overall pavement condition. Ranging between 0 and 100, a PCI of 0 would correspond to a badly deteriorated pavement with virtually no remaining life. A PCI of 100 would correspond to a new pavement or pavement with no observed distresses.

Table I – 7 relates PCI ranges to pavement condition definitions and gives a general description for each pavement condition.

Table 7

| PCI Breakdown Descriptions | | |
|-----------------------------------|------------------|--|
| PCI Range | Condition | Description |
| 90-100 | Excellent | Little or no distress. |
| 70-89 | Very Good | Little or no distress, with the exception of utility patches in good condition, or minor to moderate hairline cracks; typically lightly weathered. |
| 50-69 | Good | Light to moderate weathering, light load-related base failure, moderate linear cracking. |
| 25-40 | Poor | Moderate to severe weathering, moderate levels of base failure, moderate to heavy linear cracking. |
| 0-24 | Very Poor | Extensive weathering, moderate to heavy base failure, failed patches, extensive network of moderate to heavy linear cracking. |

The overall condition of Chico’s road network rests in the “Good” range with an average PCI of 61.

MAINTENANCE STRATEGY DEVELOPMENT

Based on the results of the condition survey and on input from the City, pavement maintenance/rehabilitation strategies were developed. A standard agreement at the outset was to identify the City’s maintenance work program for the next five (5) years, showing resource alternatives that affect the maintenance backlog and increase the overall condition of the pavement system.

Certain strategies are recommended for the most cost-effective work program. A listing of the maintenance activities utilized in the strategy development is presented in Section II. Each activity represents the type of work that has been recommended for the long-term maintenance recommendations of the City’s streets.

ANNUAL BUDGET PROJECTIONS

The budgeting process was approached with the following in mind: generate a work program for the next five (5) years based upon actual road pavement conditions and determine the funding levels required to maintain and/or improve the current level (PCI) of overall condition.

Based on current and projected pavement maintenance needs, annual work program alternatives have been prepared. A detailed work program for the City's expected annual budget at can be found in Section IV-E of this report.

SECTION II METHODOLOGY

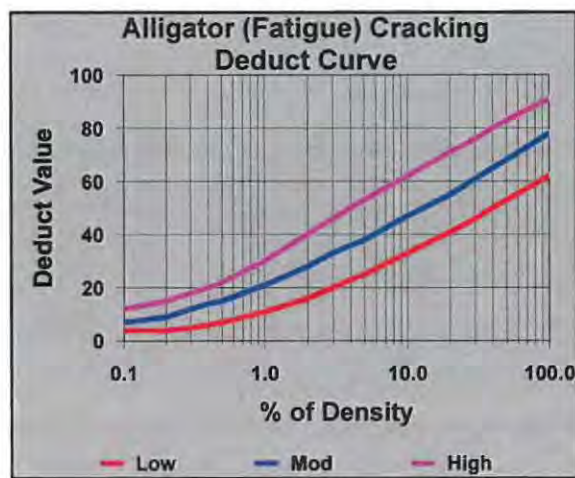
The following section provides a description of the methodology and rationale utilized in determining the recommended actions identified in this report.

Field Survey

For this inspection update, approximately 282 miles of paved streets in Chico's system were re-inspected. Data, including distress types and quantities, segment length and width, etc. were collected for all inspected roads. Using a combination of City street maps and laptop computers to collect field information, a field crew visually surveyed each of these streets. Data was categorized by street and "pavement" segment. Pavement segment limits were identified by determining the logical maintenance practices that would apply to each street and may vary from street to street, i.e. intersection to intersection, change in pavement width, drainage conditions, crown of the roadway, etc. All of the inspection data was entered into the MTC pavement management software (Streetsaver[®] Online Version 9.0)

After the data entry procedures were completed, a distress rating was calculated for each segment. The distress rating is calculated using MTC developed algorithms. The algorithm assigns each pavement section a score of 100 then deducts point values based on the pavement distresses found within the section weighted by the quantity of each distress. The algorithm assigns deduct values based on the severity (see figure below) and the density of each distress.

Figure II -1



The algorithm weighs the total deduct value within a section and calculates a total distress rating between 0 (failed) and 100 (excellent).

The PCI is an overall measure of the condition of the road surface based on a scale of zero (0) (failed) to one hundred (100) (excellent). Table II – 1 relates PCI ranges to general pavement condition definitions.

PCI Ranges
Table II – 1

| <u>PCI RANGE</u> | <u>CONDITION</u> |
|------------------|------------------|
| 90 - 100 | Excellent |
| 70 - 89 | Very Good |
| 50 - 69 | Good |
| 25 - 49 | Poor |
| 0 - 24 | Very Poor/Failed |

The summary of all road condition data and the representative PCI's is located in the Pavement Condition Index Report in Section III.

Once the PCI has been established for a pavement section, budget analyses and workload predictions commence. Predictions of future pavement performance are based on a pavement deterioration curve developed by MTC. As a pavement ages, the system predicts the PCI of the pavement based on the deterioration curve.

MAINTENANCE STRATEGY ASSIGNMENTS

The PCI is used by the system to schedule maintenance activities for each pavement segment. The MTC PMP software recommends a specific maintenance activity based on the PCI and budget constraints. Harris & Associates and the City have selected a series of maintenance activities to apply to the pavement network. The unit costs for each maintenance activity account for various construction costs including labor materials, 15% for design, 10% for construction inspection and 10% for contingency. Definitions of each maintenance activity per category are defined as follows:

1. Crack Seals - A surface treatment generally utilized to prevent entry of water or other non compressible substances into the pavement.

Crack seals are used to fill individual pavement cracks to prevent entry of water or other non-compressible substances such as sand, dirt, rocks or weeds. Crack sealant is typically used on early stage longitudinal cracks, transverse cracks and reflection cracks. It is placed over the existing pavement surface and is typically applied by the City at four (4) year intervals. The typical cost for crack sealing is \$1.00-\$1.50 per linear foot.

2. Slurry Seals - A surface treatment generally utilized to provide new wearing surfaces and prevent water penetration of the pavement surface, thereby extending pavement life.

Slurry seals are surface treatments applied to pavements with minimal surface distress to provide new wearing surfaces and extend pavement life. A slurry seal generally consists of a mixture of conventional or latex-modified emulsified asphalt, well-graded fine aggregate, mineral filler and water. It is placed over the existing pavement surface and is typically applied at eight (8) year intervals. This type of treatment was estimated by the City to cost \$2.50-\$3.50 per square yard for slurry seal.

3. Cape Seals – A surface treatment generally utilized to provide new wearing surfaces and prevent water penetration of the pavement surface, thereby extending pavement life.

Cape seals are surface treatments applied to pavements with minimal surface distress to provide new wearing surfaces and extend pavement life. A cape seal consists of a “slurry” mixture of conventional or latex-modified emulsified asphalt, well-graded fine aggregate, mineral filler and water, placed over chip seal. Asphalt rubber cape seal consists of an application of an asphalt rubber seal coat followed by an application of a slurry seal coat. This type of treatment was estimated by the City to cost \$7.50 per square yard for cape seal.

4. Overlays - The application of treatments that are more cost-effective alternatives to reconstruction of the entire pavement surfaces, but provide the required structural support.

An asphalt layer is placed over the existing pavement surface. Cold planing is typically performed prior to the overlay to provide a level surface for the overlay, reduce excessive crowning, and assure alignment with existing gutter line. This typically includes base repair and crack sealing prior to the application of an overlay. This treatment provides a new wearing surface and increased structural strength to the pavement section. An overlay is typically designed for a ten to fifteen (10-15) year life. This type of treatment was estimated by the City to cost \$18.00 - \$25.00 per square yard depending on the thickness of the overlay and the functional class of the street.

5. Reconstruction – The removal and replacement of either the pavement surface only or both the pavement and base.

Chico’s typical cost for reconstruction is \$40.00-\$80.00 per square yard. After a reconstruction, the pavement segment is again considered new with a full life extension (typically 28 years).

MAINTENANCE DECISIONS

Once the City selected the appropriate maintenance activities and unit cost used by the jurisdiction, the “Maintenance Decision Tree” (a Streetsaver phrase that list all the treatments and corresponding unit costs, found in Section V) is updated. This decision tree assigns appropriate treatments by the specific needs of the streets.

The decision tree lists costs associated with treatments on specific types of pavement according to the jurisdiction. StreetSaver® uses a decision tree to model the decision- making process that agencies follow to select a maintenance or rehabilitation strategy. The decision tree contains "branches" for each functional classification, surface type, and condition category. Jurisdictions can outline their M&R strategy, choosing a treatment for each branch.

Once the decisions were set within the system, budgets and work assignments were generated for each work program on an annual basis. Using the MTC recommended pavement deterioration curve for each pavement surface type and functional class, both current and future work requirements for each pavement segment within the City were determined.

PAVEMENT MANAGEMENT PROGRAM REPORTS

This report contains a comprehensive assemblage of pavement management reports ranging from summary reports to annual maintenance and rehabilitation schedules. These reports represent reasonable projections of pavement maintenance needs and performance based on visual condition assessments, unit cost estimates, and pavement deterioration models.

It is important to note that pavement segment dimensions and surface area, along with the action and repair costs (as presented in the reports), are accurate within tolerable limits to general project costs on average. This is noteworthy due to the "implied" accuracy of reporting length and width to the nearest foot, surface area to the nearest square foot, and action and repair unit costs and project estimates to the nearest penny and dollar, respectively, which will vary with each project.

SYSTEM MAINTENANCE

It is recommended that the City continue developing annual maintenance plans while also working towards reducing the City's present backlog of rehabilitation projects.

In order to ensure that report outputs are accurate and credible, it is essential that the integrity of all data files be maintained. This will require performing all necessary updates when changes are made to scheduling scenarios, unit cost information, historical data, etc. In addition, the entire pavement network will have to be re-inventoried at regular intervals, as noted earlier in this report. This will not only allow work to be scheduled based on the most current condition data available, but it will also provide City personnel with a means to monitor actual rates of pavement deterioration.

SECTION III

**PAVEMENT CONDITION INDEX (PCI) REPORT / REMAINING
SERVICE LIFE (RSL) REPORT (*MAP INCLUDED*)**

Section PCI Listing Report

This report lists the Pavement Condition Index (PCI) for each pavement management section.

| | |
|--------------------------|--|
| Street Name: | Street Name |
| Street ID: | Street Identifier - 6 characters |
| Section ID: | Section Identifier – 6 characters |
| From: | Beginning of Section |
| To: | End of Section |
| Length: | Length of Section (LF) |
| Width: | Width of Section (LF) |
| Functional Class: | Type of Functional Class for Section |
| Surface Type: | Type of surface for Section |
| PCI: | Pavement Condition Index (PCI) number at time of calculation |
| PCI Date: | Date of Inspection or PCI calculation |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|--------------------|--------------------|------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| ABBOCR 10 | 10 | ABBOTT CIR | BURGESS LN | WINDHAM WAY | 1,235 | 22 | 27,170 | R - Residential/Local | A - AC | 94 | 33.58 |
| ABBOCR 20 | 20 | ABBOTT CIR | ABBOTT CIR | EAST END | 230 | 22 | 5,060 | R - Residential/Local | A - AC | 94 | 33.58 |
| ACACLN 10 | 10 | ACACIA LN | HOOKER OAK AVE | CUL-DE-SAC | 452 | 36 | 17,990 | C - Collector | A - AC | 80 | 15.62 |
| ACORCR 10 | 10 | ACORN CIR | WHITE AVE | END | 210 | 34 | 7,140 | R - Residential/Local | A - AC | 77 | 24.3 |
| ADMILN 10 | 10 | ADMIRAL LN | MARIPOSA AVE | LACEWING CT | 415 | 28 | 11,620 | R - Residential/Local | A - AC | 81 | 26.84 |
| ADMILN 20 | 20 | ADMIRAL LN | LACEWING CT | VICEROY DR | 380 | 28 | 10,640 | R - Residential/Local | A - AC | 81 | 26.84 |
| AIRPBL 10 | 10 | AIRPARK BLVD | WEST END | COHASSET RD | 1,860 | 22 | 40,920 | R - Residential/Local | A - AC | 34 | 3.25 |
| AISERD 10 | 10 | AIRPORT SERVICE RD | HICKS LN | GATE | 1,140 | 22 | 25,080 | R - Residential/Local | A - AC | 70 | 19.95 |
| ALPACR 10 | 10 | ALAMEDA PARK CIR | CALIFORNIA PARK DR | ALAMEDA PARK CIR | 151 | 32 | 4,832 | C - Collector | A - AC | 27 | 0.37 |
| ALPACR 20 | 20 | ALAMEDA PARK CIR | N. CUL-DE-SAC | S. CUL-DE-SAC | 652 | 32 | 22,882 | R - Residential/Local | A - AC | 28 | 1.1 |
| ALAMAV 10 | 10 | ALAMO AVE | CUSSICK AVE | DALLAS CT W/S | 950 | 15 | 14,250 | R - Residential/Local | A - AC | 82 | 27.49 |
| ALAMAV 20 | 20 | ALAMO AVE | EAST AVE | PENSTEMON WY | 495 | 37 | 18,315 | C - Collector | A - AC | 82 | 16.58 |
| ALAMAV 30 | 30 | ALAMO AVE | EAST AVE | SOUTH CUL-DE-SAC | 1,027 | 36 | 38,772 | R - Residential/Local | A - AC | 76 | 23.66 |
| ALAMAV 40 | 40 | ALAMO AVE | HENSHAW AVE | 640 FT N/O HENSHAW AVE | 640 | 18 | 11,520 | C - Collector | A - AC | 82 | 16.58 |
| ALAMAV 50 | 50 | ALAMO AVE | PENSTEMON WAY | HENSHAW AVE | 820 | 34 | 27,880 | R - Residential/Local | A - AC | 77 | 24.21 |
| ALANLN 10 | 10 | ALAN LN | HAWTHORNE AVE | CUL-DE-SAC | 477 | 32 | 15,264 | C - Collector | A - AC | 76 | 13.84 |
| ALBAAV 10 | 10 | ALBA AVE | WHITE AVE | END | 700 | 22 | 15,400 | R - Residential/Local | A - AC | 14 | 0 |
| ALBCT 10 | 10 | ALBION CT | W. CUL-DE-SAC | E. END | 836 | 36 | 31,814 | R - Residential/Local | A - AC | 76 | 23.65 |
| ALDECT 10 | 10 | ALDEN CT | WHITEWOOD WY | CUL-DE-SAC | 191 | 28 | 7,666 | R - Residential/Local | A - AC | 79 | 25.78 |
| ALDEST 10 | 10 | ALDER ST | WOODLAND AVE | E. 8TH ST | 1,089 | 28 | 30,492 | R - Residential/Local | A - AC | 29 | 1.46 |
| ALDEST 20 | 20 | ALDER ST | E. 8TH ST | HUMBOLDT AVE | 1,069 | 28 | 29,932 | R - Residential/Local | A - AC | 30 | 1.81 |
| ALDRCT 10 | 10 | ALDRIN CT | BORMAN WY | CUL-DE-SAC | 145 | 36 | 6,938 | R - Residential/Local | A - AC | 30 | 1.8 |
| ALGOAV 10 | 10 | ALGONKIN AVE | W. 12TH AVE | W. 11TH AVE | 856 | 18 | 15,408 | R - Residential/Local | A - AC | 82 | 27.49 |
| ALGOAV 20 | 20 | ALGONKIN AVE | W. 11TH AVE | END | 390 | 22 | 8,580 | R - Residential/Local | A - AC | 97 | 34.12 |
| ALICLN 10 | 10 | ALICE LN | HAWTHORNE AVE | CUL-DE-SAC | 483 | 36 | 19,106 | C - Collector | A - AC | 29 | 0.77 |
| ALLICT 10 | 10 | ALLIE CT | MANZANITA AV | END | 140 | 27 | 3,780 | C - Collector | A - AC | 78 | 14.71 |
| ALMACR 20 | 20 | ALMANDOR CIR | EAST AVE | CUL-DE-SAC | 170 | 36 | 7,838 | R - Residential/Local | A - AC | 84 | 28.73 |
| ALMECT 10 | 10 | ALMENDIA CT | ALMENDIA DR S/S | CUL-DE-SAC | 320 | 36 | 13,320 | R - Residential/Local | A - AC | 80 | 26.21 |
| ALMEDR 10 | 10 | ALMENDIA DR | ALMENDIA CT | MARCHETTI CT | 1,730 | 36 | 65,880 | R - Residential/Local | A - AC | 66 | 17.62 |
| ALMOST 10 | 10 | ALMOND ST | HICKORY ST | POMONA AVE | 600 | 24 | 14,400 | R - Residential/Local | A - AC | 39 | 4.79 |
| ALPIST 10 | 10 | ALPINE ST | E. 8TH ST | PVMT CHNG | 558 | 38 | 21,204 | C - Collector | A - AC | 19 | 0 |
| AMANWY 10 | 10 | AMANDA WAY | HARTFORD DR | FOREST AVE | 1,402 | 36 | 50,472 | R - Residential/Local | A - AC | 69 | 19.35 |
| AMGRDR 10 | 10 | AMBER GROVE DR | ESPLANADE DR | CALISTOGA DR | 690 | 36 | 24,840 | R - Residential/Local | A - AC | 82 | 27.61 |

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|-------------------|-------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| AMBEWY | 10 | AMBER WAY | FILBERT AVE | FILBERT AVE | 1,152 | 32 | 36,864 | R - Residential/Local | A - AC | 18 | 0 |
| ADHIDR | 10 | AMBROSE HILL DR | WEYMOUTH WAY | END | 130 | 35 | 4,550 | R - Residential/Local | A - AC | 82 | 27.49 |
| ANNACT | 10 | ANNA CT | SHARI LN | CUL-DE-SAC | 340 | 32 | 10,880 | R - Residential/Local | A - AC | 79 | 25.56 |
| ARBUAV | 10A | ARBUS AV | E. 1ST AVE. | E. 3RD AVE. | 822 | 36 | 29,592 | R - Residential/Local | A - AC | 82 | 27.49 |
| ARBUAV | 10B | ARBUS AV | E. 3RD AVE | E. 5TH AVE | 850 | 36 | 30,600 | R - Residential/Local | A - AC | 19 | 0 |
| ARBUAV | 20A | ARBUS AV | E. 5TH AVE. | E. 7TH AVE. | 842 | 25 | 21,050 | R - Residential/Local | A - AC | 18 | 0 |
| ARBUAV | 20B | ARBUS AV | E. 7TH AVE. | E. LINDO AVE. | 655 | 25 | 16,375 | R - Residential/Local | A - AC | 18 | 0 |
| ARBUAV | 30 | ARBUS AV | SIERRA VIEW WY | PALMETTO AVE | 705 | 30 | 21,150 | R - Residential/Local | A - AC | 48 | 8.55 |
| ARBUAV | 40 | ARBUS AV | PALMETTO AVE | E. 1ST AVE | 962 | 32 | 30,784 | C - Collector | A - AC | 20 | 0 |
| ARBUAV | 50 | ARBUS AV | VALLOMBROSA AVE | SIERRA VIEW WY | 699 | 26 | 18,174 | R - Residential/Local | A - AC | 40 | 5.19 |
| ARCAAV | 10 | ARCADIAN AVE | SOL-WIL-LE-NO AVE | W. LINCOLN AVE | 1,140 | 29 | 33,060 | R - Residential/Local | A - AC | 0 | 0 |
| ARCAAV | 20 | ARCADIAN AVE | W. 1ST AVE. | W. 4TH AVE S/S | 1,275 | 36 | 45,900 | C - Collector | A - AC | 82 | 16.58 |
| ARCAAV | 30 | ARCADIAN AVE | W. 4TH AVE N/S | W. 6TH AVE N/S (N INT) | 944 | 36 | 33,984 | R - Residential/Local | A - AC | 82 | 27.49 |
| ARCAAV | 40 | ARCADIAN AVE | W. SACRAMENTO AVE | W. 1ST AVE. | 378 | 36 | 13,608 | R - Residential/Local | A - AC | 82 | 27.49 |
| ARCAAV | 50 | ARCADIAN AVE | W. 6TH AVE. | W. 7TH AVE. | 376 | 36 | 13,536 | R - Residential/Local | A - AC | 11 | 0 |
| ARCAAV | 60 | ARCADIAN AVE | W. 7TH AVE. | W. 8TH AVE. | 437 | 36 | 15,732 | R - Residential/Local | A - AC | 11 | 0 |
| ARCAAV | 70 | ARCADIAN AVE | W. 8TH AVE. | W. 10TH AVE | 711 | 36 | 25,596 | R - Residential/Local | A - AC | 82 | 27.49 |
| ARCHWY | 10 | ARCH WAY | MARIGOLD AVE | CACTUS AVE | 638 | 30 | 19,140 | R - Residential/Local | A - AC | 81 | 26.84 |
| ARCHWY | 20 | ARCH WAY | MARIGOLD AVE | VIRAGE LN | 965 | 36 | 34,740 | R - Residential/Local | A - AC | 80 | 26.2 |
| ARCHWY | 30 | ARCH WAY | SOUTH END | VINTAGE LN | 900 | 27 | 24,300 | R - Residential/Local | A - AC | 81 | 26.84 |
| ARCHAV | 10 | ARCHER AVE | DAYTON RD W/S | ALLEY S/S (130 FT N/O DAYTON) | 130 | 24 | 3,120 | R - Residential/Local | A - AC | 46 | 7.67 |
| ARIZWA | 10 | ARIZONA WAY | LANCE TERR | LANCE TERR | 840 | 28 | 23,520 | R - Residential/Local | A - AC | 97 | 34.13 |
| ARLIDR | 10 | ARLINGTON DR | LONGFELLOW | CEANOTHUS AVE | 1,284 | 32 | 41,088 | R - Residential/Local | A - AC | 76 | 23.65 |
| ARMICT | 10 | ARMINTA CT | ISSAC JAMES CT | END | 270 | 32 | 8,640 | R - Residential/Local | A - AC | 83 | 28.11 |
| ARROWA | 10 | ARROYO WAY | WHITE AVE | WHITE AVE | 1,240 | 33 | 40,920 | R - Residential/Local | A - AC | 20 | 0 |
| ARTEDR | 10 | ARTESIA DR | FLORAL AVE | CORTINA DR | 1,372 | 32 | 43,904 | R - Residential/Local | A - AC | 74 | 22.39 |
| ARTEDR | 20 | ARTESIA DR | FLORAL AVE | PVMT CHNG | 627 | 36 | 22,572 | R - Residential/Local | A - AC | 66 | 17.61 |
| ARTEDR | 30 | ARTESIA DR | PVMT CHNG | END | 662 | 28 | 18,536 | R - Residential/Local | A - AC | 78 | 24.92 |
| ASCOWY | 10 | ASCOLANO WAY | PICHOLINE WY | ST AUGUSTINE DR | 603 | 34 | 20,502 | R - Residential/Local | A - AC | 80 | 26.2 |
| ASHST | 10 | ASH ST | W. 5TH ST | NORTH END | 695 | 36 | 25,020 | R - Residential/Local | A - AC | 17 | 0 |
| ASHBLN | 10 | ASHBURTON LN | LUPIN AVE | TURNBRIDGE WELLES | 439 | 32 | 14,048 | R - Residential/Local | A - AC | 61 | 14.86 |
| ASHFWY | 10 | ASHFORD WAY | E. 8TH ST | WALNUT PARK DR | 180 | 32 | 5,760 | R - Residential/Local | A - AC | 80 | 26.2 |
| ASGLDR | 10 | ASPEN GLEN DR | ESPLANADE DR | BOULDER DR | 337 | 36 | 12,132 | R - Residential/Local | A - AC | 80 | 26.21 |

Criteria:

2

SS1030

MTC StreetSaver

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Section PCI/RSL Listing

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-------------------|--------------------------------|--------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| ASPEST | 10 | ASPEN ST | E 9TH ST | HUMBOLDT AVE | 675 | 38 | 25,650 | R - Residential/Local | A - AC | 55 | 11.81 |
| AUCRCT | 10 | AUBURN CREST CT | SOUTHBURY LN | END | 240 | 32 | 7,680 | R - Residential/Local | A - AC | 82 | 27.49 |
| AUOAWY | 10 | AUBURN OAK WAY | HARVEST GLEN DR | SPRINGFIELD DR | 575 | 32 | 18,400 | R - Residential/Local | A - AC | 82 | 27.48 |
| AUGLDR | 10 | AURORA GLEN DR | EATON RD | SOUTHBURY LN | 195 | 38 | 7,410 | R - Residential/Local | A - AC | 82 | 27.49 |
| AUGLDR | 20 | AURORA GLEN DR | SOUTHBURY LN | SCARLET GROVE CT | 365 | 38 | 13,870 | R - Residential/Local | A - AC | 80 | 26.21 |
| AUGLDR | 30 | AURORA GLEN DR | SCARLET GROVE CT | WEYMOUTH WAY | 230 | 38 | 8,740 | R - Residential/Local | A - AC | 82 | 27.49 |
| AUGODR | 10 | AUTUMN GOLD DR | HENSHAW AVE | HENSHAW AVE | 2,090 | 36 | 78,240 | R - Residential/Local | A - AC | 81 | 26.85 |
| AUHILN | 10 | AUTUMN HILL LN | RIDGEBROOK WY | GOLDEN BIRCH PL | 301 | 32 | 9,632 | R - Residential/Local | A - AC | 82 | 27.48 |
| AUFWY | 10 | AUTUMNFIELDS WAY | END | BANCROFT DR | 185 | 16 | 2,960 | R - Residential/Local | A - AC | 93 | 33.47 |
| AUFWY | 20 | AUTUMNFIELDS WAY | BANCROFT DR | E 20TH ST | 185 | 16 | 2,960 | R - Residential/Local | A - AC | 94 | 33.57 |
| AUTUCT | 10 | AUTUMNWOOD CT | ALMENDIA DR | CUL-DE-SAC | 320 | 36 | 13,320 | R - Residential/Local | A - AC | 42 | 5.99 |
| AUTUDR | 10 | AUTUMNWOOD DR | OAK WY | ALMENDIA DR | 1,110 | 36 | 39,960 | R - Residential/Local | A - AC | 70 | 19.96 |
| AVANWY | 10 | AVANTE WAY | FLORAL AVE | CUL-DE-SAC | 426 | 30 | 12,780 | R - Residential/Local | A - AC | 42 | 5.98 |
| AVBRCT | 10 | AVENIDA BRISA CT | PENINSULA DR | END | 300 | 32 | 9,600 | R - Residential/Local | A - AC | 85 | 29.34 |
| AVOCCT | 10 | AVOCET CT | WISTERIA LN | END | 200 | 32 | 6,400 | R - Residential/Local | A - AC | 82 | 27.49 |
| AVONWY | 10 | AVONDALE WAY | LEGACY LN | WINDROSE CT | 223 | 36 | 8,028 | R - Residential/Local | A - AC | 81 | 26.85 |
| AZALAV | 10 | AZALEA AVE | CONASSET RD | END | 495 | 32 | 15,840 | R - Residential/Local | A - AC | 27 | 0.76 |
| AZTEDR | 10 | AZTEC DR | OTTERSON DR | 50 FT W/O HUSS DR | 812 | 41 | 33,292 | C - Collector | A - AC | 82 | 16.58 |
| BST | 10 | B ST | C.L. (95 FT S/O CLEVELAND) | E. 16TH ST | 284 | 37 | 10,508 | R - Residential/Local | A - AC | 36 | 3.85 |
| BALTLP | 10 | BALTAR LOOP | MORGAN DR | MORGAN DR | 535 | 28 | 14,980 | R - Residential/Local | A - AC | 81 | 26.84 |
| BAORDR | 10 | BAMBOO ORCHARD DR | GOOSELAKE CIR | END | 380 | 28 | 10,640 | R - Residential/Local | A - AC | 82 | 27.49 |
| BANCDR | 10 | BANCROFT DR | DAWNCREST DR | AUTUMNFIELDS WAY | 615 | 27 | 16,605 | R - Residential/Local | A - AC | 94 | 33.57 |
| BANCDR | 20 | BANCROFT DR | AUTUMNFIELDS WAY | BEAUMONT AVE | 790 | 27 | 21,330 | R - Residential/Local | A - AC | 94 | 33.57 |
| BANCDR | 30 | BANCROFT DR | BEAUMONT AVE | LONGWOOD DR | 515 | 27 | 13,905 | R - Residential/Local | A - AC | 94 | 33.57 |
| BAPEDR | 10 | BANNER PEAK DR | BRUCE RD | END | 285 | 27 | 7,695 | R - Residential/Local | A - AC | 94 | 33.57 |
| BAPADR | 10 | BANNING PARK DR | IDYLLWILD CIR | IDYLLWILD CIR | 1,452 | 32 | 46,464 | C - Collector | A - AC | 81 | 16.09 |
| BATRST | 10 | BAR TRIANGLE ST | MARILYN DR | PVMT CHNG (148FT S/O LA ROCCO) | 904 | 37 | 33,448 | R - Residential/Local | A - AC | 78 | 24.92 |
| BATRST | 20 | BAR TRIANGLE ST | PARKWAY VILLAGE DR | MARILYN DR | 651 | 16 | 10,416 | R - Residential/Local | A - AC | 70 | 19.95 |
| BATRST | 30 | BAR TRIANGLE ST | PVMT CHNG (148FT S/O LA ROCCO) | FOREST AVE | 629 | 37 | 23,273 | R - Residential/Local | A - AC | 22 | 0 |
| BARKCT | 10 | BARKER CT | PENINSULA DR | END | 435 | 32 | 13,920 | R - Residential/Local | A - AC | 84 | 28.73 |

Section PCI/RSL Listing

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|------------------|-------------------------------|-------------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| BARODR | 10 | BARONI DR | REMINGTON DR | CUL-DE-SAC | 962 | 35 | 35,546 | R - Residential/Local | A - AC | 80 | 26.41 |
| BARTST | 10 | BARTLETT ST | E. 8TH ST | E. 9TH ST | 305 | 37 | 11,285 | R - Residential/Local | A - AC | 78 | 24.93 |
| BARTST | 20 | BARTLETT ST | E. 9TH ST | HUMBOLDT AVE. | 605 | 36 | 21,780 | R - Residential/Local | A - AC | 79 | 25.57 |
| BARTTR | 10 | BARTRAM TERR | MARIPOSA AVE | CUL-DE-SAC | 558 | 24 | 16,010 | R - Residential/Local | A - AC | 69 | 19.35 |
| BASSCT | 10 | BASSWOOD CT | W 11TH AVE | END | 400 | 36 | 14,400 | R - Residential/Local | A - AC | 81 | 27.06 |
| BAYAV | 10 | BAY AVE | WEST SHASTA | CATHRINE CT | 4,114 | 36 | 148,104 | C - Collector | A - AC | 31 | 1.19 |
| BEACCO | 10 | BEACHCOMBER COVE | PENDANT PL | LANCE TERR | 690 | 28 | 19,320 | R - Residential/Local | A - AC | 93 | 33.47 |
| BEACST | 10 | BEACON ST | CONCORD AVE | BRUCE RD | 935 | 32 | 29,920 | C - Collector | A - AC | 99 | 24.24 |
| BEAUAV | 10 | BEAUMONT AVE | BANCROFT DR | AUTUMNFIELDS WAY | 545 | 28 | 15,260 | R - Residential/Local | A - AC | 94 | 33.57 |
| BEDFDR | 10 | BEDFORD DR | HARTFORD DR | SPRINGFIELD DR | 1,201 | 28 | 33,628 | R - Residential/Local | A - AC | 65 | 17.05 |
| BEECST | 10 | BEECH ST | E. 18TH ST | E. 16 TH ST | 388 | 28 | 10,864 | R - Residential/Local | A - AC | 21 | 0 |
| BEGOLN | 10 | BEGONIA LN | REDWOOD WY | CUL-DE-SAC | 427 | 35 | 16,738 | R - Residential/Local | A - AC | 76 | 23.85 |
| BELACR | 10 | BELAIRE CIR | MORELAND DR | CUL-DE-SAC | 200 | 36 | 8,528 | R - Residential/Local | A - AC | 19 | 0 |
| BELGAV | 10 | BELGIUM AVE | E. 20TH ST | CUL-DE-SAC | 1,151 | 33 | 39,926 | R - Residential/Local | A - AC | 77 | 24.49 |
| BELLCT | 10 | BELLARMINE CT | MEYERS ST | PVMT CHNG (350 FT N/O MEYERS) | 350 | 40 | 14,000 | R - Residential/Local | A - AC | 40 | 5.19 |
| BELLCT | 20 | BELLARMINE CT | PVMT CHNG (350 FT N/O MEYERS) | CUL-DE-SAC | 367 | 40 | 16,507 | R - Residential/Local | A - AC | 19 | 0 |
| BENAWY | 10 | BENATAR WAY | ZENELLA WY | DOMINIC DR | 1,048 | 40 | 41,920 | C - Collector | A - AC | 36 | 2.24 |
| BEOAWY | 10 | BENDING OAK WAY | RIDGEBROOK WAY | HERITAGE OAK DR | 545 | 36 | 19,620 | R - Residential/Local | A - AC | 78 | 24.84 |
| BENTAV | 10 | BENTON AVE | E. EATON RD | CUL-DE-SAC | 516 | 33 | 18,971 | R - Residential/Local | A - AC | 66 | 17.81 |
| BERKLN | 10 | BERKELEY LN | PALMETTO AVE | CUL-DE-SAC | 445 | 32 | 16,258 | R - Residential/Local | A - AC | 21 | 0 |
| BETSWY | 10 | BETSEY WAY | BRENDA DR | SHARI LN | 992 | 36 | 35,712 | R - Residential/Local | A - AC | 78 | 25.13 |
| BIDWAV | 70 | BIDWELL AVE | BIDWELL DR | STEWART AVE | 825 | 16 | 13,200 | R - Residential/Local | A - AC | 49 | 9.2 |
| BIDWAV | 80 | BIDWELL AVE | STEWART AVE | EAST END | 2,000 | 16 | 32,000 | R - Residential/Local | A - AC | 73 | 21.99 |
| BIDWPL | 10 | BIDWELL PL | BIDWELL AVE | STEWART AVE | 390 | 24 | 9,360 | R - Residential/Local | A - AC | 67 | 18.4 |
| BIRICT | 10 | BIDWELL RIDGE CT | SPARROW HAWK LN | CUL-DE-SAC | 310 | 20 | 9,118 | R - Residential/Local | A - AC | 81 | 26.84 |
| BIRDCT | 10 | BIRDWING CT | SWALLOWTAIL WAY | END | 355 | 28 | 9,940 | R - Residential/Local | A - AC | 84 | 28.73 |
| BLOADR | 10 | BLACK OAK DR | W. LINDO AVE | CUL-DE-SAC | 1,332 | 36 | 49,752 | R - Residential/Local | A - AC | 78 | 24.93 |
| BLWAWY | 10 | BLACK WALNUT WAY | ALAMO AVE | NEW HEATHER WY | 530 | 32 | 19,064 | R - Residential/Local | A - AC | 82 | 27.49 |
| BLACCT | 10 | BLACKMUIR CT | ALMENDIA DR | CUL-DE-SAC | 318 | 36 | 13,248 | R - Residential/Local | A - AC | 80 | 26.21 |
| BLAKCT | 10 | BLACKSTONE CT | COIT TOWER WY | CUL-DE-SAC | 599 | 32 | 19,168 | R - Residential/Local | A - AC | 79 | 25.56 |
| BLAQCT | 10 | BLANQUETA CT | ST AUGUSTINE DR | CUL-DE-SAC | 586 | 32 | 20,856 | R - Residential/Local | A - AC | 78 | 24.92 |
| BLANCT | 10 | BLANTON CT | MEADOW RD | END | 160 | 32 | 5,120 | R - Residential/Local | A - AC | 47 | 8.11 |

Criteria:

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|------------------|--------------------|--------------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| BLOAV | 10 | BLOOMINGTON AVE | COIT TOWER WY | CUL-DE-SAC | 979 | 36 | 35,244 | R - Residential/Local | A - AC | 84 | 28.73 |
| BLRIAV | 10 | BLUE RIDGE AVE | RUSHMORE AVE | DIABLO AVE | 359 | 32 | 11,488 | R - Residential/Local | A - AC | 13 | 0 |
| BOEIAV | 10 | BOEING AVE | FORTRESS ST | COHASSET RD | 1,253 | 61 | 76,433 | R - Residential/Local | A - AC | 39 | 4.78 |
| BOEIAV | 20 | BOEING AVE | FORTRESS ST | END | 309 | 38 | 11,742 | R - Residential/Local | A - AC | 72 | 21.16 |
| BONARD | 10 | BONAIR RD | MANCHESTER RD | KENTFIELD RD | 536 | 32 | 17,152 | R - Residential/Local | A - AC | 64 | 16.49 |
| BORDCT | 10 | BORDEAUX CT | MONTICELLO LN | END | 475 | 36 | 17,100 | R - Residential/Local | A - AC | 39 | 4.91 |
| BORMWY | 10 | BORMAN WAY | HAWTHORNE AVE | SHEPARD LN | 977 | 32 | 31,264 | C - Collector | A - AC | 42 | 3.38 |
| BOSTDR | 10 | BOSTON DR | SARATOGA DR | COLONIAL DR | 275 | 36 | 9,900 | R - Residential/Local | A - AC | 80 | 26.2 |
| BOUCST | 10 | BOUCHER ST | CLEVELAND AVE | E. 16TH ST | 476 | 32 | 15,232 | R - Residential/Local | A - AC | 18 | 0 |
| BOULDR | 10 | BOULDER DR | CAVALIER WY | VAIL DR | 385 | 28 | 10,780 | R - Residential/Local | A - AC | 82 | 27.49 |
| BRADCT | 10 | BRADFORD CT | ALAMO AVE | CUL-DE-SAC | 720 | 36 | 27,720 | R - Residential/Local | A - AC | 75 | 23.03 |
| BRENDR | 10 | BRENDA DR | BRENDA DR | FOREST AVE | 140 | 36 | 5,040 | R - Residential/Local | A - AC | 74 | 22.39 |
| BRENDR | 20 | BRENDA DR | SHARI LN | BETSY WY | 688 | 36 | 24,768 | R - Residential/Local | A - AC | 80 | 26.2 |
| BRENNWY | 10 | BRENNI WAY | CEANOTHUS AVE | LUCY WY | 406 | 32 | 12,992 | R - Residential/Local | A - AC | 79 | 25.56 |
| BRICAV | 10 | BRICE AVE | WARNER ST | END | 396 | 36 | 14,256 | R - Residential/Local | A - AC | 20 | 0 |
| BRITLN | 10 | BRITTANY LN | MADRONE AVE | NICOLE LN | 583 | 36 | 20,988 | R - Residential/Local | A - AC | 23 | 0 |
| BROAST | 10 | BROADWAY ST | W. 11TH ST N/S | W. 16TH ST N/S | 1,818 | 40 | 72,720 | C - Collector | A - AC | 48 | 4.87 |
| BROAST | 20 | BROADWAY ST | W. 16TH ST S/S | W. 20TH ST | 1,444 | 40 | 57,760 | C - Collector | A - AC | 26 | 0.19 |
| BROAST | 30 | BROADWAY ST | W. 1ST ST S/S | W. 5TH ST N/S | 1,312 | 52 | 68,224 | C - Collector | A - AC | 81 | 16.1 |
| BROAST | 40 | BROADWAY ST | W. 5TH ST N/S | W. 8TH ST N/S | 1,000 | 52 | 52,000 | C - Collector | A - AC | 65 | 9.98 |
| BROAST | 50 | BROADWAY ST | W. 8TH ST S/S | W. 11TH ST N/S | 1,127 | 52 | 50,024 | C - Collector | A - AC | 35 | 2.03 |
| BRODCR | 10 | BRODEA CIR | PHLOX WAY | END | 205 | 32 | 6,560 | R - Residential/Local | A - AC | 16 | 0 |
| BROADR | 10 | BROOKSIDE DR | YOSEMITE DR | YOSEMITE DR | 1,528 | 32 | 48,896 | C - Collector | A - AC | 76 | 13.84 |
| BROADR | 20 | BROOKSIDE DR | YOSEMITE DR | CUL-DE-SAC | 653 | 32 | 20,896 | C - Collector | A - AC | 82 | 16.57 |
| BROOCR | 30 | BROOKVINE CIR | W. SHASTA AVE | W. SHASTA AVE | 1,236 | 36 | 49,032 | R - Residential/Local | A - AC | 66 | 17.62 |
| BRWOWY | 10 | BROOKWOOD WAY | HOOKER OAK AVE | PVMT CHNG | 484 | 36 | 17,424 | C - Collector | A - AC | 76 | 13.84 |
| BRWOWY | 20 | BROOKWOOD WAY | PVMT CHNG | CUL-DE-SAC | 800 | 36 | 28,800 | R - Residential/Local | A - AC | 21 | 0 |
| BRVADR | 10 | BROWNS VALLEY DR | CITY LIMIT (NORTH) | CITY LIMIT (SOUTH) | 1,260 | 32 | 40,320 | C - Collector | A - AC | 50 | 5.39 |
| BRUCRD | 10 | BRUCE RD | DEER CREEK HIGHWAY | HUMBOLDT RD | 1,678 | 29 | 48,662 | C - Collector | A - AC | 74 | 13 |
| BRUCRD | 20 | BRUCE RD | E. 20TH ST S/S | SKYWAY | 4,400 | 28 | 123,200 | A - Arterial | A - AC | 78 | 18.3 |
| BRUCRD | 30 | BRUCE RD | E. 8TH ST | LAKWEST DR | 1,496 | 62 | 92,752 | C - Collector | A - AC | 47 | 4.61 |
| BRUCRD | 40 | BRUCE RD | LAKE WEST DR | DEER CREEK HIGHWAY | 1,780 | 40 | 71,200 | C - Collector | A - AC | 31 | 1.17 |
| BRUCRD | 50 | BRUCE RD | PICHOLINE WY S/S | PVMT CHNG (602' S/O PICHOLINE) | 612 | 55 | 33,660 | R - Residential/Local | A - AC | 33 | 2.88 |

Criteria:

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SS1030

MTC StreetSaver

Section PCI/RSL Listing

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|--------------------|--------------------------------------|--------------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| BRUCRD | 60 | BRUCE RD | PVMT CHNG (602' S/O PICHOLINE) | E. 20TH ST S/S | 1,568 | 47 | 73,696 | C - Collector | A - AC | 70 | 11.81 |
| BRUCRD | 70 | BRUCE RD | PVMT CHNG (408' N/O PICHOLINE) | PICHOLINE WY S/S | 480 | 55 | 26,400 | A - Arterial | A - AC | 51 | 7.58 |
| BRUCRD | 80 | BRUCE RD | 480' N/O PICHOLINE | 1190' N/O PICHOLINE | 710 | 30 | 21,300 | C - Collector | A - AC | 89 | 20.15 |
| BRUCRD | 90 | BRUCE RD | 1190' N/O PICHOLINE | AUMBOIDT RD | 1,130 | 32 | 36,160 | C - Collector | A - AC | 53 | 6.21 |
| BRCLN | 10 | BRUSH CREEK LN | ROGUE RIVER DR | MYSTERY RUN | 315 | 32 | 10,080 | R - Residential/Local | A - AC | 94 | 33.79 |
| BRCLN | 20 | BRUSH CREEK LN | MYSTERY RUN | CHAMBERLAIN RUN | 280 | 32 | 8,960 | R - Residential/Local | A - AC | 94 | 33.79 |
| BRYAAV | 10 | BRYANT AVE | PALMETTO AVE | E. 1ST AVE | 1,015 | 36 | 36,540 | C - Collector | A - AC | 47 | 4.61 |
| BRYAAV | 20 | BRYANT AVE | VALLOMBROSA AVE | FILBERT AVE | 814 | 25 | 20,350 | R - Residential/Local | A - AC | 16 | 0 |
| BRYAAV | 30 | BRYANT AVE | HILLVIEW WAY | PALMETTO AVE | 310 | 36 | 11,160 | R - Residential/Local | A - AC | 19 | 0 |
| BUDLCT | 10 | BUDLEE CT | CHICO CANYON RD | END | 300 | 32 | 9,600 | R - Residential/Local | A - AC | 94 | 33.57 |
| BUNKCT | 10 | BUNKER CT | DOG LEG DR | CUL-DE-SAC | 295 | 26 | 10,138 | R - Residential/Local | A - AC | 79 | 25.56 |
| BURGLN | 10 | BURGESS LN | ABBOTT CIR | ABBOTT CIR | 345 | 22 | 7,590 | R - Residential/Local | A - AC | 94 | 33.58 |
| BURLDR | 10 | BURLINGAME DR | COIT TOWER WY | CUL-DE-SAC | 1,287 | 36 | 46,332 | R - Residential/Local | A - AC | 80 | 26.2 |
| BURNAP | 10 | BURNAP AVE | COHASSET RD | HILLARY LN | 795 | 26 | 20,670 | R - Residential/Local | A - AC | 34 | 3.25 |
| BURNAP | 20 | BURNAP AVE | HILLARY LN | TARA TERR | 503 | 26 | 13,078 | R - Residential/Local | A - AC | 73 | 21.77 |
| BURNAP | 30 | BURNAP AVE | TARA TERR | E. LASSEN AVE | 812 | 36 | 29,232 | R - Residential/Local | A - AC | 30 | 1.8 |
| BURNAP | 40 | BURNAP AVE | E. LASSEN AVE | CITY LIMITS | 1,175 | 24 | 28,200 | R - Residential/Local | A - AC | 47 | 8.22 |
| BURNAP | 50 | BURNAP AVE | E. EATON RD | CITY LIMITS | 180 | 24 | 4,320 | R - Residential/Local | A - AC | 56 | 12.35 |
| BURNDR | 10 | BURNEY DR | IDYLLWILD CIR | IDYLLWILD CIR | 1,400 | 32 | 44,800 | C - Collector | A - AC | 65 | 9.97 |
| BURAWY | 10 | BURNT RANCH WAY | SAWYERS BAR LN | ROGUE RIVER DR | 705 | 28 | 19,740 | R - Residential/Local | A - AC | 86 | 30.06 |
| BURAWY | 20 | BURNT RANCH WAY | ROGUE RIVER DR | END | 375 | 28 | 10,500 | R - Residential/Local | A - AC | 82 | 27.49 |
| CST | 10 | C ST | E. 16TH ST | E. 20TH ST | 687 | 28 | 19,236 | R - Residential/Local | A - AC | 82 | 27.49 |
| CARADR | 10 | CABARET DR | RAVENSHOE WAY | END | 350 | 33 | 11,550 | R - Residential/Local | A - AC | 78 | 24.92 |
| CABRDR | 10 | CABRILLO DR | WHITE AVE | END | 300 | 33 | 9,900 | R - Residential/Local | A - AC | 4 | 0 |
| CACTAV | 10 | CACTUS AVE | ARCH WAY | PVMT CHNG (50' N/O HARMONY PARK CIR) | 480 | 28 | 13,440 | C - Collector | A - AC | 85 | 18.06 |
| CACTAV | 20 | CACTUS AVE | EAST AVE | ARCH WAY | 825 | 25 | 20,625 | C - Collector | A - AC | 84 | 17.56 |
| CACTAV | 30 | CACTUS AVE | PVMT CHNG (50' N/O HARMONY PARK CIR) | NORTH END | 1,335 | 20 | 26,700 | C - Collector | A - AC | 69 | 11.42 |
| CADECT | 10 | CADE CT | W 12TH ST | END | 550 | 24 | 13,200 | R - Residential/Local | A - AC | 82 | 27.49 |
| CALGLN | 10 | CALGARY LN | E. 1ST AVE | CUL-DE-SAC | 392 | 32 | 14,562 | R - Residential/Local | A - AC | 18 | 0 |
| CAPADR | 10 | CALIFORNIA PARK DR | ALAMEDA PARK CIR | YOSEMITE DR | 1,206 | 42 | 50,652 | C - Collector | A - AC | 80 | 15.62 |

Section PCI/RSL Listing

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|--------------------|--------------------------|--------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| CAPADR 20 | | CALIFORNIA PARK DR | BRUCE RD | UPPER LAKE CT | 1,454 | 44 | 63,976 | C - Collector | A - AC | 78 | 14.71 |
| CAPADR 30 | | CALIFORNIA PARK DR | UPPER LAKE CT | ALAMEDA PARK CIR | 969 | 42 | 40,698 | C - Collector | A - AC | 70 | 11.81 |
| CAPADR 40 | | CALIFORNIA PARK DR | YOSEMITE DR | CANYON OAKS TERR | 725 | 36 | 26,100 | C - Collector | A - AC | 45 | 4.1 |
| CALIDR 10 | | CALISTOGA DR | 90 FT N/O CRATER LAKE DR | YELLOWSTONE DR | 1,444 | 36 | 51,984 | R - Residential/Local | A - AC | 69 | 19.36 |
| CALIDR 20 | | CALISTOGA DR | DENALI DR N/S | 90 FT N/O CRATER LAKE DR | 676 | 36 | 24,336 | R - Residential/Local | A - AC | 82 | 27.49 |
| CALIDR 30 | | CALISTOGA DR | YELLOWSTONE DR | CUL-DE-SAC | 235 | 36 | 10,260 | R - Residential/Local | A - AC | 80 | 26.21 |
| CALLN 10 | | CALLA LN | FLORAL AVE | MARIPOSA AVE | 1,278 | 36 | 46,008 | R - Residential/Local | A - AC | 39 | 4.78 |
| CAMBCT 10 | | CAMBERWELL CT | VICEROY DR | END | 165 | 36 | 5,940 | R - Residential/Local | A - AC | 80 | 26.2 |
| CAMDCT 10 | | CAMDEN CT | W. LASSEN AVE | CUL-DE-SAC | 300 | 36 | 12,600 | R - Residential/Local | A - AC | 82 | 27.49 |
| CAMEWY 10 | | CAMELLIA WAY | MEMORIAL WY | END | 464 | 28 | 12,992 | R - Residential/Local | A - AC | 31 | 2.02 |
| CAMEWY 20 | | CAMELLIA WAY | E. 1ST AVE | VALLOMBROSA AVE | 275 | 36 | 9,900 | R - Residential/Local | A - AC | 94 | 33.54 |
| CAMECT 10 | | CAMELOT CT | W. SHASTA AVE | CUL-DE-SAC | 475 | 36 | 18,900 | R - Residential/Local | A - AC | 82 | 27.49 |
| CAMEDR 10 | | CAMEO DR | COHASSET RD | CUL-DE-SAC | 1,058 | 32 | 33,856 | R - Residential/Local | A - AC | 27 | 0.76 |
| CANTCR 10 | | CANTEBURY CIR | MANCHESTER RD | CUL-DE-SAC | 265 | 36 | 11,340 | R - Residential/Local | A - AC | 74 | 22.39 |
| CAPIPL 10 | | CAPISTRANO PL | MONTEREY ST | END | 195 | 26 | 5,070 | R - Residential/Local | A - AC | 26 | 0.43 |
| CAPSC 10 | | CAPSHAW CT | WARNER ST E/S | EAST CUL-DE-SAC | 436 | 32 | 16,056 | R - Residential/Local | A - AC | 80 | 26.21 |
| CAPNCT 10 | | CAPSTONE CT | WINDECKER DR N/S | WEST WIND DR | 400 | 32 | 14,904 | R - Residential/Local | A - AC | 82 | 27.49 |
| CARARD 10 | | CARAWAY RD | GARDEN PARK DR | CUL-DE-SAC | 152 | 34 | 7,036 | R - Residential/Local | A - AC | 64 | 16.49 |
| CARDCT 10 | | CARDIFF CT | ARCH WY | CUL-DE-SAC | 187 | 32 | 5,984 | R - Residential/Local | A - AC | 80 | 26.2 |
| CARBCT 20 | | CARIBOU CT | MILL CREEK DR N/S | CUL-DE-SAC | 460 | 36 | 18,360 | R - Residential/Local | A - AC | 76 | 23.66 |
| CARICT 10 | | CARIBOU CT | CRATER LAKE DR | MILL CREEK DR S/S | 244 | 36 | 8,784 | R - Residential/Local | A - AC | 80 | 26.21 |
| CARLPL 10 | | CARLOS PL | W. END | E. END | 466 | 32 | 14,912 | R - Residential/Local | A - AC | 47 | 8.11 |
| CARLCT 10 | | CARLSBAD CT | YELLOWSTONE DR | SOUTH CUL-DE-SAC | 340 | 36 | 14,040 | R - Residential/Local | A - AC | 80 | 26.21 |
| CARLCT 20 | | CARLSBAD CT | YELLOWSTONE DR | NORTH CUL-DE-SAC | 182 | 36 | 8,352 | R - Residential/Local | A - AC | 80 | 26.21 |
| CARMPL 10 | | CARMEL PL | MONTEREY ST | END | 190 | 26 | 4,940 | R - Residential/Local | A - AC | 22 | 0 |
| CARMDR 10 | | CARMICHAEL DR | E. PARK AVE | END | 948 | 36 | 34,128 | R - Residential/Local | A - AC | 20 | 0 |
| CAROAV 10 | | CAROL AVE | PARK VISTA DR | EARL AVE | 781 | 38 | 29,678 | R - Residential/Local | A - AC | 28 | 0.89 |
| CARRWY 10 | | CARRINGTON WAY | WINGFIELD AVE | BEAUMONT AVE | 280 | 16 | 4,480 | R - Residential/Local | A - AC | 94 | 33.57 |
| CARSST 10 | | CARSON ST | NOTRE DAME BLVD | MANSFIELD CT | 174 | 37 | 6,438 | R - Residential/Local | A - AC | 8 | 0 |
| CADRCT 10 | | CASA DEL REY CT | CERES AVE | CUL-DE-SAC | 532 | 36 | 20,870 | R - Residential/Local | A - AC | 17 | 0 |
| CASITR 10 | | CASITA TERR | PLUMAS WY | CUL-DE-SAC | 186 | 36 | 8,414 | R - Residential/Local | A - AC | 21 | 0 |
| CASPCT 10 | | CASPAR CT | NEWPORT DR | CUL-DE-SAC | 315 | 36 | 13,140 | R - Residential/Local | A - AC | 77 | 24.3 |

Criteria:

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SS1030

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current | Remaining |
|-----------|------------|-------------------|--------------------------------|-------------------------------|--------|-------|--------|-----------------------|--------------|---------|-----------|
| CAPORD | 10 | CATALINA POINT RD | IDYLLWILD CIR | CUL-DE-SAC | 447 | 32 | 14,304 | C - Collector | A - AC | PCI | Life |
| CAVAWY | 10 | CAVALIER WAY | SNOWBIRD DR | CUL-DE-SAC | 1,188 | 24 | 31,224 | R - Residential/Local | A - AC | 81 | 16.09 |
| CAYMCT | 10 | CAYMAN CT | RAVEN LN W/S | END | 140 | 24 | 3,360 | R - Residential/Local | A - AC | 81 | 26.85 |
| CEANAV | 10 | CEANOTHUS AVE | PVMT CHNG (116' S/O BRENNI DR) | COLONIAL DR | 2,357 | 36 | 84,852 | C - Collector | A - AC | 82 | 27.49 |
| CEANAV | 20 | CEANOTHUS AVE | COLONIAL DR | VALLEY FORGE DR | 722 | 36 | 25,992 | C - Collector | A - AC | 80 | 15.62 |
| CEANAV | 30 | CEANOTHUS AVE | EAST AVE | PVMT CHNG (240 FT N/O EAST) | 239 | 40 | 9,560 | C - Collector | A - AC | 81 | 16.09 |
| CEANAV | 40 | CEANOTHUS AVE | KIMBERLEE LN | EAST AVE | 974 | 32 | 31,168 | C - Collector | A - AC | 42 | 3.38 |
| CEANAV | 50 | CEANOTHUS AVE | MANZANITA AVE | KIMBERLEE LN | 941 | 26 | 24,466 | C - Collector | A - AC | 17 | 0 |
| CEANAV | 60 | CEANOTHUS AVE | PVMT CHNG (240 FT N/O EAST) | PVMT CHNG (116 FT S/O BRENNI) | 1,177 | 28 | 32,956 | C - Collector | A - AC | 25 | 0 |
| CEANAV | 70 | CEANOTHUS AVE | VALLEY FORGE DR | END | 515 | 36 | 18,540 | C - Collector | A - AC | 78 | 14.71 |
| CECELN | 10 | CECELIA LN | W 11TH AVE | DIAS DR | 270 | 40 | 10,800 | R - Residential/Local | A - AC | 79 | 15.16 |
| CECIPL | 10 | CECIL PL | WEBSTER DR | CUL-DE-SAC | 191 | 37 | 8,710 | R - Residential/Local | A - AC | 80 | 26.21 |
| CEGRWY | 10 | CEDAR GROVE WAY | E. 8TH ST | END | 400 | 18 | 7,200 | R - Residential/Local | A - AC | 66 | 17.62 |
| CEDAST | 10 | CEDAR ST | W. 1ST ST | W. 3RD ST N/S | 588 | 36 | 21,168 | R - Residential/Local | A - AC | 65 | 17.04 |
| CEDAST | 20 | CEDAR ST | W. 3RD ST S/S | W. 5TH ST N/S | 622 | 36 | 22,392 | R - Residential/Local | A - AC | 53 | 10.84 |
| CEDAST | 30 | CEDAR ST | W. 5TH ST S/S | W. 8TH ST N/S | 918 | 40 | 36,720 | R - Residential/Local | A - AC | 21 | 0 |
| CEDAST | 40 | CEDAR ST | W. 8TH ST S/S | CUL-DE-SAC | 534 | 40 | 21,250 | R - Residential/Local | A - AC | 19 | 0 |
| CENTAV | 10 | CENTENNIAL AVE | CHICO CANYON RD | CITY LIMITS | 110 | 24 | 2,640 | R - Residential/Local | A - AC | 27 | 0.77 |
| CENTAV | 20 | CENTENNIAL AVE | E. 8TH ST | FOXWOOD DR | 912 | 34 | 31,008 | C - Collector | A - AC | 32 | 2.36 |
| CENTAV | 30 | CENTENNIAL AVE | FOXWOOD DR | PVMT CHNG | 661 | 34 | 22,474 | R - Residential/Local | A - AC | 65 | 9.97 |
| CEREAV | 10 | CERES AVE | CORINO REAL | PRIMROSE LN | 915 | 36 | 32,940 | C - Collector | A - AC | 67 | 18.18 |
| CEREAV | 100 | CERES AVE | SAN RAMON | EAST AVE | 1,164 | 35 | 40,740 | C - Collector | A - AC | 58 | 7.68 |
| CEREAV | 120 | CERES AVE | SAN RAMON DR | HEMMING LN | 980 | 36 | 35,280 | C - Collector | A - AC | 62 | 8.95 |
| CEREAV | 20 | CERES AVE | CORINO REAL | EAST AVE | 625 | 36 | 22,500 | C - Collector | A - AC | 30 | 0.97 |
| CEREAV | 30 | CERES AVE | E. LASSEN AVE | RIDGEWOOD DR | 695 | 36 | 25,020 | C - Collector | A - AC | 45 | 4.1 |
| CEREAV | 40 | CERES AVE | HAVEN LN | MANZANITA | 560 | 36 | 20,160 | C - Collector | A - AC | 39 | 2.91 |
| CEREAV | 50 | CERES AVE | HEMMING LN | MCKINLEY LN | 920 | 36 | 33,120 | C - Collector | A - AC | 28 | 0.57 |
| CEREAV | 60 | CERES AVE | MCKINLEY LN | RUSHMORE AVE | 1,093 | 36 | 39,348 | C - Collector | A - AC | 27 | 0.37 |
| CEREAV | 70 | CERES AVE | PRIMROSE LN | HAVEN LN | 720 | 36 | 25,920 | C - Collector | A - AC | 27 | 0.37 |
| CEREAV | 80 | CERES AVE | RIDGEWOOD DR | E. EATON RD | 827 | 36 | 29,772 | C - Collector | A - AC | 40 | 3.14 |
| CEREAV | 90 | CERES AVE | RUSHMORE AVE | E. LASSEN AVE | 820 | 36 | 29,520 | C - Collector | A - AC | 67 | 10.68 |

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|------------------|--------------------------------|---------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| CERECR | 10 | CERES CIR | CERES AVE (N) | CERES AVE (S) | 1,300 | 36 | 46,800 | R - Residential/Local | A - AC | 28 | 1.1 |
| CEMACT | 10 | CERES MANOR CT | CERES AVE | CUL-DE-SAC | 555 | 36 | 21,698 | R - Residential/Local | A - AC | 73 | 21.77 |
| CHADDR | 10 | CHADWICK DR | DEVONSHIRE DR | ROCHESTER DR | 491 | 28 | 13,748 | R - Residential/Local | A - AC | 35 | 3.63 |
| CHAMRN | 10 | CHAMBERLAIN RUN | HAYFORK CREEK | BRUSH CREEK LN | 285 | 32 | 9,120 | R - Residential/Local | A - AC | 94 | 33.58 |
| CHAMRN | 20 | CHAMBERLAIN RUN | BRUSH CREEK LN | WINDHAM WAY | 395 | 32 | 12,640 | R - Residential/Local | A - AC | 94 | 33.58 |
| CHAMWY | 10 | CHAMPLAIN WAY | COACH LITE DR | END | 700 | 36 | 25,200 | R - Residential/Local | A - AC | 83 | 28.11 |
| CHANLN | 10 | CHANCERY LN | NORTH AVE | CUL-DE-SAC | 199 | 36 | 8,882 | R - Residential/Local | A - AC | 56 | 12.29 |
| CHADLN | 10 | CHANDESE LN | CHANTEL WY | HENSHAW AVE | 490 | 36 | 17,640 | R - Residential/Local | A - AC | 80 | 26.21 |
| CHANWY | 10 | CHANTEL WAY | HENSHAW AVE | CDS | 1,275 | 36 | 48,900 | R - Residential/Local | A - AC | 80 | 26.21 |
| CHGLCT | 10 | CHESTRY GLENN CT | W 12TH AVE | END | 225 | 36 | 8,100 | R - Residential/Local | A - AC | 76 | 23.87 |
| CHERST | 10 | CHESTRY ST | W 1ST ST S/S | W 5TH ST N/S | 1,190 | 52 | 61,880 | R - Residential/Local | A - AC | 45 | 7.24 |
| CHERST | 20 | CHESTRY ST | W 5TH ST S/S | W 9TH ST N/S | 1,144 | 52 | 59,488 | R - Residential/Local | A - AC | 54 | 11.32 |
| CHERST | 30 | CHESTRY ST | W 9TH ST S/S | END | 538 | 40 | 21,520 | R - Residential/Local | A - AC | 55 | 11.81 |
| CHROLN | 10 | CHESTNUT ROSE LN | AUTUMN GOLD DR | AUTUMN GOLD DR | 1,042 | 36 | 37,512 | R - Residential/Local | A - AC | 76 | 23.66 |
| CHESST | 10 | CHESTNUT ST | W 11TH ST N/S | W 14TH ST N/S | 1,346 | 36 | 48,456 | R - Residential/Local | A - AC | 67 | 18.2 |
| CHESST | 20 | CHESTNUT ST | W 14TH ST N/S | W 16TH ST N/S | 740 | 36 | 26,640 | R - Residential/Local | A - AC | 70 | 19.96 |
| CHESST | 30 | CHESTNUT ST | W 2TH ST S/S | W 5TH ST N/S | 954 | 52 | 49,608 | R - Residential/Local | A - AC | 54 | 11.32 |
| CHESST | 40 | CHESTNUT ST | W 5TH ST S/S | W 9TH ST N/S | 1,226 | 52 | 63,752 | R - Residential/Local | A - AC | 71 | 20.56 |
| CHESST | 50 | CHESTNUT ST | W 9TH ST S/S | W 11TH ST N/S | 872 | 36 | 31,392 | R - Residential/Local | A - AC | 31 | 2.17 |
| CHCARD | 10 | CHICO CANYON RD | BRUCE RD | MANZANITA AVE | 1,600 | 32 | 51,200 | A - Arterial | A - AC | 93 | 24.84 |
| CHCARD | 20 | CHICO CANYON RD | MANZANITA AVE | PVMT CHG (122FT E/O MANZANITA) | 122 | 22 | 2,684 | R - Residential/Local | A - AC | 52 | 10.36 |
| CHCARD | 30 | CHICO CANYON RD | PVMT CHG (122FT E/O MANZANITA) | PVMT CHG (2017FT E/O MANZANITA) | 1,895 | 22 | 41,690 | R - Residential/Local | A - AC | 82 | 27.48 |
| CHCARD | 50 | CHICO CANYON RD | PVMT CHNG (336 FT E/O CHICOR) | PVMT CHNG 955 FT W/CENTENNIAL | 1,434 | 22 | 31,548 | R - Residential/Local | A - AC | 82 | 27.48 |
| CHCARD | 60 | CHICO CANYON RD | PVMT CHNG 955 FT W/CENTENNIAL | CENTENNIAL AVE | 955 | 22 | 21,010 | R - Residential/Local | A - AC | 74 | 22.39 |
| CHRID | 10 | CHICO RIVER RD | W 5TH ST | CITY LIMITS | 100 | 36 | 3,600 | R - Residential/Local | A - AC | 81 | 26.76 |
| CHICRD | 10 | CHICORY RD | CHICO CANYON RD | CHICO CANYON RD | 1,357 | 30 | 40,710 | R - Residential/Local | A - AC | 79 | 25.56 |
| CICOLP | 10 | CINDER CONE LOOP | LASSEN AVE | LASSEN AVE | 1,190 | 26 | 30,940 | R - Residential/Local | A - AC | 84 | 28.74 |
| CICDRD | 10 | CINDER CREEK DR | RUBY RIVER DR | END | 1,190 | 30 | 35,700 | R - Residential/Local | A - AC | 93 | 33.47 |
| CITRAV | 10 | CITRUS AVE | W 10TH AVE. | W 8TH AVE. | 662 | 22 | 14,564 | R - Residential/Local | A - AC | 54 | 11.32 |
| CITRAV | 20 | CITRUS AVE | 462 FT S/O W 8TH AVE | W 8TH AVE | 462 | 22 | 10,164 | R - Residential/Local | A - AC | 55 | 11.81 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current | Remaining |
|-----------|------------|---------------|-------------------------------|-------------------------------|--------|-------|---------|-----------------------|--------------|---------|-----------|
| CITRAV | 30 | CITRUS AVE | 462 FT S/O W. 8TH AVE. | W. 6TH AVE. | 350 | 22 | 7,700 | R - Residential/Local | A - AC | PCI | Life |
| CITRAV | 40 | CITRUS AVE | W. 4TH AVE N/S | W. 6TH AVE S/S | 913 | 36 | 32,868 | C - Collector | A - AC | 27 | 0.77 |
| CITRAV | 50 | CITRUS AVE | W. 2ND AVE N/S (S INT) | W. 4TH AVE S/S | 841 | 30 | 25,230 | C - Collector | A - AC | 78 | 14.72 |
| CITRAV | 60 | CITRUS AVE | W. 1ST. AVE. | W. 2ND AVE N/S (S INT) | 440 | 25 | 11,000 | R - Residential/Local | A - AC | 29 | 0.78 |
| CITRAV | 70 | CITRUS AVE | W. 1ST. AVE. | W. SACRAMENTO AVE. | 389 | 25 | 9,725 | R - Residential/Local | A - AC | 58 | 13.3 |
| CITRAV | 80 | CITRUS AVE | MANSION AVE | W. LINCOLN AVE | 887 | 29 | 25,723 | R - Residential/Local | A - AC | 53 | 10.84 |
| CLARLN | 10 | CLARA LN | HAWTHORNE AVE | CUL-DE-SAC | 230 | 32 | 9,378 | C - Collector | A - AC | 15 | 0 |
| CLARCR | 10 | CLAREMONT CIR | ARLINGTON DR | CUL-DE-SAC | 345 | 32 | 11,040 | R - Residential/Local | A - AC | 20 | 0 |
| CLARCT | 10 | CLARENCE CT | W. SHASTA AVE | CUL-DE-SAC | 466 | 36 | 18,576 | R - Residential/Local | A - AC | 62 | 15.39 |
| CLLADR | 10 | CLEAR LAKE DR | MILL CREEK DR N/S | CUL-DE-SAC | 460 | 36 | 18,360 | R - Residential/Local | A - AC | 62 | 15.4 |
| CLEACT | 10 | CLEAVES CT | RAVENSHOE WY | END | 356 | 28 | 9,968 | R - Residential/Local | A - AC | 80 | 26.21 |
| CLOTWY | 10 | CLOTILDE WAY | GILBERT LN | W. SACRAMENTO AVE | 495 | 36 | 17,820 | R - Residential/Local | A - AC | 71 | 20.55 |
| CLOUCT | 10 | CLOUD CT | BARONI DR | CUL-DE-SAC | 229 | 32 | 9,432 | R - Residential/Local | A - AC | 80 | 26.42 |
| CLOVST | 10 | CLOVER ST | E 10TH ST | HUMBOLDT AVE | 378 | 32 | 12,096 | R - Residential/Local | A - AC | 80 | 26.2 |
| COLIDR | 10 | COACH LITE DR | CHAMPLAIN DR | CUL-DE-SAC | 479 | 42 | 21,386 | R - Residential/Local | A - AC | 4 | 0 |
| COHALN | 10 | COHASSET LN | COHASSET RD | CUL-DE-SAC | 1,064 | 37 | 39,368 | R - Residential/Local | A - AC | 67 | 18.18 |
| COHARD | 10 | COHASSET RD | 810 FT E/O ESPLANADE DR | MANGROVE AVE | 2,200 | 66 | 145,200 | A - Arterial | A - AC | 51 | 9.9 |
| COHARD | 100A | COHASSET RD | PVMT CHNG (188 FT N/O BOEING) | 1550 FT FROM PVMT CHNG | 1,555 | 32 | 49,760 | R - Residential/Local | A - AC | 0 | 0 |
| COHARD | 100B | COHASSET RD | 1550 FT FROM PVMT CHNG | CITY LIMIT (725 FT N/O RYAN) | 2,959 | 32 | 94,688 | R - Residential/Local | A - AC | 91 | 32.61 |
| COHARD | 110A | COHASSET RD | THORNTREE DR N/S | 2300 FT FROM THORNTREE DR | 2,300 | 32 | 73,600 | A - Arterial | A - AC | 83 | 20.81 |
| COHARD | 110B | COHASSET RD | 2300 FT FROM THORNTREE DR | TWO OAKS DR N/S | 975 | 32 | 31,200 | A - Arterial | A - AC | 44 | 5.11 |
| COHARD | 120 | COHASSET RD | TWO OAKS DR N/S | PVMT CHNG (188 FT N/O BOEING) | 2,312 | 32 | 73,984 | R - Residential/Local | A - AC | 60 | 14.33 |
| COHARD | 20 | COHASSET RD | E EATON | THORNTREE | 2,660 | 36 | 95,760 | A - Arterial | A - AC | 35 | 2.57 |
| COHARD | 30 | COHASSET RD | EAST AVE N/S | LUPIN AVE N/S | 2,658 | 60 | 159,480 | R - Residential/Local | A - AC | 28 | 1.1 |
| COHARD | 40 | COHASSET RD | ESPLANADE DR | 810 FT E/O ESPLANADE DR | 810 | 50 | 40,500 | A - Arterial | A - AC | 82 | 20.19 |
| COHARD | 50 | COHASSET RD | HWY 99 OVERPASS N/S | MANZANITA AVE N/S | 1,295 | 60 | 77,700 | R - Residential/Local | A - AC | 84 | 28.95 |
| COHARD | 60 | COHASSET RD | LASSEN AV | E EATON RD | 757 | 70 | 52,990 | C - Collector | A - AC | 40 | 3.14 |
| COHARD | 70 | COHASSET RD | LUPIN AVE N/S | E. LASSEN AVE N/S | 2,088 | 60 | 125,280 | R - Residential/Local | A - AC | 30 | 1.8 |

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|-------------------|---------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| COHARD | 80 | COHASSET RD | MANGROVE AVE | HWY 99 OVERPASS S/S | 563 | 70 | 319,410 | R - Residential/Local | A - AC | 86 | 30.15 |
| COHARD | 90 | COHASSET RD | MANZANITA AVE N/S | EAST AVE N/S | 1,190 | 60 | 71,400 | C - Collector | A - AC | 83 | 17.28 |
| COTOWY | 10 | COIT TOWER WAY | STERLING OAKS DR | LAKWEST DR | 735 | 36 | 26,460 | C - Collector | A - AC | 83 | 17.06 |
| COTOWY | 20 | COIT TOWER WAY | LAKWEST DR | E. 8TH ST | 1,540 | 36 | 55,440 | R - Residential/Local | A - AC | 87 | 30.63 |
| COLECT | 10 | COLEMAN CT | EAST AVE | CUL-DE-SAC | 531 | 36 | 20,834 | R - Residential/Local | A - AC | 33 | 2.88 |
| COLMDR | 10 | COLMENA DR | ALMENDIA DR | W. SACRAMENTO AVE | 474 | 36 | 17,064 | R - Residential/Local | A - AC | 70 | 19.96 |
| COLODR | 10 | COLONIAL DR | CEANOTHUS AVE | HUDSON AVE | 492 | 36 | 17,712 | R - Residential/Local | A - AC | 78 | 24.92 |
| COLUAV | 10 | COLUMBUS AVE | W. SACRAMENTO AVE | END | 990 | 36 | 35,640 | R - Residential/Local | A - AC | 22 | 0 |
| COLUST | 10 | COLUSA ST | E. 8TH ST | MODOC DR | 665 | 35 | 23,275 | C - Collector | A - AC | 80 | 15.62 |
| COMMCT | 10 | COMMERCE CT | IVY ST | CUL-DE-SAC | 555 | 40 | 24,027 | R - Residential/Local | A - AC | 80 | 26.21 |
| COWECT | 10 | COMMONWEALTH CT | ST LAURENCE AVE | CHAMPLAIN WAY | 630 | 36 | 22,680 | R - Residential/Local | A - AC | 85 | 29.34 |
| COMSRD | 10 | COMSTOCK RD | PARKHURST ST | TIOGA WY | 888 | 37 | 32,856 | R - Residential/Local | A - AC | 77 | 24.3 |
| CONCAV | 10 | CONCORD AVE | E 20TH ST | END | 1,550 | 32 | 49,600 | R - Residential/Local | A - AC | 94 | 33.79 |
| CONNAV | 10 | CONNERS AVE | EAST AVE. | CONNORS CT | 218 | 36 | 7,848 | R - Residential/Local | A - AC | 84 | 28.86 |
| CONNAV | 20 | CONNORS AVE | ESPLANADE | CONNORS CT | 1,466 | 36 | 52,776 | R - Residential/Local | A - AC | 19 | 0 |
| CONNCT | 10 | CONNORS CT | CONNORS AVE | END | 274 | 36 | 9,864 | R - Residential/Local | A - AC | 47 | 8.11 |
| CONNAV | 10 | CONVAIR AVE | FORTRESS ST | MARAUDER ST | 1,020 | 40 | 13,218 | R - Residential/Local | A - AC | 70 | 19.95 |
| CONNAV | 20 | CONVAIR AVE | FORTRESS ST | END | 300 | 40 | 12,000 | R - Residential/Local | A - AC | 74 | 22.33 |
| COOKWY | 10 | COOKS WAY | MADRONE AVE | CUL-DE-SAC | 614 | 32 | 19,648 | C - Collector | A - AC | 25 | 0 |
| COPPDR | 10 | COPPERFIELD DR | WHITE CEDAR LN | COIT TOWER WAY | 965 | 32 | 30,880 | R - Residential/Local | A - AC | 80 | 26.2 |
| CORACR | 10 | CORAL CIR | CAMEO DR | CUL-DE-SAC | 241 | 34 | 10,062 | R - Residential/Local | A - AC | 44 | 6.81 |
| CORDCT | 10 | CORDELIA CT | GILBERT LN | CUL-DE-SAC | 454 | 36 | 18,145 | R - Residential/Local | A - AC | 79 | 25.57 |
| CORIRL | 10 | CORINO REAL | CERES AVE | CUL-DE-SAC | 485 | 36 | 19,178 | R - Residential/Local | A - AC | 7 | 0 |
| COOAWY | 10 | CORK OAK WAY | NATIVE OAK DR | PIN OAK LN | 450 | 22 | 9,900 | R - Residential/Local | A - AC | 97 | 34.13 |
| CORNCT | 10 | CORNICABRA CT | ASCOLANO WY | CUL-DE-SAC | 244 | 32 | 10,001 | R - Residential/Local | A - AC | 78 | 24.92 |
| CORNPL | 10 | CORNWALL PL | WINDHAM WAY | WEYMOUTH WAY | 315 | 32 | 10,080 | R - Residential/Local | A - AC | 82 | 27.49 |
| CORTDR | 10 | CORTINA DR | ARTESIA DR | FLORAL AVE | 458 | 32 | 14,656 | R - Residential/Local | A - AC | 76 | 23.65 |
| COSMCT | 10 | COSMO CT | HOLLY AVE | END | 460 | 24 | 11,040 | R - Residential/Local | A - AC | 73 | 21.78 |
| COTTAV | 10 | COTTAGE AVE | NORTH AVE | CUL-DE-SAC | 835 | 32 | 26,720 | R - Residential/Local | A - AC | 50 | 9.43 |
| COTTGR | 10 | COTTAGE CIR | COTTAGE AVE | CUL-DE-SAC | 395 | 32 | 12,640 | R - Residential/Local | A - AC | 16 | 0 |
| COUNDR | 10 | COUNTRY DR | E. PARK AVE | FAIR ST. | 2,120 | 36 | 76,320 | R - Residential/Local | A - AC | 28 | 1.11 |
| COUNLN | 10 | COUNTRYSIDE LN | W. SHASTA AVE | CUL-DE-SAC | 987 | 36 | 39,432 | R - Residential/Local | A - AC | 82 | 27.49 |
| COURCR | 10 | COURTLAND CIR | TIOGA WY | CUL-DE-SAC | 162 | 37 | 7,886 | R - Residential/Local | A - AC | 73 | 21.78 |

Section PCI/RSL Listing

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|----------------|-------------------|---------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| CRAPCT | 10 | CRABAPPLE CT | WISTERIA LN | END | 200 | 28 | 5,600 | R - Residential/Local | A - AC | 97 | 34.12 |
| CRLADR | 10 | CRATER LAKE DR | CALISTOGA DR | CARIBOU DR | 896 | 36 | 32,256 | R - Residential/Local | A - AC | 80 | 26.21 |
| CRLADR | 20 | CRATER LAKE DR | CARIBOU DR | CDS | 868 | 36 | 31,248 | R - Residential/Local | A - AC | 82 | 27.49 |
| CREECT | 10 | CREEKWOOD CT | HOOKER OAK AVE | CUL-DE-SAC | 414 | 32 | 13,248 | R - Residential/Local | A - AC | 30 | 1.8 |
| CRISAV | 10 | CRISTER AVE | VALLOMBROSA AVE | FILBERT AVE | 1,264 | 35 | 44,240 | R - Residential/Local | A - AC | 6 | 0 |
| CROMDR | 10 | CROMWELL DR | CUSSICK AVE | GRAFTON PARK DR | 574 | 36 | 22,464 | R - Residential/Local | A - AC | 82 | 27.49 |
| CRUSCT | 10 | CRUSADER CT | RYAN AVE | CUL-DE-SAC | 258 | 38 | 11,372 | R - Residential/Local | A - AC | 72 | 21.16 |
| CUSSAV | 10 | CUSSICK AVE | CROMWELL DR | HOLLY AVE | 305 | 36 | 10,980 | R - Residential/Local | A - AC | 42 | 5.99 |
| CUSSAV | 20 | CUSSICK AVE | CROMWELL DR | END (S/O ALAMO AVE) | 260 | 34 | 8,840 | R - Residential/Local | A - AC | 59 | 13.82 |
| CUSSAV | 30 | CUSSICK AVE | EAST AVE | HENSHAW AVE S/S | 1,275 | 25 | 31,875 | C - Collector | A - AC | 58 | 7.69 |
| CUSSAV | 40 | CUSSICK AVE | HENSHAW AVE N/S | W. LASSEN AVE N/S | 664 | 26 | 17,264 | C - Collector | A - AC | 21 | 0 |
| CUSSAV | 50 | CUSSICK AVE | W. LASSEN AVE N/S | W. SHASTA AVE S/S | 1,292 | 26 | 33,592 | C - Collector | A - AC | 41 | 3.16 |
| CYNDCR | 10 | CYNDI CIR | COHASSET RD | END | 520 | 36 | 18,720 | R - Residential/Local | A - AC | 23 | 0 |
| CYPRST | 10 | CYPRESS ST | E. 7TH ST | WOODLAND AVE | 950 | 38 | 36,100 | R - Residential/Local | A - AC | 67 | 18.31 |
| CYPRST | 20 | CYPRESS ST | HUMBOLDT AVE | E. 7TH ST | 1,255 | 38 | 47,690 | R - Residential/Local | A - AC | 66 | 17.62 |
| CYPRST | 30 | CYPRESS ST | PINE ST | HUMBOLDT AVE | 588 | 38 | 22,344 | R - Residential/Local | A - AC | 14 | 0 |
| DST | 10 | D ST | CLEVELAND AVE | E 19TH ST | 660 | 32 | 21,120 | R - Residential/Local | A - AC | 82 | 27.7 |
| DAHLWY | 10 | DAHLIA WAY | FUCHSIA WAY | END | 200 | 34 | 6,800 | R - Residential/Local | A - AC | 77 | 24.3 |
| DALEWY | 10 | DALE WAY | HOWARD DR | MARIPOSA AVE | 953 | 34 | 32,402 | R - Residential/Local | A - AC | 11 | 0 |
| DAPORD | 10 | DANA POINT RD | IDYLLWILD CIR | CUL-DE-SAC | 660 | 32 | 21,120 | C - Collector | A - AC | 81 | 16.09 |
| DANBWY | 10 | DANBURY WAY | ARLINGTON DR | KIMBERLEE LN | 775 | 32 | 24,800 | R - Residential/Local | A - AC | 64 | 16.49 |
| DARCT | 10 | DARTMOUTH CT | VALLEY FORGE DR | END | 165 | 32 | 5,280 | R - Residential/Local | A - AC | 78 | 24.92 |
| DARTDR | 10 | DARTWOOD DR | GATEWAY DR W/S | RICHLAND CT E/S | 225 | 36 | 8,100 | R - Residential/Local | A - AC | 66 | 17.62 |
| DAVIST | 10 | DAVIS ST | CLEVELAND ST | E. 16TH ST | 410 | 28 | 11,480 | R - Residential/Local | A - AC | 57 | 13.01 |
| DAWNDR | 10 | DAWNCREST DR | E 20TH ST | END | 620 | 16 | 9,920 | R - Residential/Local | A - AC | 94 | 33.57 |
| DAYTRD | 10 | DAYTON RD | CHERRY ST W/S | END | 534 | 36 | 19,224 | R - Residential/Local | A - AC | 55 | 11.81 |
| DAYTRD | 20 | DAYTON RD | DAYTON RD | CUL-DE-SAC | 750 | 36 | 28,800 | R - Residential/Local | A - AC | 47 | 8.11 |
| DAYTRD | 30 | DAYTON RD | DAYTON RD W/S | POMONA AVE E/S | 535 | 32 | 17,120 | R - Residential/Local | A - AC | 75 | 23.03 |
| DAYTRD | 40 | DAYTON RD | POMONA AVE E/S | McINTOSH AVE | 614 | 36 | 22,104 | R - Residential/Local | A - AC | 74 | 22.41 |
| DEANWY | 10 | DEAN WAY | NEAL DOW AVE | NEAL DOW AVE | 1,237 | 32 | 39,584 | R - Residential/Local | A - AC | 16 | 0 |
| DEGADR | 10 | DEGARMO DR | ESPLANDE | MAMMOTH LAKES DR | 2,640 | 28 | 73,920 | C - Collector | A - AC | 93 | 22.26 |
| DELYDR | 10 | DELANEY DR | YOSEMITE DR | CUL-DE-SAC | 3,000 | 32 | 96,000 | C - Collector | A - AC | 85 | 18.06 |
| DELADR | 10 | DELAWARE DR | E. LASSEN DR | E. LASSEN DR | 806 | 32 | 25,792 | R - Residential/Local | A - AC | 77 | 24.29 |

Criteria:

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SS1030

MTC StreetSaver

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|------------|------------|-------------------------------|---------------------|---------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| DENADR 10 | 10 | DENALI DR | CALISTOGA DR | ROLLINS LAKE DR W/S | 854 | 36 | 35,744 | R - Residential/Local | A - AC | 80 | 26.22 |
| DENADR 20 | 20 | DENALI DR | ROLLINS LAKE DR W/S | CUL-DE-SAC | 974 | 36 | 35,064 | R - Residential/Local | A - AC | 82 | 27.49 |
| DESCDR 10 | 10 | DESCHUTES DR | SUN RIVER DR | UPLAND DR | 535 | 30 | 16,050 | R - Residential/Local | A - AC | 72 | 21.19 |
| DESILN 10 | 10 | DESIREE LN | ROCKIN M DR | ROCKIN M DR | 365 | 28 | 10,220 | R - Residential/Local | A - AC | 83 | 28.14 |
| DEVEST 10 | 10 | DEVENEY ST | DAYTON RD | CUL-DE-SAC | 177 | 22 | 4,270 | R - Residential/Local | A - AC | 19 | 0 |
| DEVODR 10 | 10 | DEVONSHIRE DR | BEDFORD DR | SPRINGFIELD DR | 1,204 | 31 | 37,324 | R - Residential/Local | A - AC | 57 | 12.82 |
| DIABAV 10 | 10 | DIABLO AVE | BLUE RIDGE AVE | CERES AVE | 519 | 35 | 18,165 | R - Residential/Local | A - AC | 18 | 0 |
| DIABAV 20 | 20 | DIABLO AVE | CERES AVE | CUL-DE-SAC | 246 | 32 | 7,872 | R - Residential/Local | A - AC | 17 | 0 |
| DIASDR 10 | 10 | DIAS DR | CECELIA LN | MEADOW RD | 845 | 36 | 30,420 | R - Residential/Local | A - AC | 71 | 20.56 |
| DIASDR 20 | 20 | DIAS DR | MEADOW RD | END | 485 | 34 | 16,490 | R - Residential/Local | A - AC | 75 | 23.03 |
| DISCWY 10 | 10 | DISCOVERY WAY | FLORAL AVE | CUL-DE-SAC | 437 | 32 | 13,984 | R - Residential/Local | A - AC | 59 | 13.84 |
| DIXOST 10 | 10 | DIXON ST | W. LINDO AVE | S. END | 948 | 36 | 34,128 | R - Residential/Local | A - AC | 80 | 26.22 |
| DIXOST 20 | 20 | DIXON ST | W. 12TH AVE | N. END | 428 | 36 | 15,408 | R - Residential/Local | A - AC | 55 | 11.81 |
| DOLEDR 10 | 10 | DOG LEG DR | PENINSULA DR | CUL-DE-SAC | 1,093 | 20 | 21,860 | R - Residential/Local | A - AC | 82 | 27.51 |
| DOLPBD 10 | 10 | DOLPHIN BEND | LANCE TERR | PENDANT PL | 725 | 28 | 20,300 | R - Residential/Local | A - AC | 93 | 33.51 |
| DOMIDR 10 | 10 | DOMINIC DR | SKYWAY | MORROW LN | 550 | 40 | 22,000 | C - Collector | A - AC | 25 | 0.02 |
| DONNLN 10 | 10 | DONNER LN | CALIFORNIA PARK DR | CUL-DE-SAC | 403 | 33 | 15,242 | C - Collector | A - AC | 35 | 2.05 |
| DORAWY 10 | 10 | DORAL WAY | ARCH WAY | END | 170 | 28 | 4,760 | R - Residential/Local | A - AC | 83 | 28.14 |
| DORIWAY 10 | 10 | DORIS WAY | PARK VIEW LN | OAK VISTA AVE | 395 | 35 | 13,825 | R - Residential/Local | A - AC | 9 | 0 |
| DORSCT 10 | 10 | DORSET CT | RAVENSHOE WY | END | 400 | 28 | 11,200 | R - Residential/Local | A - AC | 78 | 24.96 |
| DOUGLN 10 | 10 | DOUGLAS LN | FILBERT AVE | HAWTHORNE AVE | 435 | 32 | 13,920 | C - Collector | A - AC | 73 | 12.63 |
| DOCRCT 10 | 10 | DOVE CREEK CT | MANZANITA AVE | CUL-DE-SAC | 232 | 32 | 9,442 | R - Residential/Local | A - AC | 78 | 24.96 |
| DOWNAV 10 | 10 | DOWNING AVE | E. 1ST AVE | E. 5TH AVE | 1,388 | 36 | 49,968 | R - Residential/Local | A - AC | 73 | 21.81 |
| DOWNAV 20 | 20 | DOWNING AVE | E. 5TH AVE | E. LINDO AVE | 725 | 36 | 26,100 | R - Residential/Local | A - AC | 84 | 28.76 |
| DOWNAV 30A | 30A | DOWNING AVE | FILBERT AVE | SIERRA VISTA WAY | 335 | 30 | 10,050 | R - Residential/Local | A - AC | 5 | 0 |
| DOWNAV 30B | 30B | DOWNING AVE | SIERRA VISTA WAY | HILL VIEW WY | 360 | 30 | 10,800 | R - Residential/Local | A - AC | 93 | 33.27 |
| DOWNAV 40 | 40 | DOWNING AVE | PALMETTO AVE | E. 1ST AVE | 996 | 36 | 35,856 | C - Collector | A - AC | 38 | 2.49 |
| DMLKPW 10 | 10 | DR MARTIN LUTHER KING JR PKWY | E. PARK AVE | SILVER DOLLAR WAY | 2,300 | 42 | 96,600 | C - Collector | A - AC | 41 | 3.24 |
| DMLKPW 20 | 20 | DR MARTIN LUTHER KING JR PKWY | SILVER DOLLAR WAY | E. 20TH ST | 1,965 | 54 | 106,110 | C - Collector | A - AC | 59 | 7.93 |
| DMLKPW 30 | 30 | DR MARTIN LUTHER KING JR PKWY | E. 20TH ST | END | 875 | 39 | 34,125 | R - Residential/Local | A - AC | 52 | 10.45 |
| DUFFDR 10 | 10 | DUFFY DR | RUE FRANCAIS | KONA DR | 786 | 32 | 25,152 | R - Residential/Local | A - AC | 74 | 22.43 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-------------|------------------------|---------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| DURAWY | 10 | DURANGO WAY | DIAS DR | CARLOS PL | 290 | 36 | 10,440 | R - Residential/Local | A - AC | 71 | 20.56 |
| E10TAV | 10 | E 10TH AVE | ESPLANADE DR | LABURNUM AVE | 1,008 | 20 | 20,160 | R - Residential/Local | A - AC | 91 | 32.46 |
| E10TAV | 20 | E 10TH AVE | LABURNUM AVE | E LINDO AVE | 1,397 | 20 | 27,940 | R - Residential/Local | A - AC | 93 | 33.34 |
| E10TST | 10 | E 10TH ST | CLOVER ST | BARTLETT ST | 744 | 25 | 18,600 | R - Residential/Local | A - AC | 60 | 14.34 |
| E10TST | 20 | E 10TH ST | BARTLETT ST | WILLOW ST | 650 | 25 | 16,250 | R - Residential/Local | A - AC | 70 | 19.96 |
| E10TST | 30 | E 10TH ST | WILLOW ST | LINDEN ST | 640 | 25 | 16,000 | R - Residential/Local | A - AC | 72 | 21.17 |
| E11TST | 10 | E 11TH ST | PARK AVE | NELSON ST | 615 | 32 | 19,680 | R - Residential/Local | A - AC | 78 | 24.94 |
| E12TST | 10 | E 12TH ST | MULBERRY ST | BOUCHER ST | 1,524 | 38 | 57,912 | R - Residential/Local | A - AC | 41 | 5.59 |
| E12TST | 20 | E 12TH ST | PARK AVE | MULBERRY ST | 1,109 | 44 | 48,796 | R - Residential/Local | A - AC | 82 | 27.49 |
| E13TST | 10 | E 13TH ST | PARK AVE | LOCUST ST | 294 | 46 | 13,524 | R - Residential/Local | A - AC | 82 | 27.49 |
| E14TST | 10 | E 14TH ST | MULBERRY ST | E 12TH ST | 578 | 28 | 16,184 | R - Residential/Local | A - AC | 27 | 0.77 |
| E14TST | 20 | E 14TH ST | PARK AVE | MULBERRY ST | 993 | 28 | 27,804 | R - Residential/Local | A - AC | 50 | 9.45 |
| E15TST | 10 | E 15TH ST | MULBERRY ST | PARK ST | 1,063 | 47 | 49,961 | R - Residential/Local | A - AC | 19 | 0 |
| E15TST | 20 | E 15TH ST | MULBERRY ST | BOUCHER ST | 1,198 | 47 | 56,306 | R - Residential/Local | A - AC | 27 | 0.77 |
| E16TST | 10 | E 16TH ST | 600' E/O D STREET | END S/O E 16TH STREET | 225 | 22 | 4,950 | R - Residential/Local | A - AC | 52 | 10.37 |
| E16TST | 20 | E 16TH ST | 75' W/O D STREET | 600' E/O D STREET | 675 | 22 | 14,850 | R - Residential/Local | A - AC | 64 | 16.5 |
| E16TST | 30 | E 16TH ST | BEECH ST | C.L. (75 FT W/O D ST) | 920 | 32 | 29,440 | R - Residential/Local | A - AC | 27 | 0.77 |
| E16TST | 40 | E 16TH ST | MULBERRY ST | BEECH ST | 1,444 | 28 | 40,432 | R - Residential/Local | A - AC | 44 | 6.94 |
| E16TST | 50 | E 16TH ST | N/O E 16TH STREET | S/O CLEVELAND AVE | 440 | 22 | 9,680 | R - Residential/Local | A - AC | 71 | 20.56 |
| E16TST | 60 | E 16TH ST | PARK AVE | MULBERRY ST | 992 | 44 | 43,648 | R - Residential/Local | A - AC | 82 | 27.49 |
| E17TST | 10 | E 17TH ST | PARK AVE | LAUREL ST | 1,187 | 28 | 33,236 | R - Residential/Local | A - AC | 56 | 12.3 |
| E18TST | 10 | E 18TH ST | BEECH ST | MULBERRY ST | 1,030 | 28 | 28,840 | R - Residential/Local | A - AC | 70 | 19.96 |
| E18TST | 20 | E 18TH ST | MULBERRY ST | PARK AVE | 1,082 | 28 | 30,296 | R - Residential/Local | A - AC | 65 | 17.06 |
| E19TST | 10 | E 19TH ST | LAUREL ST | C ST | 888 | 22 | 19,536 | R - Residential/Local | A - AC | 21 | 0 |
| E19TST | 20 | E 19TH ST | PARK AVE | MULBERRY ST | 1,004 | 34 | 34,136 | R - Residential/Local | A - AC | 48 | 8.66 |
| E19TST | 30 | E 19TH ST | C ST | D ST | 455 | 16 | 7,280 | R - Residential/Local | A - AC | 97 | 34.12 |
| E1STAV | 10 | E 1ST AVE | 40 FT W/O SARAH AVE | LONGFELLOW AVE | 2,422 | 37 | 89,614 | R - Residential/Local | A - AC | 65 | 17.08 |
| E1STAV | 20 | E 1ST AVE | 80 FT E/O SHERIDAN AVE | 40 FT W/O SARAH AVE | 460 | 50 | 23,000 | R - Residential/Local | A - AC | 94 | 33.59 |
| E1STAV | 30 | E 1ST AVE | ESPLANADE DR | PVMT CHING (252FT W/O MANGROVE) | 2,217 | 37 | 82,029 | C - Collector | A - AC | 77 | 14.28 |
| E1STAV | 40 | E 1ST AVE | LONGFELLOW AVE | VERBENA LN | 1,780 | 36 | 64,080 | C - Collector | A - AC | 32 | 1.42 |
| E1STAV | 50A | E 1ST AVE | MANGROVE AVE | HOLBEN AVE | 1,686 | 37 | 62,382 | R - Residential/Local | A - AC | 82 | 27.49 |

Section PCI/RSL Listing

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-------------|--------------------------------|--------------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| E1STAV | 50B | E 1ST AVE | HOLBEN AVE | 80 FT E/O SHERIDAN AVE | 414 | 37 | 15,318 | R - Residential/Local | A - AC | 94 | 33.59 |
| E1STAV | 60 | E 1ST AVE | PVMT CHNG (106 FT W/O SPANISH) | MANDRONE AVE | 566 | 36 | 20,376 | C - Collector | A - AC | 23 | 0 |
| E1STAV | 70 | E 1ST AVE | PVMT CHNG(252 FT W/O MANGROVE) | MANGROVE AVE | 252 | 48 | 12,096 | A - Arterial | A - AC | 82 | 20.19 |
| E1STAV | 80 | E 1ST AVE | VERBENA LN | PVMT CHNG (106 FT W/O SPANISH) | 902 | 36 | 32,472 | C - Collector | A - AC | 54 | 6.53 |
| E1STST | 10 | E 1ST ST | E. 2ND ST | ORIENT ST | 223 | 38 | 8,474 | R - Residential/Local | A - AC | 93 | 33.34 |
| E1STST | 20 | E 1ST ST | MAIN ST | E. 2ND ST | 750 | 38 | 28,500 | C - Collector | A - AC | 94 | 22.66 |
| E20TST | 10 | E 20TH ST | BRUCE RD | POTTER RD | 2,200 | 40 | 88,000 | C - Collector | A - AC | 82 | 16.61 |
| E20TST | 100 | E 20TH ST | WHITMAN AVE W/S | HWY 99 OVERPASS | 927 | 70 | 64,890 | C - Collector | A - AC | 62 | 8.84 |
| E20TST | 110 | E 20TH ST | DAWNCREST DR | POTTER RD | 1,460 | 30 | 43,800 | C - Collector | A - AC | 92 | 21.75 |
| E20TST | 20 | E 20TH ST | FOREST AVE W/S | NOTRE DAME BLVD W/S | 1,500 | 60 | 90,000 | A - Arterial | A - AC | 61 | 10.93 |
| E20TST | 30 | E 20TH ST | FRANKLIN ST W/S | WHITMAN AVE W/S | 1,632 | 65 | 106,080 | C - Collector | A - AC | 57 | 7.41 |
| E20TST | 40 | E 20TH ST | HWY 99 OVERPASS E/S | MALL ENTR(124 FT E/O BUSINESS) | 1,022 | 78 | 79,716 | R - Residential/Local | A - AC | 58 | 13.33 |
| E20TST | 50 | E 20TH ST | HWY 99 OVERPASS W/S | HWY 99 OVERPASS E/S | 200 | 70 | 14,000 | A - Arterial | A - AC | 79 | 18.8 |
| E20TST | 60 | E 20TH ST | MALL ENTR(124 FT E/O BUSINESS) | FOREST AVE W/S | 1,106 | 70 | 77,420 | A - Arterial | A - AC | 66 | 13.17 |
| E20TST | 70 | E 20TH ST | MULBERRY ST W/S | FRANKLIN ST W/S | 1,338 | 65 | 86,970 | R - Residential/Local | A - AC | 52 | 10.61 |
| E20TST | 80 | E 20TH ST | NOTRE DAME BLVD W/S | BRUCE RD | 2,570 | 41 | 105,370 | C - Collector | A - AC | 72 | 12.23 |
| E20TST | 90 | E 20TH ST | PARK AVE | MULBERRY ST W/S | 988 | 65 | 64,220 | R - Residential/Local | A - AC | 83 | 28.33 |
| E21SST | 10 | E 21ST ST | FAIR ST | ELM ST | 717 | 28 | 20,076 | R - Residential/Local | A - AC | 45 | 7.44 |
| E22NST | 10 | E 22ND ST | FAIR ST | MULBERRY ST | 235 | 22 | 5,170 | R - Residential/Local | A - AC | 21 | 0 |
| E22NST | 30 | E 22ND ST | LAUREL ST | ELM ST | 312 | 17 | 5,304 | R - Residential/Local | A - AC | 74 | 22.41 |
| E23RST | 10 | E 23RD ST | FAIR ST | MULBERRY ST | 275 | 25 | 6,875 | R - Residential/Local | A - AC | 28 | 1.12 |
| E23RST | 20 | E 23RD ST | MULBERRY ST | ELM ST | 620 | 25 | 15,500 | R - Residential/Local | A - AC | 80 | 26.22 |
| E2NDAV | 10 | E 2ND AVE | ESPLANADE DR | OLEANDER AVE | 494 | 24 | 11,856 | R - Residential/Local | A - AC | 29 | 1.46 |
| E2NDAV | 20 | E 2ND AVE | OLEANDER AVE | LABURNUM AVE | 454 | 22 | 9,988 | C - Collector | A - AC | 22 | 0 |
| E2NDAV | 30 | E 2ND AVE | LABURNUM AVE | SPRUCE AVE | 450 | 24 | 10,800 | C - Collector | A - AC | 37 | 2.47 |
| E2NDAV | 40 | E 2ND AVE | SPRUCE AVE | PALM AVE | 468 | 24 | 11,232 | C - Collector | A - AC | 52 | 5.95 |
| E2NDST | 10 | E 2ND ST | MAIN ST | E. 1ST ST | 769 | 38 | 29,222 | R - Residential/Local | A - AC | 82 | 27.49 |
| E3RDVAV | 10 | E 3RD AVE | ESPLANADE DR | LABURNUM AVE | 964 | 28 | 26,992 | R - Residential/Local | A - AC | 42 | 6 |
| E3RDVAV | 20 | E 3RD AVE | LABURNUM AVE | MANGROVE AVE | 1,498 | 26 | 38,948 | C - Collector | A - AC | 54 | 6.51 |

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-------------|--------------------|----------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| E3RDAV | 30 | E 3RD AVE | MANGROVE AVE | SHERMAN AVE | 1,034 | 34 | 35,156 | R - Residential/Local | A - AC | 82 | 27.49 |
| E3RDST | 10 | E 3RD ST | PINE ST | MAIN ST | 1,725 | 38 | 65,550 | R - Residential/Local | A - AC | 81 | 26.86 |
| E4THAV | 10 | E 4TH AVE | ESPLANADE | OLEANDER AVE | 510 | 26 | 13,260 | R - Residential/Local | A - AC | 34 | 3.27 |
| E4THAV | 20 | E 4TH AVE | OLEANDER AVE | LABURNUM AVE | 414 | 26 | 10,764 | R - Residential/Local | A - AC | 18 | 0 |
| E4THAV | 30 | E 4TH AVE | LABURNUM AVE | SPRUCE AVE | 495 | 26 | 12,870 | R - Residential/Local | A - AC | 71 | 11.82 |
| E4THAV | 40 | E 4TH AVE | SPRUCE AVE | PALM AVE | 472 | 26 | 12,272 | C - Collector | A - AC | 80 | 15.64 |
| E4THST | 10 | E 4TH ST | MAIN ST | ORIENT ST | 981 | 38 | 37,278 | R - Residential/Local | A - AC | 80 | 26.22 |
| E4THST | 20 | E 4TH ST | ORIENT ST | WOODLAND AVE | 1,008 | 38 | 38,304 | R - Residential/Local | A - AC | 82 | 27.49 |
| E5THAV | 10 | E 5TH AVE | 155FT E/ARBUTUS AV | SHERIDAN AV | 898 | 36 | 32,328 | A - Arterial | A - AC | 83 | 20.66 |
| E5THAV | 20 | E 5TH AVE | ESPLANADE DR | LABURNUM AVE | 979 | 40 | 39,160 | R - Residential/Local | A - AC | 94 | 33.59 |
| E5THAV | 30 | E 5TH AVE | LABURNUM AVE | MANGROVE AVE | 1,470 | 26 | 38,220 | C - Collector | A - AC | 96 | 23.7 |
| E5THAV | 40 | E 5TH AVE | MANCHESTER RD | E. LINDO AVE | 942 | 35 | 32,970 | R - Residential/Local | A - AC | 36 | 3.66 |
| E5THAV | 50 | E 5TH AVE | MANGROVE AVE | C.L. (155 FT E/O ALBUTRUS) | 980 | 32 | 31,360 | C - Collector | A - AC | 80 | 15.64 |
| E5THAV | 60 | E 5TH AVE | SHERIDAN AVE | MANCHESTER RD | 1,708 | 35 | 59,780 | R - Residential/Local | A - AC | 50 | 9.47 |
| E5THST | 10 | E 5TH ST | MAIN ST | ORIENT ST | 1,058 | 36 | 38,088 | R - Residential/Local | A - AC | 77 | 24.3 |
| E5THST | 20 | E 5TH ST | ORIENT ST | PINE ST | 610 | 36 | 21,960 | R - Residential/Local | A - AC | 79 | 25.58 |
| E5THST | 30 | E 5TH ST | PINE ST | WOODLAND AVE | 900 | 36 | 32,400 | R - Residential/Local | P - PCC | 81 | 55.98 |
| E6THAV | 10 | E 6TH AVE | ESPLANADE DR | LABURNUM AVE | 924 | 34 | 31,416 | R - Residential/Local | A - AC | 57 | 12.8 |
| E6THAV | 20 | E 6TH AVE | LABURNAM | PALM | 998 | 22 | 21,956 | C - Collector | A - AC | 57 | 7.39 |
| E6THST | 10 | E 6TH ST | FLUME ST | PINE ST | 977 | 36 | 35,172 | R - Residential/Local | A - AC | 42 | 6 |
| E6THST | 20 | E 6TH ST | MAIN ST | FLUME ST | 589 | 36 | 21,204 | R - Residential/Local | A - AC | 43 | 6.41 |
| E6THST | 30 | E 6TH ST | PINE ST | POPLAR ST | 660 | 36 | 23,760 | R - Residential/Local | P - PCC | 65 | 31.55 |
| E6THST | 40 | E 6TH ST | POPLAR ST | SYCAMORE ST | 660 | 36 | 23,760 | R - Residential/Local | A - AC | 74 | 22.41 |
| E7THAV | 10 | E 7TH AVE | ESPLANADE DR | LABURNUM AVE | 1,014 | 34 | 34,476 | R - Residential/Local | A - AC | 32 | 2.53 |
| E7THAV | 20 | E 7TH AVE | LABURNUM AVE | PALM AVE | 980 | 34 | 33,320 | R - Residential/Local | A - AC | 91 | 32.46 |
| E7THAV | 30 | E 7TH AVE | PALM AVE | MANGROVE AVE | 494 | 36 | 17,784 | C - Collector | A - AC | 8 | 0 |
| E7THAV | 40 | E 7TH AVE | MANGROVE AVE | SHERMAN AVE | 1,082 | 30 | 32,460 | R - Residential/Local | A - AC | 18 | 0 |
| E7THST | 10 | E 7TH ST | LINDEN ST | WOODLAND AVE | 1,025 | 36 | 36,900 | R - Residential/Local | A - AC | 81 | 26.86 |
| E7THST | 20 | E 7TH ST | MAIN ST | ORIENT ST | 986 | 36 | 35,496 | R - Residential/Local | A - AC | 12 | 0 |
| E7THST | 30 | E 7TH ST | ORIENT ST | PINE ST | 657 | 36 | 23,652 | R - Residential/Local | A - AC | 71 | 20.56 |
| E7THST | 40 | E 7TH ST | PINE ST | POPLAR ST | 660 | 36 | 23,760 | R - Residential/Local | P - PCC | 36 | 6.79 |
| E7THST | 50 | E 7TH ST | POPLAR ST | LINDEN ST | 1,251 | 36 | 45,036 | R - Residential/Local | A - AC | 78 | 24.94 |
| E8THAV | 10 | E 8TH AVE | 225 FT E/ESPLANADE | LABURNAM | 737 | 22 | 16,214 | C - Collector | A - AC | 82 | 16.59 |

Criteria:

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SS1030

MTC StreetSaver

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|--------------------------|------------------------------|-----------------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| E8THAV | 20 | E 8TH AVE | ESPLANADE DR | C.L. (225 FT E/O ESPLANADE) | 225 | 30 | 6,750 | A - Arterial | A - AC | 82 | 20.19 |
| E8THAV | 30 | E 8TH AVE | LABURNAM | PALM | 995 | 22 | 21,890 | C - Collector | A - AC | 82 | 16.59 |
| E8THST | 10 | E 8TH ST | ALPINE ST | GLENN ST | 750 | 18 | 13,500 | C - Collector | A - AC | 94 | 22.68 |
| E8THST | 20 | E 8TH ST | EL MONTE AVE | PARKWOOD DR | 1,305 | 18 | 23,490 | C - Collector | A - AC | 95 | 23.09 |
| E8THST | 30 | E 8TH ST | FOREST AVE | EL MONTE AVE | 1,469 | 18 | 26,442 | C - Collector | A - AC | 94 | 22.68 |
| E8THST | 40 | E 8TH ST | GLENN ST | FOREST AVE | 1,988 | 18 | 35,784 | C - Collector | A - AC | 95 | 23.09 |
| E8THST | 50 | E 8TH ST | HWY 32 | ALPINE ST | 1,950 | 28 | 54,600 | C - Collector | A - AC | 94 | 22.68 |
| E8THST | 60 | E 8TH ST | PARKWOOD DR | BRUCE RD | 944 | 28 | 26,432 | C - Collector | A - AC | 54 | 6.53 |
| E9THAV | 10 | E 9TH AVE | ESPLANADE DR | OLEANDER AVE | 510 | 36 | 18,360 | R - Residential/Local | A - AC | 34 | 3.27 |
| E9THAV | 40 | E 9TH AVE | MANGROVE AVE | E. LINDO AVE | 225 | 36 | 8,100 | R - Residential/Local | A - AC | 65 | 17.06 |
| EEATRD | 10 | E EATON RD | BURNAP AV | COHASSET | 2,616 | 36 | 94,176 | C - Collector | A - AC | 59 | 8.03 |
| EEATRD | 100 | E EATON RD | LUPIN AVE | FLORAL AVE | 705 | 62 | 43,710 | A - Arterial | A - AC | 74 | 16.47 |
| EEATRD | 110 | E EATON RD | MORSEMAN AV | BURNAP AV | 1,455 | 36 | 52,380 | C - Collector | A - AC | 40 | 2.95 |
| EEATRD | 30 | E EATON RD | CERES AVE | LASSEN AVE | 1,351 | 52 | 70,252 | C - Collector | A - AC | 39 | 2.72 |
| EEATRD | 40 | E EATON RD | COHASSET RD | CERES AVE | 1,929 | 36 | 69,444 | A - Arterial | A - AC | 69 | 14.24 |
| EEATRD | 50 | E EATON RD | ESPLANADE DR | HWY 99 RAMP W/S | 843 | 55 | 46,365 | R - Residential/Local | A - AC | 32 | 2.53 |
| EEATRD | 60 | E EATON RD | GODMAN AV | MORSEMAN AVE | 1,670 | 36 | 60,120 | A - Arterial | A - AC | 33 | 2.05 |
| EEATRD | 70A | E EATON RD | HWY 99 RAMP E SIDE | 370 FT FROM HWY 99 RAMP E SIDE | 370 | 36 | 13,320 | A - Arterial | A - AC | 82 | 20.34 |
| EEATRD | 70B | E EATON RD | 370 FT FROM HWY 99 RAMP E | GODMAN AVE | 1,701 | 36 | 61,236 | A - Arterial | A - AC | 38 | 3.31 |
| EEATRD | 90 | E EATON RD | LASSEN AVE | LUPIN AVE | 1,831 | 62 | 113,522 | C - Collector | A - AC | 26 | 0.21 |
| EFWIAV | 10 | E FRANCES WILLARD AVE | ESPLANADE DR | OLEANDER AVE | 453 | 36 | 16,308 | R - Residential/Local | A - AC | 82 | 27.49 |
| ELASAV | 100 | E LASSEN AVE | RAINIER LN | FLORAL AVE | 821 | 60 | 49,260 | C - Collector | A - AC | 66 | 10.36 |
| ELASAV | 20 | E LASSEN AVE | BURNAP AVE E/S | COHASSET RD | 1,710 | 37 | 63,270 | C - Collector | A - AC | 86 | 18.61 |
| ELASAV | 30 | E LASSEN AVE | CERES AVE | RAINIER LN | 1,529 | 50 | 76,450 | C - Collector | A - AC | 56 | 7.11 |
| ELASAV | 40 | E LASSEN AVE | HWY 99 OVERPASS | EL PASO WY W/S | 1,750 | 37 | 64,750 | C - Collector | A - AC | 86 | 18.61 |
| ELASAV | 50 | E LASSEN AVE | COHASSET RD | CERES AVE | 1,535 | 50 | 76,750 | C - Collector | A - AC | 60 | 8.34 |
| ELASAV | 60 | E LASSEN AVE | EL PASO WY W/S | BURNAP AVE E/S | 2,362 | 37 | 87,394 | C - Collector | A - AC | 86 | 18.61 |
| ELASAV | 70 | E LASSEN AVE | ESPLANADE | MEFFORD WY | 840 | 30 | 25,200 | C - Collector | A - AC | 46 | 4.37 |
| ELASAV | 90 | E LASSEN AVE | MEFFORD WY | 99 OVERPASS | 890 | 30 | 26,700 | C - Collector | A - AC | 30 | 0.96 |
| ELICAV | 10 | E LINCOLN AVE | ESPLANADE DR | OLEANDER AVE | 462 | 36 | 16,632 | R - Residential/Local | A - AC | 31 | 2 |

Section PCI/RSL Listing

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|------------------|---------------------------|-----------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| ELINAV | 10 | E LINDO AVE | C.L. (145 FT E/O ARBUTUS) | W. END | 675 | 25 | 16,875 | R - Residential/Local | A - AC | 10 | 0 |
| ELINAV | 20 | E LINDO AVE | E. 5TH AVE | LONGFELLOW AVE | 1,243 | 32 | 39,776 | R - Residential/Local | A - AC | 43 | 6.43 |
| ELINAV | 30 | E LINDO AVE | FLORENCE LN | CUL-DE-SAC | 2,062 | 25 | 51,550 | R - Residential/Local | A - AC | 18 | 0 |
| ELINAV | 40 | E LINDO AVE | FLORENCE LN | E. 5TH AVE | 652 | 25 | 16,300 | R - Residential/Local | A - AC | 21 | 0 |
| ELINAV | 50 | E LINDO AVE | LABURNUM AVE | PALM AVE | 1,110 | 18 | 19,980 | R - Residential/Local | A - AC | 71 | 20.5 |
| ELINAV | 60 | E LINDO AVE | PALM AVE | MANGROVE AVE | 1,020 | 22 | 22,440 | R - Residential/Local | A - AC | 21 | 0 |
| EPARAV | 10 | E PARK AVE | 216 FT E/O COUNTRY DR | HWY 99 PCC OVERPASS | 900 | 36 | 32,400 | R - Residential/Local | A - AC | 82 | 27.7 |
| EPARAV | 20 | E PARK AVE | 75 FT W/O COUNTRY DR | 216 FT E/O COUNTRY DR | 355 | 74 | 26,270 | R - Residential/Local | A - AC | 80 | 26.42 |
| EPARAV | 30 | E PARK AVE | PARK AVE E/S | 75 FT W/O COUNTRY DR | 2,385 | 62 | 147,870 | C - Collector | A - AC | 61 | 8.52 |
| ESACAV | 10 | E SACRAMENTO AVE | ESPLANADE DR | OLEANDER AVE | 493 | 33 | 16,269 | C - Collector | A - AC | 36 | 2.25 |
| ESACAV | 20 | E SACRAMENTO AVE | OLEANDER AVE | LABURNUM AVE | 443 | 33 | 14,619 | C - Collector | A - AC | 36 | 2.25 |
| ESACAV | 30 | E SACRAMENTO AVE | LABURNUM AVE | PALM AVE | 1,007 | 33 | 33,231 | C - Collector | A - AC | 25 | 0 |
| ESHAAV | 10 | E SHASTA AVE | ESPLANADE | MONTICELLO LN | 800 | 36 | 28,800 | R - Residential/Local | A - AC | 20 | 0 |
| EWASAV | 10 | E WASHINGTON AVE | ESPLANADE DR | OLEANDER AVE | 506 | 32 | 16,192 | R - Residential/Local | A - AC | 20 | 0 |
| EWASAV | 20 | E WASHINGTON AVE | OLEANDER AVE | CUL-DE-SAC | 803 | 32 | 27,714 | R - Residential/Local | A - AC | 25 | 0.1 |
| EALACT | 10 | EAGLE LAKE CT | GREENFIELD DR | SOUTH END | 390 | 36 | 14,040 | R - Residential/Local | A - AC | 62 | 15.41 |
| EALACT | 20 | EAGLE LAKE CT | GREENFIELD DR | NORTH END | 370 | 36 | 13,320 | R - Residential/Local | A - AC | 82 | 27.49 |
| EARLAV | 50 | EARL AVE | E. 8TH ST | PARK VISTA DR | 847 | 38 | 32,186 | C - Collector | A - AC | 17 | 0 |
| EASTAV | 10 | EAST AVE | CACTUS AVE | MANZANITA AVE | 1,066 | 28 | 29,848 | C - Collector | A - AC | 89 | 20.18 |
| EASTAV | 100 | EAST AVE | MARIGOLD AVE | CACTUS AVE | 1,275 | 28 | 35,700 | C - Collector | A - AC | 87 | 19.13 |
| EASTAV | 110 | EAST AVE | MARIPOSA AVE | CEANOTHUS AVE | 1,278 | 60 | 76,680 | C - Collector | A - AC | 89 | 20.18 |
| EASTAV | 120 | EAST AVE | NORTH AVE | CERES AVE | 1,268 | 60 | 76,080 | C - Collector | A - AC | 87 | 19.13 |
| EASTAV | 20 | EAST AVE | CEANOTHUS AVE | MARIGOLD AVE | 1,324 | 44 | 58,256 | C - Collector | A - AC | 87 | 19.13 |
| EASTAV | 30 | EAST AVE | CERES AVE | FLORAL AVE | 1,275 | 60 | 76,500 | R - Residential/Local | A - AC | 89 | 31.59 |
| EASTAV | 40 | EAST AVE | COHASSET RD | NORTH AVE | 1,271 | 60 | 76,260 | C - Collector | A - AC | 87 | 19.13 |
| EASTAV | 50 | EAST AVE | CONNORS AVE E/S | HWY 99 NB ON-RAMP E/S | 733 | 60 | 43,980 | C - Collector | A - AC | 48 | 4.88 |
| EASTAV | 60 | EAST AVE | EL PASO WY | COHASSET RD | 2,524 | 60 | 151,440 | R - Residential/Local | A - AC | 37 | 4.04 |
| EASTAV | 70 | EAST AVE | ESPLANADE DR E/S | CONNORS AVE E/S | 1,500 | 60 | 90,000 | C - Collector | A - AC | 82 | 16.59 |
| EASTAV | 80 | EAST AVE | FLORAL AVE | MARIPOSA AVE | 1,272 | 60 | 76,320 | R - Residential/Local | A - AC | 89 | 31.59 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|------------------|--------------------------------|--------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| EASTAV | 90 | EAST AVE | HWY 99 NB ON-RAMP E/S | EL PASO WY | 966 | 60 | 57,960 | C - Collector | A - AC | 28 | 0.6 |
| EAWDAV | 10 | EASTWOOD AVE | PINE ST | POPLAR ST | 612 | 25 | 15,300 | R - Residential/Local | A - AC | 20 | 0 |
| EATORD | 10 | EATON RD | WEST CITY LIMITS | ROGUE RIVER DR | 645 | 32 | 20,640 | C - Collector | A - AC | 82 | 16.59 |
| EATORD | 170 | EATON RD | FLORAL AVE | MARIPOSA AVE | 1,330 | 26 | 34,580 | C - Collector | A - AC | 96 | 23.72 |
| EATORD | 180 | EATON RD | MARIPOSA AVE | GRASSHOPPER CREEK WAY | 1,175 | 41 | 48,175 | C - Collector | A - AC | 96 | 23.72 |
| EATORD | 190 | EATON RD | GRASSHOPPER CREEK WAY | CEANOOTHUS AVE | 185 | 50 | 9,250 | C - Collector | A - AC | 96 | 23.72 |
| EATORD | 20 | EATON RD | ROGUE RIVER DR | PENZANCE AVE | 1,850 | 32 | 59,200 | C - Collector | A - AC | 82 | 16.59 |
| EATORD | 200 | EATON RD | CEANOOTHUS AVE | MARIGOLD AVE | 1,295 | 44 | 56,980 | C - Collector | A - AC | 83 | 17.1 |
| EATORD | 210 | EATON RD | MARIGOLD AVE | ST LAWRENCE AVE | 1,050 | 44 | 46,200 | C - Collector | A - AC | 83 | 17.1 |
| EATORD | 30 | EATON RD | PENZANCE AVE | AURORA GLEN DR | 1,375 | 32 | 44,000 | C - Collector | A - AC | 82 | 16.59 |
| EATORD | 40 | EATON RD | AURORA GLEN DR | ESPLANADE | 1,510 | 54 | 81,540 | C - Collector | A - AC | 60 | 8.21 |
| EATORD | 60 | EATON RD | PENDANT PL | MANZANITA AVE | 1,110 | 32 | 35,520 | C - Collector | A - AC | 98 | 24.15 |
| EAVDR | 10 | EATON VILLAGE DR | LEGACY LN | W. EATON RD | 546 | 36 | 19,656 | R - Residential/Local | A - AC | 82 | 27.49 |
| ECHOLN | 10 | ECHO LN | COMSTOCK RD | CUL-DE-SAC | 240 | 37 | 10,523 | R - Residential/Local | A - AC | 77 | 24.3 |
| EGRELN | 10 | EGRET LN | HIDEAWAY PARK | W 12TH AVE | 320 | 20 | 6,400 | R - Residential/Local | A - AC | 82 | 27.49 |
| ELCEDR | 10 | EL CERRITO DR | EL VARANO WAY | EL PASO WAY | 1,620 | 36 | 58,320 | R - Residential/Local | A - AC | 18 | 0 |
| ELCOCR | 10 | EL CORTEZ CIR | WHITE AVE | END | 235 | 32 | 7,520 | R - Residential/Local | A - AC | 21 | 0 |
| ELDOST | 10 | EL DORADO ST | E. 8TH ST | MODOC DR | 988 | 35 | 34,580 | C - Collector | A - AC | 60 | 8.34 |
| ELOMCT | 10 | EL MIRASOL CT | EL VARANO WAY | END | 215 | 36 | 7,740 | R - Residential/Local | A - AC | 73 | 21.81 |
| ELMOAV | 10 | EL MONTE AVE | HUMBOLDT RD | HWY 32 | 430 | 32 | 13,760 | C - Collector | A - AC | 96 | 23.63 |
| ELMOAV | 20 | EL MONTE AVE | HWY 32 | DORADO CERRO | 870 | 32 | 27,840 | C - Collector | A - AC | 81 | 16.13 |
| ELMOAV | 40 | EL MONTE AVE | LA QUINTA ST | E 8TH ST | 1,140 | 32 | 36,480 | C - Collector | A - AC | 56 | 7.11 |
| ELPAWY | 10 | EL PASO WAY | C.L. (135 FT N/O EL VARANO) | E. LASSEN AVE | 934 | 36 | 33,624 | C - Collector | A - AC | 19 | 0 |
| ELPAWY | 20 | EL PASO WAY | C.L. (135 FT N/O EL VARANO) | EL CERRITO DR | 563 | 35 | 19,705 | C - Collector | A - AC | 96 | 23.63 |
| ELPAWY | 30 | EL PASO WAY | EAST AV | WHITE AV | 743 | 32 | 23,776 | C - Collector | A - AC | 16 | 0 |
| ELPAWY | 40 | EL PASO WAY | EL RENO | EAST AV | 275 | 35 | 9,625 | C - Collector | A - AC | 96 | 23.63 |
| ELPAWY | 50 | EL PASO WAY | ELCERRITO DR | EL RENO DR | 491 | 36 | 17,676 | C - Collector | A - AC | 96 | 23.63 |
| ELREDR | 10 | EL RENO DR | EL PASO WAY | WHITE AVE | 940 | 36 | 33,840 | R - Residential/Local | A - AC | 18 | 0 |
| ELVAWY | 10 | EL VARANO WAY | WHITE AVE | EL CERRITO DR | 840 | 36 | 30,240 | R - Residential/Local | A - AC | 22 | 0 |
| ELVAWY | 20 | EL VARANO WAY | EL CERRITO DR | EL PASO WAY | 290 | 36 | 10,440 | R - Residential/Local | A - AC | 23 | 0 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|------------------------|------------------------|--------|-------|---------|--------------------------|--------------|-------------|----------------|
| ELVAWY | 30 | EL VARANO WAY | EL PASO WAY | END | 530 | 36 | 19,080 | R - Residential/Local | A - AC | 75 | 23.06 |
| ELDCT | 10 | ELDERBERRY CT | CALIFORNIA PARK DR | PLUMWOOD CT | 185 | 33 | 6,105 | R - Residential/Local | A - AC | 75 | 23.06 |
| ELISCT | 10 | ELISHA CT | COHASSET RD | END | 515 | 30 | 15,450 | R - Residential/Local | A - AC | 94 | 33.6 |
| ELLEAV | 10 | ELLENE AVE | 450' E/O MANZANITA AVE | NORTH AVE | 850 | 36 | 30,600 | R - Residential/Local | A - AC | 9 | 0 |
| ELLEAV | 20 | ELLENE AVE | MANZANITA AVE | 450' E/O MANZANITA AVE | 450 | 34 | 15,300 | R - Residential/Local | A - AC | 23 | 0 |
| ELMST | 10 | ELM ST | E. 12TH ST | E. 16TH ST | 601 | 28 | 16,828 | R - Residential/Local | A - AC | 19 | 0 |
| ELMST | 20 | ELM ST | E. 16TH ST | E. 20TH ST | 1,045 | 36 | 37,620 | R - Residential/Local | A - AC | 24 | 0 |
| ELMST | 30 | ELM ST | E. 21ST ST | E. 23RD ST | 1,224 | 22 | 26,928 | R - Residential/Local | A - AC | 26 | 0.43 |
| ELMEST | 10 | ELMER ST | POMONA | N/END | 684 | 22 | 15,048 | R - Residential/Local | A - AC | 26 | 0.43 |
| ELVECR | 10 | ELVERTA CIR | MADERA LN | END | 150 | 36 | 5,400 | R - Residential/Local | A - AC | 20 | 0 |
| EMLACT | 10 | EMERALD LAKE CT | AMBER GROVE DR | SOUTH CUL-DE-SAC | 286 | 36 | 10,296 | R - Residential/Local | A - AC | 80 | 26.22 |
| EMLACT | 20 | EMERALD LAKE CT | AMBER GROVE DR | NORTH CUL-DE-SAC | 286 | 36 | 10,296 | R - Residential/Local | A - AC | 80 | 26.22 |
| EMERWY | 10 | EMERSON WAY | HARTFORD DR | NOTRE DAME BLVD | 420 | 18 | 7,560 | R - Residential/Local | A - AC | 94 | 33.6 |
| EMILWY | 10 | EMILIO WAY | PANAMA AVE | END | 385 | 32 | 12,320 | R - Residential/Local | A - AC | 72 | 21.19 |
| EMPECT | 10 | EMPEROR CT | ADMIRAL LN | END | 165 | 28 | 4,620 | R - Residential/Local | A - AC | 76 | 23.69 |
| ENGLST | 10 | ENGLAND ST | ZACHARY CT | END | 619 | 22 | 13,618 | C - Collector | A - AC | 82 | 16.61 |
| ENGLST | 20 | ENGLAND ST | MATSON ST | ZACHARY CT | 925 | 24 | 22,200 | R - Residential/Local | A - AC | 75 | 22.95 |
| ERMACT | 10 | ERMA CT | COMMERCE CT | END | 400 | 28 | 11,200 | R - Residential/Local | A - AC | 79 | 25.48 |
| ESCAWY | 10 | ESCALLONIA WAY | ARCH WY | KONA DR | 933 | 36 | 33,588 | R - Residential/Local | A - AC | 64 | 16.52 |
| ESPLAN | 10 | ESPLANADE | 514 FT S/O MEMORIAL WY | W. LINCOLN AVE N/S | 1,368 | 25 | 34,200 | R - Residential/Local | A - AC | 79 | 25.58 |
| ESPLAN | 100 | ESPLANADE | LEORA CT | THREE SEVENS LN | 2,430 | 32 | 77,760 | MiA - Minor Arterial (4) | A - AC | 35 | 2.59 |
| ESPLAN | 110 | ESPLANADE | MISSION RANCH BLVD N/S | EAST AVE N/S | 1,270 | 60 | 76,200 | R - Residential/Local | A - AC | 74 | 22.41 |
| ESPLAN | 120 | ESPLANADE | N/O INNSBROOK | LEORA CT | 1,900 | 60 | 114,000 | MiA - Minor Arterial (4) | A - AC | 54 | 8.65 |
| ESPLAN | 140 | ESPLANADE | W. 11TH AVE N/S | COHASSET RD S/S | 950 | 54 | 51,300 | R - Residential/Local | A - AC | 71 | 20.56 |
| ESPLAN | 150 | ESPLANADE | W. 11TH AVE N/S | W. 7TH AVE N/S | 1,775 | 25 | 44,375 | R - Residential/Local | A - AC | 75 | 23.03 |
| ESPLAN | 160 | ESPLANADE | W. 3RD AVE N/S | W. LINCOLN AVE N/S | 1,970 | 25 | 49,250 | C - Collector | A - AC | 81 | 16.11 |
| ESPLAN | 170 | ESPLANADE | W. 3RD AVE N/S | W. 7TH AVE N/S | 1,775 | 25 | 44,375 | R - Residential/Local | A - AC | 81 | 26.86 |
| ESPLAN | 180 | ESPLANADE | W. 7TH AVE N/S | W. 11TH AVE N/S | 1,775 | 25 | 44,375 | R - Residential/Local | A - AC | 84 | 28.86 |
| ESPLAN | 190 | ESPLANADE | W. 7TH AVE N/S | W. 3RD AVE N/S | 1,775 | 25 | 44,375 | R - Residential/Local | A - AC | 79 | 25.58 |
| ESPLAN | 20 | ESPLANADE | AMBER GROVE DR N/S | GREENFIELD DR N/S | 925 | 60 | 55,500 | R - Residential/Local | A - AC | 63 | 15.95 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-------------|------------------------------------|----------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| ESPLAN | 200 | ESPLANADE | W. LINCOLN AVE N/S | 514 FT S/O MEMORIAL WY | 1,365 | 25 | 34,125 | R - Residential/Local | A - AC | 76 | 23.66 |
| ESPLAN | 210 | ESPLANADE | W. LINCOLN AVE N/S | E. 3RD AVE N/S | 1,970 | 25 | 49,250 | C - Collector | A - AC | 79 | 15.18 |
| ESPLAN | 220 | ESPLANADE | YELLOWSTONE DR N/S | EATON RD N/S | 950 | 60 | 57,000 | R - Residential/Local | A - AC | 71 | 20.56 |
| ESPLAN | 30 | ESPLANADE | CITY LIMITS (485 FT N/EATON) | INNSBROOK WY | 1,209 | 36 | 43,524 | C - Collector | A - AC | 45 | 4.12 |
| ESPLAN | 40 | ESPLANADE | COHASSET RD S/S | MISSION RANCH BLVD N/S | 1,584 | 60 | 95,040 | C - Collector | A - AC | 18 | 0 |
| ESPLAN | 50 | ESPLANADE | E. LASSEN AVE N/S | E. SHASTA AVE N/S | 1,325 | 60 | 79,500 | C - Collector | A - AC | 57 | 7.39 |
| ESPLAN | 60 | ESPLANADE | E. SHASTA AVE N/S | AMBER GROVE DR N/S | 1,075 | 60 | 64,500 | C - Collector | A - AC | 65 | 9.98 |
| ESPLAN | 70 | ESPLANADE | EAST AVE N/S | E. LASSEN AVE N/S | 1,975 | 60 | 118,500 | C - Collector | A - AC | 65 | 9.98 |
| ESPLAN | 80 | ESPLANADE | EATON RD N/S | C.L. (485 FT N/O EATON) | 485 | 60 | 29,100 | A - Arterial | A - AC | 40 | 4.04 |
| ESPLAN | 90 | ESPLANADE | GREENFIELD DR N/S | YELLOWSTONE DR N/S | 950 | 60 | 57,000 | R - Residential/Local | A - AC | 71 | 20.56 |
| ESFRNB | 10 | ESPLANADE | FRNT NB 62 FT S/O MEMORIAL WY | E. FRANCES WILLARD AVE S/S | 705 | 19 | 13,395 | A - Arterial | A - AC | 41 | 4.34 |
| ESFRNB | 20 | ESPLANADE | FRNT NB E. 1ST AVE N/S | E. 4TH AVE S/S | 1,200 | 19 | 21,780 | R - Residential/Local | A - AC | 58 | 13.31 |
| ESFRNB | 30 | ESPLANADE | FRNT NB E. 4TH AVE N/S | E. 8TH AVE S/S | 1,600 | 19 | 30,400 | R - Residential/Local | A - AC | 79 | 25.58 |
| ESFRNB | 40 | ESPLANADE | FRNT NB E. 9TH AVE N/S | E. 11TH AVE S/S | 800 | 19 | 28,890 | R - Residential/Local | A - AC | 14 | 0 |
| ESFRNB | 50 | ESPLANADE | FRNT NB E. FRANCES WILLARD AVE N/S | E. 1ST AVE S/S | 705 | 19 | 13,395 | C - Collector | A - AC | 82 | 16.59 |
| ESFRSB | 10 | ESPLANADE | FRNT SB W. 11TH AVE S/S | W. 7TH AVE N/S | 1,605 | 19 | 30,495 | R - Residential/Local | A - AC | 17 | 0 |
| ESFRSB | 20 | ESPLANADE | FRNT SB W. 1ST AVE S/S | BIDWELL MANSION | 1,644 | 19 | 31,236 | R - Residential/Local | A - AC | 27 | 0.77 |
| ESFRSB | 30 | ESPLANADE | FRNT SB W. 4TH AVE S/S | W. 1ST AVE N/S | 1,210 | 19 | 22,990 | C - Collector | A - AC | 67 | 10.7 |
| ESFRSB | 40 | ESPLANADE | FRNT SB W. 7TH AVE S/S | W. 4TH AVE N/S | 1,200 | 19 | 22,800 | R - Residential/Local | A - AC | 76 | 23.66 |
| ESFTNB | 10 | ESPLANADE | FTNT NB E. 8TH AVE N/S | E. 9TH AVE S/S | 400 | 19 | 7,600 | R - Residential/Local | A - AC | 15 | 0 |
| ESTAWY | 10 | ESTATES WAY | EARL AVE | CUL-DE-SAC | 1,200 | 37 | 46,043 | R - Residential/Local | A - AC | 57 | 12.82 |
| ESTERD | 10 | ESTES RD | W. 22ND ST | 1280 FT S/O W. 22ND ST | 1,280 | 20 | 25,600 | R - Residential/Local | A - AC | 55 | 11.81 |
| EUGEAV | 10 | EUGENE AVE | LINDO | N/END | 485 | 26 | 12,610 | C - Collector | A - AC | 17 | 0 |
| FAIRST | 10 | FAIR ST | COUNTRY DR | END | 743 | 40 | 29,720 | R - Residential/Local | A - AC | 82 | 27.49 |
| FAIRST | 20 | FAIR ST | E. 20TH ST | E. 21ST ST | 625 | 55 | 34,375 | R - Residential/Local | A - AC | 58 | 13.42 |
| FAIRST | 30 | FAIR ST | E. 21ST ST | E. 23RD ST | 1,278 | 55 | 70,290 | R - Residential/Local | A - AC | 37 | 4.02 |
| FAIRST | 40 | FAIR ST | E. 23RD ST | E. PARK AVE | 1,628 | 60 | 97,680 | R - Residential/Local | A - AC | 40 | 5.19 |
| FAIRST | 50 | FAIR ST | E. PARK AVE | SCOTT AVE | 821 | 32 | 26,272 | R - Residential/Local | A - AC | 29 | 1.46 |
| FAIRST | 60 | FAIR ST | PASEO COMPANEROS | PVMT CHNG | 1,627 | 24 | 39,048 | R - Residential/Local | A - AC | 81 | 26.86 |
| FAIRST | 70 | FAIR ST | PVMT CHNG | COUNTRY DR | 452 | 36 | 16,272 | R - Residential/Local | A - AC | 49 | 9 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|----------------|---------------|-------------------------------|--------|-------|--------|--------------------------|--------------|-------------|----------------|
| FAIRST | 80 | FAIR ST | SCOTT AVE | PASEO COMPANEROS | 744 | 32 | 23,808 | R - Residential/Local | A - AC | 51 | 9.91 |
| FAIRAV | 10 | FAIRCHILD AVE | MARAUDER ST | END | 1,365 | 40 | 54,600 | R - Residential/Local | A - AC | 67 | 18.31 |
| FARCR | 10 | FAIROAKS CIR | MANZANITA AVE | | 93 | 44 | 5,438 | R - Residential/Local | A - AC | 59 | 13.84 |
| FAWYAL | 10 | FAIRWAY AL | E 3RD ST | CUL-DE-SAC | 835 | 14 | 11,690 | R - Residential/Local | A - AC | 33 | 2.9 |
| FAWYAL | 20 | FAIRWAY AL | E. 5TH AVE | E. 7TH AVE | 875 | 14 | 12,250 | R - Residential/Local | P - PCC | 78 | 50.5 |
| FARGCT | 10 | FARGA CT | PICHOLINE WY | CUL-DE-SAC | 178 | 31 | 7,878 | R - Residential/Local | A - AC | 80 | 26.24 |
| FARMV | 10 | FARMINGTON AVE | BURLINGAME DR | BRUCE RD | 917 | 36 | 33,012 | R - Residential/Local | A - AC | 83 | 28.14 |
| FERNV | 10 | FERN AVE | W 11TH AVE | W 12TH AVE | 820 | 30 | 24,600 | R - Residential/Local | A - AC | 79 | 25.58 |
| FERNV | 20 | FERN AVE | W 12TH AVE | W LINDO AVE | 575 | 22 | 12,650 | R - Residential/Local | A - AC | 51 | 9.91 |
| FILBAV | 10 | FILBERT AVE | ARBUTUS AVE | CITY LIMIT (NEAR SIERRA VIEW) | 998 | 28 | 27,944 | R - Residential/Local | A - AC | 29 | 1.46 |
| FILBAV | 20 | FILBERT AVE | CRISTER AVE | MADRONE AVE | 1,565 | 32 | 50,080 | C - Collector | A - AC | 75 | 13.45 |
| FILBAV | 30 | FILBERT AVE | DOWNING AVE | MOSS AVE | 1,420 | 32 | 45,440 | R - Residential/Local | A - AC | 23 | 0 |
| FILBAV | 40 | FILBERT AVE | LARCH ST | CUL-DE-SAC | 489 | 34 | 18,494 | R - Residential/Local | A - AC | 14 | 0 |
| FILBAV | 50 | FILBERT AVE | MADRONE AVE | LARCH ST | 1,162 | 34 | 39,508 | C - Collector | A - AC | 16 | 0 |
| FILBAV | 60 | FILBERT AVE | MOSS AVE | CRISTER AVE | 1,350 | 32 | 43,200 | R - Residential/Local | A - AC | 73 | 21.81 |
| FILBAV | 70 | FILBERT AVE | REY WY | DOWNING AVE | 962 | 25 | 24,050 | R - Residential/Local | A - AC | 16 | 0 |
| FIRST | 10 | FIR ST | E. 8TH ST | HUMBOLDT RD | 717 | 32 | 22,944 | C - Collector | A - AC | 86 | 18.61 |
| FIRECT | 10 | FIREFLY CT | MANZANITA AVE | CUL-DE-SAC | 504 | 34 | 19,004 | R - Residential/Local | A - AC | 78 | 24.96 |
| FIIRCT | 10 | FIVE IRON CT | DELANEY | END | 180 | 30 | 5,400 | R - Residential/Local | A - AC | 85 | 29.37 |
| FIMIRD | 10 | FIVE MILE RD | UPPER PARK RD | END | 1,340 | 18 | 24,120 | R - Residential/Local | A - AC | 61 | 14.89 |
| FLORAV | 10 | FLORAL AVE | DISCOVERY WY | LASSEN AVE | 850 | 32 | 27,200 | C - Collector | A - AC | 79 | 15.2 |
| FLORAV | 20 | FLORAL AVE | E. EATON RD | DISCOVERY WY | 1,616 | 48 | 77,568 | R - Residential/Local | A - AC | 73 | 21.81 |
| FLORAV | 30 | FLORAL AVE | EAST AVE | PATRICIA DR | 775 | 36 | 27,900 | MiA - Minor Arterial (4) | A - AC | 15 | 0 |
| FLORAV | 40 | FLORAL AVE | EAST AVE | RAVENSHOE WY | 531 | 52 | 27,612 | R - Residential/Local | A - AC | 77 | 24.41 |
| FLORAV | 50 | FLORAL AVE | GLENSHIRE LN | E. EATON RD | 1,170 | 56 | 65,520 | R - Residential/Local | A - AC | 65 | 17.08 |
| FLORAV | 60 | FLORAL AVE | MANZANITA AVE | ORCHARD LN | 844 | 36 | 30,384 | A - Arterial | A - AC | 45 | 5.61 |
| FLORAV | 70 | FLORAL AVE | ORCHARD LN | PATRICIA DR | 1,002 | 36 | 36,072 | R - Residential/Local | A - AC | 27 | 0.79 |
| FLORAV | 80 | FLORAL AVE | RAVENSHOE WY | GLENSHIRE LN | 1,251 | 56 | 70,056 | R - Residential/Local | A - AC | 74 | 22.43 |
| FLORLN | 10 | FLORENCE LN | E. LINDO AVE | CUL-DE-SAC | 197 | 35 | 8,688 | R - Residential/Local | A - AC | 6 | 0 |
| FUMMST | 10 | FLUME ST | E. 2ND ST | E. 5TH ST | 870 | 50 | 43,500 | R - Residential/Local | A - AC | 70 | 19.96 |
| FUMMST | 20 | FLUME ST | E. 5TH ST | E. 8TH ST | 950 | 50 | 47,500 | R - Residential/Local | A - AC | 82 | 27.49 |
| FUMMST | 30 | FLUME ST | E. 8TH ST | HUMBOLDT AVE | 562 | 50 | 28,100 | R - Residential/Local | A - AC | 77 | 24.3 |
| FLYVST | 10 | FLYING V ST | FOREST AVE | BAR TRIANGLE ST | 1,033 | 37 | 38,221 | R - Residential/Local | A - AC | 47 | 8.13 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|--------------------|--------------------------------|--------------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| FOREAV | 10 | FOREST AVE | AMANDA WAY | SPRINGFIELD DR | 1,764 | 62 | 109,368 | R - Residential/Local | A - AC | 78 | 24.96 |
| FOREAV | 20 | FOREST AVE | DEER CREEK HIGHWAY | HUMBOLDT RD | 450 | 61 | 27,450 | C - Collector | A - AC | 94 | 22.68 |
| FOREAV | 30 | FOREST AVE | E. 8TH ST | FOREST CREEK CIR N/S | 1,052 | 29 | 30,508 | R - Residential/Local | A - AC | 89 | 31.59 |
| FOREAV | 40 | FOREST AVE | FOREST CREEK CIR N/S | DEER CREEK HWY | 1,330 | 29 | 38,570 | R - Residential/Local | A - AC | 89 | 31.59 |
| FOREAV | 50 | FOREST AVE | HUMBOLDT RD | AMANDA WAY | 1,342 | 61 | 81,862 | R - Residential/Local | A - AC | 78 | 25.05 |
| FOREAV | 60 | FOREST AVE | NOTRE DAME BLVD | SKYWAY | 1,824 | 44 | 80,256 | C - Collector | A - AC | 72 | 12.23 |
| FOREAV | 70 | FOREST AVE | PVMT CHNG (655 FT S/O E. 20TH) | NOTRE DAME BLVD | 4,007 | 64 | 256,448 | R - Residential/Local | A - AC | 78 | 24.96 |
| FOREAV | 80 | FOREST AVE | SPRINGFIELD DR | PVMT CHNG (655 FT S/O E. 20TH) | 1,654 | 62 | 102,548 | R - Residential/Local | A - AC | 71 | 20.59 |
| FOCRCR | 10 | FOREST CREEK CIR | FOREST AVE | PVMT CHNG | 1,116 | 33 | 36,828 | R - Residential/Local | A - AC | 55 | 11.83 |
| FOCRCR | 20 | FOREST CREEK CIR | PVMT CHNG | FOREST AVE | 816 | 33 | 26,928 | R - Residential/Local | A - AC | 22 | 0 |
| FORTST | 10 | FORTRESS ST | AIRPARK BLVD | RYAN AVE | 2,145 | 61 | 130,845 | R - Residential/Local | A - AC | 68 | 18.8 |
| FORTST | 20 | FORTRESS ST | BOEING AVE | AIRPARK BLVD | 1,573 | 61 | 95,953 | R - Residential/Local | A - AC | 71 | 20.59 |
| FORTST | 30 | FORTRESS ST | SIKORSKY AVE | BOEING AVE | 662 | 44 | 29,128 | R - Residential/Local | A - AC | 37 | 4.04 |
| FOXWDR | 10 | FOXWOOD DR | WALNUT PARK DR | CENTENNIAL AVE | 175 | 32 | 5,600 | R - Residential/Local | A - AC | 79 | 25.6 |
| FRANWY | 10 | FRANCISCAN WAY | PANAMA AVE | PANAMA AVE | 1,300 | 32 | 41,600 | R - Residential/Local | A - AC | 31 | 2.17 |
| FRANST | 10 | FRANKLIN ST | E. 20TH ST | END | 251 | 28 | 9,812 | R - Residential/Local | A - AC | 82 | 27.49 |
| FREDCT | 10 | FREDA CT | WEBSTER DR | CUL-DE-SAC | 224 | 37 | 9,931 | R - Residential/Local | A - AC | 69 | 19.37 |
| FREMST | 10 | FREMONT ST | NOTRE DAME BLVD | END | 362 | 35 | 12,670 | R - Residential/Local | A - AC | 21 | 0 |
| FRONCR | 10 | FRONTIER CIR | COHASSET RD | CUL-DE-SAC | 442 | 33 | 14,586 | R - Residential/Local | A - AC | 2 | 0 |
| FUCHWY | 10 | FUCHSIA WAY | HOLLY AVE | END | 480 | 32 | 15,360 | R - Residential/Local | A - AC | 68 | 18.78 |
| GALAWY | 20 | GALLATIN GATEWAY | CINDER CREEK DR | CEANOTHUS AVE | 930 | 30 | 27,900 | R - Residential/Local | A - AC | 93 | 33.36 |
| GAPADR | 10 | GARDEN PARK DR | CERES AVE | E. CUL-DE-SAC | 623 | 32 | 19,936 | R - Residential/Local | A - AC | 40 | 5.21 |
| GAPADR | 20 | GARDEN PARK DR | CERES AVE | W. CUL-DE-SAC | 658 | 32 | 21,056 | R - Residential/Local | A - AC | 60 | 14.36 |
| GAVATR | 10 | GARDEN VALLEY TERR | PRESERVATION OAK DR | POTTER RD | 470 | 30 | 14,100 | R - Residential/Local | A - AC | 91 | 32.48 |
| GATELN | 10 | GATEWAY LN | W. SACRAMENTO AVE | DARTWOOD DR | 702 | 36 | 27,072 | R - Residential/Local | A - AC | 77 | 24.3 |
| GENELN | 10 | GENEVA LN | NORTH AVE | CUL-DE-SAC | 675 | 32 | 21,600 | R - Residential/Local | A - AC | 10 | 0 |
| GILBLN | 10 | GILBERT LN | W. 8TH AVE | CORDELIA CT E/S | 472 | 36 | 16,992 | R - Residential/Local | A - AC | 77 | 24.3 |
| GLPELN | 10 | GLACIER PEAK LN | E. EATON RD | CUL-DE-SAC | 677 | 32 | 21,664 | R - Residential/Local | A - AC | 37 | 4.04 |
| GLBRCT | 10 | GLENBROOK CT | COHASSET RD | END | 500 | 22 | 11,000 | R - Residential/Local | A - AC | 84 | 28.76 |
| GLENST | 10 | GLENN ST | E. 8TH ST | PVMT CHNG | 815 | 35 | 28,525 | C - Collector | A - AC | 75 | 13.45 |
| GLENST | 20 | GLENN ST | PVMT CHNG | MODOC DR | 426 | 32 | 13,632 | R - Residential/Local | A - AC | 81 | 26.88 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------------|--------------------------------|--------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| GLENCT | 10 | GLENOAK CT | HOOKER OAK AVE | CUL-DE-SAC | 388 | 36 | 15,686 | C - Collector | A - AC | 67 | 10.72 |
| GLENLN | 10 | GLENSHIRE LN | CERES AVE | W. CUL-DE-SAC | 521 | 36 | 20,474 | C - Collector | A - AC | 60 | 8.34 |
| GLENLN | 20 | GLENSHIRE LN | CERES AVE | FLORAL AVE | 1,262 | 36 | 45,432 | C - Collector | A - AC | 61 | 8.66 |
| GLENLN | 30 | GLENSHIRE LN | FLORAL AVE | 650 FT E/O FLORAL AVE | 650 | 36 | 23,400 | R - Residential/Local | A - AC | 59 | 13.84 |
| GLENLN | 40 | GLENSHIRE LN | 650' E/O FLORAL AVE | MARIPOSA AVE | 660 | 28 | 18,480 | R - Residential/Local | A - AC | 82 | 27.51 |
| GLEVCT | 10 | GLENVIEW CT | PINYON HILLS DR | CUL-DE-SAC | 409 | 32 | 13,088 | R - Residential/Local | A - AC | 81 | 26.88 |
| GLWOAV | 30 | GLENWOOD AVE | OAK WAY | WAXING WAY | 810 | 22 | 17,820 | C - Collector | A - AC | 97 | 23.81 |
| GLWOAV | 40 | GLENWOOD AVE | WAXING WAY | W SACRAMENTO | 810 | 22 | 17,820 | C - Collector | A - AC | 97 | 23.81 |
| GODMAV | 10 | GODMAN AVE | LASSEN AV | WATERFORD | 1,550 | 34 | 52,700 | C - Collector | A - AC | 74 | 13.06 |
| GODMAV | 20 | GODMAN AVE | WATERFORD | EATON AV | 2,502 | 36 | 90,072 | C - Collector | A - AC | 84 | 17.61 |
| GOBIPL | 10 | GOLDEN BIRCH PL | AUTUMN HILL LN | ROSE RIVER AVE | 164 | 32 | 5,248 | R - Residential/Local | A - AC | 82 | 27.53 |
| GOLDCT | 10 | GOLDENEYE CT | IDYLLWILD CIR | CUL-DE-SAC | 564 | 32 | 18,048 | R - Residential/Local | A - AC | 66 | 17.67 |
| GOLACR | 10 | GOOSELAKE CIR | PENZANCE AVE | END | 440 | 20 | 8,800 | R - Residential/Local | A - AC | 94 | 33.59 |
| GOLACR | 20 | GOOSELAKE CIR | GOOSELAKE CIR | END | 1,230 | 26 | 31,980 | R - Residential/Local | A - AC | 94 | 33.59 |
| GOSSLN | 10 | GOSSAMER LN | VISTAMONT WAY | PARNASSIAN AVE | 340 | 28 | 9,520 | R - Residential/Local | A - AC | 81 | 26.9 |
| GRPADR | 10 | GRAFTON PARK DR | CUSSICK AVE | CROMWELL DR N/S | 756 | 36 | 29,016 | R - Residential/Local | A - AC | 82 | 27.49 |
| GRVWY | 10 | GRANITE VIEW WAY | YOSEMITE DR | HALF DOME WAY | 280 | 32 | 8,960 | R - Residential/Local | A - AC | 84 | 28.78 |
| GRASCT | 10 | GRASS CT | MEADOW RD | END | 380 | 32 | 12,160 | R - Residential/Local | A - AC | 50 | 9.45 |
| GRCRWY | 10 | GRASSHOPPER CREEK WAY | GALLATIN GATEWAY | END | 350 | 30 | 10,500 | R - Residential/Local | A - AC | 93 | 33.53 |
| GRAYCT | 10 | GRAYEAGLE CT | NEWPORT DR | CUL-DE-SAC | 315 | 36 | 13,140 | R - Residential/Local | A - AC | 77 | 24.3 |
| GRFDR | 10 | GREENFIELD DR | ESPLANADE DR | CALISTOGA DR | 733 | 36 | 26,388 | R - Residential/Local | A - AC | 60 | 14.34 |
| GREENLN | 10 | GREENHAVEN LN | 140 FT S/O GREENWICH DR | NORTH CUL-DE-SAC | 840 | 36 | 32,040 | R - Residential/Local | A - AC | 47 | 8.11 |
| GREEDR | 10 | GREENWICH DR | GREENHAVEN LN W/S | W. 8TH AVE | 1,510 | 36 | 54,360 | R - Residential/Local | A - AC | 23 | 0 |
| GREEDR | 20 | GREENWICH DR | N. CHERRY ST | GREENHAVEN LN W/S | 1,672 | 36 | 60,192 | R - Residential/Local | A - AC | 21 | 0 |
| GREEDR | 30 | GREENWICH DR | W. 8TH AVE | W. 11TH AVE | 1,283 | 32 | 41,056 | R - Residential/Local | A - AC | 82 | 27.49 |
| GREGCT | 10 | GREG CT | MODOC DR | CUL-DE-SAC | 144 | 36 | 7,069 | R - Residential/Local | A - AC | 57 | 12.84 |
| GRPALN | 10 | GRIFFITH PARK LN | IDYLLWILD CIR | CUL-DE-SAC | 469 | 32 | 15,008 | C - Collector | A - AC | 81 | 16.15 |
| GUYNAV | 10 | GUYNN AVE | CITY LIMIT(640 FT S/O HENSHAW) | HENSHAW AVE | 640 | 18 | 11,520 | C - Collector | A - AC | 80 | 15.64 |
| GUYNAV | 20 | GUYNN AVE | HENSHAW AVE | CITY LIMIT(640 FT N/O HENSHAW) | 640 | 18 | 11,520 | C - Collector | A - AC | 20 | 0 |
| GUYNAV | 30 | GUYNN AVE | W. LINDO AVE | END OF BRIDGE | 165 | 18 | 2,970 | R - Residential/Local | A - AC | 46 | 7.79 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|------------------------------|--------------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| HADOWY | 10 | HALF DOME WAY | SOUTH END | NORTH END | 620 | 28 | 17,360 | R - Residential/Local | A - AC | 82 | 27.53 |
| HANCDR | 10 | HANCOCK DR | SARATOGA DR | CUL-DE-SAC | 475 | 36 | 18,818 | R - Residential/Local | A - AC | 58 | 13.35 |
| HARTDR | 10 | HARTFORD DR | FOREST AVE | END | 1,230 | 37 | 45,510 | R - Residential/Local | A - AC | 64 | 16.54 |
| HAGLDR | 10 | HARVEST GLEN DR | ROSE RIVER AVE | AUBURN OAK WY | 394 | 32 | 12,608 | R - Residential/Local | A - AC | 74 | 22.45 |
| HASTST | 10 | HASTINGS ST | EAST END | WEST CUL-DE-SAC | 725 | 32 | 27,409 | R - Residential/Local | A - AC | 82 | 27.49 |
| HACRDR | 10 | HAT CREEK DR | CALISTOGA DR | CLEAR LAKE DR | 262 | 36 | 9,432 | R - Residential/Local | A - AC | 71 | 20.56 |
| HAVELN | 10 | HAVEN LN | CERES AVE | WEST END | 635 | 36 | 22,860 | R - Residential/Local | A - AC | 19 | 0 |
| HAWTAV | 10 | HAWTHORNE AVE | ALAN LN | VERBENA LN | 1,100 | 35 | 38,500 | C - Collector | A - AC | 81 | 16.15 |
| HAWTAV | 20 | HAWTHORNE AVE | MOSS AVE | ALAN LN | 557 | 35 | 19,495 | C - Collector | A - AC | 75 | 13.47 |
| HAWTAV | 30 | HAWTHORNE AVE | VERBENA LN | MADRONE AVE | 1,250 | 35 | 43,750 | C - Collector | A - AC | 79 | 15.22 |
| HAZEST | 10 | HAZEL ST | W. 11TH ST N/S | NORTH END | 478 | 36 | 17,208 | R - Residential/Local | A - AC | 39 | 4.91 |
| HAZEST | 20 | HAZEL ST | W. 11TH ST S/S | IVY ST | 1,121 | 32 | 35,872 | C - Collector | A - AC | 49 | 5.14 |
| HAZEST | 30 | HAZEL ST | W. 2ND ST S/S | W. 5TH ST N/S | 882 | 52 | 45,864 | R - Residential/Local | A - AC | 37 | 4.02 |
| HAZEST | 40 | HAZEL ST | W. 5TH ST S/S | W. 8TH ST N/S | 938 | 52 | 48,776 | R - Residential/Local | A - AC | 79 | 25.58 |
| HAZEST | 50 | HAZEL ST | W. 8TH ST | W. 9TH ST | 278 | 52 | 14,456 | R - Residential/Local | A - AC | 94 | 33.59 |
| HAZEST | 60 | HAZEL ST | W. 9TH ST S/S | END | 335 | 34 | 11,390 | R - Residential/Local | A - AC | 22 | 0 |
| HAZEST | 70 | HAZEL ST | IVY ST | W. 12TH ST | 905 | 32 | 28,960 | R - Residential/Local | A - AC | 11 | 0 |
| HEARCT | 10 | HEARTWOOD CT | BENDING OAK WY | CUL-DE-SAC | 246 | 32 | 10,065 | R - Residential/Local | A - AC | 80 | 26.26 |
| HEATCR | 10 | HEATHER CIR | E. 5TH AVE | CUL-DE-SAC | 575 | 34 | 21,418 | R - Residential/Local | A - AC | 38 | 4.58 |
| HEGALN | 10 | HEGAN LN | HUSS DR | MIDWAY | 3,200 | 37 | 118,400 | R - Residential/Local | A - AC | 97 | 34.1 |
| HELEWY | 10 | HELENA WAY | CAVALIER WY | VAIL DR | 365 | 28 | 10,220 | R - Residential/Local | A - AC | 80 | 26.39 |
| HEMLST | 10 | HEMLOCK ST | E. 12TH ST | E. 16TH ST | 1,110 | 37 | 41,070 | R - Residential/Local | A - AC | 80 | 26.33 |
| HEMLST | 20 | HEMLOCK ST | E. 16TH ST | E. 20TH ST | 1,260 | 37 | 46,620 | R - Residential/Local | A - AC | 82 | 27.49 |
| HEMMLN | 10 | HEMMING LN | CERES AVE | CUL-DE-SAC | 513 | 32 | 16,416 | R - Residential/Local | A - AC | 66 | 17.67 |
| HENSVA | 10 | HENSHAW AVE | 106 FT W/O LOWELL DR (W INT) | CUSSICK AVE | 1,964 | 36 | 70,704 | C - Collector | A - AC | 61 | 8.64 |
| HENSVA | 20 | HENSHAW AVE | ALAMO AVE W/S | PVMT CHNG (567 FT W/O ALAMO) | 567 | 25 | 14,175 | C - Collector | A - AC | 75 | 13.43 |
| HENSVA | 30 | HENSHAW AVE | CUSSICK AVE | PVMT CHNG (594 FT W/O CUSSICK) | 594 | 25 | 14,850 | C - Collector | A - AC | 7 | 0 |
| HENSVA | 40 | HENSHAW AVE | ESPLANADE DR | 106 FT W/O LOWELL DR (W INT) | 1,336 | 36 | 48,096 | R - Residential/Local | A - AC | 72 | 21.17 |
| HENSVA | 50 | HENSHAW AVE | GUYNN AVE W/S | CITY LIMIT (650 FT W/O GUYNN) | 650 | 30 | 19,500 | C - Collector | A - AC | 35 | 2.03 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|--|------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| HENSAV | 60 | HENSHAW AVE | PVMT CHNG (567 FT W/O GUYNN AVE W/S ALAMO) | | 752 | 30 | 22,560 | C - Collector | A - AC | 46 | 4.37 |
| HENSAV | 70 | HENSHAW AVE | PVMT CHNG (594 FT W/O ALAMO AVE W/S CUSSICK) | | 892 | 30 | 26,760 | C - Collector | A - AC | 82 | 16.59 |
| HERFCIR | 10 | HERFAX CIR | W. 11TH AVE | W/O HERFAX CIR | 1,175 | 28 | 32,900 | R - Residential/Local | A - AC | 82 | 27.49 |
| HEOADR | 10 | HERITAGE OAK DR | SPRINGFIELD DR | END | 1,014 | 32 | 32,448 | R - Residential/Local | A - AC | 76 | 23.71 |
| HICKST | 10 | HICKORY ST | W. 5TH ST N/S | ALMOND ST | 820 | 36 | 29,520 | R - Residential/Local | A - AC | 21 | 0 |
| HICKST | 20 | HICKORY ST | W. 5TH ST N/S | NORTH END | 510 | 36 | 18,360 | R - Residential/Local | A - AC | 26 | 0.43 |
| HICRDR | 10 | HIDDEN CREEK DR | MILL CREEK DR | SANDY COVE DR | 945 | 36 | 34,020 | R - Residential/Local | A - AC | 76 | 23.66 |
| HIDEPK | 10 | HIDEAWAY PARK | W.12TH AVE | END | 670 | 32 | 21,440 | R - Residential/Local | A - AC | 82 | 27.49 |
| HIGHCR | 10 | HIGHLAND CIR | CERES AVE | CUL-DE-SAC | 950 | 36 | 35,918 | R - Residential/Local | A - AC | 24 | 0 |
| HIGHLN | 10 | HIGHTOWER LNDG | RUBY RIVER DR | GALLATIN GATEWAY | 570 | 32 | 18,240 | R - Residential/Local | A - AC | 93 | 33.38 |
| HILDWY | 10 | HILDA WY | MANZANITA AVE | CUL-DE-SAC | 250 | 36 | 10,328 | R - Residential/Local | A - AC | 22 | 0 |
| HIVIWY | 10 | HILL VIEW WAY | DOWNING AVE | BRYANT AVE | 410 | 30 | 12,300 | R - Residential/Local | A - AC | 18 | 0 |
| HIVIWY | 20 | HILL VIEW WAY | NEAL DOW AVE | DOWNING AVE | 504 | 30 | 15,120 | R - Residential/Local | A - AC | 89 | 31.61 |
| HIVIWY | 30 | HILL VIEW WAY | ARBUTUS AVE | TOYON WY | 610 | 36 | 21,960 | R - Residential/Local | A - AC | 18 | 0 |
| HILLLN | 10 | HILLARY LN | BURNAP AVE | CUL-DE-SAC | 616 | 36 | 23,894 | R - Residential/Local | A - AC | 48 | 8.59 |
| HILLCR | 10 | HILLSBORO CIR | DANBURY WY | CUL-DE-SAC | 550 | 32 | 17,600 | R - Residential/Local | A - AC | 57 | 12.84 |
| HOBAST | 10 | HOBART ST | W. 1ST AVE N/S | W. 4TH AVE S/S | 1,300 | 30 | 39,000 | C - Collector | A - AC | 57 | 7.39 |
| HOBAST | 20 | HOBART ST | W. 4TH AVE N/S | W. 6TH AVE S/S | 913 | 25 | 22,825 | C - Collector | A - AC | 35 | 2.03 |
| HOBAST | 30 | HOBART ST | W. SACRAMENTO AVE | W. 1ST AVE S/S | 384 | 40 | 15,360 | R - Residential/Local | A - AC | 32 | 2.53 |
| HOLBAV | 10 | HOLBEN AVE | E. 1ST AVE | MARGUERITE AVE | 685 | 36 | 24,660 | R - Residential/Local | A - AC | 68 | 18.78 |
| HOLLAV | 10 | HOLLY AVE | 120 FT S/KNOTS GLEN | W. 12TH AVE | 418 | 36 | 15,048 | C - Collector | A - AC | 65 | 9.98 |
| HOLLAV | 20 | HOLLY AVE | C.L. (130 FT S/O KNOTTS GLEN) | W. LINDO AVE N/S | 595 | 36 | 21,420 | C - Collector | A - AC | 67 | 10.7 |
| HOLLAV | 30 | HOLLY AVE | W. 11TH AVE | SOUTH END | 569 | 44 | 25,036 | C - Collector | A - AC | 73 | 12.61 |
| HOLLAV | 40 | HOLLY AVE | W. 12TH AVE | W. 11TH AVE | 867 | 20 | 17,340 | C - Collector | A - AC | 65 | 9.98 |
| HOLLAV | 50 | HOLLY AVE | W. LINDO AVE N/S | EAST AVE | 1,750 | 36 | 59,760 | C - Collector | A - AC | 73 | 12.61 |
| HOLTNL | 10 | HOLT LN | FLORAL AVE | DIRT ROAD | 400 | 16 | 6,400 | R - Residential/Local | A - AC | 53 | 10.89 |
| HOMEWY | 10 | HOMWOOD WAY | HASTINGS ST | HENSHAW AVE | 160 | 32 | 5,120 | R - Residential/Local | A - AC | 82 | 27.49 |
| HOOAAV | 10 | HOOKER OAK AVE | CRISTER AVE | MADRONE AVE | 1,573 | 35 | 55,055 | R - Residential/Local | A - AC | 80 | 26.33 |
| HOOAAV | 20 | HOOKER OAK AVE | JUNIPER ST | MANZANITA AVE | 1,880 | 32 | 60,160 | C - Collector | A - AC | 18 | 0 |
| HOOAAV | 30 | HOOKER OAK AVE | LARCH ST | JUNIPER ST | 1,535 | 36 | 55,260 | C - Collector | A - AC | 21 | 0 |
| HOOAAV | 40 | HOOKER OAK AVE | MADRONE AVE | LARCH ST | 1,143 | 36 | 41,148 | C - Collector | A - AC | 30 | 1.03 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|---------------|--------------------------------|--------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| HOOPCR | 10 | HOOPA CIR | SEQUOYAH AVE | END | 325 | 36 | 11,700 | R - Residential/Local | A - AC | 18 | 0 |
| HOWADR | 10 | HOWARD DR | MANZANITA AVE | MARIPOSA AVE | 1,538 | 32 | 49,216 | R - Residential/Local | A - AC | 16 | 0 |
| HUDSAV | 10 | HUDSON AVE | SARATOGA DR | VALLEY FORGE DR | 766 | 36 | 27,576 | R - Residential/Local | A - AC | 81 | 26.9 |
| HUDSAV | 20 | HUDSON AVE | VALLEY FORGE DR | ST LAWRENCE AVE | 569 | 36 | 20,484 | R - Residential/Local | A - AC | 80 | 26.26 |
| HUDSAV | 30 | HUDSON AVE | ST LAWRENCE AVE | MIDDLETON AVE | 580 | 36 | 20,880 | R - Residential/Local | A - AC | 79 | 25.62 |
| HUGGAV | 10 | HUGGINS AVE | CRISTER AVE | C.L. (370' W/O CRISTER AVE) | 370 | 38 | 14,060 | R - Residential/Local | A - AC | 60 | 14.39 |
| HUMBAV | 20 | HUMBOLDT AVE | PVMT CHNG (218 FT W/O BRUCE) | PVMT CHNG (538 FT E/O BRUCE) | 858 | 22 | 18,876 | R - Residential/Local | A - AC | 37 | 4.02 |
| HUMBAV | 30 | HUMBOLDT AVE | PVMT CHNG (984 FT E/O LINDEN) | PVMT CHNG (218 FT W/O BRUCE) | 636 | 36 | 22,896 | R - Residential/Local | A - AC | 20 | 0 |
| HUMBAV | 40 | HUMBOLDT AVE | LINDEN ST | PVMT CHNG (984 FT E/O LINDEN) | 984 | 36 | 35,424 | R - Residential/Local | A - AC | 24 | 0 |
| HUMBAV | 50 | HUMBOLDT AVE | ALDER ST | LINDEN ST | 967 | 22 | 21,274 | R - Residential/Local | A - AC | 37 | 4.02 |
| HUMBAV | 60 | HUMBOLDT AVE | PINE ST | ALDER ST | 1,017 | 34 | 34,578 | R - Residential/Local | A - AC | 54 | 11.32 |
| HUMBAV | 70 | HUMBOLDT AVE | ORIENT ST | PINE ST | 672 | 34 | 22,848 | R - Residential/Local | A - AC | 54 | 11.44 |
| HUMBAV | 80 | HUMBOLDT AVE | MAIN ST | ORIENT ST | 1,023 | 36 | 36,828 | R - Residential/Local | A - AC | 66 | 17.62 |
| HUMBRD | 10 | HUMBOLDT RD | 2785 FT E/O STILSON RD | CITY LIMIT | 3,200 | 19 | 60,800 | C - Collector | A - AC | 0 | 0 |
| HUMBRD | 100 | HUMBOLDT RD | PVMT CHNG (223 FT E/O STILSON) | RD | 2,562 | 19 | 48,678 | C - Collector | A - AC | 8 | 0 |
| HUMBRD | 110 | HUMBOLDT RD | PVMT CHNG (425 FT W/O BRUCE) | RD | 425 | 27 | 11,475 | C - Collector | A - AC | 72 | 12.25 |
| HUMBRD | 20 | HUMBOLDT RD | E/O BRUCE RD | PVMT CHNG (223 FT E/O STILSON) | 700 | 27 | 18,900 | C - Collector | A - AC | 76 | 13.9 |
| HUMBRD | 30 | HUMBOLDT RD | EL MONTE AVE | PVMT CHG (1290FT E/O EL MONTE) | 1,290 | 27 | 34,830 | C - Collector | A - AC | 96 | 23.43 |
| HUMBRD | 40 | HUMBOLDT RD | FIR ST | FIRE TRAINING CENTER DRIVEWAY | 780 | 30 | 23,400 | C - Collector | A - AC | 15 | 0 |
| HUMBRD | 50 | HUMBOLDT RD | FIRE TRAINING CENTER DRIVEWAY | HUNTER CT | 2,354 | 32 | 75,328 | C - Collector | A - AC | 77 | 14.33 |
| HUMBRD | 60 | HUMBOLDT RD | FOREST AVE | EL MONTE AVE | 1,420 | 28 | 39,760 | C - Collector | A - AC | 96 | 23.43 |
| HUMBRD | 80 | HUMBOLDT RD | HUNTER CT | FOREST AVE | 361 | 33 | 11,913 | C - Collector | A - AC | 25 | 0.04 |
| HUMBRD | 90 | HUMBOLDT RD | PVMT CHG (1290FT E/O EL MONTE) | PVMT CHNG (425 FT W/O BRUCE) | 1,637 | 25 | 40,925 | C - Collector | A - AC | 23 | 0 |
| HUNTCT | 10 | HUNTER CT | HUMBOLDT RD | CUL-DE-SAC | 376 | 29 | 13,147 | C - Collector | A - AC | 68 | 10.74 |
| HUNTDR | 10 | HUNTINGTON DR | E. 20TH ST | WESTMINISTER CT | 271 | 37 | 10,027 | R - Residential/Local | A - AC | 80 | 26.26 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|--------------------------------|-------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| HUNTR | 20 | HUNTINGTON DR | WESTMINISTER CT | MARILYN DR | 1,354 | 37 | 50,098 | R - Residential/Local | A - AC | 80 | 26.26 |
| HUSSDR | 10 | HUSS DR | AZTEC DR S/S | HEGAN LN | 1,302 | 37 | 48,174 | C - Collector | A - AC | 73 | 12.61 |
| HUTCST | 10 | HUTCHINSON ST | 20TH | LAWLWR | 320 | 17 | 5,440 | C - Collector | A - AC | 82 | 16.63 |
| IDYLCR | 10 | IDYLLWILD CIR | BURNEY DR | CATALINA POINT DR | 1,092 | 36 | 39,312 | C - Collector | A - AC | 58 | 7.74 |
| IDYLCR | 20 | IDYLLWILD CIR | CATALINA POINT RD | LAKWEST DR | 676 | 36 | 24,336 | C - Collector | A - AC | 61 | 8.68 |
| IDYLCR | 30 | IDYLLWILD CIR | LAKWEST DR | PRINCESS T.J. CT | 948 | 36 | 34,128 | C - Collector | A - AC | 72 | 12.25 |
| IDYLCR | 40 | IDYLLWILD CIR | LAVA CAP CT | PENINSULA DR | 288 | 37 | 10,656 | C - Collector | A - AC | 81 | 16.15 |
| IDYLCR | 50 | IDYLLWILD CIR | PRINCESS T.J. CT | YOSEMITE DR | 1,096 | 36 | 39,456 | C - Collector | A - AC | 74 | 13.06 |
| IDYLCR | 60 | IDYLLWILD CIR | YOSEMITE DR | LAVA CAP CT | 160 | 35 | 5,600 | C - Collector | A - AC | 80 | 15.68 |
| IDYLCR | 70 | IDYLLWILD CIR | YOSEMITE DR | BURNEY DR | 999 | 36 | 35,964 | C - Collector | A - AC | 73 | 12.65 |
| INNSWY | 10 | INNSBROOK WY | ESPLANADE | DIRT ROAD | 395 | 22 | 8,690 | R - Residential/Local | A - AC | 80 | 26.22 |
| INYOST | 10 | INYO ST | GLENN ST | MODOC DR | 998 | 33 | 32,934 | R - Residential/Local | A - AC | 79 | 25.62 |
| IRENST | 10 | IRENE ST | CONNORS AVE | WHITE AVE | 400 | 36 | 14,400 | R - Residential/Local | A - AC | 27 | 0.77 |
| IRIQAV | 10 | IRIQUOIS AVE | W LINDO AVE | W 11TH AVE | 425 | 32 | 13,600 | R - Residential/Local | A - AC | 47 | 8.11 |
| IRVWY | 10 | IRVING WAY | PATRICIA DR | CUL-DE-SAC | 395 | 32 | 14,658 | R - Residential/Local | A - AC | 79 | 25.62 |
| ISJAV | 10 | ISAAC JAMES AVE | FOREST AVE | END | 670 | 30 | 20,100 | R - Residential/Local | A - AC | 84 | 28.78 |
| IVYST | 10 | IVY ST | PVMT CHNG (238 FT N/O SEVILLE) | MEYERS ST | 708 | 60 | 42,480 | C - Collector | A - AC | 0 | 0 |
| IVYST | 20 | IVY ST | PVMT CHNG (238 FT N/O SEVILLE) | NORTH END (440' N/O COMMERCE) | 716 | 40 | 28,640 | C - Collector | A - AC | 0 | 0 |
| IVYST | 30 | IVY ST | W. 11TH ST S/S | SOUTH END | 894 | 30 | 26,820 | C - Collector | A - AC | 78 | 14.73 |
| IVYST | 40 | IVY ST | W. 1ST ST N/S | W. 5TH ST N/S | 1,304 | 50 | 65,200 | C - Collector | A - AC | 0 | 0 |
| IVYST | 50 | IVY ST | W. 5TH ST S/S | W. 9TH ST N/S | 1,224 | 50 | 61,200 | C - Collector | A - AC | 0 | 0 |
| IVYST | 60 | IVY ST | W. 9TH ST S/S | W. 11TH ST S/S | 874 | 52 | 43,448 | C - Collector | A - AC | 78 | 14.73 |
| JACKDR | 10 | JACKIE DR | FLORAL AVE | END | 600 | 36 | 21,600 | R - Residential/Local | A - AC | 78 | 24.98 |
| JACOC | 10 | JACOBS CT | ALAMO AVE | CUL-DE-SAC | 430 | 32 | 17,280 | R - Residential/Local | A - AC | 58 | 13.31 |
| JANCT | 10 | JAN CT | FOREST AVE | CUL-DE-SAC | 202 | 36 | 9,072 | R - Residential/Local | A - AC | 77 | 24.5 |
| JARDWY | 10 | JARDIN WAY | VALLOMBROSA AVE | END | 700 | 24 | 16,800 | R - Residential/Local | A - AC | 78 | 24.98 |
| JASPDR | 10 | JASPER DR | NOTRE DAME BLVD | NIAGARA WY | 1,040 | 37 | 38,480 | R - Residential/Local | A - AC | 80 | 26.22 |
| JEANLN | 10 | JEAN LN | MARIPOSA AVE | CUL-DE-SAC | 621 | 32 | 19,872 | R - Residential/Local | A - AC | 27 | 0.82 |
| JENNWY | 10 | JENNY WAY | MARIPOSA AVE | CUL-DE-SAC | 573 | 35 | 21,848 | R - Residential/Local | A - AC | 38 | 4.45 |
| JEROP | 10 | JEROME PL | LARCH ST | CUL-DE-SAC | 345 | 32 | 11,040 | R - Residential/Local | A - AC | 18 | 0 |
| JORDCT | 10 | JORDANNE CT | IDYLLWILD CIR | CUL-DE-SAC | 209 | 36 | 9,242 | R - Residential/Local | A - AC | 80 | 26.42 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|----------------|--------------------|-----------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| JOTRRD | 10 | JOSHUA TREE RD | E. LASSEN AVE | C.L. (184 FT S/O WATERFORD) | 1,300 | 42 | 54,600 | C - Collector | A - AC | 55 | 6.84 |
| JOYLN | 10 | JOY LN | LARCH ST | CUL-DE-SAC | 280 | 32 | 8,960 | R - Residential/Local | A - AC | 63 | 15.99 |
| JOYLN | 20 | JOY LN | OAK VISTA AVE | LARCH ST | 270 | 36 | 9,720 | R - Residential/Local | A - AC | 85 | 29.39 |
| JUDYLN | 10 | JUDY LN | LARCH ST | CUL-DE-SAC | 287 | 32 | 9,184 | R - Residential/Local | A - AC | 71 | 20.61 |
| JUNIST | 10 | JUNIPER ST | NIGHTSONG LN | HOOKER OAK AVE | 619 | 22 | 13,618 | C - Collector | A - AC | 31 | 1.23 |
| JUNIST | 20 | JUNIPER ST | VALLOMBROSA AVE | NIGHTSONG LN | 537 | 36 | 19,332 | R - Residential/Local | A - AC | 77 | 24.34 |
| KAREDR | 10 | KAREN DR | MOSS AVE | CUL-DE-SAC | 668 | 36 | 25,766 | R - Residential/Local | A - AC | 67 | 18.24 |
| KAREDR | 20 | KAREN DR | PALMETTO AVE | MOSS AVE | 1,221 | 32 | 39,072 | R - Residential/Local | A - AC | 45 | 7.29 |
| KAREDR | 30 | KAREN DR | PALMETTO AVE | CUL-DE-SAC | 421 | 32 | 13,472 | C - Collector | A - AC | 75 | 13.47 |
| KENRLN | 10 | KENRICK LN | BEDFORD DR | NOTRE DAME BLVD | 475 | 28 | 13,300 | R - Residential/Local | A - AC | 94 | 33.63 |
| KENTRD | 10 | KENTFIELD RD | E. 1ST AVE | KINGSTON CIR | 771 | 32 | 24,672 | R - Residential/Local | A - AC | 32 | 2.21 |
| KENTRD | 20 | KENTFIELD RD | KINGSTON CIR | BONAIR RD | 397 | 32 | 12,704 | R - Residential/Local | A - AC | 69 | 19.41 |
| KERILN | 10 | KERI LN | MARIPOSA AVE | KIMBERLEE LN | 1,266 | 32 | 40,512 | R - Residential/Local | A - AC | 43 | 6.45 |
| KERNST | 10 | KERN ST | E. 8TH ST | MODOC DR | 1,570 | 33 | 51,810 | C - Collector | A - AC | 73 | 12.65 |
| KESTCT | 10 | KESTREL CT | IDYLLWILD CIR | CUL-DE-SAC | 593 | 32 | 18,976 | R - Residential/Local | A - AC | 58 | 13.35 |
| KEVICR | 10 | KEVIN CT | SHARI LN | CUL-DE-SAC | 406 | 32 | 12,992 | R - Residential/Local | A - AC | 72 | 21.22 |
| KEYSCT | 10 | KEYSTONE CT | HENSHAW AVE | END | 205 | 21 | 4,305 | R - Residential/Local | A - AC | 82 | 27.49 |
| KIMBLN | 10 | KIMBERLEE LN | CEANOTHUS AVE | CUL-DE-SAC | 1,082 | 34 | 38,656 | R - Residential/Local | A - AC | 41 | 5.63 |
| KINGCR | 10 | KINGSTON CIR | KENTFIELD RD | CUL-DE-SAC | 348 | 36 | 14,246 | R - Residential/Local | A - AC | 68 | 18.82 |
| KNGLCT | 10 | KNOTTS GLEN CT | HOLLY AVE | CUL-DE-SAC | 528 | 32 | 19,000 | R - Residential/Local | A - AC | 82 | 27.49 |
| KOBELP | 10 | KOBE LOOP | CACTUS AVE | END | 265 | 17 | 4,505 | R - Residential/Local | A - AC | 84 | 28.78 |
| KONADR | 10 | KONA DR | LUCY WY | MARIGOLD AVE | 740 | 35 | 25,900 | R - Residential/Local | A - AC | 73 | 21.83 |
| KONADR | 20 | KONA DR | MARIGOLD AVE | DUFFY DR | 480 | 35 | 16,800 | R - Residential/Local | A - AC | 69 | 19.41 |
| LACACT | 10 | LA CASA CT | PASEO HACIENDAS CT | END | 275 | 28 | 7,700 | R - Residential/Local | A - AC | 83 | 28.16 |
| LALECT | 10 | LA LEITA CT | E. 12TH ST | LA LEITA CT | 291 | 28 | 8,148 | R - Residential/Local | A - AC | 21 | 0 |
| LALECT | 20 | LA LEITA CT | W. CUL-DE-SAC | E. CUL-DE-SAC | 321 | 28 | 11,306 | R - Residential/Local | A - AC | 54 | 11.32 |
| LAMEDR | 10 | LA MESA DR | NORTH AVE | END | 475 | 36 | 17,100 | R - Residential/Local | A - AC | 22 | 0 |
| LARODR | 10 | LA ROCCO DR | BAR TRIANGLE ST | FLYING V ST | 1,017 | 37 | 37,629 | R - Residential/Local | A - AC | 78 | 24.98 |
| LAVIWAY | 10 | LA VISTA WAY | WARNER ST | END | 250 | 36 | 9,000 | R - Residential/Local | A - AC | 61 | 14.99 |
| LABUAV | 10 | LABURNUM AVE | E. 10TH AVE | E. LINDO AVE | 541 | 36 | 19,476 | R - Residential/Local | A - AC | 82 | 27.49 |
| LABUAV | 20 | LABURNUM AVE | E. 9TH AVE | E. 10TH AVE | 420 | 26 | 10,920 | R - Residential/Local | A - AC | 82 | 27.49 |
| LABUAV | 30 | LABURNUM AVE | E. 8TH AVE | E. 9TH AVE | 414 | 26 | 10,764 | R - Residential/Local | A - AC | 82 | 27.49 |
| LABUAV | 40 | LABURNUM AVE | E. 7TH AVE | E. 8TH AVE | 402 | 24 | 9,648 | R - Residential/Local | A - AC | 82 | 27.49 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|---------------------------|-----------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| LABUAV 50 | | LABURNUM AVE | E. 6TH AVE | E. 7TH AVE | 416 | 24 | 9,984 | R - Residential/Local | A - AC | 53 | 10.84 |
| LABUAV 60 | | LABURNUM AVE | E. 5TH AVE | E. 6TH AVE | 407 | 36 | 14,652 | R - Residential/Local | A - AC | 34 | 3.27 |
| LABUAV 70 | | LABURNUM AVE | E. 2ND AVE | E. 5TH AVE | 1,285 | 36 | 46,260 | R - Residential/Local | A - AC | 21 | 0 |
| LABUAV 80 | | LABURNUM AVE | E. 2ND AVE | E. 1ST AVE | 411 | 36 | 14,796 | R - Residential/Local | A - AC | 38 | 4.41 |
| LABUAV 90 | | LABURNUM AVE | E. 1ST AVE | WASHINGTON AV | 618 | 36 | 22,248 | R - Residential/Local | A - AC | 0 | 0 |
| LACECT 10 | | LACEWING CT | ADMIRAL LN | END | 505 | 28 | 14,140 | R - Residential/Local | A - AC | 73 | 21.83 |
| LAPORD 10 | | LAGUNA POINT RD | IDYLLWILD CIR | CUL-DE-SAC | 469 | 36 | 18,602 | R - Residential/Local | A - AC | 80 | 26.26 |
| LAIRD 10 | | LAIRD DR | W. 12TH AVE | END | 508 | 14 | 7,112 | R - Residential/Local | A - AC | 18 | 0 |
| LAMECT 10 | | LAKE MEAD CT | SILVER LAKE DR | CUL-DE-SAC | 450 | 36 | 18,000 | R - Residential/Local | A - AC | 82 | 27.49 |
| LAKEDR 10 | | LAKEWEST DR | BANNING PARK DR | CUL-DE-SAC | 203 | 37 | 9,154 | R - Residential/Local | A - AC | 78 | 24.98 |
| LAKEDR 20 | | LAKEWEST DR | BRUCE RD | IDYLLWILD CIR | 1,448 | 37 | 53,576 | C - Collector | A - AC | 79 | 15.22 |
| LAKEDR 30 | | LAKEWEST DR | IDYLLWILD CIR | BANNING PARK DR | 1,647 | 37 | 60,939 | C - Collector | A - AC | 76 | 13.9 |
| LAKEDR 40 | | LAKEWEST DR | COIT TOWER WY | BRUCE RD | 183 | 38 | 6,954 | C - Collector | A - AC | 82 | 16.63 |
| LAKEWY 10 | | LAKWOOD WAY | WOODSIDE LN | MANZANITA AVE | 1,190 | 33 | 39,270 | R - Residential/Local | A - AC | 79 | 25.62 |
| LAKOCT 10 | | LAKODA CT | SYCAMORE DR | CUL-DE-SAC | 763 | 28 | 21,364 | R - Residential/Local | A - AC | 70 | 20.01 |
| LANACT 10 | | LANAI CT | KONA DR | CUL-DE-SAC | 347 | 35 | 13,938 | R - Residential/Local | A - AC | 73 | 21.98 |
| LANCTR 10 | | LANCE TERR | BEACHCOMBER COVE | DOLPHIN BEND | 300 | 28 | 8,400 | R - Residential/Local | A - AC | 93 | 33.53 |
| LANCTR 20 | | LANCE TERR | DOLPHIN BEND | END | 424 | 32 | 13,568 | R - Residential/Local | A - AC | 93 | 33.53 |
| LARGST 10 | | LARCH ST | HOOKER OAK AVE | FILBERT AVE | 670 | 36 | 24,120 | C - Collector | A - AC | 75 | 13.47 |
| LARGST 20 | | LARCH ST | PVMT CHNG | HOOKER OAK AVE | 534 | 36 | 19,224 | R - Residential/Local | A - AC | 80 | 26.26 |
| LARGST 30 | | LARCH ST | VALLOMBROSA AVE | PVMT CHNG | 663 | 36 | 23,868 | R - Residential/Local | A - AC | 80 | 26.26 |
| LARELN 10 | | LAREDO LN | NIAGRA WY | END | 123 | 37 | 4,551 | R - Residential/Local | A - AC | 80 | 26.22 |
| LARKLN 10 | | LARKSPUR LN | REDWOOD WY | CUL-DE-SAC | 201 | 35 | 8,828 | R - Residential/Local | A - AC | 67 | 18.24 |
| LATALN 10 | | LATANIA LN | REDWOOD WY | CUL-DE-SAC | 222 | 32 | 7,104 | R - Residential/Local | A - AC | 76 | 23.71 |
| LAURST 10 | | LAUREL ST | E. 12TH ST | E. 18TH ST | 1,306 | 28 | 36,568 | R - Residential/Local | A - AC | 23 | 0 |
| LAURST 20 | | LAUREL ST | E. 18TH ST | DEAD END | 436 | 28 | 12,208 | R - Residential/Local | A - AC | 29 | 1.46 |
| LAURST 30 | | LAUREL ST | E. 21ST ST | N. END | 235 | 18 | 4,230 | R - Residential/Local | A - AC | 82 | 27.49 |
| LAURST 60 | | LAUREL ST | E. 19TH ST | END | 110 | 32 | 3,520 | R - Residential/Local | A - AC | 38 | 4.56 |
| LAVACT 10 | | LAVA CAP CT | IDYLLWILD CIR | CUL-DE-SAC | 199 | 26 | 7,642 | R - Residential/Local | A - AC | 81 | 26.9 |
| LAWLST 10 | | LAWLER ST | ENGLAND ST | MATSON ST | 925 | 22 | 20,350 | R - Residential/Local | A - AC | 82 | 27.53 |
| LAWNDR 10 | | LAWNWOOD DR | W LINDO AVE | END | 540 | 32 | 17,280 | R - Residential/Local | A - AC | 71 | 20.68 |
| LAWTDR 10 | | LAWTON DR | CERES AVE | END | 640 | 36 | 23,040 | R - Residential/Local | A - AC | 55 | 11.85 |
| LAZYLN 10 | | LAZY S LN | 360 W/O STILSON CANYON RD | END | 1,788 | 22 | 39,336 | R - Residential/Local | A - AC | 80 | 26.42 |

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|----------------|-----------------------------|-----------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| LEAFCT | 10 | LEAFWOOD CT | W. 8TH AVE | CUL-DE-SAC | 230 | 36 | 10,080 | R - Residential/Local | A - AC | 29 | 1.46 |
| LEGALN | 10 | LEGACY LN | SIERRA SPRINGS DR | EAST CUL-DE-SAC | 1,260 | 36 | 45,360 | R - Residential/Local | A - AC | 82 | 27.49 |
| LEGALN | 20 | LEGACY LN | SIERRA SPRINGS DR | WEST CUL-DE-SAC | 427 | 36 | 17,172 | R - Residential/Local | A - AC | 80 | 26.22 |
| LEGIAY | 10 | LEGION AVE | CITRUS AVE | ARCADIAN AVE | 620 | 29 | 17,980 | R - Residential/Local | A - AC | 19 | 0 |
| LEGIAY | 20 | LEGION AVE | CITRUS AVE | WARNER ST | 710 | 40 | 28,400 | R - Residential/Local | A - AC | 82 | 27.49 |
| LELACT | 10 | LELAND CT | SIERRA SPRINGS DR | CUL-DE-SAC | 455 | 32 | 16,664 | R - Residential/Local | A - AC | 56 | 12.3 |
| LEORCT | 10 | LEORA CT | 491' E/O ESPLANADE | 546' E/O ESPLANADE | 55 | 36 | 1,980 | R - Residential/Local | A - AC | 71 | 20.77 |
| LEORCT | 20 | LEORA CT | 546' E/O ESPLANADE | CUL-DE-SAC | 270 | 36 | 11,604 | R - Residential/Local | A - AC | 71 | 20.77 |
| LEORCT | 30 | LEORA CT | ESPLANADE | 491' E/O ESPLANADE | 491 | 36 | 17,676 | R - Residential/Local | A - AC | 55 | 11.81 |
| LEVLN | 10 | LEVI LN | LANCE TERR | PENDANT PL | 785 | 28 | 21,980 | R - Residential/Local | A - AC | 93 | 33.53 |
| LEXIDR | 10 | LEXINGTON DR | E. EATON RD | FLORAL AVE | 1,351 | 36 | 48,636 | R - Residential/Local | A - AC | 52 | 10.42 |
| LIBEST | 10 | LIBERATOR ST | BOEING AVE | SIKORSKY AVE | 720 | 38 | 27,360 | R - Residential/Local | A - AC | 21 | 0 |
| LIBEST | 20 | LIBERATOR ST | BOEING AVE | FAIRCHILD AVE | 517 | 48 | 24,816 | R - Residential/Local | A - AC | 73 | 21.83 |
| LIISCT | 10 | LIDO ISLAND CT | CEANOTHUS AVE | END | 250 | 30 | 7,500 | R - Residential/Local | A - AC | 83 | 28.16 |
| LILALN | 10 | LILAC LN | VALLOMBROSA AVE | CUL-DE-SAC | 788 | 35 | 29,373 | R - Residential/Local | A - AC | 74 | 22.45 |
| LINDST | 10 | LINDEN ST | WOODLAND AVE | E. 8TH ST | 571 | 35 | 19,985 | R - Residential/Local | A - AC | 59 | 13.82 |
| LINDST | 20 | LINDEN ST | E. 9TH ST | E. 8TH ST | 308 | 28 | 8,624 | R - Residential/Local | A - AC | 82 | 27.49 |
| LINDST | 30 | LINDEN ST | E. 9TH ST | HUMBOLDT AVE | 575 | 28 | 16,100 | R - Residential/Local | A - AC | 20 | 0 |
| LINDLN | 10 | LINDO LN | PILLSBURY RD | COHASSET RD | 1,220 | 32 | 39,040 | R - Residential/Local | A - AC | 94 | 33.63 |
| LIGAWY | 10 | LIONSGATE WAY | E 8TH ST | LIONSGATE AVE (ROAD WIDENS) | 460 | 22 | 10,120 | R - Residential/Local | A - AC | 94 | 33.63 |
| LIGAWY | 20 | LIONSGATE WAY | LIONSGATE WAY (ROAD WIDENS) | FOREST AVE | 195 | 44 | 8,580 | R - Residential/Local | A - AC | 94 | 33.63 |
| LOBECT | 10 | LOBELIA CT | FLORAL AVE | END | 620 | 32 | 19,840 | R - Residential/Local | A - AC | 82 | 27.53 |
| LOBOWY | 10 | LOBO WAY | ARTESIA | JACKIE DR | 363 | 32 | 11,616 | R - Residential/Local | A - AC | 73 | 21.83 |
| LOCKAV | 10 | LOCKHEED AVE | FORTRESS ST | MARAUDER ST | 1,009 | 40 | 40,360 | R - Residential/Local | A - AC | 77 | 24.34 |
| LOCKAV | 20 | LOCKHEED AVE | FORTRESS ST | END | 282 | 40 | 11,280 | R - Residential/Local | A - AC | 69 | 19.41 |
| LOCUST | 10 | LOCUST ST | E. 12TH ST | E. 16TH ST | 1,234 | 44 | 54,296 | R - Residential/Local | A - AC | 82 | 27.49 |
| LOCUST | 20 | LOCUST ST | E. 16TH ST | E. 20TH ST | 1,118 | 44 | 49,192 | R - Residential/Local | A - AC | 82 | 27.49 |
| LOPLN | 10 | LODGE PINE LN | W. 8TH AVE | CUL-DE-SAC | 740 | 36 | 30,894 | R - Residential/Local | A - AC | 82 | 27.49 |
| LOMBLN | 10 | LOMBARD LN | WHITE AVE | END | 905 | 30 | 27,150 | R - Residential/Local | A - AC | 41 | 5.59 |
| LONDCT | 10 | LONDON CT | SOUTHBURY LN | END | 195 | 30 | 5,850 | R - Residential/Local | A - AC | 80 | 26.22 |
| LONGAV | 10 | LONGFELLOW AVE | E. 1ST AVE | MANZANITA AVE | 956 | 37 | 28,786 | C - Collector | A - AC | 31 | 1.23 |
| LONGDR | 10 | LONGWOOD DR | POTTER RD | BANCROFT DR | 320 | 24 | 7,680 | R - Residential/Local | A - AC | 93 | 33.53 |

Criteria:

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SS1030

MTC StreetSaver

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|---------------|------------------------|----------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| LONGDR | 20 | LONGWOOD DR | BANCROFT DR | END | 430 | 28 | 12,040 | R - Residential/Local | A - AC | 94 | 33.63 |
| LORACT | 10 | LORAYNE CT | W. 4TH AVE | CUL-DE-SAC | 242 | 32 | 9,848 | R - Residential/Local | A - AC | 49 | 9 |
| LORILN | 10 | LORINDA LN | PILLSBURY RD | PORTAL DR | 815 | 30 | 24,450 | R - Residential/Local | A - AC | 93 | 33.53 |
| LORILN | 20 | LORINDA LN | PORTAL DR | COHASSET RD | 530 | 30 | 15,900 | R - Residential/Local | A - AC | 94 | 33.6 |
| LOSSWY | 10 | LOSSE WAY | W LINDO AVE | END | 555 | 34 | 18,870 | R - Residential/Local | A - AC | 80 | 26.22 |
| LOVEAV | 10 | LOVELL AVE | DESCHUTES DR | WHITEWOOD WY | 355 | 30 | 10,650 | R - Residential/Local | A - AC | 58 | 13.35 |
| LOWEDR | 10 | LOWELL DR | HENSHAW AVE | HENSHAW AVE | 815 | 30 | 27,450 | R - Residential/Local | A - AC | 80 | 26.22 |
| LOLACT | 10 | LOWER LAKE CT | CALIFORNIA PARK DR | CUL-DE-SAC | 460 | 32 | 16,738 | C - Collector | A - AC | 81 | 16.15 |
| LUCICT | 10 | LUCIANO CT | PENINSULA DR | END | 310 | 32 | 9,920 | R - Residential/Local | A - AC | 83 | 28.16 |
| LUCKWY | 10 | LUCKIE WAY | SOUTH END | NORTH END | 305 | 36 | 10,980 | R - Residential/Local | A - AC | 27 | 0.82 |
| LUCYWY | 10 | LUCY WAY | CEANOTHUS AVE | PVMT CHNG | 983 | 36 | 35,388 | R - Residential/Local | A - AC | 78 | 24.98 |
| LUCYWY | 20 | LUCY WAY | PVMT CHNG | BRENNI WY | 421 | 36 | 15,156 | R - Residential/Local | A - AC | 81 | 26.9 |
| LUCYWY | 30 | LUCY WAY | CEANOTHUS AVE | END | 570 | 28 | 15,960 | R - Residential/Local | A - AC | 79 | 25.62 |
| LUPIAV | 10 | LUPIN AVE | CERES AVE | E. EATON RD | 728 | 36 | 26,208 | C - Collector | A - AC | 71 | 11.86 |
| LUPIAV | 20 | LUPIN AVE | COHASSET | SAN VERBENA WY | 903 | 35 | 31,605 | C - Collector | A - AC | 22 | 0 |
| LUPIAV | 30 | LUPIN AVE | NORTH AVE | CERES AVE | 1,283 | 36 | 46,188 | C - Collector | A - AC | 49 | 5.18 |
| LUPIAV | 40 | LUPIN AVE | SAN VERBENA WY | NORTH AVE | 404 | 36 | 14,544 | C - Collector | A - AC | 22 | 0 |
| LYNNLN | 10 | LYNN LN | HAWTHORNE AVE | CUL-DE-SAC | 479 | 36 | 18,962 | R - Residential/Local | A - AC | 75 | 23.08 |
| LYNNCR | 10 | LYNNWOOD CIR | TOM POLK AVE | END | 160 | 32 | 5,120 | R - Residential/Local | A - AC | 21 | 0 |
| MACDAV | 10 | MACDONALD AVE | ESPLANADE DR | REED PARK DR | 834 | 36 | 30,024 | R - Residential/Local | A - AC | 18 | 0 |
| MACYAV | 10 | MACY AVE | PALMETTO AVE | NAOMI AVE | 715 | 36 | 25,740 | C - Collector | A - AC | 65 | 9.98 |
| MACYAV | 20 | MACY AVE | PALMETTO AVE | S. END | 514 | 36 | 18,504 | C - Collector | A - AC | 82 | 16.59 |
| MADDCT | 10 | MADDIE CT | DUFFY DR | CUL-DE-SAC | 396 | 34 | 15,332 | R - Residential/Local | A - AC | 77 | 24.34 |
| MADELN | 10 | MADERA LN | PILLSBURY RD | END | 675 | 36 | 24,300 | R - Residential/Local | A - AC | 17 | 0 |
| MADRAV | 10 | MADRONE AVE | HAWTHORNE AVE | N. END | 518 | 36 | 18,648 | C - Collector | A - AC | 54 | 6.55 |
| MADRAV | 20 | MADRONE AVE | HOOKER OAK AVE | HAWTHORNE AVE | 1,134 | 24 | 27,216 | C - Collector | A - AC | 92 | 21.76 |
| MADRAV | 30 | MADRONE AVE | MANZANITA AVE | S. END | 644 | 36 | 23,184 | C - Collector | A - AC | 19 | 0 |
| MADRAV | 40 | MADRONE AVE | VALLOMBROSA AVE | HOOKER OAK AVE | 990 | 28 | 27,720 | C - Collector | A - AC | 93 | 22.25 |
| MAGNAV | 10 | MAGNOLIA AVE | W. FRANCES WILLARD AVE | W. LINCOLN AVE | 225 | 23 | 5,175 | R - Residential/Local | A - AC | 18 | 0 |
| MAGNAV | 20 | MAGNOLIA AVE | W. SACRAMENTO AVE | W. 1ST AVE N/S | 385 | 25 | 9,625 | R - Residential/Local | A - AC | 50 | 9.45 |
| MAGNAV | 30 | MAGNOLIA AVE | W. 1ST AVE | W. 3RD AVE | 867 | 28 | 24,276 | R - Residential/Local | A - AC | 54 | 11.32 |
| MAGNAV | 40 | MAGNOLIA AVE | W. 3RD AVE | W. 4TH AVE | 440 | 36 | 15,840 | R - Residential/Local | A - AC | 0 | 0 |
| MAGNAV | 50 | MAGNOLIA AVE | W. 4TH AVE | W. 5TH AVE | 406 | 36 | 14,616 | R - Residential/Local | A - AC | 82 | 27.49 |

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|---------------|--------------------------------|------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| MAGNAV | 60 | MAGNOLIA AVE | W. 6TH AVE N/S | W. 7TH AVE N/S | 430 | 36 | 15,480 | R - Residential/Local | A - AC | 31 | 2.17 |
| MAGNAV | 70 | MAGNOLIA AVE | W. 7TH AVE N/S | W. 8TH AVE S/S | 415 | 25 | 10,375 | R - Residential/Local | A - AC | 94 | 33.59 |
| MAGNAV | 80 | MAGNOLIA AVE | W. 8TH AVE | W. 11TH AVE | 1,273 | 28 | 35,644 | R - Residential/Local | A - AC | 86 | 29.94 |
| MAGNAV | 90 | MAGNOLIA AVE | W. 6TH AVE | W. 5TH AVE | 440 | 32 | 14,080 | R - Residential/Local | A - AC | 83 | 28.03 |
| MAINST | 10 | MAIN ST | PARK AVE (225 FT S/O HUMBOLDT) | W. 9TH ST N/S | 400 | 52 | 20,800 | A - Arterial | A - AC | 68 | 13.79 |
| MAINST | 20 | MAIN ST | W. 1ST ST N/S | 514 FT S/O MEMORIAL WY | 342 | 45 | 15,390 | C - Collector | A - AC | 60 | 8.32 |
| MAINST | 30 | MAIN ST | W. 5TH ST N/S | W. 1ST ST N/S | 1,360 | 52 | 70,720 | C - Collector | A - AC | 54 | 6.51 |
| MAINST | 40 | MAIN ST | W. 9TH ST N/S | W. 5TH ST S/S | 1,260 | 52 | 65,520 | R - Residential/Local | A - AC | 46 | 7.68 |
| MANCRD | 10 | MANCHESTER RD | E. 5TH AVE | N. CUL-DE-SAC | 398 | 36 | 16,046 | R - Residential/Local | A - AC | 74 | 22.45 |
| MANCRD | 20 | MANCHESTER RD | KENTFIELD RD | E. 5TH AVE | 1,285 | 32 | 41,120 | R - Residential/Local | A - AC | 24 | 0 |
| MANDCT | 10 | MANDALAY CT | ESPLANADE | END | 505 | 27 | 13,635 | R - Residential/Local | A - AC | 82 | 27.49 |
| MANGAV | 10 | MANGROVE AVE | E. 1ST AVE | E. 5TH AVE | 1,702 | 66 | 112,332 | R - Residential/Local | A - AC | 82 | 27.49 |
| MANGAV | 20 | MANGROVE AVE | E. 5TH AVE | E. 9TH AVE | 1,717 | 66 | 113,322 | R - Residential/Local | A - AC | 82 | 27.49 |
| MANGAV | 30 | MANGROVE AVE | E. 9TH AVE | COHASSET RD | 790 | 66 | 52,140 | R - Residential/Local | A - AC | 82 | 27.49 |
| MANGAV | 40 | MANGROVE AVE | PALMETTO AVE | E. 1ST AVE | 1,299 | 66 | 85,734 | A - Arterial | A - AC | 82 | 20.19 |
| MANGAV | 50 | MANGROVE AVE | VALLAMBROSA AVE | PALMETTO AVE | 1,577 | 66 | 104,982 | C - Collector | A - AC | 81 | 16.11 |
| MANGAV | 60 | MANGROVE AVE | WOODLAND AVE | VALLAMBROSA AVE | 467 | 36 | 16,812 | A - Arterial | A - AC | 66 | 12.94 |
| MANNV | 10 | MANNING AVE | GLENSHIRE LN | END | 150 | 20 | 3,000 | R - Residential/Local | A - AC | 80 | 26.26 |
| MANOCR | 10 | MANOR CIR | LAWTON DR | END | 495 | 32 | 15,840 | R - Residential/Local | A - AC | 54 | 11.37 |
| MANSCT | 10 | MANSFIELD CT | HUNTINGTON DR | MARILYN DR | 1,271 | 26 | 33,046 | R - Residential/Local | A - AC | 71 | 20.61 |
| MANSV | 10 | MANSION AVE | ARCADIAN AVE | CITRUS AVE | 630 | 29 | 18,270 | R - Residential/Local | A - AC | 16 | 0 |
| MANAAV | 10 | MANZANITA AVE | CEANOTHUS AVE | MADRONE AVE | 1,351 | 36 | 48,636 | C - Collector | A - AC | 40 | 2.97 |
| MANAAV | 100 | MANZANITA AVE | TOLUMNE DR | HOOKER OAK AV | 2,245 | 30 | 67,350 | A - Arterial | A - AC | 94 | 24.9 |
| MANAAV | 110 | MANZANITA AVE | FLOREAL AVE | MARIPOSA AVE | 1,350 | 40 | 54,000 | C - Collector | A - AC | 71 | 11.86 |
| MANAAV | 20 | MANZANITA AVE | CERES AVE | FLOREAL AVE | 1,234 | 36 | 44,424 | C - Collector | A - AC | 71 | 11.86 |
| MANAAV | 30 | MANZANITA AVE | COHASSET RD | ELLENE AVE | 1,175 | 36 | 42,300 | C - Collector | A - AC | 83 | 17.12 |
| MANAAV | 40 | MANZANITA AVE | ELLENE AVE | MORELAND | 1,460 | 28 | 40,880 | C - Collector | A - AC | 93 | 22.25 |
| MANAAV | 50 | MANZANITA AVE | HOOKER OAK AVE | CENTENIAL AVE | 1,232 | 28 | 34,496 | R - Residential/Local | A - AC | 93 | 33.53 |
| MANAAV | 60 | MANZANITA AVE | JENOOKA LN | TOLUMNE DR | 1,950 | 30 | 58,500 | C - Collector | A - AC | 30 | 1.03 |
| MANAAV | 70 | MANZANITA AVE | MARIGOLD AVE | JENOKE LN | 684 | 32 | 21,888 | C - Collector | A - AC | 30 | 1.03 |
| MANAAV | 80 | MANZANITA AVE | MARIPOSA AVE | CEANOTHUS AVE | 1,688 | 38 | 64,144 | C - Collector | A - AC | 67 | 10.38 |
| MANAAV | 90 | MANZANITA AVE | MORELAND | CERES AVE | 1,820 | 38 | 69,160 | C - Collector | A - AC | 78 | 14.77 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|----------------|-----------------------------|--------------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| MANZCT 10 | | MANZANITA CT | COHASSET RD | SOUTH CUL-DE-SAC | 1,281 | 32 | 40,992 | R - Residential/Local | A - AC | 20 | 0 |
| MANZCT 20 | | MANZANITA CT | COHASSET RD | NORTH CUL-DE-SAC | 1,074 | 32 | 34,368 | R - Residential/Local | A - AC | 24 | 0 |
| MAPLST 10 | | MAPLE ST | W. 4TH ST | W. 5TH ST | 290 | 29 | 8,410 | R - Residential/Local | A - AC | 25 | 0 |
| MARAST 10 | | MARAUDER ST | AIRPARK BLVD | LOCKHEED AVE | 569 | 40 | 22,760 | R - Residential/Local | A - AC | 63 | 16.06 |
| MARAST 20 | | MARAUDER ST | BOEING AVE | AIRPARK BLVD | 1,532 | 36 | 55,152 | R - Residential/Local | A - AC | 40 | 5.23 |
| MARAST 30 | | MARAUDER ST | LOCKHEED AVE | RYAN AVE | 1,583 | 40 | 63,320 | R - Residential/Local | A - AC | 80 | 26.26 |
| MARAST 40 | | MARAUDER ST | PVMT CHNG (690 FT N/O RYAN) | RYAN AVE | 974 | 32 | 31,168 | R - Residential/Local | A - AC | 82 | 27.53 |
| MARAST 50 | | MARAUDER ST | RYAN AVE | PVMT CHNG (690' FROM RYAN AVE) | 690 | 36 | 24,840 | L - Local (7) | A - AC | 76 | 23.71 |
| MARCTT 10 | | MARCHETTI CT | ALMENDIA DR | END | 377 | 36 | 13,572 | R - Residential/Local | A - AC | 80 | 26.22 |
| MARCWY 10 | | MARCI WAY | MARIGOLD AVE | END | 285 | 24 | 6,840 | R - Residential/Local | A - AC | 81 | 26.9 |
| MACICT 10 | | MARCIA CT | MORSEMAN AV | END | 130 | 30 | 3,900 | C - Collector | A - AC | 70 | 11.64 |
| MARGAV 10 | | MARGUERITE AVE | MILDRED AVE | HOLBEN AVE | 499 | 36 | 17,964 | R - Residential/Local | A - AC | 66 | 17.63 |
| MAGDAV 10 | | MARIGOLD AVE | ARCH WY | PVMT CHNG (148 FT N/O KONA) | 1,070 | 52 | 55,640 | C - Collector | A - AC | 67 | 10.38 |
| MAGDAV 20 | | MARIGOLD AVE | EAST AVE | ARCH WY | 882 | 36 | 31,752 | C - Collector | A - AC | 43 | 3.67 |
| MAGDAV 40 | | MARIGOLD AVE | PVMT CHNG (148 FT N/O KONA) | MIDDLETOWN AVE | 1,475 | 35 | 51,625 | C - Collector | A - AC | 71 | 11.86 |
| MAGDAV 30 | | MARIGOLD AVE | MANZANITA AVE (N INT) | EAST AVE | 761 | 36 | 27,396 | C - Collector | A - AC | 41 | 3.36 |
| MARIDR 10 | | MARILYN DR | BAR TRIANGLE ST | MANSFIELD CT | 161 | 17 | 2,737 | R - Residential/Local | A - AC | 67 | 18.24 |
| MARIDR 20 | | MARILYN DR | PARKWAY VILLAGE DR | BAR TRIANGLE ST | 1,226 | 32 | 39,232 | R - Residential/Local | A - AC | 80 | 26.26 |
| MARIAV 10 | | MARIPOSA AVE | EAST AVE | RAVENSHOE WY | 522 | 38 | 19,836 | R - Residential/Local | A - AC | 80 | 26.33 |
| MARIAV 20 | | MARIPOSA AVE | MANZANITA AVE N/S | EAST AVE | 2,836 | 36 | 102,096 | R - Residential/Local | A - AC | 50 | 9.49 |
| MARIAV 30 | | MARIPOSA AVE | TIERRA ROSA LN | WANDERER LN | 540 | 36 | 19,440 | A - Arterial | A - AC | 66 | 12.98 |
| MARIAV 40 | | MARIPOSA AVE | RAVENSHOE WY | TIERRA ROSA LN | 942 | 36 | 33,912 | R - Residential/Local | A - AC | 80 | 26.26 |
| MARIAV 50 | | MARIPOSA AVE | ROAD NARROWS | END PAVEMENT | 220 | 17 | 3,740 | L - Local (7) | A - AC | 77 | 24.34 |
| MARIAV 60 | | MARIPOSA AVE | EATON RD | LASSEN AVE | 1,000 | 30 | 30,000 | L - Local (7) | A - AC | 92 | 32.87 |
| MARJAV 10 | | MARJORIE AVE | CITY LIMIT | CUL-DE-SAC | 587 | 36 | 22,460 | R - Residential/Local | A - AC | 14 | 0 |
| MARLCT 10 | | MARLIN CT | MARIGOLD AVE | CUL-DE-SAC | 558 | 32 | 17,856 | R - Residential/Local | A - AC | 79 | 25.62 |
| MARSWY 10 | | MARS WAY | W 10TH AVE | W 9TH AVE | 415 | 26 | 10,790 | R - Residential/Local | A - AC | 82 | 27.49 |
| MARSWY 20 | | MARS WAY | W. 9TH AVE | END | 290 | 16 | 4,640 | R - Residential/Local | P - PCC | 89 | 66.36 |
| MARVCR 10 | | MARVIN CIR | MARVIN WY | CUL-DE-SAC | 154 | 36 | 7,262 | R - Residential/Local | A - AC | 75 | 23.08 |
| MARVWY 10 | | MARVIN WAY | KAREN DR | MOSS AVE | 650 | 36 | 23,400 | C - Collector | A - AC | 73 | 12.65 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|--------------------------------|--------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| MARYLN | 10 | MARYDITH LN | W. CUL-DE-SAC | E. CUL-DE-SAC | 560 | 32 | 22,129 | R - Residential/Local | A - AC | 66 | 17.67 |
| MATST | 10 | MATSON ST | ENGLAND ST | S/ END | 846 | 22 | 18,612 | R - Residential/Local | A - AC | 79 | 25.62 |
| MAYEDR | 10 | MAYETTE DR | ALMENDIA DR | W. SACRAMENTO AVE | 475 | 36 | 17,100 | R - Residential/Local | A - AC | 80 | 26.22 |
| MCCALN | 10 | MC CARTHUR LN | W 12TH AVE | END | 315 | 32 | 10,080 | R - Residential/Local | A - AC | 74 | 22.41 |
| MCGIST | 10 | MCGIE ST | ENGLAND | BELGIUM AVE | 200 | 20 | 4,000 | R - Residential/Local | A - AC | 82 | 27.7 |
| MCINAV | 10 | MCINTOSH AVE | DAYTON | WEST END | 644 | 32 | 20,608 | R - Residential/Local | A - AC | 50 | 9.45 |
| MCKILN | 10 | MCKINLEY LN | CERES AVE | CUL-DE-SAC | 342 | 32 | 10,944 | R - Residential/Local | A - AC | 62 | 15.45 |
| MEADR | 10 | MEADOW RD | SOUTH END | W 8TH AVE | 825 | 36 | 29,700 | R - Residential/Local | A - AC | 94 | 33.59 |
| MEARDR | 20 | MEADOW RD | W 8TH AVE | GRASS CT | 610 | 38 | 23,180 | R - Residential/Local | A - AC | 47 | 8.11 |
| MEARDR | 30 | MEADOW RD | GRASS CT | W 11TH AVE | 575 | 38 | 21,850 | R - Residential/Local | A - AC | 75 | 23.04 |
| MEADCT | 10 | MEADOWHAWK CT | LAKODA CT | CUL-DE-SAC | 183 | 28 | 5,124 | R - Residential/Local | A - AC | 67 | 18.24 |
| MECHST | 10 | MECHOOPDA ST | W. SACRAMENTO AVE | W. 1ST AVE | 428 | 32 | 13,696 | R - Residential/Local | A - AC | 69 | 19.37 |
| MEGAWY | 10 | MEGAN WAY | TUOLUMNE DR | NOYO CT | 194 | 36 | 6,984 | R - Residential/Local | A - AC | 82 | 27.53 |
| MEMOWY | 10 | MEMORIAL WAY | ESPLANADE DR | VALLOMBROSA AVE | 1,300 | 54 | 70,200 | R - Residential/Local | A - AC | 68 | 18.78 |
| MENLWY | 10 | MENLO WAY | ESPLANADE DR | AUTUMN CREEK APT. | 358 | 37 | 13,246 | R - Residential/Local | A - AC | 20 | 0 |
| MERLCT | 10 | MERLE CT | MODOC DR | CUL-DE-SAC | 192 | 36 | 8,797 | R - Residential/Local | A - AC | 55 | 11.85 |
| MEVECT | 10 | MESA VERDE CT | VOLCANO DR | END | 530 | 30 | 15,900 | R - Residential/Local | A - AC | 81 | 26.86 |
| METAWY | 10 | METALMARK WAY | VISTAMONT WAY | PARNASSIAN AVE | 350 | 26 | 9,100 | R - Residential/Local | A - AC | 84 | 28.78 |
| MEYEST | 10 | MEYERS ST | PARK AVE | PVMT CHG(178FT W/O BELLARMINI) | 1,550 | 40 | 62,000 | R - Residential/Local | A - AC | 34 | 3.27 |
| MEYEST | 20 | MEYERS ST | PVM CHG(178 FT W/O BELLARMINI) | IVY ST | 287 | 40 | 11,480 | R - Residential/Local | A - AC | 22 | 0 |
| MICRCT | 10 | MIDDLE CREEK CT | SIERRA SPRINGS DR | END | 534 | 32 | 17,088 | R - Residential/Local | A - AC | 82 | 27.49 |
| MIDDV | 10 | MIDDLETOWN AVE | MARIGOLD AVE | ST LAURENCE AVE | 510 | 36 | 18,360 | R - Residential/Local | A - AC | 77 | 24.34 |
| MIDDV | 20 | MIDDLETOWN AVE | ST LAURENCE AVE | END | 585 | 36 | 21,060 | R - Residential/Local | A - AC | 80 | 26.26 |
| MIDWAY | 10 | MIDWAY | E. PARK AVE | CITY LIMITS | 1,985 | 37 | 73,445 | R - Residential/Local | A - AC | 54 | 11.39 |
| MILDV | 10 | MILDRED AVE | MARGUERITE AVE | E. 1ST AVE | 854 | 36 | 30,744 | R - Residential/Local | A - AC | 66 | 17.63 |
| MICRDR | 10 | MILL CREEK DR | CLEAR LAKE DR | CUL-DE-SAC | 1,700 | 36 | 69,400 | R - Residential/Local | A - AC | 75 | 23.04 |
| MILLST | 10 | MILL ST | E. 12TH ST | HUMBOLDT AVE | 656 | 28 | 18,368 | R - Residential/Local | A - AC | 26 | 0.44 |
| MILLAV | 10 | MILLER AVE | CHICO RIVER RD | CAMPER LN | 590 | 21 | 12,390 | C - Collector | A - AC | 66 | 10.19 |
| MINDCT | 10 | MINDY CT | FOREST CREEK CIR | CUL-DE-SAC | 131 | 36 | 6,774 | R - Residential/Local | A - AC | 55 | 11.85 |
| MIONWY | 10 | MIONE WAY | E. 1ST AVE | CUL-DE-SAC (S/END) | 193 | 32 | 6,176 | C - Collector | A - AC | 95 | 23.11 |
| MIONWY | 20 | MIONE WAY | E. 1ST AVE | CUL-DE-SAC | 200 | 32 | 8,418 | C - Collector | A - AC | 95 | 23.11 |

Section PC/IRSL Listing

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|----------------------|--------------------------------|-------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| MIRABL 10 | | MISSION RANCH BLVD | ESPLANADE DR #400 | MISSION RANCH APTS. #400 | 1,439 | 36 | 51,804 | R - Residential/Local | A - AC | 80 | 26.22 |
| MIRABL 20 | | MISSION RANCH BLVD | MISSION RANCH APT. #400 | HOLLY AVE | 1,350 | 35 | 47,250 | C - Collector | A - AC | 80 | 15.64 |
| MSAFCR 10 | | MISSION SANTA FE CIR | MISSION SANTA FE W/S | MISSION SANTA FE E/S | 1,276 | 28 | 35,728 | R - Residential/Local | A - AC | 82 | 27.49 |
| MODODR 10 | | MODOC DR | ALPINE ST | KERN ST | 1,130 | 33 | 37,290 | R - Residential/Local | A - AC | 70 | 20.01 |
| MODODR 20 | | MODOC DR | KERN ST | FOREST AVE | 1,167 | 33 | 38,511 | R - Residential/Local | A - AC | 35 | 3.31 |
| MONCDR 10 | | MONTCLAIR DR | MANZANITA AVE | CUL-DE-SAC | 500 | 36 | 19,328 | R - Residential/Local | A - AC | 10 | 0 |
| MOCTAV 10 | | MONTECITO AVE | 311 FT N/MISSION RANCH | 180 FT S/MISSION SANTA FE CIR | 793 | 32 | 25,376 | R - Residential/Local | A - AC | 82 | 27.49 |
| MONTWY 10 | | MONTEGO WAY | GLENWOOD AVE | RIVER WOOD LOOP (North) | 490 | 18 | 8,820 | R - Residential/Local | A - AC | 94 | 33.69 |
| MONTST 10 | | MONTEREY ST | E LASSEN AVE | CAPISTRANO PL | 500 | 36 | 18,000 | R - Residential/Local | A - AC | 16 | 0 |
| MONTST 20 | | MONTEREY ST | CAPISTRANO PL | PANAMA AVE | 775 | 36 | 27,900 | R - Residential/Local | A - AC | 20 | 0 |
| MONTAV 10 | | MONTICELLO LN | CITY LIMIT(130FT S/O CAVALIER) | VAIL DR | 520 | 28 | 14,560 | R - Residential/Local | A - AC | 80 | 26.22 |
| MONYST 10 | | MONTREAL LN | VALLEY FORGE DR | END | 335 | 32 | 10,720 | R - Residential/Local | A - AC | 80 | 26.27 |
| MORADR 10 | | MORAGA DR | MANZANITA AVE (W) | MANZANITA AVE (E) | 1,600 | 36 | 57,600 | R - Residential/Local | A - AC | 5 | 0 |
| MORED 10 | | MORELAND DR | MANZANITA AVE (W) | MANZANITA AVE (E) | 740 | 36 | 26,640 | R - Residential/Local | A - AC | 18 | 0 |
| MORGDR 10 | | MORGAN DR | GOODMAN AVE | JOSHUA TREE RD | 710 | 28 | 19,880 | R - Residential/Local | A - AC | 83 | 28.17 |
| MORRLN 10 | | MORROW LN | NOTRE DAME BLVD | W. END | 368 | 45 | 16,560 | C - Collector | A - AC | 23 | 0 |
| MORRLN 20 | | MORROW LN | NOTRE DAME BLVD | ZANELLA WY | 854 | 32 | 27,328 | C - Collector | A - AC | 59 | 7.9 |
| MORRLN 30 | | MORROW LN | ZANELLA WY | E. END | 1,464 | 28 | 40,992 | C - Collector | A - AC | 12 | 0 |
| MORSV 20 | | MORSEMAN AVE | HALIE CT | E EATON RD | 1,245 | 32 | 39,840 | C - Collector | A - AC | 24 | 0 |
| MOSSAV 10 | | MOSS AVE | HAWTHORNE AVE | KAREN DR | 1,039 | 36 | 37,404 | C - Collector | A - AC | 29 | 0.83 |
| MOVIAY 10 | | MOUNTAIN VIEW AVE | CRISTER AVE | MADRONE AVE | 1,608 | 28 | 45,024 | R - Residential/Local | A - AC | 62 | 15.46 |
| MOYEWY 10 | | MOYER WAY | W LINDO AVE | W 11TH AVE | 1,723 | 40 | 68,920 | A - Arterial | A - AC | 72 | 15.55 |
| MTWHCT 10 | | MT WHITNEY CT | YELLOWSTONE DR | SOUTH CUL-DE-SAC | 340 | 36 | 14,040 | R - Residential/Local | A - AC | 80 | 26.22 |
| MTWHCT 20 | | MT WHITNEY CT | YELLOWSTONE DR | NORTH CUL-DE-SAC | 250 | 36 | 10,800 | R - Residential/Local | A - AC | 82 | 27.49 |
| MULBST 10 | | MULBERRY ST | E. 16TH ST | E. 20TH ST | 1,185 | 44 | 52,140 | R - Residential/Local | A - AC | 29 | 1.47 |
| MULBST 20 | | MULBERRY ST | PINE ST | E. 16TH ST | 648 | 44 | 28,512 | R - Residential/Local | A - AC | 51 | 9.91 |
| MULBST 50 | | MULBERRY ST | E. 21ST ST | END | 359 | 38 | 13,642 | R - Residential/Local | A - AC | 30 | 1.82 |
| MYRTAV 10 | | MYRTLE AVE | 890' W/O NORTH AVE | NORTH AVE | 890 | 36 | 32,040 | R - Residential/Local | A - AC | 94 | 33.63 |
| MYRTAV 20 | | MYRTLE AVE | MANZANITA AVE | 890' N/O NORTH AVE | 525 | 25 | 13,125 | C - Collector | A - AC | 24 | 0 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|-------------------------------|-------------------------------|--------|-------|--------|---------------------------|--------------|-------------|----------------|
| MYSTRN | 10 | MYSTERY RUN | BRUSH CREEK LN | WINDHAM WAY | 575 | 32 | 18,400 | R - Residential/Local | A - AC | 94 | 33.77 |
| NCEDST | 10 | N CEDAR ST | W. SACRAMENTO AVE | W. 4TH AVE | 1,750 | 30 | 52,500 | R - Residential/Local | A - AC | 11 | 0 |
| NCHEST | 10 | N CHERRY ST | PVMT CHNG (470 FT N/O W. 4TH) | W. 8TH AVE | 1,327 | 36 | 47,772 | R - Residential/Local | A - AC | 26 | 0.29 |
| NCHEST | 20 | N CHERRY ST | W. 4TH AVE | PVMT CHNG (470 FT N/O W. 4TH) | 470 | 36 | 16,920 | R - Residential/Local | A - AC | 53 | 11.03 |
| NANCLN | 10 | NANCY LN | HAWTHORNE AVE | CUL-DE-SAC | 320 | 32 | 12,258 | C - Collector | A - AC | 22 | 0 |
| NANCLN | 20 | NANCY LN | HAWTHORNE AVE | N. CUL-DE-SAC | 479 | 36 | 18,962 | C - Collector | A - AC | 17 | 0 |
| NAOMAV | 10 | NAOMI AVE | MILDRED AVE | HOLBEN AVE | 495 | 36 | 17,820 | R - Residential/Local | A - AC | 68 | 18.78 |
| NAOADR | 10 | NATIVE OAK DR | CORK OAK WAY | YOSEMITE DR | 615 | 26 | 15,990 | R - Residential/Local | A - AC | 0 | 0 |
| NAOADR | 20 | NATIVE OAK DR | CORK OAK WAY | BRUCE RD | 2,435 | 26 | 63,310 | RI - Rural Interstate (1) | A - AC | 0 | 0 |
| NATOCT | 10 | NATOMA CT | MILL ST | NATOMA CT | 335 | 28 | 9,380 | R - Residential/Local | A - AC | 19 | 0 |
| NATOCT | 20 | NATOMA CT | N. CUL-DE-SAC | S. CUL-DE-SAC | 360 | 28 | 12,398 | R - Residential/Local | A - AC | 37 | 4.02 |
| NAVADR | 10 | NAVARRO DR | EAST AVE | ALBION CT | 998 | 36 | 35,928 | R - Residential/Local | A - AC | 78 | 24.99 |
| NEDOAV | 20 | NEAL DOW AVE | E. 1ST AVE | PALMETTO AVE | 1,000 | 37 | 37,000 | R - Residential/Local | A - AC | 92 | 32.87 |
| NEDOAV | 30 | NEAL DOW AVE | E. 1ST AVE | E. 5TH AVE | 1,450 | 37 | 53,650 | R - Residential/Local | A - AC | 68 | 18.83 |
| NEDOAV | 40 | NEAL DOW AVE | PALMETTO AVE | HILL VIEW WAY | 343 | 30 | 10,290 | C - Collector | A - AC | 86 | 18.79 |
| NEDOAV | 50 | NEAL DOW AVE | E 5TH AVE | E LINDO AVE | 975 | 36 | 35,100 | L - Local (7) | A - AC | 59 | 13.87 |
| NELNST | 10 | NELSON ST | E. 11TH ST | E. 12TH ST | 344 | 32 | 11,008 | R - Residential/Local | A - AC | 82 | 27.49 |
| NEVACT | 10 | NEVADILLO CT | VIA MISSION DR | CUL-DE-SAC | 282 | 32 | 9,024 | R - Residential/Local | A - AC | 66 | 17.68 |
| NEDACR | 10 | NEW DAWN CIR | WEBSTER DR | WEBSTER DR | 1,878 | 37 | 69,486 | R - Residential/Local | A - AC | 75 | 23.04 |
| NEHEWY | 10 | NEW HEATHER WAY | BLACK WALNUT WY N/S | PENSTEMON WY S/S | 530 | 32 | 19,064 | R - Residential/Local | A - AC | 82 | 27.49 |
| NEWPDR | 10 | NEWPORT DR | PRAIRIE CREEK DR | SIERRA SPRINGS DR | 742 | 36 | 26,712 | R - Residential/Local | A - AC | 80 | 26.22 |
| NEWPDR | 20 | NEWPORT DR | SIERRA SPRINGS DR | CUL-DE-SAC | 437 | 36 | 17,532 | R - Residential/Local | A - AC | 82 | 27.49 |
| NIAGWY | 10 | NIAGARA WAY | PARKHURST ST | END | 598 | 37 | 22,126 | R - Residential/Local | A - AC | 68 | 18.78 |
| NICOLN | 10 | NICOLE LN | MADRONE AVE | CUL-DE-SAC | 1,331 | 36 | 49,634 | R - Residential/Local | A - AC | 36 | 3.69 |
| NICALN | 10 | NILES CANYON LN | ROGUE RIVER DR | END | 275 | 32 | 8,800 | R - Residential/Local | A - AC | 94 | 33.59 |
| NOELCT | 10 | NOEL CT | W 12TH AVE | END | 290 | 36 | 10,440 | R - Residential/Local | A - AC | 30 | 1.67 |
| NORHWY | 10 | NORD HWY | ESPLANADE | END OF BRIDGE OVER CREEK | 2,990 | 32 | 95,680 | C - Collector | A - AC | 48 | 4.88 |
| NORMAV | 10 | NORMAL AVE | W. 10TH ST N/S | END (N/O 10TH ST) | 305 | 45 | 13,725 | R - Residential/Local | A - AC | 53 | 10.85 |
| NORMAV | 20 | NORMAL AVE | W. 10TH ST S/S | W. 14TH ST N/S | 1,490 | 45 | 67,050 | R - Residential/Local | A - AC | 52 | 10.38 |
| NORMAV | 30 | NORMAL AVE | W. 14TH ST S/S | W. 16TH ST N/S | 688 | 25 | 17,200 | R - Residential/Local | A - AC | 19 | 0 |
| NORMAV | 40 | NORMAL AVE | W. 16TH ST S/S | W. 22TH ST N/S | 1,918 | 25 | 47,950 | R - Residential/Local | A - AC | 20 | 0 |

Section PCI/RSL Listing

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|-------------------------------|-------------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| NORMAV 50 | | NORMAL AVE | W. 2TH ST S/S | W. 5TH ST N/S | 882 | 52 | 45,864 | R - Residential/Local | A - AC | 22 | 0 |
| NORMAV 60 | | NORMAL AVE | W. 5TH ST S/S | W. 9TH ST N/S | 1,148 | 52 | 59,696 | R - Residential/Local | A - AC | 56 | 12.3 |
| NORTAV 10 | | NORTH AVE | PELICAN PARK DR | MYRTLE AVE | 620 | 35 | 21,700 | C - Collector | A - AC | 96 | 23.75 |
| NORTAV 20 | | NORTH AVE | EAST AVE | VIA VERONA | 785 | 40 | 31,400 | C - Collector | A - AC | 31 | 1.24 |
| NORTAV 30 | | NORTH AVE | LUPIN AVE | TURNBRIDGE WELLES | 440 | 32 | 14,080 | C - Collector | A - AC | 68 | 10.75 |
| NORTAV 40 | | NORTH AVE | MANZANITA AVE | PELICAN PARK DR | 820 | 35 | 28,700 | C - Collector | A - AC | 52 | 6 |
| NORTAV 50 | | NORTH AVE | MYRTLE AVE | COTTAGE AVE | 930 | 35 | 32,550 | C - Collector | A - AC | 96 | 23.75 |
| NORTAV 60 | | NORTH AVE | VERDE DR | LUPIN AVE | 858 | 37 | 31,746 | C - Collector | A - AC | 39 | 2.75 |
| NORTAV 70 | | NORTH AVE | VIA VERONA DR | VERDE DR | 989 | 37 | 36,593 | C - Collector | A - AC | 39 | 2.75 |
| NORTAV 80 | | NORTH AVE | COTTAGE AVE | EAST AVE | 560 | 38 | 21,280 | C - Collector | A - AC | 20 | 0 |
| NOVACT 10 | | NORTH VALLEY CT | EAST AVE | END | 405 | 28 | 11,340 | R - Residential/Local | A - AC | 83 | 28.17 |
| NODABL 10 | | NOTRE DAME BLVD | KENRICK LN | EMERSON WAY | 700 | 40 | 28,000 | A - Arterial | A - AC | 94 | 24.9 |
| NODABL 20 | | NOTRE DAME BLVD | E. 20TH ST | JASPER DR | 1,333 | 60 | 79,980 | A - Arterial | A - AC | 54 | 8.35 |
| NODABL 30 | | NOTRE DAME BLVD | FOREST AVE | SKYWAY | 1,050 | 60 | 63,000 | A - Arterial | A - AC | 87 | 22.49 |
| NODABL 40 | | NOTRE DAME BLVD | JASPER DR | FOREST AVE | 1,971 | 60 | 118,260 | R - Residential/Local | A - AC | 44 | 6.88 |
| NODABL 50 | | NOTRE DAME BLVD | PVMT CHNG (948 FT S/O SKYWAY) | END (NEIGHBORHOOD CHURCH) | 2,336 | 36 | 84,096 | C - Collector | A - AC | 49 | 5.19 |
| NODABL 60 | | NOTRE DAME BLVD | SKYWAY | PVMT CHNG (948 FT S/O SKYWAY) | 948 | 42 | 39,816 | A - Arterial | A - AC | 62 | 11.36 |
| NODABL 70 | | NOTRE DAME BLVD | HUMBOLDT RD | SOUTH END | 1,845 | 42 | 77,490 | C - Collector | A - AC | 83 | 17.13 |
| NOYOCT 10 | | NOYO CT | ALBION CT | CUL-DE-SAC | 918 | 36 | 34,766 | R - Residential/Local | A - AC | 78 | 24.99 |
| OAKDR 10 | | OAK DR | WHITE AVE | WHITE AVE | 1,515 | 32 | 48,480 | R - Residential/Local | A - AC | 15 | 0 |
| OALAAV 10 | | OAK LAWN AVE | W. SACRAMENTO AVE | CITY LIMITS | 460 | 28 | 12,880 | R - Residential/Local | A - AC | 37 | 4.18 |
| OAKMCT 10 | | OAK MANOR CT | VALOMBROSA AVE | END | 380 | 30 | 11,400 | L - Local (7) | A - AC | 78 | 24.99 |
| OAMICT 10 | | OAK MILL CT | BLACK OAK DR | CUL-DE-SAC | 247 | 36 | 10,692 | R - Residential/Local | A - AC | 81 | 26.76 |
| OAPAAV 10 | | OAK PARK AVE | W. 1ST ST | 820 FT FROM W. 1ST ST | 820 | 36 | 29,520 | C - Collector | A - AC | 92 | 21.75 |
| OAPAAV 20 | | OAK PARK AVE | 820 FT FROM W. 1ST ST | CITY LIMITS | 350 | 36 | 12,600 | R - Residential/Local | A - AC | 8 | 0 |
| OAKST 10 | | OAK ST | W. 1ST ST | W. 5TH ST N/S | 1,283 | 36 | 46,188 | R - Residential/Local | A - AC | 82 | 27.49 |
| OAKST 20 | | OAK ST | W. 5TH ST S/S | W. 7TH ST N/S | 620 | 36 | 22,320 | R - Residential/Local | A - AC | 18 | 0 |
| OAKST 30 | | OAK ST | W. 7TH ST S/S | W. 9TH ST N/S | 620 | 36 | 22,320 | R - Residential/Local | A - AC | 19 | 0 |
| OAVIAV 10 | | OAK VISTA AVE | DORIS WY | PARVIEW LN | 765 | 32 | 24,480 | R - Residential/Local | A - AC | 18 | 0 |
| OAVIAV 20 | | OAK VISTA AVE | MADRONE AVE | DORIS WY | 438 | 24 | 10,512 | C - Collector | A - AC | 14 | 0 |
| OAKWY 10 | | OAK WAY | AUTUMWOOD DR W/S | GLENWOOD AVE | 1,495 | 32 | 47,840 | R - Residential/Local | A - AC | 52 | 10.38 |
| OAKWY 20 | | OAK WAY | NORD AVE | 157 FT W/O ADLAR CT | 915 | 36 | 32,940 | R - Residential/Local | A - AC | 80 | 26.22 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|--------------|-----------------------------|------------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| OAKWY | 30 | OAK WAY | AUTMWOOD DR | 157 FT W/O ADLAR CT | 810 | 30 | 24,300 | R - Residential/Local | A - AC | 61 | 14.88 |
| OAKDST | 10 | OAKDALE ST | W. 11TH ST | W. 14TH ST N/S | 1,034 | 25 | 25,850 | R - Residential/Local | A - AC | 22 | 0 |
| OAKDST | 20 | OAKDALE ST | W. 14TH ST | W. 15TH ST | 323 | 25 | 8,075 | R - Residential/Local | A - AC | 18 | 0 |
| OAKDST | 30 | OAKDALE ST | W. 15TH ST | W. 16TH ST | 332 | 25 | 8,300 | R - Residential/Local | A - AC | 29 | 1.47 |
| OAKDST | 40 | OAKDALE ST | W. 16TH ST | W. 17TH ST | 300 | 25 | 7,500 | R - Residential/Local | A - AC | 19 | 0 |
| OAKDST | 50 | OAKDALE ST | W. 17TH ST | W. 18TH ST | 300 | 25 | 7,500 | R - Residential/Local | A - AC | 82 | 27.49 |
| OLEAAV | 10 | OLEANDER AVE | E. 1ST AVE | E. 3TH AVE | 840 | 37 | 31,080 | R - Residential/Local | A - AC | 82 | 27.49 |
| OLEAAV | 20 | OLEANDER AVE | E. 3TH AVE | E. 5TH AVE | 858 | 37 | 31,746 | R - Residential/Local | A - AC | 80 | 26.22 |
| OLEAAV | 30 | OLEANDER AVE | E. 5TH AVE | E. 6TH AVE | 407 | 37 | 15,059 | R - Residential/Local | A - AC | 66 | 17.63 |
| OLEAAV | 40 | OLEANDER AVE | E. 6TH AVE | E. 7TH AVE | 407 | 22 | 8,954 | R - Residential/Local | A - AC | 94 | 33.59 |
| OLEAAV | 50 | OLEANDER AVE | E. 7TH AVE | E. 9TH AVE | 845 | 22 | 18,590 | R - Residential/Local | A - AC | 86 | 29.94 |
| OLEAAV | 60 | OLEANDER AVE | E. 9TH AVE | E. 10TH AVE | 398 | 16 | 6,368 | R - Residential/Local | A - AC | 93 | 33.34 |
| OLEAAV | 70 | OLEANDER AVE | E. FRANCES WILLARD | E. 1ST AVE | 1,135 | 43 | 48,805 | C - Collector | A - AC | 33 | 1.61 |
| OLEAAV | 80 | OLEANDER AVE | MEMORIAL WY | E. FRANCES WILLARD | 841 | 43 | 36,163 | R - Residential/Local | A - AC | 47 | 8.11 |
| OLIVST | 10 | OLIVE ST | E. 12TH ST | CUL-DE-SAC | 584 | 28 | 17,850 | R - Residential/Local | A - AC | 82 | 27.49 |
| OLIVST | 20 | OLIVE ST | E. 3RD ST | E. 7TH ST | 1,176 | 37 | 43,512 | R - Residential/Local | A - AC | 56 | 12.3 |
| OLIVST | 30 | OLIVE ST | E. 7TH ST | HUMBOLDT AVE | 1,052 | 46 | 48,392 | R - Residential/Local | A - AC | 30 | 1.82 |
| OLYMLN | 10 | OLYMPUS LN | E. EATON RD | CUL-DE-SAC | 172 | 32 | 5,504 | R - Residential/Local | A - AC | 49 | 9.05 |
| ORANST | 10 | ORANGE ST | W. 2ND ST | W. 5TH ST N/S | 953 | 52 | 49,556 | R - Residential/Local | A - AC | 66 | 17.63 |
| ORANST | 20 | ORANGE ST | W. 5TH ST S/S | W. DAYTON RD | 1,477 | 52 | 76,804 | R - Residential/Local | A - AC | 82 | 27.49 |
| ORCHDR | 10 | ORCHARD DR | ESPLANADE DR | END | 469 | 42 | 19,698 | R - Residential/Local | A - AC | 58 | 13.31 |
| ORCHLN | 10 | ORCHARD LN | FLORAL AVE | CUL-DE-SAC | 519 | 34 | 19,514 | R - Residential/Local | A - AC | 4 | 0 |
| ORIENT | 10 | ORIENT ST | E. 1ST ST | E. 5TH ST | 750 | 46 | 34,500 | R - Residential/Local | A - AC | 34 | 3.27 |
| ORIENT | 20 | ORIENT ST | E. 5TH ST | E. 8TH ST | 944 | 46 | 43,424 | R - Residential/Local | A - AC | 27 | 0.78 |
| ORIENT | 30 | ORIENT ST | E. 8TH ST | HUMBOLDT AVE | 600 | 46 | 27,600 | R - Residential/Local | A - AC | 41 | 5.59 |
| OROVAV | 10 | ORVILLE AVE | W. 8TH ST S/S | PARK AVE (225' S/O HUMBOLDT) | 780 | 36 | 28,080 | C - Collector | A - AC | 45 | 4.08 |
| OTTEDR | 10 | OTTERSON DR | HEGAN LN | CUL-DE-SAC | 2,530 | 40 | 102,697 | C - Collector | A - AC | 81 | 16.11 |
| OVERCT | 10 | OVERSEER CT | HUMBOLDT RD | END | 355 | 28 | 9,940 | R - Residential/Local | A - AC | 97 | 34.13 |
| OXFOWY | 10 | OXFORD WAY | PARKHURST ST (WEST SEGMENT) | END | 104 | 37 | 3,848 | R - Residential/Local | A - AC | 80 | 26.22 |
| OXFOWY | 20 | OXFORD WAY | PARKHURST ST (EAST SEGMENT) | END | 80 | 37 | 2,960 | R - Residential/Local | A - AC | 82 | 27.49 |
| PALIDR | 10 | PALISADES DR | YOSEMITE DR | END PVMT CHNG | 562 | 36 | 20,232 | C - Collector | A - AC | 63 | 9.35 |

Section PCI/RSL Listing

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|--------------------|-------------------------------|---------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| PALMAV 10 | | PALM AVE | E. SACRAMENTO AVE | E. 1ST AVE | 300 | 36 | 10,800 | C - Collector | A - AC | 17 | 0 |
| PALMAV 20 | | PALM AVE | E. 1ST AVE | E. 3RD AVE | 827 | 22 | 18,194 | C - Collector | A - AC | 22 | 0 |
| PALMAV 30 | | PALM AVE | 3RD AVE | 5TH AVE | 887 | 22 | 19,514 | C - Collector | A - AC | 82 | 16.59 |
| PALMAV 40 | | PALM AVE | E. 5TH AVE | E. 7TH AVE | 858 | 22 | 18,876 | C - Collector | A - AC | 82 | 16.59 |
| PALMAV 50 | | PALM AVE | E. 7TH AVE | E. 9TH AVE | 847 | 22 | 18,634 | C - Collector | A - AC | 82 | 16.59 |
| PALMAV 60 | | PALM AVE | E. 9TH AVE | E. 10TH AVE | 400 | 22 | 8,800 | C - Collector | A - AC | 82 | 16.59 |
| PALMAV 70 | | PALM AVE | E. 10TH AVE | E. LINDO AVE | 520 | 22 | 11,440 | C - Collector | A - AC | 82 | 16.59 |
| PAMTAV 10 | | PALMETTO AVE | HWY 99 OVERPASS W/S | MOSS AVE | 2,625 | 36 | 94,500 | C - Collector | A - AC | 76 | 13.85 |
| PAMTAV 20 | | PALMETTO AVE | EL CAMINO WY | HWY 99 OVERPASS W/S | 915 | 36 | 32,940 | C - Collector | A - AC | 65 | 9.99 |
| PAMTAV 30 | | PALMETTO AVE | EL CAMINO WY | BERKELEY LN | 467 | 36 | 16,812 | C - Collector | A - AC | 77 | 14.29 |
| PAMTAV 40 | | PALMETTO AVE | BERKELEY LN | ARBUTUS AVE | 407 | 36 | 14,652 | C - Collector | A - AC | 96 | 23.7 |
| PAMTAV 50 | | PALMETTO AVE | MANGROVE AVE | ARBUTUS AVE | 801 | 36 | 28,836 | C - Collector | A - AC | 82 | 16.59 |
| PAALST 10 | | PALO ALTO ST | BLACKSTONE CT | FARMINGTON AVE | 652 | 36 | 23,472 | R - Residential/Local | A - AC | 82 | 27.54 |
| PALOLN 10 | | PALOMAR LN | YOSEMITE DR | CUL-DE-SAC | 150 | 35 | 7,212 | R - Residential/Local | A - AC | 78 | 24.99 |
| PAMEWY 10 | | PAMELA WAY | PARKWOOD DR | CUL-DE-SAC | 344 | 35 | 13,833 | R - Residential/Local | A - AC | 19 | 0 |
| PANAAY 10 | | PANAMA AVE | ESPLANADE DR | MONTEREY ST | 1,780 | 28 | 52,158 | R - Residential/Local | A - AC | 21 | 0 |
| PANAAY 20 | | PANAMA AVE | TOM POLK AVE | EMILIO WAY | 450 | 28 | 12,600 | L - Local (7) | A - AC | 30 | 1.51 |
| PARKAV 20 | | PARK AVE | MAIN ST (225 FT S/O HUMBOLDT) | W. 14TH ST N/S | 1,480 | 60 | 88,800 | R - Residential/Local | A - AC | 73 | 21.79 |
| PARKAV 30 | | PARK AVE | W. 14TH ST N/S | W. 20TH ST N/S | 2,000 | 60 | 120,000 | R - Residential/Local | A - AC | 73 | 21.79 |
| PARKAV 40 | | PARK AVE | W. 20TH ST N/S | WESTFIELD LN N/S | 1,735 | 60 | 104,100 | R - Residential/Local | A - AC | 80 | 26.22 |
| PARKAV 50 | | PARK AVE | WESTFIELD LN N/S | EAST PARK AVE S/S | 1,684 | 60 | 101,040 | R - Residential/Local | A - AC | 81 | 26.86 |
| PAVILN 10 | | PARK VIEW LN | OAK VISTA AVE | CUL-DE-SAC | 608 | 32 | 19,456 | R - Residential/Local | A - AC | 19 | 0 |
| PARKDR 10 | | PARK VISTA DR | E. 8TH ST | EARL AVE | 1,353 | 38 | 51,414 | C - Collector | A - AC | 24 | 0 |
| PARKST 10 | | PARKHURST ST | E. OXFORD WY | END | 705 | 37 | 26,085 | R - Residential/Local | A - AC | 77 | 24.3 |
| PARKST 20 | | PARKHURST ST | NOTRE DAME BLVD | E. OXFORD WY | 944 | 37 | 34,928 | R - Residential/Local | A - AC | 77 | 24.3 |
| PARKCT 10 | | PARKTREE CT | HOOKER OAK AVE | CUL-DE-SAC | 257 | 32 | 8,224 | C - Collector | A - AC | 24 | 0 |
| PAVIDR 10 | | PARKWAY VILLAGE DR | FOREST AVE | BAR TRIANGLE ST | 1,258 | 33 | 41,514 | R - Residential/Local | A - AC | 81 | 26.91 |
| PAWDDR 10 | | PARKWOOD DR | E. 8TH ST | CUL-DE-SAC | 1,000 | 35 | 36,793 | C - Collector | A - AC | 6 | 0 |
| PARLCT 10 | | PARLIAMENT CT | VALLEY FORGE DR | END | 465 | 32 | 14,880 | R - Residential/Local | A - AC | 81 | 26.91 |
| PARMRD 10 | | PARMAC RD | COHASSET RD | RIO LINDO AVE | 1,488 | 36 | 53,568 | C - Collector | A - AC | 77 | 14.29 |
| PARNAY 10 | | PARNASSIAN AVE | VICEROY DR | WANDERER LN | 775 | 24 | 18,600 | R - Residential/Local | A - AC | 84 | 28.79 |
| PARNAY 20 | | PARNASSIAN AVE | WANDERER LN | LUCY WAY | 490 | 24 | 11,760 | R - Residential/Local | A - AC | 84 | 28.86 |

Criteria:

40
SS1030

MTC StreetSaver

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------------|-----------------|--------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| PARQDR | 10 | PARQUE DR | MARIPOSA AVE | PC @ CUL-DE-SAC | 720 | 16 | 11,520 | R - Residential/Local | A - AC | 18 | 0 |
| PAHACT | 10 | PASEO HACIENDAS CT | CEANOTHUS AV | END | 500 | 32 | 16,000 | C - Collector | A - AC | 80 | 15.69 |
| PAGRCT | 10 | PASO GRANDE CT | W. SHASTA AVE | CUL-DE-SAC | 487 | 36 | 19,332 | R - Residential/Local | A - AC | 30 | 1.82 |
| PASSCT | 10 | PASSIFLORA CT | ARCH WY | CUL-DE-SAC | 358 | 36 | 14,606 | R - Residential/Local | A - AC | 80 | 26.27 |
| PATCDR | 10 | PATCHES DR | TOWSER RD | CUL-DE-SAC | 484 | 32 | 15,488 | R - Residential/Local | A - AC | 80 | 26.27 |
| PATHWY | 10 | PATHFINDER WAY | DOG LEG DR | CUL-DE-SAC | 381 | 37 | 15,905 | R - Residential/Local | A - AC | 81 | 26.91 |
| PATDR | 10 | PATRICIA DR | CERES AVE | FLORAL AVE | 1,300 | 36 | 46,800 | R - Residential/Local | A - AC | 75 | 23.09 |
| PATRWY | 10 | PATRIOT WAY | HANCOCK DR | COACH LITE DR | 260 | 36 | 9,360 | R - Residential/Local | A - AC | 75 | 23.09 |
| PAULPL | 10 | PAULETAH PL | FLORAL AVE | WHITEWOOD WAY | 830 | 28 | 23,240 | R - Residential/Local | A - AC | 79 | 25.63 |
| PEACLN | 10 | PEACOCK LN | CERES AVE | EAST END | 300 | 22 | 6,600 | R - Residential/Local | A - AC | 33 | 2.59 |
| PEARCT | 10 | PEARMAN CT | ALAMO AVE | CUL-DE-SAC | 275 | 36 | 11,700 | R - Residential/Local | A - AC | 82 | 27.49 |
| PEPADR | 10 | PELICAN PARK DR | NORTH AVE | CUL-DE-SAC | 545 | 32 | 17,440 | R - Residential/Local | A - AC | 80 | 26.27 |
| PENDR | 10 | PENDANT PLACE | EATON RD | BEACHCOMBER COVE | 880 | 28 | 24,640 | L - Local (7) | A - AC | 94 | 33.54 |
| PENIDR | 10 | PENINSULA DR | PATHFINDER WY | DELANEY DR (NORTH) | 2,390 | 37 | 88,430 | C - Collector | A - AC | 85 | 18.13 |
| PENNWY | 10 | PENNISUE WAY | KAREN DR | MOSS AVE | 675 | 32 | 21,600 | R - Residential/Local | A - AC | 15 | 0 |
| PENSWY | 10 | PENSTEMON WAY | ALAMO AVE | NEW HEATHER WY | 530 | 32 | 19,064 | R - Residential/Local | A - AC | 82 | 27.49 |
| PENZAV | 10 | PENZANCE AVE | SOUTH END | EATON RD | 305 | 36 | 10,980 | R - Residential/Local | A - AC | 80 | 26.22 |
| PENZAV | 20 | PENZANCE AVE | EATON RD | WEYMOUTH WAY | 656 | 38 | 24,928 | R - Residential/Local | A - AC | 82 | 27.49 |
| PENZAV | 30 | PENZANCE AVE | WEYMOUTH WAY | DEGARMO DR | 590 | 38 | 22,420 | R - Residential/Local | A - AC | 82 | 27.49 |
| PEMEWY | 10 | PETERSON MEMORIAL WAY | VALLOMBROSA WAY | VALLOMBROSA AVE | 11,987 | 12 | 143,844 | R - Residential/Local | A - AC | 34 | 2.95 |
| PHLOWY | 10 | PHLOX WAY | COHASSET RD | END | 720 | 32 | 23,040 | R - Residential/Local | A - AC | 21 | 0 |
| PHYLCT | 10 | PHYLLIS CT | PENINSULA DR | END | 275 | 32 | 8,800 | R - Residential/Local | A - AC | 86 | 30 |
| PICALN | 10 | PICASO LN | HOLLY AVE | END | 480 | 28 | 13,440 | R - Residential/Local | A - AC | 82 | 27.49 |
| PICHWY | 10 | PICHOLINE WAY | ASCOLANO WY | CUL-DE-SAC | 1,306 | 34 | 49,107 | R - Residential/Local | A - AC | 80 | 26.27 |
| PICHWY | 20 | PICHOLINE WAY | BARONI WY | POTTER RD | 1,257 | 35 | 43,995 | R - Residential/Local | A - AC | 81 | 26.97 |
| PICHWY | 30 | PICHOLINE WAY | BRUCE RD | BARONI WY | 996 | 35 | 34,860 | R - Residential/Local | A - AC | 80 | 26.27 |
| PICHWY | 40 | PICHOLINE WAY | POTTER RD | ASCOLANO WY | 638 | 33 | 21,054 | R - Residential/Local | A - AC | 80 | 26.27 |
| PICUCT | 10 | PICUAL CT | PICHOLINE WY | CUL-DE-SAC | 526 | 32 | 16,832 | R - Residential/Local | A - AC | 80 | 26.27 |
| PIEDCR | 10 | PIEDMONT CIR | MORELAND DR | CUL-DE-SAC | 200 | 36 | 8,528 | R - Residential/Local | A - AC | 22 | 0 |
| PILLRD | 10 | PILLSBURY RD | COHASSET RD | PLAZA WY | 1,923 | 48 | 92,304 | C - Collector | A - AC | 79 | 15.23 |
| PILLRD | 20 | PILLSBURY RD | EAST AVE | PORTAL DR | 306 | 32 | 9,792 | R - Residential/Local | A - AC | 94 | 33.64 |
| PILLRD | 30 | PILLSBURY RD | PLAZA WY | EAST AVE | 508 | 48 | 24,384 | R - Residential/Local | A - AC | 74 | 22.46 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|---------------------|---------------------------|------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| PILLRD | 40 | PILLSBURY RD | COHASSET RD | MADERA LN | 1,110 | 36 | 39,960 | L - Local (7) | A - AC | 94 | 33.64 |
| PILLRD | 50 | PILLSBURY RD | MADERA LN | PORTAL DR | 1,220 | 30 | 36,600 | L - Local (7) | A - AC | 94 | 33.64 |
| PINEST | 10 | PINE ST | E. 5TH ST | E. 8TH ST | 1,034 | 38 | 39,292 | R - Residential/Local | A - AC | 66 | 17.63 |
| PINEST | 20 | PINE ST | E. 8TH ST | HUMBOLDT AVE | 976 | 38 | 37,088 | R - Residential/Local | A - AC | 82 | 27.49 |
| PINEST | 30 | PINE ST | HUMBOLDT AVE | MULBERRY ST | 648 | 38 | 24,624 | R - Residential/Local | A - AC | 10 | 0 |
| PINEST | 40 | PINE ST | E. 3RD ST | E. 5TH ST | 665 | 38 | 25,270 | R - Residential/Local | A - AC | 75 | 23.04 |
| PINECR | 10 | PINECREST CIR | E. 5TH AVE | END | 180 | 32 | 5,760 | R - Residential/Local | A - AC | 15 | 0 |
| PIHIDR | 10 | PINYON HILLS DR | BANNING PARK DR | PVMT CHNG (1191 FT WEST) | 1,191 | 32 | 38,112 | R - Residential/Local | A - AC | 77 | 24.35 |
| PIHIDR | 20 | PINYON HILLS DR | PVMT CHNG (1191 FT W) | LAKEWEST DR | 454 | 32 | 14,528 | R - Residential/Local | A - AC | 79 | 25.63 |
| PIPEAV | 10 | PIPER AVE | FORTRESS ST | END | 248 | 40 | 9,920 | R - Residential/Local | A - AC | 69 | 19.42 |
| PIPEAV | 20 | PIPER AVE | FORTRESS ST | END | 250 | 110 | 27,500 | R - Residential/Local | A - AC | 86 | 29.89 |
| PLAZWY | 10 | PLAZA WAY | PILLSBURY RD | END | 345 | 34 | 11,730 | R - Residential/Local | A - AC | 5 | 0 |
| PLUMWY | 10 | PLUMAS WAY | NEAL DOW AVE | CUL-DE-SAC | 684 | 36 | 24,624 | R - Residential/Local | A - AC | 20 | 0 |
| PLUMCT | 10 | PLUMWOOD CT | CUL-DE-SAC | CALIFORNIA PARK DR | 800 | 33 | 26,400 | C - Collector | A - AC | 69 | 11.12 |
| POMOAV | 10 | POMONA AVE | ALMOND ST S/S | BRIDGE N/S | 850 | 30 | 25,500 | R - Residential/Local | A - AC | 43 | 6.52 |
| POMOAV | 20 | POMONA AVE | MILLER AVE | ALMOND ST S/S | 1,200 | 21 | 25,200 | R - Residential/Local | A - AC | 44 | 6.83 |
| POPLST | 10 | POPLAR ST | E. 6TH ST | E. 9TH ST | 874 | 36 | 31,464 | R - Residential/Local | A - AC | 64 | 16.5 |
| POPLST | 20 | POPLAR ST | E. 6TH ST | WOODLAND AVE | 482 | 36 | 17,352 | R - Residential/Local | A - AC | 53 | 10.85 |
| POPLST | 30 | POPLAR ST | E. 9TH ST | HUMBOLDT AVE | 738 | 36 | 26,568 | R - Residential/Local | A - AC | 66 | 17.63 |
| POPPST | 10 | POPPY ST | MCINTOSH AVE | END | 460 | 20 | 9,200 | R - Residential/Local | A - AC | 26 | 0.44 |
| POVITR | 10 | POPPY VIEW TERR | GARDEN VALLEY TERR | E 20TH ST | 490 | 32 | 15,680 | R - Residential/Local | A - AC | 89 | 31.62 |
| PORTDR | 10 | PORTAL DR | PILLSBURY RD | LORINDA LN | 1,140 | 32 | 36,480 | R - Residential/Local | A - AC | 94 | 33.64 |
| PORTWY | 10 | PORTER WAY | RIO LINDO AVE | CUL-DE-SAC | 417 | 32 | 15,448 | R - Residential/Local | A - AC | 28 | 1.12 |
| POSAWY | 10 | POSADA WAY | JOSHUA TREE | CUL-DE-SAC | 641 | 38 | 25,926 | R - Residential/Local | A - AC | 47 | 8.16 |
| POTTRD | 10 | POTTER RD | NORTH END (N/O PICHOLINE) | SOUTH END (S/O ST AUGUSTINE) | 1,078 | 33 | 35,574 | R - Residential/Local | A - AC | 80 | 26.27 |
| POTTRD | 30 | POTTER RD | E. 20TH ST | END | 1,200 | 32 | 38,400 | L - Local (7) | A - AC | 92 | 32.88 |
| PRCRDR | 10 | PRAIRIE CREEK DR | SANDY COVE DR N/S | LEGACY LN | 794 | 36 | 28,584 | R - Residential/Local | A - AC | 80 | 26.22 |
| PREMCT | 10 | PREMIER CT | VIA MISSION DR | CUL-DE-SAC | 578 | 32 | 18,496 | R - Residential/Local | A - AC | 78 | 24.99 |
| PROADR | 10 | PRESERVATION OAK DR | POTTER RD | GARDEN VALLEY TERR | 650 | 24 | 15,600 | R - Residential/Local | A - AC | 91 | 32.51 |
| PROADR | 20 | PRESERVATION OAK DR | GARDEN VALLEY TERR | E 20TH ST | 510 | 32 | 16,320 | R - Residential/Local | A - AC | 89 | 31.62 |

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|----------------|--------------------------|--------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| PRIMLN | 10 | PRIMROSE LN | CERES AVE | CUL-DE-SAC | 555 | 37 | 20,535 | R - Residential/Local | A - AC | 25 | 0.15 |
| PRTJCT | 10 | PRINCESS TJ CT | IDYLLWILD CIR | CUL-DE-SAC | 129 | 36 | 6,702 | R - Residential/Local | A - AC | 75 | 23.09 |
| PURCLN | 10 | PURCELL LN | HWY 32 | RUSKIN ST | 515 | 28 | 14,420 | R - Residential/Local | A - AC | 94 | 33.59 |
| RADCLN | 10 | RADCLIFF LN | W. 8TH AVE | RICHLAND CT | 223 | 36 | 8,028 | R - Residential/Local | A - AC | 77 | 24.3 |
| RACRDR | 10 | RAE CREEK DR | CINDER CREEK DR | RAE CREEK DR (LEFT TURN) | 845 | 30 | 25,350 | R - Residential/Local | A - AC | 94 | 33.64 |
| RACRDR | 20 | RAE CREEK DR | RAE CREEK DR (LEFT TURN) | PVMT ENDS | 390 | 32 | 12,480 | R - Residential/Local | A - AC | 94 | 33.64 |
| RAFAST | 10 | RAFAEL ST | PANAMA | E LASSEN AV | 1,384 | 36 | 49,824 | C - Collector | A - AC | 24 | 0 |
| RAINLN | 10 | RAINIER LN | E. LASSEN AVE | CUL-DE-SAC | 277 | 32 | 8,864 | R - Residential/Local | A - AC | 59 | 13.88 |
| RALEBL | 10 | RALEY BLVD | FOREST AVE | BRUCE RD | 1,246 | 37 | 46,102 | A - Arterial | A - AC | 65 | 12.73 |
| RALLCR | 10 | RALLAND CIR | NORTH AVE | END | 520 | 35 | 18,200 | C - Collector | A - AC | 4 | 0 |
| RAMAWY | 10 | RAMADA WAY | EAST AVE | END | 485 | 36 | 17,460 | R - Residential/Local | A - AC | 3 | 0 |
| RANCDR | 10 | RANCHERIA DR | MECHOOPDA ST | N. CEDAR ST | 1,230 | 37 | 45,510 | R - Residential/Local | A - AC | 78 | 24.94 |
| RAVELN | 10 | RAVEN LN | W. SACRAMENTO AVE | CUL-DE-SAC | 508 | 28 | 14,224 | R - Residential/Local | A - AC | 82 | 27.49 |
| RAVEWY | 10 | RAVENSHOE WAY | FLORAL AVE | MARIPOSA AVE | 1,287 | 32 | 41,184 | R - Residential/Local | A - AC | 78 | 24.99 |
| REDELP | 10 | REDEEMERS LOOP | GODMAN AVE | GODMAN AVE | 1,245 | 28 | 34,860 | R - Residential/Local | A - AC | 94 | 33.64 |
| REDWWY | 10 | REDWOOD WAY | LILAC LN | HOOKER OAK AVE | 1,519 | 32 | 48,608 | C - Collector | A - AC | 75 | 13.48 |
| REPADR | 10 | REED PARK DR | SOUTH END | CUL-DE-SAC | 415 | 36 | 16,658 | R - Residential/Local | A - AC | 19 | 0 |
| REGEDR | 10 | REGENCY DR | GREENWICH DR | YORK DR | 450 | 28 | 12,600 | R - Residential/Local | A - AC | 82 | 27.49 |
| REMIDR | 10 | REMINGTON DR | BRUCE DR | BARONI DR | 969 | 35 | 33,915 | R - Residential/Local | A - AC | 73 | 21.84 |
| RENECR | 10 | RENEE CIR | WENDY WY | CUL-DE-SAC | 163 | 36 | 7,586 | R - Residential/Local | A - AC | 78 | 24.99 |
| REYCR | 10 | REY CIR | REY WY | CUL-DE-SAC | 416 | 35 | 16,353 | R - Residential/Local | A - AC | 20 | 0 |
| REYWY | 10 | REY WAY | VALLOMBROSA AVE | SIERRA VISTA WY | 1,122 | 28 | 31,416 | R - Residential/Local | A - AC | 65 | 17.11 |
| RHONLN | 10 | RHONDA LN | FOREST CREEK CIR | VERMILLION CIR | 198 | 32 | 6,336 | R - Residential/Local | A - AC | 29 | 1.17 |
| RICHCT | 10 | RICHLAND CT | DARTWOOD DR | CUL-DE-SAC | 610 | 36 | 23,760 | R - Residential/Local | A - AC | 77 | 24.3 |
| RIDGWY | 10 | RIDGEBROOK WAY | CREEKHOLLOW DR | AUBURN OAK WY | 1,195 | 32 | 38,240 | R - Residential/Local | A - AC | 80 | 26.27 |
| RIDGDR | 10 | RIDGEWOOD DR | E. LASSEN AVE | CERES AVE | 1,040 | 36 | 37,440 | R - Residential/Local | A - AC | 38 | 4.46 |
| RINGWY | 10 | RINGTAIL WAY | VISTAMONT WAY | PARNASSIAN AVE | 345 | 28 | 9,660 | R - Residential/Local | A - AC | 84 | 28.8 |
| RICHWY | 10 | RIO CHICO WAY | W. 1ST ST | W. 1ST ST | 1,120 | 25 | 28,000 | R - Residential/Local | A - AC | 22 | 0 |
| RILIAV | 10 | RIO LINDO AVE | COHASSET RD | PARMAC RD | 1,421 | 36 | 51,156 | C - Collector | A - AC | 26 | 0.25 |
| RILIAV | 20A | RIO LINDO AVE | PARMAC RD | 700 FT FROM PARMAC RD | 700 | 36 | 25,200 | C - Collector | A - AC | 73 | 12.76 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|------------------------|-----------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| RILIAV | 20B | RIO LINDO AVE | 700 FT FROM PARAMAC RD | PORTER WY | 1,075 | 36 | 38,700 | C - Collector | A - AC | 25 | 0 |
| RILIAV | 30 | RIO LINDO AVE | PORTER WY | ESPLANADE DR | 810 | 36 | 29,160 | C - Collector | A - AC | 24 | 0 |
| RITCCR | 10 | RITCHIE CIR | W 12TH AVE | END | 840 | 38 | 31,920 | R - Residential/Local | A - AC | 80 | 26.22 |
| RIBELN | 10 | RIVER BEND LN | W LINDO AVE | CUL-DE-SAC | 610 | 36 | 23,760 | R - Residential/Local | A - AC | 80 | 26.22 |
| RIOADR | 10 | RIVER OAKS DR | NORTH AVE | LUPIN AVE | 755 | 32 | 24,160 | R - Residential/Local | A - AC | 33 | 2.59 |
| RIWOLP | 10 | RIVER WOOD LOOP | MONTEGO WAY | MONTEGO WAY | 1,045 | 28 | 29,260 | R - Residential/Local | A - AC | 94 | 33.54 |
| RIVICT | 10 | RIVIERA CT | FILBERT AVE | END | 415 | 24 | 9,960 | R - Residential/Local | A - AC | 85 | 29.4 |
| ROBADR | 10 | ROBAILEY DR | BAR TRIANGLE ST | NOTRE DAME BLVD | 333 | 37 | 12,321 | R - Residential/Local | A - AC | 60 | 14.4 |
| ROBADR | 20 | ROBAILEY DR | LA ROCCO DR | BAR TRIANGLE ST | 1,335 | 37 | 49,395 | R - Residential/Local | A - AC | 80 | 26.27 |
| ROBECT | 10 | ROBERTO CT | WEBSTER DR | CUL-DE-SAC | 263 | 37 | 11,374 | R - Residential/Local | A - AC | 79 | 25.58 |
| ROCHDR | 10 | ROCHESTER DR | DEVONSHIRE DR | CHADWICK DR | 263 | 28 | 7,364 | R - Residential/Local | A - AC | 20 | 0 |
| ROCMDR | 10 | ROCKIN M DR | EATON RD | DESIREE LN | 515 | 28 | 14,420 | R - Residential/Local | A - AC | 82 | 27.55 |
| ROCMDR | 20 | ROCKIN M DR | DESIREE LN | GODMAN AVE | 675 | 28 | 18,900 | R - Residential/Local | A - AC | 82 | 27.55 |
| RORIDR | 10 | ROGUE RIVER DR | WINDHAM WAY | BURNT RANCH WAY | 400 | 28 | 11,200 | R - Residential/Local | A - AC | 82 | 27.49 |
| RORIDR | 20 | ROGUE RIVER DR | BURNT RANCH WAY | END | 940 | 28 | 26,320 | R - Residential/Local | A - AC | 82 | 27.49 |
| RORIDR | 30 | ROGUE RIVER DR | BRUSH CREEK LN | WINDHAM WAY | 850 | 32 | 27,200 | R - Residential/Local | A - AC | 97 | 34.12 |
| ROLADR | 10 | ROLLINS LAKE DR | DENALI DR | CRATER LAKE DR | 518 | 36 | 18,648 | R - Residential/Local | A - AC | 80 | 26.22 |
| ROLLN | 10 | ROLLIS LN | FOREST CREEK CIR | VERMILLION CIR | 217 | 33 | 7,161 | R - Residential/Local | A - AC | 36 | 3.7 |
| ROOHT | 10 | ROOHR CT | ISSAC JAMES AVE | END | 194 | 32 | 6,208 | R - Residential/Local | A - AC | 83 | 28.17 |
| RORIAV | 10 | ROSE RIVER AVE | GOLDEN BIRCH PL | HARVEST GLEN DR | 250 | 26 | 6,500 | R - Residential/Local | A - AC | 81 | 26.91 |
| ROSECR | 10 | ROSEMARY CIR | VERBENA LN | CUL-DE-SAC | 405 | 36 | 16,298 | R - Residential/Local | A - AC | 18 | 0 |
| ROSSLN | 10 | ROSSETTI LN | NORD AVE | RUSKIN ST | 505 | 26 | 13,130 | R - Residential/Local | A - AC | 94 | 33.59 |
| ROTHST | 10 | ROTH ST | E 20TH ST | ENGLAND ST | 1,026 | 22 | 22,572 | C - Collector | A - AC | 82 | 16.64 |
| ROXACT | 10 | ROXANNE CT | NEW DAWN CIR | CUL-DE-SAC | 437 | 37 | 18,061 | R - Residential/Local | A - AC | 60 | 14.34 |
| ROXBCT | 10 | ROXBURY CT | HENSHAW AVE | CUL-DE-SAC | 670 | 32 | 23,544 | R - Residential/Local | A - AC | 82 | 27.49 |
| ROGLN | 10 | ROYAL GLEN LN | WEDGEWOOD LN | CUL-DE-SAC | 728 | 32 | 27,000 | R - Residential/Local | A - AC | 67 | 18.2 |
| ROYCLN | 10 | ROYCROFT LN | NORD AVE | RUSKIN ST | 470 | 22 | 10,340 | R - Residential/Local | A - AC | 94 | 33.59 |
| RUBICT | 10 | RUBICON CT | E 8TH ST | END | 360 | 16 | 5,760 | R - Residential/Local | A - AC | 87 | 30.57 |
| RURIDR | 10 | RUBY RIVER DR | CINDER CREEK DR | HIGHTOWER LNDG | 340 | 26 | 8,840 | R - Residential/Local | A - AC | 94 | 33.54 |
| RURIDR | 20 | RUBY RIVER DR | HIGHTOWER LNDG | END | 460 | 26 | 11,960 | R - Residential/Local | A - AC | 94 | 33.54 |
| RUFNAN | 10 | RUE FRANCAIS | MARIGOLD AVE | DUFFY DR | 533 | 32 | 17,056 | R - Residential/Local | A - AC | 77 | 24.35 |
| RUOACT | 10 | RUGOSA OAK CT | SOUTHBURY LN | END | 170 | 32 | 5,440 | R - Residential/Local | A - AC | 82 | 27.49 |
| RUSHCT | 10 | RUSH CT | MEADOW RD | END | 370 | 32 | 11,840 | R - Residential/Local | A - AC | 29 | 1.47 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|------------------|------------------|----------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| RUSHAV | 10 | RUSHMORE AVE | BLUE RIDGE AVE | CERES AVE | 643 | 32 | 20,576 | R - Residential/Local | A - AC | 19 | 0 |
| RUSKST | 10 | RUSKIN ST | PURCELL LN | ROSSETTI LN | 615 | 28 | 17,220 | R - Residential/Local | A - AC | 94 | 33.59 |
| RYANAV | 10 | RYAN AVE | FORTRESS ST | END | 385 | 26 | 10,010 | R - Residential/Local | A - AC | 23 | 0 |
| RYANAV | 20 | RYAN AVE | FORTRESS ST | MARAUDER ST | 1,054 | 61 | 64,294 | R - Residential/Local | A - AC | 75 | 23.09 |
| RYANAV | 30 | RYAN AVE | MARAUDER ST | COHASSET RD | 1,483 | 50 | 74,150 | R - Residential/Local | A - AC | 67 | 18.25 |
| SAGECT | 10 | SAGEBRUSH CT | DIXON ST | END | 170 | 32 | 5,440 | R - Residential/Local | A - AC | 78 | 24.94 |
| SALEAV | 10 | SALE AVE | CEANOTHUS AVE | LUCY WY | 494 | 36 | 17,784 | R - Residential/Local | A - AC | 72 | 21.23 |
| SALEST | 10 | SALEM ST | W. 11TH ST N/S | W. 15TH ST S/S | 1,550 | 38 | 58,900 | R - Residential/Local | A - AC | 70 | 19.96 |
| SALEST | 20 | SALEM ST | W. 15TH ST S/S | W. 20TH ST N/S | 1,710 | 22 | 37,620 | R - Residential/Local | A - AC | 31 | 2.17 |
| SALEST | 30 | SALEM ST | W. 1ST ST S/S | W. 5TH ST N/S | 1,312 | 49 | 64,288 | R - Residential/Local | A - AC | 91 | 32.46 |
| SALEST | 40 | SALEM ST | W. 20TH ST S/S | CUL-DE-SAC | 428 | 32 | 14,156 | R - Residential/Local | A - AC | 82 | 27.49 |
| SALEST | 50 | SALEM ST | W. 5TH ST S/S | W. 9TH ST N/S | 1,254 | 49 | 61,446 | R - Residential/Local | A - AC | 74 | 22.41 |
| SALEST | 60 | SALEM ST | W. 9TH ST S/S | W. 11TH ST N/S | 865 | 49 | 42,385 | R - Residential/Local | A - AC | 26 | 0.44 |
| SAANDR | 10 | SAN ANTONIO DR | ALAMO AVE | CUL-DE-SAC | 715 | 36 | 27,540 | R - Residential/Local | A - AC | 78 | 24.94 |
| SAJOST | 10 | SAN JOSE ST | RAFAEL ST | LASSEN | 1,250 | 36 | 45,000 | C - Collector | A - AC | 13 | 0 |
| SAMICT | 10 | SAN MIGUEL CT | E. EATON RD | CUL-DE-SAC | 177 | 33 | 7,784 | R - Residential/Local | A - AC | 63 | 16 |
| SAPACT | 10 | SAN PABLO CT | NORTH AVE | CUL-DE-SAC | 516 | 36 | 20,294 | R - Residential/Local | A - AC | 38 | 4.46 |
| SANRDR | 10 | SAN RAMON DR | NORTH AVE | CUL-DE-SAC | 900 | 32 | 28,800 | R - Residential/Local | A - AC | 48 | 8.6 |
| SANRDR | 20 | SAN RAMON DR | CERES AVE | NORTH AVE | 1,340 | 37 | 49,580 | L - Local (7) | A - AC | 18 | 0 |
| SAVEWY | 10 | SAN VERBENA WAY | LUPIN AVE | CUL-DE-SAC | 459 | 32 | 16,706 | R - Residential/Local | A - AC | 78 | 24.99 |
| SAVEWY | 20 | SAN VERBENA WAY | NORTH AVE | LUPIN AVE | 865 | 36 | 31,140 | L - Local (7) | A - AC | 22 | 0 |
| SANDCR | 10 | SANDRA CIR | PATRICIA DR | CUL-DE-SAC | 180 | 36 | 8,198 | R - Residential/Local | A - AC | 78 | 24.99 |
| SACODR | 10 | SANDY COVE DR | PRAIRIE CREEK DR | CUL-DE-SAC | 1,274 | 36 | 47,664 | R - Residential/Local | A - AC | 81 | 26.86 |
| SANTCT | 10 | SANTANA CT | CERES AVE | CUL-DE-SAC | 545 | 36 | 21,338 | R - Residential/Local | A - AC | 54 | 11.38 |
| SARAAV | 10 | SARAH AVE | PALMETTO | E. 1ST AVE | 1,062 | 36 | 38,232 | R - Residential/Local | A - AC | 34 | 2.95 |
| SARADR | 10 | SARATOGA DR | BOSTON DR | HANCOCK DR | 837 | 36 | 30,132 | R - Residential/Local | A - AC | 79 | 25.63 |
| SATIWW | 10 | SATINWOOD WAY | E. EATON RD | CUL-DE-SAC | 477 | 33 | 17,684 | R - Residential/Local | A - AC | 67 | 18.25 |
| SAURCT | 10 | SAURAT CT | TRENTA DR | END | 120 | 32 | 3,840 | R - Residential/Local | A - AC | 82 | 27.49 |
| SAUSST | 10 | SAUSALITO ST | PALO ALTO ST | COIT TOWER WY | 339 | 36 | 12,204 | R - Residential/Local | A - AC | 81 | 26.91 |
| SABALN | 10 | SAWYERS BAR LN | BURN RANCH WAY | ROGUE RIVER DR | 400 | 28 | 11,200 | R - Residential/Local | A - AC | 82 | 27.49 |
| SCGRCT | 10 | SCARLET GROVE CT | AURORA GLEN WAY | END | 145 | 30 | 4,350 | R - Residential/Local | A - AC | 82 | 27.49 |
| SCOTAV | 10 | SCOTT AVE | E. PARK AVE | FAIR ST | 788 | 35 | 27,580 | R - Residential/Local | A - AC | 55 | 11.81 |
| SEABLN | 10 | SEABIRD LN | PELICAN PARK DR | CUL-DE-SAC | 421 | 32 | 13,472 | R - Residential/Local | A - AC | 77 | 24.35 |

Section PCI/RSL Listing

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-------------------|------------------|----------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| SEBACT | 10 | SEBASTIAN CT | SHARI LN | CUL-DE-SAC | 260 | 36 | 11,078 | R - Residential/Local | A - AC | 79 | 25.63 |
| SEOACT | 10 | SECLUDED OAKS CT | E 8TH ST | END | 490 | 28 | 13,720 | R - Residential/Local | A - AC | 84 | 28.8 |
| SEDOAV | 10 | SEDONA AVE | SOUTH END | ARCH WAY | 185 | 29 | 5,365 | R - Residential/Local | A - AC | 83 | 28.17 |
| SEDOAV | 20 | SEDONA AVE | ARCH WAY | CACTUS AVE | 740 | 27 | 19,980 | R - Residential/Local | A - AC | 85 | 29.4 |
| SEDOAV | 30 | SEDONA AVE | SEDONA AVE | SEDONA AVE | 295 | 27 | 7,965 | R - Residential/Local | A - AC | 83 | 28.17 |
| SEQUAV | 10 | SEQUOYAH AVE | END | ZUNI AVE | 265 | 36 | 9,540 | R - Residential/Local | A - AC | 18 | 0 |
| SEQUAV | 20 | SEQUOYAH AVE | ZUNI AVE | W LINDO AVE | 1,108 | 12 | 13,296 | R - Residential/Local | A - AC | 47 | 8.11 |
| SEQUAV | 30 | SEQUOYAH AVE | HOLLY AVE | END | 1,407 | 30 | 42,210 | R - Residential/Local | A - AC | 24 | 0 |
| SEQUAV | 40 | SEQUOYAH AVE | LAIRD DR | END | 220 | 14 | 3,080 | R - Residential/Local | A - AC | 82 | 27.49 |
| SECRWA | 10 | SESPE CREEK WAY | WINDHAM WAY | END | 180 | 27 | 4,860 | R - Residential/Local | A - AC | 82 | 27.49 |
| SEVECT | 10 | SEVILLANO CT | PICHOLINE WY | CUL-DE-SAC | 105 | 75 | 7,875 | R - Residential/Local | A - AC | 80 | 26.27 |
| SEVICT | 10 | SEVILLE CT | IVY ST | CUL-DE-SAC | 262 | 40 | 12,307 | R - Residential/Local | A - AC | 47 | 8.11 |
| SHANCT | 10 | SHANNON CT | FOREST CREEK CIR | CUL-DE-SAC | 136 | 36 | 6,781 | R - Residential/Local | A - AC | 29 | 1.17 |
| SHARLN | 10 | SHARI LN | SEBASTIAN CT | BRENDA DR | 796 | 36 | 28,656 | R - Residential/Local | A - AC | 76 | 23.72 |
| SHARLN | 20 | SHARI LN | SPRINGFIELD DR | SEBASTIAN CT | 836 | 36 | 30,096 | R - Residential/Local | A - AC | 75 | 23.09 |
| SHEACT | 10 | SHEARWATER CT | IDYLLWILD CIR | CUL-DE-SAC | 518 | 32 | 16,576 | R - Residential/Local | A - AC | 74 | 22.46 |
| SHEPLN | 10 | SHEPARD LN | S. CUL-DE-SAC | N. END | 300 | 32 | 9,600 | C - Collector | A - AC | 48 | 4.93 |
| SHERAV | 10 | SHERIDAN AVE | E. LINDO AVE | E. 5TH AVE | 1,439 | 22 | 31,658 | R - Residential/Local | A - AC | 94 | 33.59 |
| SHERAV | 20 | SHERIDAN AVE | E. 5TH AVE | E. 1ST AVE | 1,434 | 36 | 51,624 | R - Residential/Local | A - AC | 94 | 33.59 |
| SHERAV | 30 | SHERIDAN AVE | E. 1ST AVE | PALMETO AVE | 1,017 | 36 | 36,612 | R - Residential/Local | A - AC | 21 | 0 |
| SHERAV | 40 | SHERIDAN AVE | PALMETO AVE | FILBERT AVE | 1,017 | 36 | 36,612 | R - Residential/Local | A - AC | 94 | 33.59 |
| SHERAV | 50 | SHERIDAN AVE | FILBERT AVE | VALLOMBROSA AV | 797 | 36 | 28,692 | R - Residential/Local | A - AC | 40 | 5.19 |
| SHRMVAV | 10 | SHERMAN AVE | E. 1ST AVE | E. 5TH AVE | 1,122 | 36 | 40,392 | R - Residential/Local | A - AC | 21 | 0 |
| SHRMVAV | 20 | SHERMAN AVE | E. 5TH AVE | E. LINDO AVE | 1,350 | 36 | 48,600 | R - Residential/Local | A - AC | 92 | 32.82 |
| SHOACT | 10 | SHIMMERING OAK CT | HOOKER OAK AVE | CUL-DE-SAC | 476 | 32 | 15,232 | C - Collector | A - AC | 34 | 1.66 |
| SHOSAV | 10 | SHOSHONEE AVE | W 12TH AVE | SEQUOYAH AVE | 415 | 20 | 8,300 | R - Residential/Local | A - AC | 48 | 8.55 |
| SHOSAV | 20 | SHOSHONEE AVE | SEQUOYAH AVE | W 11TH AVE | 420 | 20 | 8,400 | R - Residential/Local | A - AC | 54 | 11.33 |
| SHOAWY | 10 | SHUMARD OAK WAY | NATIVE OAK DR | PIN OAK LN | 410 | 22 | 9,020 | R - Residential/Local | A - AC | 97 | 34.13 |
| SIOADR | 10 | SIERRA OAKS DR | W. 12TH AVE | BLACK OAK DR | 355 | 36 | 12,780 | R - Residential/Local | A - AC | 77 | 24.3 |
| SISPDR | 10 | SIERRA SPRINGS DR | SANDY COVE DR | ROYAL GLEN LN | 1,302 | 36 | 46,872 | R - Residential/Local | A - AC | 81 | 26.86 |
| SIERWY | 10 | SIERRA VIEW WAY | ARBUTUS AVE | FILBERT AVE | 1,018 | 22 | 22,396 | R - Residential/Local | A - AC | 39 | 4.8 |
| SIVIWY | 10 | SIERRA VISTA WAY | REY WAY | DOWNING AVE | 988 | 28 | 27,664 | R - Residential/Local | A - AC | 92 | 33.17 |
| SIKOAV | 10 | SIKORSKY AVE | FORTRESS ST | THUNDERBOLT ST | 306 | 41 | 12,546 | R - Residential/Local | A - AC | 41 | 5.73 |

Criteria:

46
SS1030

MTC StreetSaver

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current | Remaining |
|-----------|------------|---------------------|----------------------------|----------------------------|--------|-------|---------|-----------------------|--------------|---------|-----------|
| SILKWAY | 10 | SILKWOOD WAY | FLORAL AVE | CUL-DE-SAC | 456 | 32 | 14,592 | R - Residential/Local | A - AC | PCI | Life |
| SIDOWY | 10 | SILVER DOLLAR WAY | WHITMAN AVE | END | 818 | 40 | 32,720 | C - Collector | A - AC | 49 | 5.14 |
| SILADR | 10 | SILVER LAKE DR | CALISTOGA DR | HIDDEN CREEK DR | 1,416 | 36 | 50,976 | R - Residential/Local | A - AC | 82 | 27.49 |
| SILADR | 20 | SILVER LAKE DR | HIDDEN CREEK DR | CUL-DE-SAC | 508 | 36 | 18,288 | R - Residential/Local | A - AC | 71 | 20.57 |
| SILODR | 10 | SILVER OAK DR | GLENSHIRE LN | CUL-DE-SAC | 730 | 36 | 28,165 | R - Residential/Local | A - AC | 51 | 9.96 |
| SIESCT | 10 | SILVERADO ESTATE CT | ALAMO AVE | CUL-DE-SAC | 674 | 36 | 26,064 | R - Residential/Local | A - AC | 80 | 26.22 |
| SILVCT | 10 | SILVERHILL CT | DIXON ST | END | 175 | 30 | 5,250 | R - Residential/Local | A - AC | 76 | 23.67 |
| SIAACT | 10 | SIR AARON CT | IDYLLWILD CIR | CUL-DE-SAC | 182 | 36 | 8,270 | R - Residential/Local | A - AC | 81 | 26.91 |
| SIANCT | 10 | SIR ANDREW CT | PARKWAY VILLAGE DR | CUL-DE-SAC | 404 | 37 | 16,672 | R - Residential/Local | A - AC | 78 | 24.99 |
| SKYCCCT | 10 | SKYCREEK CT | MARAUDER ST | END | 510 | 26 | 13,260 | R - Residential/Local | A - AC | 82 | 27.55 |
| SKYMCR | 10 | SKYMOUNTAIN CIR | SKYMOUNTAIN CIR | SPRINGFIELD DR | 132 | 37 | 4,884 | R - Residential/Local | A - AC | 44 | 6.88 |
| SKYMCR | 20 | SKYMOUNTAIN CIR | WILDFLOWER CT | WINDMILL CT | 1,834 | 37 | 67,858 | R - Residential/Local | A - AC | 48 | 8.6 |
| SKYMCR | 30 | SKYMOUNTAIN CIR | WINDMILL CT | WILDFLOWER CT | 650 | 37 | 2,405 | R - Residential/Local | A - AC | 23 | 0 |
| SKYWCT | 10 | SKYWALKER CT | CEANOTHUS AVE | END | 474 | 22 | 10,428 | C - Collector | A - AC | 63 | 9.35 |
| SKYWY | 10 | SKYWAY | 892 FT W/O NOTRE DAME BLVD | W/O NOTRE DAME BLVD | 892 | 84 | 3,746 | R - Residential/Local | A - AC | 94 | 33.64 |
| SKYWY | 20 | SKYWAY | BRUCE RD W/S | CITY LIMIT (W/O POTTER RD) | 2,938 | 60 | 176,280 | R - Residential/Local | A - AC | 87 | 30.57 |
| SKYWY | 30 | SKYWAY | HWY 99 PCC OVERPASS | 892 FT W/O NOTRE DAME BLVD | 444 | 50 | 22,200 | R - Residential/Local | A - AC | 94 | 33.64 |
| SKYWY | 40 | SKYWAY | NOTRE DAME BLVD W/S | BRUCE RD W/S | 2,113 | 30 | 63,390 | C - Collector | A - AC | 83 | 17.13 |
| SMBRCT | 10 | SMITH BROTHERS CT | WARNER ST W/S | WEST CUL-DE-SAC | 390 | 32 | 14,584 | R - Residential/Local | A - AC | 75 | 23.04 |
| SNOWDR | 10 | SNOWBIRD DR | CAVALIER WY | VAIL DR | 412 | 28 | 11,536 | R - Residential/Local | A - AC | 75 | 23.04 |
| SWLNAV | 10 | SO-WIL-LE-NO AVE | ESPLANADE DR | ARCADIAN AVE | 770 | 20 | 15,400 | R - Residential/Local | A - AC | 18 | 0 |
| SOMEPL | 10 | SOMERSET PL | END | CUL-DE-SAC | 350 | 36 | 18,684 | R - Residential/Local | A - AC | 66 | 17.63 |
| SONOLN | 10 | SOUTH PARK DR | WOODLAND AVE | E 7TH ST | 2,835 | 18 | 51,030 | C - Collector | A - AC | 33 | 1.45 |
| SOUTDR | 10 | SOUTHAMPTON DR | GREENWICH DR | YORK DR | 460 | 28 | 12,880 | R - Residential/Local | A - AC | 82 | 27.49 |
| SOUTLN | 10 | SOUTHBURY LN | WEYMOUTH WAY | LONDON CT | 715 | 30 | 21,450 | R - Residential/Local | A - AC | 82 | 27.49 |
| SOUTLN | 20 | SOUTHBURY LN | LONDON CT | AURORA GLEN DR | 840 | 30 | 25,200 | R - Residential/Local | A - AC | 82 | 27.49 |
| SOUTLN | 30 | SOUTHBURY LN | AURORA GLEN DR | WHITE BLOSSOM DR | 630 | 30 | 18,900 | R - Residential/Local | A - AC | 82 | 27.49 |
| SOHADR | 10 | SOUTHAMPTON DR | GREENWICH DR | YORK DR | 460 | 30 | 13,800 | R - Residential/Local | A - AC | 82 | 27.49 |
| SPOACR | 10 | SPANISH OAKS CIR | E. 1ST AVE | CUL-DE-SAC | 295 | 36 | 12,338 | C - Collector | A - AC | 50 | 5.46 |
| SPHALN | 10 | SPARROW HAWK LN | CHICORY RD | END | 893 | 20 | 17,860 | R - Residential/Local | A - AC | 84 | 28.86 |

Section PCI/RSL Listing

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|------------|------------|------------------|-------------------------------|-------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| SPICCT 10 | 10 | SPICEBUSH CT | WALNUT PARK DR | CUL-DE-SAC | 126 | 35 | 6,203 | R - Residential/Local | A - AC | 80 | 26.28 |
| SPINWY 10 | 10 | SPINNAKER WAY | FIREFLY CT | CUL-DE-SAC | 271 | 32 | 8,672 | R - Residential/Local | A - AC | 77 | 24.41 |
| SPRICT 10 | 10 | SPRINGBROOK CT | GREENWICH DR | CUL-DE-SAC | 486 | 36 | 19,296 | R - Residential/Local | A - AC | 22 | 0 |
| SPRIDR 10 | 10 | SPRINGFIELD DR | AUBURN OAK WY | SHARI LN | 1,500 | 63 | 94,500 | R - Residential/Local | A - AC | 31 | 1.87 |
| SPRIDR 20 | 20 | SPRINGFIELD DR | FOREST AVE | HERITAGE OAK DR | 1,495 | 63 | 94,185 | R - Residential/Local | A - AC | 23 | 0 |
| SPRIDR 30 | 30 | SPRINGFIELD DR | FOREST AVE E/S | PVMT CHNG (710 FT E/O FOREST) | 710 | 37 | 26,270 | R - Residential/Local | A - AC | 78 | 25 |
| SPRIDR 40 | 40 | SPRINGFIELD DR | HERITAGE OAK DR | AUBURN OAK WY | 996 | 63 | 62,748 | R - Residential/Local | A - AC | 23 | 0 |
| SPRIDR 50 | 50 | SPRINGFIELD DR | PVMT CHNG (710 FT E/O FOREST) | E. END | 551 | 36 | 19,836 | R - Residential/Local | A - AC | 79 | 25.64 |
| SPRIDR 60 | 60 | SPRINGFIELD DR | SHARI LN | FOREST AVE | 1,124 | 63 | 70,812 | R - Residential/Local | A - AC | 37 | 4.08 |
| SPRUAV 10 | 10 | SPRUCE AVE | END | E. 1ST AVE | 496 | 32 | 15,872 | C - Collector | A - AC | 16 | 0 |
| SPRUAV 100 | 100 | SPRUCE AVE | E. 10TH AVE | E. LINDO AVE | 377 | 22 | 8,294 | C - Collector | A - AC | 86 | 18.59 |
| SPRUAV 20 | 20 | SPRUCE AVE | E. 1ST AVE | E. 2ND AVE | 393 | 32 | 12,576 | C - Collector | A - AC | 27 | 0.39 |
| SPRUAV 30 | 30 | SPRUCE AVE | E. 2ND AVE | E. 3RD AVE | 405 | 26 | 10,530 | C - Collector | A - AC | 7 | 0 |
| SPRUAV 40 | 40 | SPRUCE AVE | E. 3RD AVE | E. 4TH AVE | 420 | 26 | 10,920 | C - Collector | A - AC | 86 | 18.59 |
| SPRUAV 50 | 50 | SPRUCE AVE | E. 4TH AVE | E. 5TH AVE | 415 | 20 | 8,300 | C - Collector | A - AC | 89 | 20.16 |
| SPRUAV 60 | 60 | SPRUCE AVE | E. 5TH AVE | E. 7TH AVE | 856 | 24 | 20,544 | C - Collector | A - AC | 91 | 21.21 |
| SPRUAV 70 | 70 | SPRUCE AVE | E. 7TH AVE | E. 8TH AVE | 421 | 22 | 9,262 | C - Collector | A - AC | 88 | 19.64 |
| SPRUAV 80 | 80 | SPRUCE AVE | E. 8TH AVE | E. 9TH AVE | 421 | 22 | 9,262 | C - Collector | A - AC | 88 | 19.64 |
| SPRUAV 90 | 90 | SPRUCE AVE | E. 9TH AVE | E. 10TH AVE | 417 | 22 | 9,174 | C - Collector | A - AC | 86 | 18.59 |
| STAUDR 10 | 10 | ST AUGUSTINE DR | POTTER RD | PICHOLINE WY | 1,636 | 35 | 57,260 | R - Residential/Local | A - AC | 80 | 26.28 |
| STFRDR 10 | 10 | ST FRANCIS DR | HOOKER OAK AVE | HOOKER OAK AVE | 1,355 | 36 | 48,780 | R - Residential/Local | A - AC | 77 | 24.36 |
| STHELN 10 | 10 | ST HELENS LN | E. EATON RD | CUL-DE-SAC | 278 | 32 | 8,896 | R - Residential/Local | A - AC | 27 | 0.49 |
| STJACT 10 | 10 | ST JAMES CT | HOOKER OAK AVE | CUL-DE-SAC | 178 | 36 | 8,126 | R - Residential/Local | A - AC | 6 | 0 |
| STLAAV 10 | 10 | ST LAWRENCE AVE | E. EATON RD | HUDSON AVE | 1,460 | 36 | 52,560 | R - Residential/Local | A - AC | 82 | 27.55 |
| STMICT 10 | 10 | ST MICHAEL CT | MONTICELLO LN | END | 475 | 34 | 16,150 | R - Residential/Local | A - AC | 39 | 5 |
| STADWY 10 | 10 | STADIUM WAY | WARNER ST | END | 250 | 36 | 9,000 | R - Residential/Local | A - AC | 19 | 0 |
| STANCT 10 | 10 | STANSBURY CT | FOREST AVE | CUL-DE-SAC | 972 | 35 | 35,896 | R - Residential/Local | A - AC | 56 | 12.36 |
| STATWY 10 | 10 | STATICE WAY | E 23RD | YARROW DR | 302 | 25 | 7,550 | R - Residential/Local | A - AC | 79 | 25.78 |
| STERCT 10 | 10 | STERLING CT | MARILYN DR | CUL-DE-SAC | 491 | 37 | 19,891 | R - Residential/Local | A - AC | 72 | 21.23 |
| STOADR 10 | 10 | STERLING OAKS DR | WHITE CEDAR LN | COIT TOWER WAY | 950 | 32 | 30,400 | R - Residential/Local | A - AC | 82 | 27.55 |
| STOADR 20 | 20 | STERLING OAKS DR | COIT TOWER WAY | BRUCE RD | 150 | 32 | 4,800 | R - Residential/Local | A - AC | 85 | 29.41 |
| STEVWY 10 | 10 | STEVIEJOE WAY | SHERIDAN AVE | END | 155 | 36 | 5,580 | R - Residential/Local | A - AC | 18 | 0 |

Section PCI/RSL Listing

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-------------------|-----------------------|----------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| STEWAY 10 | 10 | STEWART AVE | BIDWELL AVE | BIDWELL PL | 1,370 | 26 | 35,620 | R - Residential/Local | A - AC | 75 | 23.24 |
| STEWAY 20 | 20 | STEWART AVE | BIDWELL PL | HWY 32 | 205 | 24 | 4,920 | R - Residential/Local | A - AC | 68 | 18.98 |
| STCARD 10 | 10 | STILSON CANYON RD | HUMBOLDT RD | PVMT CHNG (CATTLE GUARD) | 536 | 32 | 17,152 | R - Residential/Local | A - AC | 78 | 25 |
| STCARD 20 | 20 | STILSON CANYON RD | 590' E/O CATTLE GUARD | ONE LANE BRIDGE CITY LIMIT | 2,890 | 24 | 69,360 | R - Residential/Local | A - AC | 32 | 2.23 |
| STCARD 30 | 30 | STILSON CANYON RD | CATTLE GUARD | 590' E/O OF CATTLE GUARD | 590 | 24 | 14,160 | L - Local (7) | A - AC | 94 | 33.64 |
| STONDR 10 | 10 | STONEBRIDGE DR | W. SHASTA AVE | W. SHASTA AVE | 1,296 | 36 | 51,192 | R - Residential/Local | A - AC | 66 | 17.63 |
| STONCT 10 | 10 | STONEHAVEN CT | LONGWOOD DR | END | 430 | 26 | 11,180 | R - Residential/Local | A - AC | 94 | 33.64 |
| STPOWY 10 | 10 | STONE POINT WAY | PINYON HILLS DR | CUL-DE-SAC | 332 | 36 | 13,752 | R - Residential/Local | A - AC | 73 | 21.85 |
| STRAWY 10 | 10 | STRATFORD WAY | CUSSICK AVE | CUL-DE-SAC | 620 | 36 | 24,120 | R - Residential/Local | A - AC | 82 | 27.49 |
| SURIDR 10 | 10 | SUN RIVER DR | WHITEWOOD WY | DESCHUTES DR | 372 | 30 | 11,160 | R - Residential/Local | A - AC | 81 | 26.92 |
| SUNFCT 10 | 10 | SUNFLOWER CT | EAST AVE | END | 570 | 30 | 17,100 | R - Residential/Local | A - AC | 81 | 26.92 |
| SUNLDR 10 | 10 | SUNLAND DR | CERES AVE (N) | CERES AVE (S) | 1,345 | 36 | 48,420 | R - Residential/Local | A - AC | 18 | 0 |
| SUNSAV 10 | 10 | SUNSET AVE | E 3RD AVE | E 5TH AVE | 840 | 34 | 28,560 | R - Residential/Local | A - AC | 18 | 0 |
| SUNSAV 20 | 20 | SUNSET AVE | E 5TH AVE | E 7TH AVE | 835 | 38 | 31,730 | R - Residential/Local | A - AC | 20 | 0 |
| SUNSAV 30 | 30 | SUNSET AVE | E 7TH AVE | E LINDO AVE | 565 | 38 | 21,470 | R - Residential/Local | A - AC | 18 | 0 |
| SUNSAV 40 | 40 | SUNSET AVE | E. 1ST AVE | END | 353 | 36 | 12,708 | R - Residential/Local | A - AC | 55 | 11.81 |
| SURRLN 10 | 10 | SURREY LN | W. SHASTA AVE | SOMERSET PL | 393 | 36 | 14,148 | R - Residential/Local | A - AC | 47 | 8.11 |
| SWTAWY 10 | 10 | SWALLOWTAIL WAY | WANDERER LN | VICEROY DR | 760 | 28 | 21,280 | R - Residential/Local | A - AC | 81 | 26.92 |
| SWTAWY 20 | 20 | SWALLOWTAIL WAY | WANDERER LN | END | 300 | 26 | 7,800 | L - Local (7) | A - AC | 94 | 33.65 |
| SWOAWY 10 | 10 | SWAMP OAK WAY | CORK OAK WAY | END OF PVMT | 140 | 22 | 3,080 | R - Residential/Local | A - AC | 97 | 34.13 |
| SWWAFA 10 | 10 | SWEETWATER FALLS | MARIPOSA AVE | CINDER CREEK DR | 680 | 30 | 20,400 | R - Residential/Local | A - AC | 93 | 33.4 |
| SWWAFA 20 | 20 | SWEETWATER FALLS | CINDER CREEK DR | GRASSHOPPER CREEK WAY | 540 | 30 | 16,200 | R - Residential/Local | A - AC | 94 | 33.54 |
| SWWAFA 30 | 30 | SWEETWATER FALLS | MARIPOSA AVE | END | 250 | 30 | 7,500 | L - Local (7) | A - AC | 94 | 33.54 |
| SWITLN 10 | 10 | SWITZERLAND LN | ENGLAND ST | END | 222 | 24 | 5,328 | R - Residential/Local | A - AC | 78 | 25 |
| SYCADR 10 | 10 | SYCAMORE DR | 1877' W/O HICKS LN | 2541' W/O HICKS LN | 664 | 30 | 19,920 | R - Residential/Local | A - AC | 75 | 23.09 |
| SYCADR 20 | 20 | SYCAMORE DR | 218' W/O HICKS LN | 1877' W/O HICKS LN | 1,659 | 22 | 36,498 | R - Residential/Local | A - AC | 23 | 0 |
| SYCADR 30 | 30 | SYCAMORE DR | 2541' W/O HICKS LN | CUL-DE-SAC | 116 | 22 | 2,552 | R - Residential/Local | A - AC | 18 | 0 |
| SYCADR 40 | 40 | SYCAMORE DR | HICKS LN | 218' W/O HICKS LN | 218 | 28 | 6,104 | R - Residential/Local | A - AC | 15 | 0 |
| SYCAST 10 | 10 | SYCAMORE ST | WOODLAND AVE | E. 8TH ST | 753 | 35 | 26,355 | R - Residential/Local | A - AC | 58 | 13.31 |
| SYCAST 20 | 20 | SYCAMORE ST | E. 9TH ST | HUMBOLDT AVE | 676 | 18 | 12,168 | R - Residential/Local | A - AC | 70 | 19.96 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|------------------|------------------------|------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| TALBSR | 10 | TALBERT DR | FOREST AVE | BAR TRIANGLE ST | 1,245 | 37 | 46,065 | R - Residential/Local | A - AC | 64 | 16.56 |
| TEMPWY | 10 | TEMPERANCE WAY | FOREST AVE | END | 565 | 32 | 18,080 | R - Residential/Local | A - AC | 81 | 26.92 |
| TERRDR | 10 | TERRACE DR | N/E 1ST AVE (WEST END) | N/E 1ST AVE (EAST END) | 1,485 | 25 | 37,125 | C - Collector | A - AC | 95 | 23.12 |
| TERRDR | 20 | TERRACE DR | S/E 1ST AVE (WEST END) | S/E 1ST AVE (EAST END) | 1,465 | 25 | 36,625 | C - Collector | A - AC | 95 | 23.12 |
| TERRLN | 10 | TERRACE LN | DOWNING AVE | CUL-DE-SAC | 186 | 36 | 8,414 | R - Residential/Local | A - AC | 76 | 23.73 |
| THOMLN | 10 | THOMAS LN | HAWTHORNE AVE | CUL-DE-SAC | 225 | 32 | 9,218 | C - Collector | A - AC | 14 | 0 |
| THOACT | 10 | THREE OAKS CT | MARAUDER ST | END | 370 | 26 | 9,620 | R - Residential/Local | A - AC | 82 | 27.55 |
| THUNST | 10 | THUNDERBOLT ST | SIKORSKY AVE | BOEING AVE | 674 | 38 | 25,612 | R - Residential/Local | A - AC | 33 | 2.59 |
| TIFFWY | 10 | TIFFANY WAY | W 12TH AVE | RITCHIE CIR | 620 | 38 | 23,560 | R - Residential/Local | A - AC | 80 | 26.22 |
| TILDEN | 10 | TILDEN LN | CALIFORNIA PARK DR | CUL-DE-SAC | 522 | 33 | 19,169 | C - Collector | A - AC | 33 | 1.45 |
| TICKRWY | 10 | TINKER CREEK WAY | NILES CANYON | WINDHAM WAY | 1,205 | 28 | 33,740 | R - Residential/Local | A - AC | 97 | 34.12 |
| TIOGWY | 10 | TIOGA WAY | PARKHURST ST | N/O COMSTOCK RD | 502 | 37 | 18,574 | R - Residential/Local | A - AC | 80 | 26.22 |
| TIOGWY | 20 | TIOGA WAY | S/O COMSTOCK RD | JASPER DR | 229 | 37 | 8,473 | R - Residential/Local | A - AC | 58 | 13.31 |
| TITLWY | 10 | TITLEIST WAY | DELANEY DR | DELANEY DR | 625 | 30 | 18,750 | R - Residential/Local | A - AC | 86 | 30.06 |
| TOPOAV | 10 | TOM POLK AVE | NORTH END | PANAMA AVE | 465 | 22 | 10,230 | R - Residential/Local | A - AC | 34 | 2.96 |
| TOPOAV | 20 | TOM POLK AVE | PANAMA AVE | EAST AVE | 615 | 36 | 22,140 | R - Residential/Local | A - AC | 67 | 18.26 |
| TOPOAV | 30 | TOM POLK AVE | EAST AVE | WHITE AVE | 615 | 36 | 22,140 | R - Residential/Local | A - AC | 91 | 32.52 |
| TOPOCR | 10 | TOM POLK CIR | TOM POLK AVE | TOM POLK CIR | 1,485 | 24 | 35,640 | R - Residential/Local | A - AC | 53 | 10.96 |
| TONEWY | 10 | TONEA WAY | CURVE IN ROAD | CUL-DE-SAC | 1,015 | 36 | 38,258 | R - Residential/Local | A - AC | 26 | 0.44 |
| TONEWY | 20 | TONEA WAY | ESPLANADE | CUL-DE-SAC | 757 | 32 | 26,242 | R - Residential/Local | A - AC | 18 | 0 |
| TOFLCT | 10 | TOP FLIGHT CT | DELANEY | END | 180 | 30 | 5,400 | R - Residential/Local | A - AC | 83 | 28.18 |
| TOHACT | 10 | TOP HAND CT | W. SHASTA AVE | CUL-DE-SAC | 470 | 36 | 18,720 | R - Residential/Local | A - AC | 64 | 16.62 |
| TORICT | 10 | TORINO CT | BRENNI WY | CUL-DE-SAC | 170 | 32 | 7,458 | R - Residential/Local | A - AC | 78 | 25 |
| TOWSRD | 10 | TOWSER RD | REMINGTON DR | CUL-DE-SAC | 413 | 37 | 17,089 | R - Residential/Local | A - AC | 74 | 22.47 |
| TOYOWY | 10 | TOYON WAY | HILL VIEW WY | PALMETTO AVE | 320 | 36 | 11,520 | R - Residential/Local | A - AC | 32 | 2.53 |
| TRACLN | 10 | TRACY LN | CERES AVE | WEST END | 550 | 36 | 19,800 | R - Residential/Local | A - AC | 32 | 2.23 |
| TRADCT | 10 | TRADEWINDS CT | LEXINGTON DR | CUL-DE-SAC | 150 | 33 | 4,950 | R - Residential/Local | A - AC | 58 | 13.37 |
| TRENDR | 10 | TRENTA DR | MOYER WAY | W LINDO AVE | 835 | 34 | 28,390 | R - Residential/Local | A - AC | 82 | 27.49 |
| TRIEWY | 10 | TRIESTE WY | TRACY LN | CUL-DE-SAC | 520 | 36 | 18,720 | R - Residential/Local | A - AC | 27 | 0.49 |
| TRINLN | 10 | TRINITY LN | CALIFORNIA PARK DR | CUL-DE-SAC | 331 | 33 | 12,866 | C - Collector | A - AC | 37 | 2.31 |
| TURIWY | 10 | TULE RIVER WAY | ROGUE RIVER DR | BURNT RANCH WAY | 320 | 28 | 8,960 | R - Residential/Local | A - AC | 82 | 27.49 |
| TUOLDR | 10 | TUOLUMNE DR | EAST AVE | ALBION CT | 940 | 36 | 33,840 | R - Residential/Local | A - AC | 78 | 25 |

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|------------------------|-----------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| TUWELL | 10 | TURNBRIDGE | WELLES ASHBURTON LN | CUL-DE-SAC | 514 | 32 | 16,448 | R - Residential/Local | A - AC | 73 | 21.85 |
| TUWELL | 20 | TURNBRIDGE | WELLES LUPIN AVE | ASHBURTON LN | 694 | 32 | 22,208 | R - Residential/Local | A - AC | 67 | 18.26 |
| UPLADR | 10 | UPLAND DR | WHITEWOOD WY | DESCHUTES DR | 360 | 30 | 10,800 | R - Residential/Local | A - AC | 55 | 11.87 |
| UPLACT | 10 | UPPER LAKE CT | CALIFORNIA PARK DR | CUL-DE-SAC | 404 | 32 | 14,946 | R - Residential/Local | A - AC | 81 | 26.92 |
| UPPARD | 10 | UPPER PARK RD | WILDWOOD AVE | FIVE MILE RD | 2,640 | 28 | 73,920 | C - Collector | A - AC | 60 | 8.38 |
| UPPARD | 20 | UPPER PARK RD | FIVE MILE RD | GOLF COURSE RD | 2,810 | 25 | 70,250 | C - Collector | A - AC | 21 | 0 |
| UPPARD | 30 | UPPER PARK RD | GOLF COURSE RD | 23458 FT E/O GOLF COURSE RD | 1,700 | 23 | 39,100 | C - Collector | A - AC | 61 | 8.7 |
| VAILDR | 10 | VAIL DR | ASPEN GLEN DR | SNOWBIRD DR | 1,017 | 24 | 24,408 | R - Residential/Local | A - AC | 82 | 27.49 |
| VALHPL | 10 | VALHALLA PL | EAST AVE | END | 595 | 28 | 16,660 | R - Residential/Local | A - AC | 84 | 28.8 |
| VALILN | 10 | VALINE LN | MEYERS ST | CUL-DE-SAC | 450 | 40 | 19,827 | R - Residential/Local | A - AC | 22 | 0 |
| VAFODR | 10 | VALLEY FORGE DR | CEANOTHUS AVE | MARIGOLD AVE | 715 | 36 | 25,740 | R - Residential/Local | A - AC | 79 | 25.64 |
| VAFODR | 20 | VALLEY FORGE DR | CEANOTHUS AVE | END | 935 | 36 | 33,660 | L - Local (7) | A - AC | 79 | 25.64 |
| VAOADR | 10 | VALLEY OAK DR | OAK WY | WOODVIEW DR | 855 | 36 | 32,580 | R - Residential/Local | A - AC | 74 | 22.41 |
| VALLAV | 10 | VALLOMBROSA AVE | SHERIDAN AVE | VALLOMBROSA LN W/S | 3,060 | 28 | 85,680 | R - Residential/Local | A - AC | 81 | 26.92 |
| VALLAV | 100 | VALLOMBROSA AVE | SHERIDAN AVE | 550 FT E/O ARBUTUS AVE | 1,680 | 24 | 40,320 | R - Residential/Local | A - AC | 45 | 7.37 |
| VALLAV | 20 | VALLOMBROSA AVE | 307 FT W/O ARBUTUS AVE | 550 FT E/O ARBUTUS AVE | 890 | 26 | 23,140 | R - Residential/Local | A - AC | 74 | 22.41 |
| VALLAV | 30 | VALLOMBROSA AVE | CAMELLIA WAY | MANGROVE AVE | 560 | 62 | 34,720 | A - Arterial | A - AC | 75 | 16.91 |
| VALLAV | 40 | VALLOMBROSA AVE | CRISTER AVE W/S | MADRONE AVE W/S | 1,768 | 28 | 49,504 | R - Residential/Local | A - AC | 67 | 18.26 |
| VALLAV | 50 | VALLOMBROSA AVE | LILAC LN W/S | MANZANITA AVE | 2,836 | 28 | 79,408 | R - Residential/Local | A - AC | 75 | 23.09 |
| VALLAV | 60 | VALLOMBROSA AVE | MADRONE AVE W/S | LILAC LN W/S | 2,242 | 30 | 67,260 | C - Collector | A - AC | 63 | 9.36 |
| VALLAV | 70 | VALLOMBROSA AVE | MANGROVE AVE | 307 FT W/O ARBUTUS AVE | 1,203 | 70 | 84,210 | R - Residential/Local | A - AC | 67 | 18.2 |
| VALLAV | 90 | VALLOMBROSA AVE | VALLOMBROSA LN W/S | CRISTER AVE W/S | 875 | 28 | 24,500 | A - Arterial | A - AC | 80 | 19.31 |
| VALLCR | 10 | VALLOMBROSA CIR | VALLOMBROSA AVE | CUL-DE-SAC | 454 | 35 | 17,683 | R - Residential/Local | A - AC | 18 | 0 |
| VALLWY | 10 | VALLOMBROSA WAY | VALLOMBROSA AVE | END | 1,235 | 36 | 44,460 | R - Residential/Local | A - AC | 82 | 27.49 |
| VENECT | 10 | VENETIAN CT | W LASSEN AVE | END | 280 | 24 | 6,720 | R - Residential/Local | A - AC | 89 | 31.57 |
| VERBAV | 10 | VERBENA AVE | E 1ST AVE | HAWTHORNE AVE | 667 | 30 | 20,010 | C - Collector | A - AC | 31 | 1.04 |
| VERBLN | 10 | VERBENA LN | HAWTHORNE AV | SOUTH END | 275 | 15 | 4,125 | C - Collector | A - AC | 16 | 0 |
| VERDCT | 10 | VERDE CT | NORTH AVE | CUL-DE-SAC | 518 | 32 | 16,576 | R - Residential/Local | A - AC | 80 | 26.28 |
| VERDDR | 10 | VERDE DR | NORTH AV | END | 550 | 35 | 19,250 | C - Collector | A - AC | 19 | 0 |
| VERMCR | 10 | VERMILLION CIR | N. CUL-DE-SAC | S. CUL-DE-SAC | 874 | 34 | 31,755 | R - Residential/Local | A - AC | 41 | 5.78 |

Section PCI/RSL Listing

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current | Remaining |
|-----------|------------|----------------|--------------------------|--------------------------|--------|-------|--------|-----------------------|--------------|---------|-----------|
| VIFLCT | 10 | VIA FLORA CT | NORTH AVE | CUL-DE-SAC | 513 | 32 | 16,416 | R - Residential/Local | A - AC | PCI | Life |
| VIMDR | 10 | VIA MISSION DR | BARONI DR | POTTER RD | 1,409 | 35 | 49,315 | R - Residential/Local | A - AC | 77 | 24.36 |
| VIMDR | 20 | VIA MISSION DR | BRUCE DR | BARONI DR | 916 | 36 | 32,976 | R - Residential/Local | A - AC | 80 | 26.28 |
| VIVECR | 10 | VIA VERONA CIR | CERES AVE | END | 500 | 36 | 18,000 | R - Residential/Local | A - AC | 22 | 0 |
| VIVEDR | 10 | VIA VERONA DR | CERES AVE | NORTH AVE | 1,265 | 36 | 45,540 | R - Residential/Local | A - AC | 19 | 0 |
| VICEDR | 10 | VICEROY DR | MARIPOSA AVE | SWALLOWTAIL WAY | 500 | 36 | 18,000 | R - Residential/Local | A - AC | 80 | 26.28 |
| VICEDR | 20 | VICEROY DR | SWALLOWTAIL WAY | PARMASSIAN AVE | 630 | 36 | 22,680 | R - Residential/Local | A - AC | 82 | 27.55 |
| VICEDR | 30 | VICEROY DR | PARMASSIAN AVE | ARCH WAY | 295 | 36 | 10,620 | R - Residential/Local | A - AC | 82 | 27.55 |
| VICKCT | 10 | VICKSBURG CT | MILL CREEK DR | CUL-DE-SAC | 460 | 36 | 18,360 | R - Residential/Local | A - AC | 76 | 23.67 |
| VICTWY | 10 | VICTORIA WAY | JUNIPER ST | CUL-DE-SAC | 456 | 32 | 14,592 | R - Residential/Local | A - AC | 76 | 23.73 |
| VIQACT | 10 | VILLA OAK CT | BLACK OAK DR | CUL-DE-SAC | 245 | 36 | 10,692 | R - Residential/Local | A - AC | 82 | 27.49 |
| VILLN | 10 | VILLAGE LN | E. 1ST AVE | KAREN DR | 515 | 36 | 18,540 | R - Residential/Local | A - AC | 43 | 6.47 |
| VINTCT | 10 | VINTAGE CT | MARIGOLD AVE | CUL-DE-SAC | 206 | 34 | 8,872 | R - Residential/Local | A - AC | 78 | 25 |
| VIRALN | 10 | VIRAGE LN | ARCH WAY | ARCH WAY | 320 | 32 | 10,240 | R - Residential/Local | A - AC | 83 | 28.18 |
| VISTWY | 10 | VISTAMONT WAY | LUCY WAY | WANDERER LN | 505 | 28 | 14,140 | R - Residential/Local | A - AC | 83 | 28.18 |
| VISTWY | 20 | VISTAMONT WAY | WANDERER LN | VICEROY DR | 760 | 28 | 21,280 | R - Residential/Local | A - AC | 81 | 26.92 |
| VOLCDR | 10 | VOLCANO DR | CRATER LAKE DR | DENALI DR | 520 | 36 | 18,720 | R - Residential/Local | A - AC | 82 | 27.5 |
| W10TAV | 10 | W 10TH AVE | ESPLANADE | MAGNOLIA AVE | 474 | 30 | 14,220 | R - Residential/Local | A - AC | 48 | 8.55 |
| W10TAV | 20 | W 10TH AVE | MAGNOLIA AVE | CITRUS AVE | 805 | 24 | 19,320 | R - Residential/Local | A - AC | 18 | 0 |
| W10TST | 10 | W 10TH ST | IVY ST | CHERRY ST | 298 | 36 | 10,728 | R - Residential/Local | A - AC | 76 | 23.67 |
| W10TST | 20 | W 10TH ST | SALEM ST | HAZEL ST | 900 | 36 | 32,400 | R - Residential/Local | A - AC | 24 | 0 |
| W11TAV | 10 | W 11TH AVE | 105 FT E/O SAVANNAH LN | 112 FT W/O SUN CIRCLE CT | 757 | 32 | 24,224 | R - Residential/Local | A - AC | 24 | 0 |
| W11TAV | 20 | W 11TH AVE | 112 FT W/O SUN CIRCLE CT | HOLLY AVE | 1,186 | 32 | 37,952 | R - Residential/Local | A - AC | 53 | 10.85 |
| W11TAV | 30 | W 11TH AVE | ESPLANADE DR | 105 FT E/O SAVANNAH LN | 986 | 25 | 24,650 | R - Residential/Local | A - AC | 38 | 4.41 |
| W11TAV | 40 | W 11TH AVE | HOLLY AVE | MEADOW DR | 978 | 32 | 31,296 | R - Residential/Local | A - AC | 78 | 24.94 |
| W11TAV | 50 | W 11TH AVE | MEADOW DR | MOYER AVE | 2,159 | 36 | 77,724 | C - Collector | A - AC | 61 | 8.64 |
| W11TST | 10 | W 11TH ST | BROADWAY ST W/S | CHESTNUT ST E/S | 956 | 36 | 34,416 | C - Collector | A - AC | 23 | 0 |
| W11TST | 20 | W 11TH ST | CHESTNUT ST W/S | WEST CUL-DE-SAC | 823 | 45 | 37,035 | C - Collector | A - AC | 21 | 0 |
| W11TST | 30 | W 11TH ST | PARK AVE | BROADWAY ST E/S | 556 | 45 | 25,020 | C - Collector | A - AC | 25 | 0.01 |
| W12TAV | 10 | W 12TH AVE | WLINDO AV | ZUNI | 918 | 20 | 18,360 | C - Collector | A - AC | 82 | 16.59 |
| W12TAV | 20 | W 12TH AVE | ZUNI AV | HOLLY AV | 1,495 | 32 | 47,840 | C - Collector | A - AC | 63 | 9.3 |

Criteria:

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SS1030

MTC StreetSaver

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-------------|-----------------|-----------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| W12TAV | 30 | W 12TH AVE | HOLLY AVE | FERN AVE | 2,960 | 32 | 94,720 | C - Collector | A - AC | 82 | 16.59 |
| W12TAV | 40 | W 12TH AVE | FERN AVE | MOYER WY | 589 | 36 | 21,204 | C - Collector | A - AC | 82 | 16.59 |
| W12TST | 10 | W 12TH ST | BROADWAY ST W/S | COBBLESTONE DR | 1,741 | 25 | 44,135 | C - Collector | A - AC | 58 | 7.7 |
| W12TST | 20 | W 12TH ST | OAKDALE ST W/S | BROADWAY ST E/S | 296 | 35 | 10,360 | C - Collector | A - AC | 80 | 15.64 |
| W12TST | 30 | W 12TH ST | PARK AVE | OAKDALE ST W/S | 345 | 45 | 15,525 | R - Residential/Local | A - AC | 69 | 19.37 |
| W13TST | 10 | W 13TH ST | NORMAL AVE | CHESTNUT ST | 305 | 30 | 9,150 | R - Residential/Local | A - AC | 42 | 6 |
| W13TST | 20 | W 13TH ST | PARK AVE | BROADWAY ST | 580 | 25 | 14,500 | C - Collector | A - AC | 17 | 0 |
| W14TST | 10 | W 14TH ST | BROADWAY ST | WEST END | 1,212 | 36 | 43,632 | C - Collector | A - AC | 57 | 7.4 |
| W14TST | 20 | W 14TH ST | PARK AVE | BROADWAY ST | 635 | 25 | 15,875 | R - Residential/Local | A - AC | 50 | 9.45 |
| W15TST | 10 | W 15TH ST | BROADWAY ST | CHESTNUT ST | 960 | 30 | 28,800 | C - Collector | A - AC | 28 | 0.59 |
| W15TST | 20 | W 15TH ST | PARK AVE | BROADWAY ST | 590 | 30 | 17,700 | R - Residential/Local | A - AC | 69 | 19.37 |
| W16TST | 10 | W 16TH ST | BROADWAY ST W/S | CHESTNUT ST W/S | 1,082 | 45 | 48,690 | C - Collector | A - AC | 49 | 5.14 |
| W16TST | 20 | W 16TH ST | PARK AVE | BROADWAY ST E/S | 635 | 45 | 28,575 | C - Collector | A - AC | 54 | 6.51 |
| W17TST | 10 | W 17TH ST | BROADWAY ST W/S | WEST END | 830 | 25 | 20,750 | C - Collector | A - AC | 25 | 0.01 |
| W17TST | 20 | W 17TH ST | PARK AVE | BROADWAY ST E/S | 590 | 45 | 26,550 | C - Collector | A - AC | 27 | 0.39 |
| W18TST | 10 | W 18TH ST | BROADWAY ST W/S | WEST END | 885 | 25 | 22,125 | C - Collector | A - AC | 28 | 0.59 |
| W18TST | 20 | W 18TH ST | PARK AVE | BROADWAY ST E/S | 635 | 36 | 22,860 | C - Collector | A - AC | 33 | 1.61 |
| W19TST | 10 | W 19TH ST | BROADWAY | NORMAL AVE | 741 | 24 | 17,784 | R - Residential/Local | A - AC | 26 | 0.44 |
| W19TST | 20 | W 19TH ST | PARK AVE | BROADWAY ST E/S | 635 | 36 | 22,860 | C - Collector | A - AC | 2 | 0 |
| W1STAV | 10 | W 1ST AVE | CITRUS AVE W/S | WARNER ST E/S | 820 | 36 | 29,520 | R - Residential/Local | A - AC | 41 | 5.6 |
| W1STAV | 20 | W 1ST AVE | ESPLANADE DR | CITRUS AVE W/S | 1,510 | 36 | 54,360 | R - Residential/Local | A - AC | 57 | 12.81 |
| W1STAV | 30 | W 1ST AVE | WARNER ST W/S | N. CEDAR ST E/S | 1,444 | 36 | 51,984 | R - Residential/Local | A - AC | 20 | 0 |
| W1STST | 10 | W 1ST ST | MAIN ST W/S | SALEM ST W/S | 650 | 50 | 32,500 | R - Residential/Local | A - AC | 93 | 33.34 |
| W1STST | 20 | W 1ST ST | WALNUT ST W/S | OAK ST W/S | 340 | 36 | 12,240 | R - Residential/Local | A - AC | 57 | 12.81 |
| W1STST | 30 | W 1ST ST | WARNER ST | WALNUT ST E/S | 1,245 | 50 | 62,250 | R - Residential/Local | A - AC | 83 | 28.13 |
| W20TST | 10 | W 20TH ST | PARK AVE | NORMAL AVE | 1,376 | 36 | 49,536 | R - Residential/Local | A - AC | 16 | 0 |
| W21SST | 10 | W 21ST ST | PARK AVE | SALEM ST | 1,066 | 36 | 38,376 | R - Residential/Local | A - AC | 81 | 26.86 |
| W22NST | 10 | W 22ND ST | PARK AVE | ESTES RD | 1,633 | 36 | 58,788 | R - Residential/Local | A - AC | 80 | 26.33 |
| W2NDAV | 10 | W 2ND AVE | CITRUS AVE W/S | WARNER ST E/S | 795 | 25 | 19,875 | R - Residential/Local | A - AC | 32 | 2.54 |
| W2NDAV | 20 | W 2ND AVE | ESPLANADE DR | CITRUS AVE E/S | 1,396 | 25 | 34,900 | R - Residential/Local | A - AC | 46 | 7.68 |
| W2NDAV | 30 | W 2ND AVE | WARNER ST W/S | N. CEDAR ST E/S | 1,445 | 36 | 52,020 | R - Residential/Local | A - AC | 49 | 9 |
| W2NDST | 10 | W 2ND ST | IVY ST W/S | WALNUT ST E/S | 1,340 | 50 | 67,000 | R - Residential/Local | A - AC | 93 | 33.34 |
| W2NDST | 20 | W 2ND ST | MAIN ST W/S | SALEM ST W/S | 582 | 50 | 29,100 | R - Residential/Local | A - AC | 93 | 33.34 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-------------|----------------------------|----------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| W2NDST 30 | 30 | W 2ND ST | SALEM ST W/S | IVY ST W/S | 1,340 | 50 | 67,000 | R - Residential/Local | A - AC | 93 | 33.34 |
| W2NDST 40 | 40 | W 2ND ST | WALNUT ST W/S | OAK ST | 300 | 36 | 10,800 | R - Residential/Local | A - AC | 32 | 2.54 |
| W3RDST 10 | 10 | W 3RD AVE | ARCADIAN AVE W/S | WEST END | 234 | 36 | 8,424 | R - Residential/Local | A - AC | 65 | 17.06 |
| W3RDST 20 | 20 | W 3RD AVE | ESPLANADE DR | ARCADIAN AVE E/S | 975 | 25 | 24,375 | R - Residential/Local | A - AC | 50 | 9.45 |
| W3RDST 10 | 10 | W 3RD ST | IVY ST W/S | ORANGE ST E/S | 645 | 36 | 23,220 | R - Residential/Local | A - AC | 54 | 11.33 |
| W3RDST 20 | 20 | W 3RD ST | MAIN ST W/S | SALEM ST E/S | 586 | 36 | 21,096 | R - Residential/Local | A - AC | 93 | 33.34 |
| W3RDST 30 | 30 | W 3RD ST | ORANGE ST W/S | WALNUT ST E/S | 645 | 36 | 23,220 | R - Residential/Local | A - AC | 21 | 0 |
| W3RDST 40 | 40 | W 3RD ST | PVMT CHNG (144 FT W/O OAK) | SANTA CLARA AVE | 1,162 | 36 | 41,832 | R - Residential/Local | A - AC | 12 | 0 |
| W3RDST 50 | 50 | W 3RD ST | SALEM ST W/S | IVY ST E/S | 1,285 | 36 | 46,260 | R - Residential/Local | A - AC | 27 | 0.78 |
| W3RDST 60 | 60 | W 3RD ST | WALNUT ST W/S | PVMT CHNG (144 FT W/O OAK) | 478 | 37 | 17,686 | R - Residential/Local | A - AC | 85 | 29.35 |
| W4THAV 10 | 10 | W 4TH AVE | CITRUS AVE W/S | WARNER ST W/S | 865 | 36 | 31,140 | C - Collector | A - AC | 82 | 16.59 |
| W4THAV 20 | 20 | W 4TH AVE | ESPLANADE DR | CITRUS AVE W/S | 1,506 | 36 | 54,216 | C - Collector | A - AC | 82 | 16.59 |
| W4THAV 30 | 30 | W 4TH AVE | N. CEDAR ST E/S | WEST CUL-DE-SAC | 495 | 36 | 19,790 | R - Residential/Local | A - AC | 32 | 2.54 |
| W4THAV 40 | 40 | W 4TH AVE | WARNER ST W/S | N. CEDAR ST E/S | 1,445 | 36 | 52,020 | R - Residential/Local | A - AC | 82 | 27.5 |
| W4THST 10 | 10 | W 4TH ST | BROADWAY ST E/S | MAIN ST W/S | 296 | 36 | 10,656 | R - Residential/Local | A - AC | 82 | 27.5 |
| W4THST 20 | 20 | W 4TH ST | CEDAR ST | WALNUT ST E/S | 300 | 36 | 10,800 | R - Residential/Local | A - AC | 55 | 11.81 |
| W4THST 30 | 30 | W 4TH ST | IVY ST W/S | ORANGE ST E/S | 650 | 36 | 23,400 | R - Residential/Local | A - AC | 14 | 0 |
| W4THST 40 | 40 | W 4TH ST | OAK ST S/S | MAPLE ST | 970 | 36 | 34,920 | R - Residential/Local | A - AC | 26 | 0.44 |
| W4THST 50 | 50 | W 4TH ST | SALEM ST E/S | BROADWAY ST W/S | 290 | 36 | 10,440 | R - Residential/Local | A - AC | 82 | 27.5 |
| W4THST 60 | 60 | W 4TH ST | SALEM ST W/S | IVY ST E/S | 1,285 | 36 | 46,260 | R - Residential/Local | A - AC | 37 | 4.03 |
| W4THST 70 | 70 | W 4TH ST | WALNUT ST W/S | OAK ST W/S | 334 | 36 | 12,024 | R - Residential/Local | A - AC | 58 | 13.31 |
| W5THAV 10 | 10 | W 5TH AVE | ESPLANADE DR | ARCADIAN AVE | 975 | 36 | 35,100 | R - Residential/Local | A - AC | 94 | 33.59 |
| W5THST 10 | 10 | W 5TH ST | HICKORY ST W/S | MILLER AVE E/S | 790 | 36 | 28,440 | R - Residential/Local | A - AC | 28 | 1.12 |
| W5THST 20 | 20 | W 5TH ST | IVY ST W/S | WALNUT ST E/S | 1,340 | 36 | 48,240 | R - Residential/Local | A - AC | 60 | 14.35 |
| W5THST 30 | 30 | W 5TH ST | MAIN ST W/S | SALEM ST E/S | 586 | 36 | 21,096 | R - Residential/Local | A - AC | 54 | 11.33 |
| W5THST 40 | 40 | W 5TH ST | SALEM ST W/S | IVY ST E/S | 1,285 | 36 | 46,260 | R - Residential/Local | A - AC | 81 | 26.86 |
| W5THST 50 | 50 | W 5TH ST | WALNUT ST W/S | HICKORY ST W/S | 1,000 | 36 | 36,000 | R - Residential/Local | A - AC | 48 | 8.55 |
| W6THAV 10 | 10 | W 6TH AVE | ARCADIAN AVE W/S | WARNER ST W/S | 1,360 | 25 | 34,000 | R - Residential/Local | A - AC | 46 | 7.79 |
| W6THAV 20 | 20 | W 6TH AVE | ESPLANADE DR | MAGNOLIA AVE W/S | 515 | 36 | 18,540 | R - Residential/Local | A - AC | 73 | 21.79 |
| W6THAV 30 | 30 | W 6TH AVE | MAGNOLIA AVE W/S | ARCADIAN AVE E/S | 460 | 36 | 16,560 | R - Residential/Local | A - AC | 94 | 33.59 |
| W6THAV 40 | 40 | W 6TH AVE | WARNER ST W/S | N. CHERRY ST W/S | 516 | 36 | 18,576 | R - Residential/Local | A - AC | 37 | 4.03 |
| W6THST 10 | 10 | W 6TH ST | CEDAR ST W/S | WALNUT ST E/S | 302 | 36 | 10,872 | R - Residential/Local | A - AC | 19 | 0 |

Section PCI/RSL Listing

Printed: 03/31/2016

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------------|--------------------------------|-------------------------|--------|-------|---------|-----------------------|--------------|-------------|----------------|
| W6THST | 20 | W 6TH ST | IVY ST W/S | ORANGE ST E/S | 645 | 36 | 23,220 | R - Residential/Local | A - AC | 46 | 7.68 |
| W6THST | 30 | W 6TH ST | MAIN ST W/S | SALEM ST E/S | 586 | 36 | 21,096 | R - Residential/Local | A - AC | 66 | 17.63 |
| W6THST | 40 | W 6TH ST | SALEM ST W/S | IVY ST E/S | 1,285 | 36 | 46,260 | R - Residential/Local | A - AC | 60 | 14.35 |
| W6THST | 50 | W 6TH ST | WALNUT ST W/S | OAK ST | 300 | 36 | 10,800 | R - Residential/Local | A - AC | 14 | 0 |
| W6THST | 60 | W 6TH ST | ORANGE ST | WEST END | 110 | 40 | 4,400 | R - Residential/Local | A - AC | 5 | 0 |
| W6THST | 70 | W 6TH ST | CEDAR ST | EAST END | 120 | 22 | 2,640 | R - Residential/Local | A - AC | 14 | 0 |
| W7THAV | 10 | W 7TH AVE | ESPLANADE DR | ARCADIAN AVE E/S | 945 | 25 | 23,625 | R - Residential/Local | A - AC | 0 | 0 |
| W7THST | 10 | W 7TH ST | HICKORY ST E/S | POMONA AVE | 640 | 24 | 15,360 | R - Residential/Local | A - AC | 1 | 0 |
| W7THST | 20 | W 7TH ST | IVY ST W/S | ORANGE ST E/S | 645 | 36 | 46,152 | R - Residential/Local | A - AC | 55 | 11.81 |
| W7THST | 30 | W 7TH ST | MAIN ST W/S | SALEM ST E/S | 586 | 36 | 21,096 | R - Residential/Local | A - AC | 54 | 11.33 |
| W7THST | 40 | W 7TH ST | ORANGE ST W/S | WALNUT ST E/S | 637 | 36 | 22,932 | R - Residential/Local | A - AC | 3 | 0 |
| W7THST | 50 | W 7TH ST | SALEM ST W/S | IVY ST E/S | 1,285 | 36 | 46,260 | R - Residential/Local | A - AC | 42 | 6.11 |
| W7THST | 60 | W 7TH ST | WALNUT ST W/S | HICKORY ST E/S | 950 | 36 | 34,200 | R - Residential/Local | A - AC | 24 | 0 |
| W8THAV | 10 | W 8TH AVE | CITY LIMIT(137 FT E/O CHERRY) | GREENWICH DR W/S | 2,058 | 25 | 51,450 | R - Residential/Local | A - AC | 82 | 27.5 |
| W8THAV | 20 | W 8TH AVE | FORTY NINER CT N/S | W. SACRAMENTO AVE | 905 | 36 | 32,580 | R - Residential/Local | A - AC | 61 | 14.88 |
| W8THAV | 30 | W 8TH AVE | GREENWICH DR W/S | NORD AVE | 1,252 | 30 | 37,560 | R - Residential/Local | A - AC | 72 | 21.18 |
| W8THAV | 40 | W 8TH AVE | NORD AVE | FORTY NINER CT N/S | 935 | 36 | 33,660 | R - Residential/Local | A - AC | 82 | 27.5 |
| W8THAV | 50 | W 8TH AVE | CITY LIMIT (137 FT E/O CHERRY) | ESPLANADE | 1,497 | 28 | 41,916 | R - Residential/Local | A - AC | 82 | 27.5 |
| W8THST | 10 | W 8TH ST | WALNUT ST W/S | OAK ST | 300 | 32 | 9,600 | R - Residential/Local | A - AC | 18 | 0 |
| W9THAV | 10 | W 9TH AVE | ESPLANADE DR | MAGNOLIA AVE | 238 | 32 | 7,616 | R - Residential/Local | A - AC | 27 | 0.78 |
| W9THST | 10 | W 9TH ST | WALNUT ST W/S | END | 468 | 32 | 14,976 | R - Residential/Local | A - AC | 34 | 3.27 |
| WEASAV | 30 | W EAST AVE | GUYNN AVE | ALAMO AVE | 1,300 | 58 | 75,400 | C - Collector | A - AC | 68 | 11.07 |
| WEASAV | 40 | W EAST AVE | ALAMO AVE | HOLLY AVE | 1,440 | 58 | 83,520 | C - Collector | A - AC | 73 | 12.61 |
| WEASAV | 50 | W EAST AVE | HOLLY AVE | ILAHEE LN | 380 | 62 | 23,560 | C - Collector | A - AC | 82 | 16.59 |
| WEASAV | 60 | W EAST AVE | ILAHEE LN | HARVEST PARK CT | 945 | 58 | 54,810 | C - Collector | A - AC | 80 | 15.64 |
| WEASAV | 70 | W EAST AVE | HARVEST PARK CT | ESPLANADE | 1,985 | 74 | 146,890 | C - Collector | A - AC | 82 | 16.59 |
| WEATRD | 10 | W EATON RD | 505 FT W/O ESPLANADE DR | W/O EATON VILLAGE DR | 980 | 36 | 35,280 | A - Arterial | A - AC | 34 | 2.31 |
| WEATRD | 20 | W EATON RD | ESPLANADE DR | 505 FT W/O ESPLANADE DR | 505 | 55 | 27,775 | A - Arterial | A - AC | 80 | 19.26 |
| WFRWAV | 10 | W FRANCES WILLARD AVE | ESPLANADE DR | CITRUS AVE | 1,436 | 29 | 41,644 | R - Residential/Local | A - AC | 57 | 12.81 |
| WLASAV | 10 | W LASSEN AVE | CAMDEN CT E/S | CUSSICK AVE | 1,820 | 30 | 54,600 | C - Collector | A - AC | 17 | 0 |

Criteria:

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SS1030

MTC StreetSaver

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|------------------|------------------------|------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| WLASAV | 20 | W LASSEN AVE | ESPLANADE DR | CAMDEN CT E/S | 1,480 | 30 | 44,400 | C - Collector | A - AC | 27 | 0.39 |
| WLCNAV | 10 | W LINCOLN AVE | CITRUS AVE | ESPLANADE DR | 1,454 | 29 | 42,166 | R - Residential/Local | A - AC | 19 | 0 |
| WLINAV | 10 | W LINDO AVE | CHANNEL VIEW LN W/S | HOLLY AVE W/S | 1,300 | 30 | 39,000 | R - Residential/Local | A - AC | 80 | 26.33 |
| WLINAV | 20 | W LINDO AVE | NORD | RR TRACKS | 716 | 30 | 21,480 | A - Arterial | A - AC | 46 | 5.92 |
| WLINAV | 30 | W LINDO AVE | CHANNEL VIEW LN W/S | W. 11TH AVE | 2,820 | 26 | 73,320 | R - Residential/Local | A - AC | 67 | 18.2 |
| WLINAV | 40 | W LINDO AVE | RR TRACKS | FERN AVE | 1,386 | 28 | 38,808 | R - Residential/Local | A - AC | 80 | 26.22 |
| WLINAV | 50 | W LINDO AVE | FERN AVE | LAWNWOOD DR | 1,724 | 28 | 48,272 | R - Residential/Local | A - AC | 71 | 20.57 |
| WLINAV | 60 | W LINDO AVE | LAWNWOOD DR | HOLLY AVE | 1,777 | 28 | 49,756 | R - Residential/Local | A - AC | 59 | 13.94 |
| WSACAV | 10 | W SACRAMENTO AVE | CEDAR | NORD | 787 | 36 | 28,332 | R - Residential/Local | A - AC | 8 | 0 |
| WSACAV | 20 | W SACRAMENTO AVE | ESPLANADE DR | WARNER ST E/S | 2,200 | 36 | 79,200 | R - Residential/Local | A - AC | 76 | 23.67 |
| WSACAV | 30 | W SACRAMENTO AVE | NORD AV | RAVEN AV | 1,805 | 36 | 64,980 | C - Collector | A - AC | 68 | 11.07 |
| WSACAV | 40 | W SACRAMENTO AVE | WARNER | CEDAR ST | 1,353 | 36 | 48,708 | R - Residential/Local | A - AC | 65 | 17.06 |
| WSACAV | 50 | W SACRAMENTO AVE | WESTMONT DR WEST SIDE | GLENWOOD DR | 2,308 | 36 | 83,088 | R - Residential/Local | A - AC | 64 | 16.51 |
| WSACAV | 60 | W SACRAMENTO AVE | RAVEN LN W/S | WESTMONT DR W/S | 2,186 | 36 | 78,696 | C - Collector | A - AC | 45 | 4.12 |
| WSHAAV | 10 | W SHASTA AVE | 160 FT W/O TOP HAND CT | CUSSICK AVE E/S | 1,313 | 25 | 32,825 | C - Collector | A - AC | 33 | 1.61 |
| WSHAAV | 20 | W SHASTA AVE | ESPLANADE DR | 160 FT W/O TOP HAND CT | 2,000 | 36 | 72,000 | C - Collector | A - AC | 41 | 3.16 |
| WALELN | 10 | WALES LN | WEYMOUTH WAY | WINDHAM WAY | 250 | 32 | 8,000 | R - Residential/Local | A - AC | 82 | 27.5 |
| WALLST | 10 | WALL ST | HUMBOLT AVE | E 9TH ST | 190 | 46 | 8,740 | R - Residential/Local | A - AC | 77 | 24.31 |
| WALLST | 20 | WALL ST | E 9TH ST | E 8TH ST | 260 | 46 | 11,960 | R - Residential/Local | A - AC | 75 | 23.04 |
| WALLST | 30 | WALL ST | E 8TH ST | E 7TH ST | 310 | 46 | 14,260 | R - Residential/Local | A - AC | 70 | 19.97 |
| WALLST | 40 | WALL ST | E 6TH ST | E 5TH ST | 315 | 46 | 14,490 | R - Residential/Local | A - AC | 54 | 11.33 |
| WALLST | 50 | WALL ST | E 4TH ST | E 3RD ST | 315 | 46 | 14,490 | R - Residential/Local | A - AC | 89 | 31.57 |
| WALLST | 60 | WALL ST | E 3RD ST | E 2ND ST | 315 | 46 | 14,490 | R - Residential/Local | A - AC | 91 | 32.46 |
| WALLST | 70 | WALL ST | E 2ND ST | E 1ST ST | 215 | 46 | 9,890 | R - Residential/Local | A - AC | 91 | 32.46 |
| WAPADR | 10 | WALNUT PARK DR | S. CUL-DE-SAC | N. CUL-DE-SAC | 1,322 | 35 | 48,063 | R - Residential/Local | A - AC | 80 | 26.28 |
| WALNST | 10 | WALNUT ST | 9TH ST S/S | DAYTON RD | 800 | 36 | 28,800 | R - Residential/Local | A - AC | 80 | 26.22 |
| WANDLN | 10 | WANDERER LN | MARIPOSA AVE | VISTAMONT WAY | 765 | 28 | 21,420 | R - Residential/Local | A - AC | 81 | 26.92 |
| WANDLN | 20 | WANDERER LN | VISTAMONT WAY | CEANOTHUS AVE | 510 | 28 | 14,280 | R - Residential/Local | A - AC | 83 | 28.18 |
| WARNST | 10 | WARNER ST | 1ST AVE N/S | 4TH AVE S/S | 1,286 | 36 | 46,296 | R - Residential/Local | A - AC | 56 | 12.31 |
| WARNST | 20 | WARNER ST | 4TH AVE N/S | 6TH AVE S/S | 912 | 36 | 32,832 | R - Residential/Local | A - AC | 82 | 27.5 |
| WARNST | 30 | WARNER ST | 6TH AVE N/S | NORTH END | 333 | 42 | 13,986 | R - Residential/Local | A - AC | 82 | 27.5 |

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|----------------|--------------------------|--------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| WARNST 40 | | WARNER ST | LEGION AVE S/S | W. SACRAMENTO AVE N/S | 1,300 | 53 | 68,900 | R - Residential/Local | A - AC | 67 | 18.2 |
| WARNST 50 | | WARNER ST | W. 1ST ST N/S | LEGION AVE S/S | 1,475 | 36 | 53,100 | R - Residential/Local | A - AC | 72 | 21.18 |
| WARNST 60 | | WARNER ST | W. SACRAMENTO AVE N/S | 1ST AVE N/S | 455 | 50 | 22,750 | R - Residential/Local | A - AC | 82 | 27.5 |
| WAXWY 10 | | WAXWING WAY | WISTERIA LN | GLENWOOD AVE | 485 | 28 | 13,580 | R - Residential/Local | A - AC | 82 | 27.5 |
| WAYNLN 10 | | WAYNE LN | FILBERT AVE | HAWTHORNE AVE | 395 | 32 | 12,640 | R - Residential/Local | A - AC | 23 | 0 |
| WEBSDR 10 | | WEBSTER DR | NOTRE DAME BLVD | END | 931 | 37 | 34,447 | R - Residential/Local | A - AC | 80 | 26.22 |
| WEDGLN 10 | | WEDGEWOOD LN | WESTIN LN | ROYAL GLEN LN | 210 | 32 | 9,920 | R - Residential/Local | A - AC | 82 | 27.5 |
| WELLCT 10 | | WELLINGTON CT | ROXBURY CT | CUL-DE-SAC | 184 | 32 | 7,992 | R - Residential/Local | A - AC | 82 | 27.5 |
| WENDWY 10 | | WENDY WAY | E. 1ST AVE | CUL-DE-SAC | 932 | 32 | 29,824 | C - Collector | A - AC | 80 | 15.7 |
| WEMWDR 10 | | WEST WIND DR | CAPSTONE CT S/S | OAK WY | 700 | 32 | 22,400 | R - Residential/Local | A - AC | 82 | 27.5 |
| WEDACT 10 | | WESTERDAHL CT | MARIGOLD AVE | WEST END | 525 | 30 | 15,750 | R - Residential/Local | A - AC | 97 | 34.13 |
| WESTCT 10 | | WESTGROVE CT | W. SHASTA AVE | CUL-DE-SAC | 632 | 36 | 26,552 | R - Residential/Local | A - AC | 80 | 26.22 |
| WESTLN 10 | | WESTIN LN | EATON VILLAGE DR | WEDGEWOOD LN | 242 | 32 | 7,904 | R - Residential/Local | A - AC | 80 | 26.22 |
| WEMICT 10 | | WESTMINSTER CT | HUNTINGTON DR | WESTMINSTER CT | 225 | 37 | 8,325 | R - Residential/Local | A - AC | 80 | 26.28 |
| WEMICT 20 | | WESTMINSTER CT | N. CUL-DE-SAC | S. CUL-DE-SAC | 395 | 37 | 16,340 | R - Residential/Local | A - AC | 29 | 1.18 |
| WYMOWY 10 | | WEYMOUTH WAY | WEST END | PENZANCE AVE | 1,145 | 30 | 34,350 | R - Residential/Local | A - AC | 81 | 26.86 |
| WYMOWY 20 | | WEYMOUTH WAY | PENZANCE AVE | AURORA GLEN DR | 915 | 30 | 27,450 | R - Residential/Local | A - AC | 80 | 26.22 |
| WYMOWY 30 | | WEYMOUTH WAY | AURORA GLEN DR | WINE BLOSSOM DR | 560 | 30 | 16,800 | R - Residential/Local | A - AC | 82 | 27.5 |
| WHISWY 10 | | WHISTLER WAY | CAVALIER WY | VAIL DR | 380 | 28 | 10,640 | R - Residential/Local | A - AC | 80 | 26.22 |
| WHEAV 10 | | WHITE AVE | E. LASSEN AVE | PVMT CHNG | 1,413 | 37 | 52,281 | R - Residential/Local | A - AC | 19 | 0 |
| WHEAV 20 | | WHITE AVE | ESPLANADE DR | LOMBARD LN | 1,973 | 36 | 71,028 | R - Residential/Local | A - AC | 25 | 0.1 |
| WHEAV 30 | | WHITE AVE | PVMT CHNG | EAST AVE | 1,775 | 37 | 65,675 | L - Local (7) | A - AC | 94 | 33.54 |
| WHEAV 40 | | WHITE AVE | EAST AVE | SOUTH END | 1,180 | 33 | 38,940 | L - Local (7) | A - AC | 94 | 33.54 |
| WHCELN 10 | | WHITE CEDAR LN | COPPERFIELD DR | STERLING OAKS DR | 315 | 32 | 10,080 | R - Residential/Local | A - AC | 82 | 27.55 |
| WHITPL 10 | | WHITEHALL PL | CENTENNIAL AVE | CUL-DE-SAC | 927 | 32 | 32,930 | R - Residential/Local | A - AC | 81 | 26.97 |
| WHWOWY 10 | | WHITEWOOD WAY | CERES AVE | FLORAL AVE | 1,270 | 29 | 36,830 | R - Residential/Local | A - AC | 76 | 23.73 |
| WHWOWY 20 | | WHITEWOOD WAY | LUPIN AVE | CERES AVE | 658 | 35 | 23,030 | R - Residential/Local | A - AC | 72 | 21.23 |
| WHWOWY 30 | | WHITEWOOD WAY | FLORAL AVE | 630 FT E/O FLORAL AVE | 630 | 32 | 20,160 | L - Local (7) | A - AC | 78 | 25 |
| WHWOWY 40 | | WHITEWOOD WAY | 630 FTE/O FLORAL AVE | END | 535 | 28 | 14,980 | L - Local (7) | A - AC | 91 | 32.52 |
| WILDCT 10 | | WILDFLOWER CT | FOREST AVE | CUL-DE-SAC | 431 | 36 | 17,316 | R - Residential/Local | A - AC | 43 | 6.47 |
| WILDVA 10 | | WILDWOOD AVE | MANZANITA AVE | GATE | 1,785 | 34 | 60,690 | C - Collector | A - AC | 75 | 13.49 |
| WILLST 10 | | WILLOW ST | E. 10TH ST | HUMBOLDT AVE | 275 | 36 | 9,900 | R - Residential/Local | A - AC | 40 | 5.2 |

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| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|-----------------|-------------------------------|-------------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| WILLST | 20 | WILLOW ST | E. 7TH ST | E. 8TH ST | 293 | 24 | 7,032 | R - Residential/Local | A - AC | 26 | 0.44 |
| WILLST | 30 | WILLOW ST | E. 9TH ST | E. 10TH ST | 275 | 36 | 9,900 | R - Residential/Local | A - AC | 6 | 0 |
| WBRWY | 10 | WILLOWBROOK WAY | GREENWICH DR | CUL-DE-SAC | 288 | 36 | 12,168 | R - Residential/Local | A - AC | 33 | 2.65 |
| WNCCT | 10 | WINCHESTER CT | GREENWICH DR | END | 160 | 28 | 5,880 | R - Residential/Local | A - AC | 80 | 26.22 |
| WINDCT | 10 | WINDBRIDGE CT | PINYON HILLS DR | CUL-DE-SAC | 262 | 36 | 11,232 | R - Residential/Local | A - AC | 76 | 23.73 |
| WINDDR | 10 | WINDECKER DR | CAPSTONE CT | OAK WY | 575 | 32 | 20,504 | R - Residential/Local | A - AC | 82 | 27.5 |
| WINDWY | 10 | WINDHAM WAY | ABBOTT CIR | ROGUE RIVER DR | 945 | 26 | 24,570 | R - Residential/Local | A - AC | 82 | 27.5 |
| WINDWY | 20 | WINDHAM WAY | ROGUE RIVER DR | WALES LN | 1,240 | 32 | 39,680 | R - Residential/Local | A - AC | 82 | 27.5 |
| WINDWY | 30 | WINDHAM WAY | WALES LN | CORNWALL PL | 475 | 32 | 15,200 | R - Residential/Local | A - AC | 82 | 27.5 |
| WIMLCT | 10 | WINDMILL CT | SKYMOUNTAIN CIR | CUL-DE-SAC | 175 | 31 | 7,605 | R - Residential/Local | A - AC | 28 | 0.83 |
| WIRSC | 10 | WINDROSE CT | EAST END | WEST CUL-DE-SAC | 650 | 32 | 25,073 | R - Residential/Local | A - AC | 82 | 27.5 |
| WISOWY | 10 | WINDSOR WAY | GREENWICH DR | END | 855 | 28 | 23,940 | R - Residential/Local | A - AC | 82 | 27.5 |
| WIBLDR | 10 | WINE BLOSSOM DR | WEYMOUTH WAY | SOUTHBURY LN | 315 | 34 | 10,710 | R - Residential/Local | A - AC | 82 | 27.5 |
| WINGAV | 10 | WINGFIELD AVE | BANCROFT DR | END | 545 | 28 | 15,260 | R - Residential/Local | A - AC | 94 | 33.54 |
| WISTLN | 10 | WISTERIA LN | GLENWOOD AVE | END | 930 | 28 | 26,040 | R - Residential/Local | A - AC | 82 | 27.5 |
| WITTDR | 10 | WITMEIER DR | FOREST AVE | CUL-DE-SAC | 566 | 37 | 20,942 | R - Residential/Local | A - AC | 72 | 21.23 |
| WOODLN | 10 | WOODCREST LN | VALLOMBROSA AVE | CUL-DE-SAC | 354 | 32 | 11,328 | R - Residential/Local | A - AC | 70 | 20.02 |
| WOODAV | 10 | WOODLAND AVE | CYPRESS ST | SYCAMORE ST | 1,200 | 30 | 36,000 | R - Residential/Local | P - PCC | 86 | 60.26 |
| WOODAV | 20 | WOODLAND AVE | PVMT CHNG (489 FT E/O 7TH ST) | E. CUL-DE-SAC | 546 | 24 | 13,104 | R - Residential/Local | A - AC | 30 | 1.82 |
| WOODAV | 30 | WOODLAND AVE | SYCAMORE ST | PVMT CHNG (489 FT E/O 7TH ST) | 2,040 | 30 | 61,200 | R - Residential/Local | P - PCC | 91 | 69.66 |
| WOODAV | 40 | WOODLAND AVE | E. 4TH ST | PINE ST | 500 | 32 | 16,000 | R - Residential/Local | A - AC | 61 | 14.88 |
| WOODCT | 10 | WOODMINSTER CT | GREENHAVEN LN | CUL-DE-SAC | 318 | 36 | 13,248 | R - Residential/Local | A - AC | 26 | 0.44 |
| WOROLN | 10 | WOODROSE LN | GARDEN PARK DR | CUL-DE-SAC | 119 | 34 | 5,914 | R - Residential/Local | A - AC | 45 | 7.31 |
| WOSILN | 10 | WOODSIDE LN | LONGFELLOW | LAKEWOOD WY | 875 | 33 | 28,875 | R - Residential/Local | A - AC | 77 | 24.36 |
| WOVILN | 10 | WOODVIEW LN | VALLEY OAK DR W/S | AUTUMNWOOD DR | 304 | 36 | 10,944 | R - Residential/Local | A - AC | 46 | 7.68 |
| WREXCT | 10 | WREX CT | MEYERS ST | CUL-DE-SAC | 450 | 40 | 19,827 | R - Residential/Local | A - AC | 29 | 1.47 |
| WYNDCT | 10 | WYNDHAM CT | ASPEN GLENN DR | END | 230 | 28 | 6,440 | R - Residential/Local | A - AC | 82 | 27.5 |
| WYSOCT | 10 | WYSONG CT | PARKWAY VILLAGE DR | CUL-DE-SAC | 390 | 37 | 16,073 | R - Residential/Local | A - AC | 80 | 26.28 |
| YALECR | 10 | YALE CIR | YALE WAY | END | 110 | 75 | 8,250 | R - Residential/Local | A - AC | 71 | 20.57 |
| YALEWY | 10 | YALE WAY | LAWNWOOD DR | END | 510 | 36 | 18,360 | R - Residential/Local | A - AC | 71 | 20.57 |
| YARRDR | 10 | YARROW DR | 195 FT N/STACE WY | 97 FT S/STACE WY | 292 | 25 | 7,300 | R - Residential/Local | A - AC | 83 | 28.33 |
| YELDR | 10 | YELLOWSTONE DR | ESPLANADE DR | PRAIRIE CREEK DR | 1,252 | 36 | 45,072 | R - Residential/Local | A - AC | 70 | 19.97 |

Criteria:

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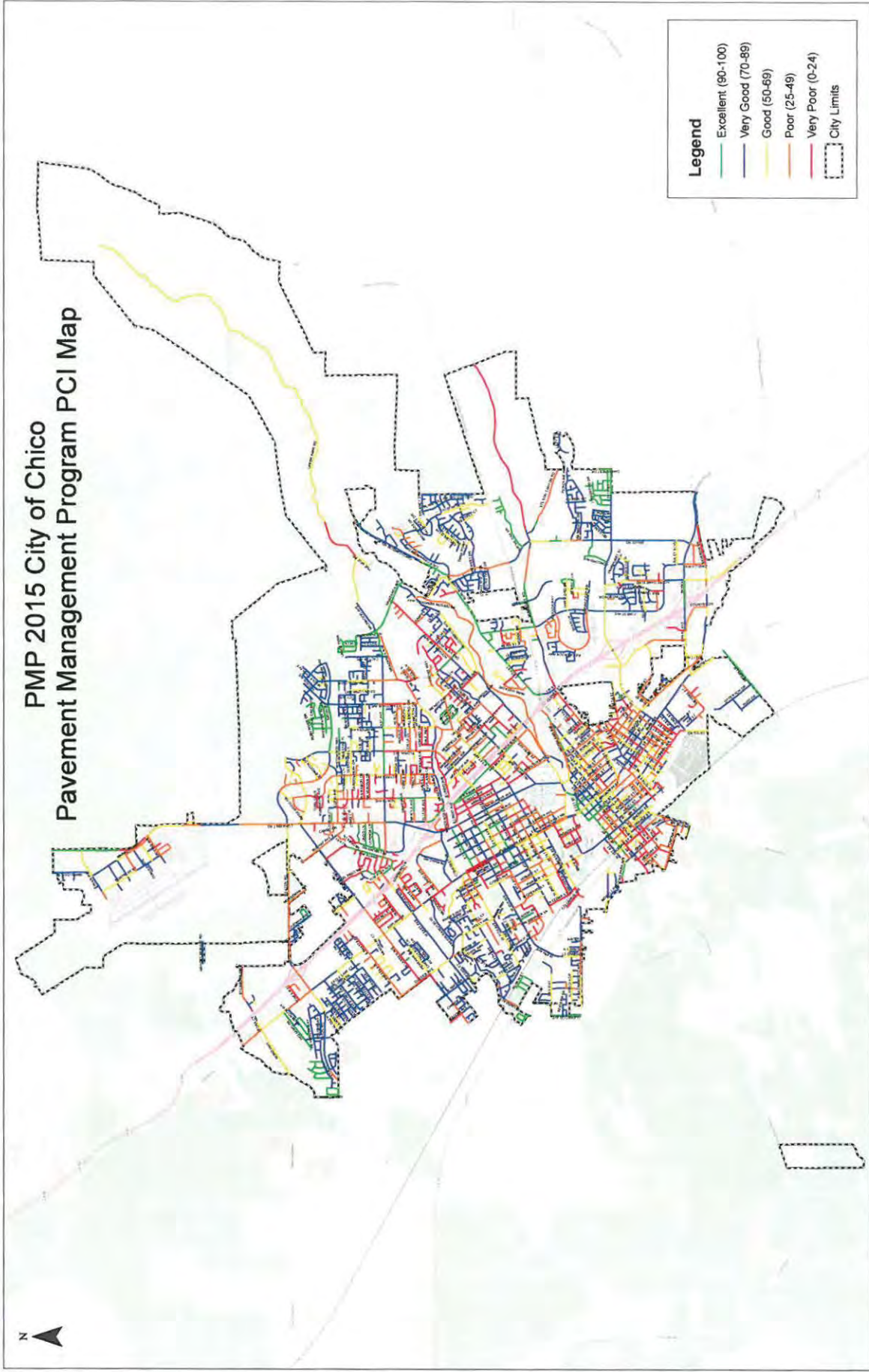
MTC StreetSaver

| Street ID | Section ID | Street Name | From | To | Length | Width | Area | Functional Class | Surface Type | Current PCI | Remaining Life |
|-----------|------------|----------------|----------------------------|----------------------------|--------|-------|--------|-----------------------|--------------|-------------|----------------|
| YORKDR | 10 | YORK DR | WINDSOR WAY | SOUTHAMPTON DR | 485 | 26 | 12,610 | R - Residential/Local | A - AC | 82 | 27.5 |
| YOSEDR | 10 | YOSEMITE DR | BLUE OAK PARK PVMT CHNG | CALIFORNIA PARK DR | 455 | 35 | 15,925 | C - Collector | A - AC | 79 | 15.24 |
| YOSEDR | 20 | YOSEMITE DR | DEER CREEK HIGHWAY | IDYLLWILD CIR | 1,605 | 35 | 56,175 | C - Collector | A - AC | 59 | 8.06 |
| YOSEDR | 30 | YOSEMITE DR | DELANEY DR | PALOMAR LN | 810 | 35 | 28,350 | C - Collector | A - AC | 70 | 11.5 |
| YOSEDR | 40 | YOSEMITE DR | IDYLLWILD CIR | DELANEY DR | 1,019 | 35 | 35,665 | C - Collector | A - AC | 58 | 7.75 |
| YOSEDR | 50 | YOSEMITE DR | N/O CALIFORNIA PARK DR | CUL-DE-SAC | 548 | 33 | 20,027 | R - Residential/Local | A - AC | 66 | 17.68 |
| YOSEDR | 60 | YOSEMITE DR | PALOMAR LN | BLUE OAK PARK PVMT CHNG | 1,205 | 35 | 42,175 | C - Collector | A - AC | 75 | 13.49 |
| YOSEDR | 70 | YOSEMITE DR | PIN OAK LN | NATIVE OAK DR | 390 | 32 | 12,480 | R - Residential/Local | A - AC | 97 | 34.13 |
| ZACHCT | 10 | ZACHARY CT | REMLINGTON DR | CUL-DE-SAC | 470 | 36 | 16,920 | R - Residential/Local | A - AC | 80 | 26.28 |
| ZANEWY | 10 | ZANELLA WAY | SKYWAY | MORROWLN | 1,174 | 40 | 46,960 | C - Collector | A - AC | 30 | 0.99 |
| ZINNWW | 10 | ZINNIA WAY | FUCHSIA WAY | END | 390 | 36 | 14,040 | R - Residential/Local | A - AC | 80 | 26.22 |
| ZICACT | 10 | ZION CANYON CT | ROLLINS LAKE DR | CUL-DE-SAC | 552 | 32 | 19,768 | R - Residential/Local | A - AC | 80 | 26.22 |
| ZUNIAY | 10 | ZUNI AVE | W 12TH AVE | W 11TH AVE | 860 | 24 | 20,640 | R - Residential/Local | A - AC | 64 | 16.51 |

Total Section Length: 1,476,292

Total Section Area: 52,418,939

PMP 2015 City of Chico Pavement Management Program PCI Map



- Legend**
- Excellent (90-100)
 - Very Good (70-89)
 - Good (50-69)
 - Poor (25-49)
 - Very Poor (0-24)
 - City Limits

Map creation and GIS completed by  Harris & Associates.
Sharing the Power. One planet at a time.

SECTION IV
BUDGET ANALYSIS REPORTS

- A. Budget Needs Report - Five Year**
- B. Average PCI by Annual Funding Chart**
- C. Deferred Maintenance Cost Trend by Annual Funding Chart**
- D. Budget Scenario Cost and Network Summaries**
- E. Expected Annual Budget - \$1M (*Map included*)**

A. NEEDS – PROJECTED PCI/COST SUMMARY – FIVE YEAR

NEEDS - PROJECTED PCI/COST SUMMARY

This report highlights the impact of projected needs funding (and lack thereof) on pavement system condition. The report also provides a cost estimate of the funding needed to bring the streets to an optimal preventive maintenance level. Finally, an “Untreated PCI” is listed. This value is an accurate portrayal of average street condition.

| | |
|-----------------------|--|
| Year: | Year of Projection |
| PCI Treated: | Average Street PCI with suggested treatments applied |
| PCI Untreated: | Present average untreated street PCI for year. This value is most accurate reflection of present PCI |
| Cost: | Cost per year to apply suggested treatments |
| PM Cost: | Total cost over the period of analysis spent on preventative maintenance |
| % PM: | Percent of total cost over the period of analysis spent on preventative maintenance |
| Total Cost: | Total cost over the period of analysis to bring streets to optimal maintenance level. |

Needs - Projected PCI/Cost Summary

Inflation Rate = 3.00 % Printed: 02/10/2016

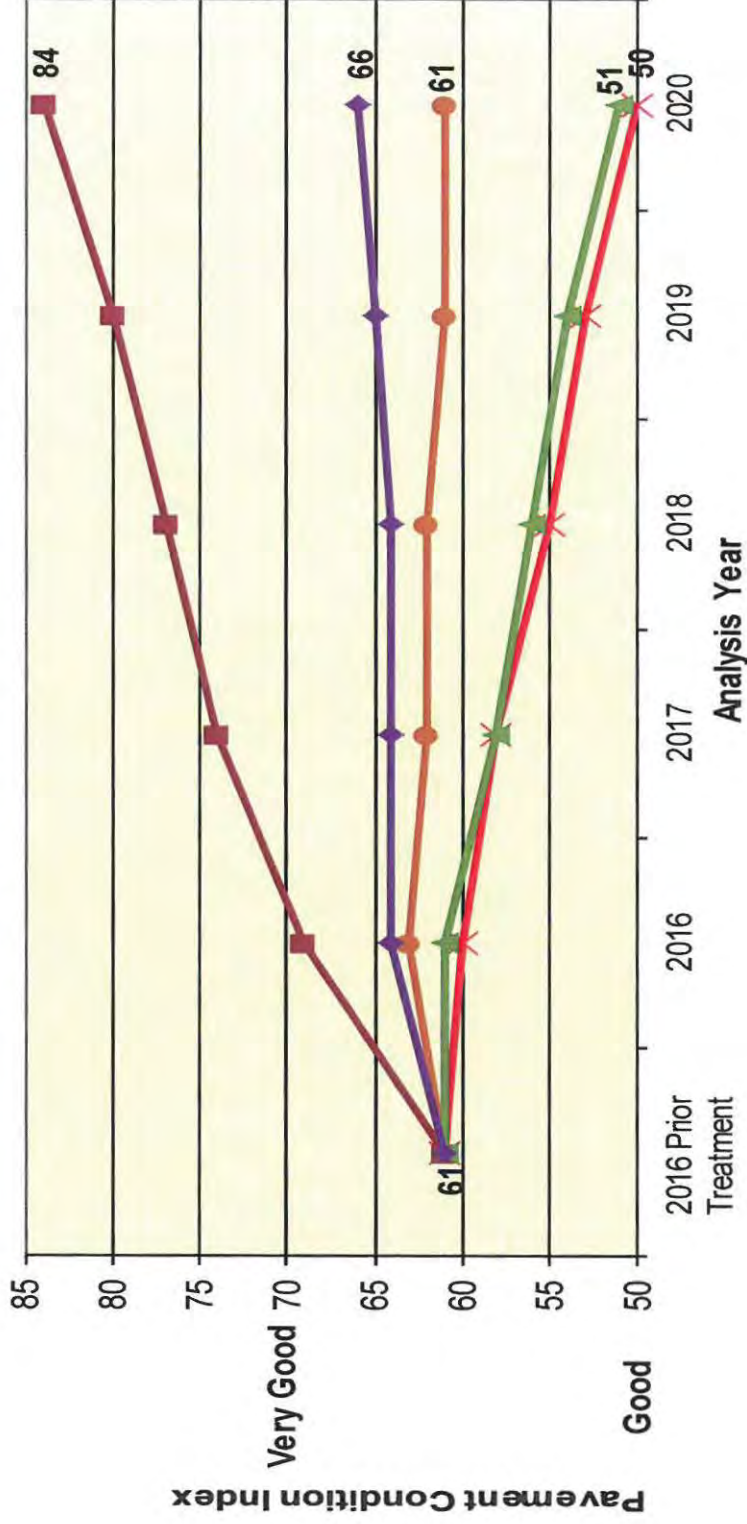
| Year | PCI Treated | PCI Untreated | PM Cost | Rehab Cost | Cost | |
|------|-------------|---------------|-------------|---------------|------------------|---------------|
| 2016 | 79 | 60 | \$5,906,812 | \$60,435,567 | \$66,342,379 | |
| 2017 | 80 | 57 | \$340,830 | \$17,757,751 | \$18,098,581 | |
| 2018 | 82 | 55 | \$582,795 | \$15,549,688 | \$16,132,483 | |
| 2019 | 83 | 52 | \$237,403 | \$15,660,101 | \$15,897,504 | |
| 2020 | 84 | 50 | \$98,073 | \$8,514,529 | \$8,612,602 | |
| | | | % PM | PM Total Cost | Rehab Total Cost | Total Cost |
| | | | 5.73% | \$7,165,913 | \$117,917,636 | \$125,083,549 |

B. AVERAGE PCI BY ANNUAL FUNDING CHART

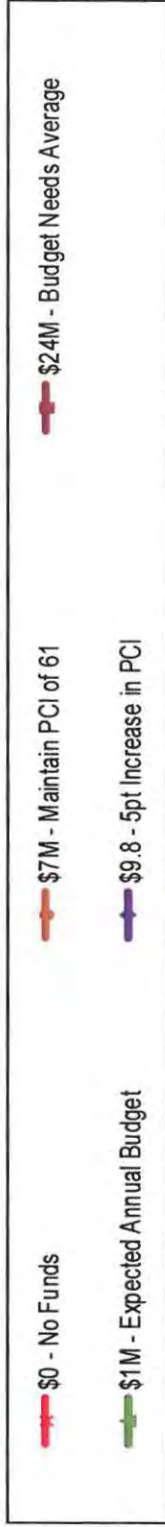
AVERAGE PAVEMENT CONDITION INDEX (PCI) BY ANNUAL FUNDING LEVEL

This graph compares 5 different annual budget scenarios and their impact on overall pavement condition over a five year budget analysis period.

Pavement Condition Index by Annual Funding Level



Annual Funding Level

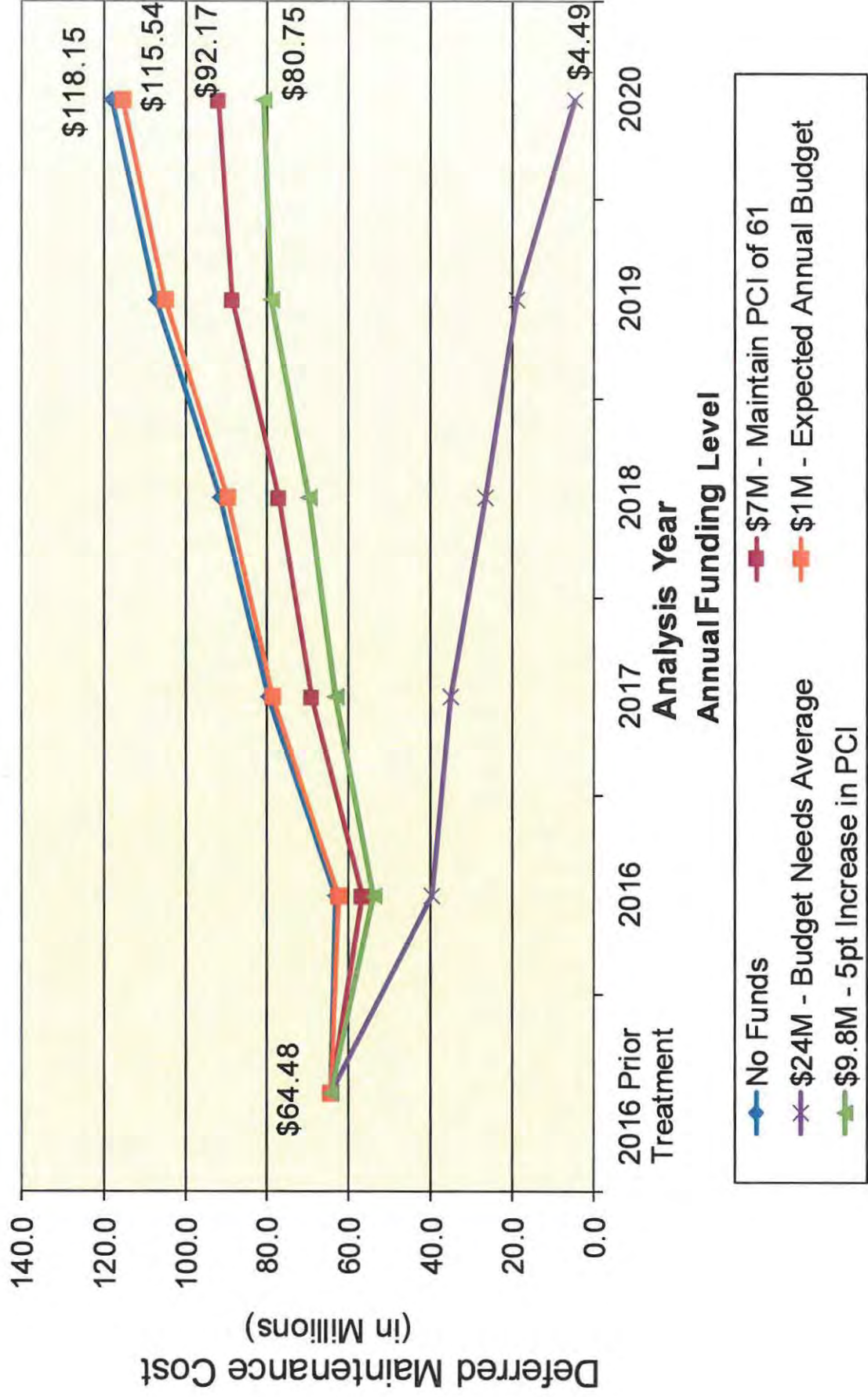


C. DEFERRED MAINTENANCE COST TREND BY ANNUAL FUNDING CHART

DEFERRED MAINTENANCE COST TREND VS. ANNUAL FUNDING LEVEL

This graph compares 5 different annual budget scenarios and their impact on deferred maintenance costs (maintenance backlog) over each year of the five-year budget analysis period. Deferred maintenance is the amount of necessary rehabilitation not performed each year due to budget shortfalls

Deferred Maintenance Cost by Annual Funding Level



D. BUDGET SCENARIO COST AND NETWORK SUMMARIES

SCENARIOS – COST SUMMARY REPORT

A Cost Summary report is provided in this section for each annual budget level analyzed. This specific report indicates the distribution of pavement funding between various condition levels of streets. The top portion of this report shows the interest and inflation assumptions.

The central part of the report lists the analysis year, percentage of that budget that is assigned to preventive maintenance, annual budget, activities distribution of the budget between rehabilitation (by condition category within rehabilitation, preventative maintenance, stop gap, and deferred maintenance costs. Condition Category refers to the MTC defined pavement conditions –Excellent (I), Very Good (II), Good (III), Poor (IV), and Very Poor (V). “Stop Gap” is a factored cost applied for emergency repairs (i.e. pothole repair) needed to minimally maintain a section where a section’s suggested rehabilitation maintenance is deferred. “Funded Stop Gap” refers to the budget amount allocated for stop gap and “Unmet Stop Gap” refers to any deferred stop gap costs. “Deferred” refers to the dollar cost of backlogged rehabilitation. “Surplus PM” values appear if the budget percentage split between rehabilitation and preventive maintenance is too heavily weighted to preventive maintenance. In such a case, the percentage of funding to rehabilitation would be increased.

The bottom part of the report shows a summary of the total costs spent on rehabilitation, preventative maintenance, and stop gap (funded and unmet) according to functional class.

SCENARIOS – NETWORK CONDITION SUMMARY REPORT

This report details present and projected network condition trends based on the annual budget set for an individual scenario. Perhaps the most important piece of information from this report is found on the first page of the report. The table under “Project Network Average PCI by Year” shows what the overall network condition trend is when the suggested treatments are applied in comparison to when the treatments are not applied.

THESE REPORTS ARE PROVIDED FOR EACH OF THE COMPARISON SCENARIOS

Scenarios - Cost Summary

Interest: 3.00%

Inflation: 3.00%

Printed: 03/31/2016

Scenario: 2015 PMP - No Funds

| Year | PM | Budget | Rehabilitation | Preventative Maintenance | Surplus PM | Deferred | Stop Gap | | | |
|------|----|--------|----------------|--------------------------|-------------|----------|----------|---------------|---------|-----------|
| 2016 | 0% | \$0 | II | \$0 | Non-Project | \$0 | \$0 | \$63,310,339 | Funded | \$0 |
| | | | III | \$0 | | | | | Unmet | \$785,367 |
| | | | IV | \$0 | | | | | Project | \$0 |
| | | | V | \$0 | | | | | | |
| | | | Total | \$0 | | | | | | |
| | | | Project | \$0 | | | | | | |
| 2017 | 0% | \$0 | II | \$0 | Non-Project | \$0 | \$0 | \$79,570,023 | Funded | \$0 |
| | | | III | \$0 | | | | | Unmet | \$223,634 |
| | | | IV | \$0 | | | | | Project | \$0 |
| | | | V | \$0 | | | | | | |
| | | | Total | \$0 | | | | | | |
| | | | Project | \$0 | | | | | | |
| 2018 | 0% | \$0 | II | \$0 | Non-Project | \$0 | \$0 | \$91,679,252 | Funded | \$0 |
| | | | III | \$0 | | | | | Unmet | \$177,000 |
| | | | IV | \$0 | | | | | Project | \$0 |
| | | | V | \$0 | | | | | | |
| | | | Total | \$0 | | | | | | |
| | | | Project | \$0 | | | | | | |
| 2019 | 0% | \$0 | II | \$0 | Non-Project | \$0 | \$0 | \$107,499,144 | Funded | \$0 |
| | | | III | \$0 | | | | | Unmet | \$187,070 |
| | | | IV | \$0 | | | | | Project | \$0 |
| | | | V | \$0 | | | | | | |
| | | | Total | \$0 | | | | | | |
| | | | Project | \$0 | | | | | | |
| 2020 | 0% | \$0 | II | \$0 | Non-Project | \$0 | \$0 | \$118,150,276 | Funded | \$0 |
| | | | III | \$0 | | | | | Unmet | \$134,372 |
| | | | IV | \$0 | | | | | Project | \$0 |
| | | | V | \$0 | | | | | | |
| | | | Total | \$0 | | | | | | |
| | | | Project | \$0 | | | | | | |

| Summary | | | | |
|---------------------|----------------|--------------|-----------------|--------------------|
| Functional Class | Rehabilitation | Prev. Maint. | Funded Stop Gap | Unmet Stop Gap |
| Arterial | \$0 | \$0 | \$0 | \$72,764 |
| Collector | \$0 | \$0 | \$0 | \$490,558 |
| Residential/Local | \$0 | \$0 | \$0 | \$944,121 |
| Grand Total: | \$0 | \$0 | \$0 | \$1,507,443 |

Scenarios - Network Condition Summary

Interest: 3%

Inflation: 3%

Printed: 03/31/2016

Scenario: 2015 PMP - No Funds

| Year | Budget | PM | Year | Budget | PM | Year | Budget | PM |
|------|--------|----|------|--------|----|------|--------|----|
| 2016 | \$0 | 0% | 2018 | \$0 | 0% | 2020 | \$0 | 0% |
| 2017 | \$0 | 0% | 2019 | \$0 | 0% | | | |

Projected Network Average PCI by year

| Year | Never Treated | With Selected Treatment | Treated Centerline Miles | Treated Lane Miles |
|------|---------------|-------------------------|--------------------------|--------------------|
| 2016 | 60 | 60 | 0 | 0 |
| 2017 | 58 | 58 | 0 | 0 |
| 2018 | 55 | 55 | 0 | 0 |
| 2019 | 53 | 53 | 0 | 0 |
| 2020 | 50 | 50 | 0 | 0 |

Percent Network Area by Functional Class and Condition Category

Condition in base year 2016, prior to applying treatments.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 1.5% | 12.4% | 35.1% | 0.0% | 49.0% |
| II / III | 1.1% | 6.5% | 11.2% | 0.0% | 18.8% |
| IV | 0.9% | 5.6% | 11.5% | 0.0% | 18.0% |
| V | 0.5% | 4.1% | 9.5% | 0.0% | 14.1% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Condition in year 2016 after schedulable treatments applied.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 1.5% | 12.4% | 35.1% | 0.0% | 49.0% |
| II / III | 1.1% | 6.5% | 11.2% | 0.0% | 18.8% |
| IV | 0.9% | 5.6% | 11.5% | 0.0% | 18.0% |
| V | 0.5% | 4.1% | 9.5% | 0.0% | 14.1% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Condition in year 2020 after schedulable treatments applied.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 1.0% | 7.6% | 28.5% | 0.0% | 37.1% |
| II / III | 1.0% | 7.1% | 13.8% | 0.0% | 21.9% |
| IV | 0.9% | 5.5% | 9.1% | 0.0% | 15.4% |
| V | 1.1% | 8.4% | 16.0% | 0.0% | 25.6% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Interest: 3.00%

Inflation: 3.00%

Printed: 04/01/2016

Scenario: 2015 PMP - Expected Annual Budget
\$1M

| Year | PM | Budget | Rehabilitation | Preventative Maintenance | Surplus PM | Deferred | Stop Gap | | |
|------|----|-------------|----------------|--------------------------|-------------|----------|---------------|--------|-----------|
| 2016 | 6% | \$1,000,000 | II | \$0 | Non-Project | \$0 | \$62,371,556 | Funded | \$61,217 |
| | | | III | \$658,904 | Non-Project | | | Unmet | \$717,614 |
| | | | IV | \$279,882 | Project | \$0 | | | |
| | | | V | \$0 | | | | | |
| | | | Total | \$938,786 | | | | | |
| | | | Project | \$0 | | | | | |
| 2017 | 6% | \$1,000,000 | II | \$1,700 | Non-Project | \$0 | \$78,417,554 | Funded | \$60,838 |
| | | | III | \$597,778 | Non-Project | | | Unmet | \$160,332 |
| | | | IV | \$339,689 | Project | \$0 | | | |
| | | | V | \$0 | | | | | |
| | | | Total | \$939,167 | | | | | |
| | | | Project | \$0 | | | | | |
| 2018 | 6% | \$1,000,000 | II | \$0 | Non-Project | \$0 | \$89,872,170 | Funded | \$61,534 |
| | | | III | \$558,455 | Non-Project | | | Unmet | \$112,779 |
| | | | IV | \$380,015 | Project | \$0 | | | |
| | | | V | \$0 | | | | | |
| | | | Total | \$938,470 | | | | | |
| | | | Project | \$0 | | | | | |
| 2019 | 6% | \$1,000,000 | II | \$0 | Non-Project | \$0 | \$105,012,034 | Funded | \$60,778 |
| | | | III | \$118,480 | Non-Project | | | Unmet | \$126,292 |
| | | | IV | \$820,748 | Project | \$0 | | | |
| | | | V | \$0 | | | | | |
| | | | Total | \$939,228 | | | | | |
| | | | Project | \$0 | | | | | |
| 2020 | 6% | \$1,000,000 | II | \$0 | Non-Project | \$0 | \$115,539,980 | Funded | \$61,542 |
| | | | III | \$33,431 | Non-Project | | | Unmet | \$70,579 |
| | | | IV | \$905,031 | Project | \$0 | | | |
| | | | V | \$0 | | | | | |
| | | | Total | \$938,462 | | | | | |
| | | | Project | \$0 | | | | | |

Summary

| Functional Class | Rehabilitation | Prev. Maint. | Funded Stop Gap | Unmet Stop Gap |
|---------------------|--------------------|--------------|------------------|--------------------|
| Arterial | \$1,278,119 | \$0 | \$5,761 | \$56,910 |
| Collector | \$3,199,240 | \$0 | \$100,487 | \$386,364 |
| Residential/Local | \$216,754 | \$0 | \$199,661 | \$744,322 |
| Grand Total: | \$4,694,113 | \$0 | \$305,909 | \$1,187,596 |

Scenarios - Network Condition Summary

Interest: 3% Inflation: 3% Printed: 04/01/2016

Scenario: 2015 PMP - Expected Annual Budget
\$1M

| Year | Budget | PM | Year | Budget | PM | Year | Budget | PM |
|------|-------------|----|------|-------------|----|------|-------------|----|
| 2016 | \$1,000,000 | 6% | 2018 | \$1,000,000 | 6% | 2020 | \$1,000,000 | 6% |
| 2017 | \$1,000,000 | 6% | 2019 | \$1,000,000 | 6% | | | |

Projected Network Average PCI by year

| Year | Never Treated | With Selected Treatment | Treated Centerline Miles | Treated Lane Miles |
|------|---------------|-------------------------|--------------------------|--------------------|
| 2016 | 60 | 61 | 2.39 | 4.78 |
| 2017 | 58 | 58 | 1.84 | 3.68 |
| 2018 | 55 | 56 | 1.43 | 2.87 |
| 2019 | 53 | 54 | 1.66 | 3.32 |
| 2020 | 50 | 52 | 1.35 | 2.69 |

Percent Network Area by Functional Class and Condition Category

Condition in base year 2016, prior to applying treatments.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 1.5% | 12.4% | 35.1% | 0.0% | 49.0% |
| II / III | 1.1% | 6.5% | 11.2% | 0.0% | 18.8% |
| IV | 0.9% | 5.6% | 11.5% | 0.0% | 18.0% |
| V | 0.5% | 4.1% | 9.5% | 0.0% | 14.1% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Condition in year 2016 after schedulable treatments applied.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 1.8% | 12.8% | 35.1% | 0.0% | 49.8% |
| II / III | 1.1% | 6.0% | 11.2% | 0.0% | 18.3% |
| IV | 0.7% | 5.6% | 11.5% | 0.0% | 17.8% |
| V | 0.5% | 4.1% | 9.5% | 0.0% | 14.1% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Condition in year 2020 after schedulable treatments applied.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 1.9% | 9.9% | 28.7% | 0.0% | 40.5% |
| II / III | 1.0% | 6.1% | 13.6% | 0.0% | 20.8% |
| IV | 0.0% | 4.1% | 9.0% | 0.0% | 13.1% |
| V | 1.1% | 8.4% | 16.0% | 0.0% | 25.6% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Interest: 3.00%

Inflation: 3.00%

Printed: 04/01/2016

Scenario: 2015 PMP - Maintain PCI of 61 - \$7M

| Year | PM | Budget | Rehabilitation | Preventative Maintenance | Surplus PM | Deferred | Stop Gap | | | |
|------|----|-------------|----------------|--------------------------|-------------|-----------|--------------|--------------|-----------|-----------|
| 2016 | 6% | \$7,000,000 | II | \$6,551 | Non-Project | \$0 | \$56,731,632 | Funded | \$421,294 | |
| | | | III | \$1,616,190 | | | | Unmet | \$311,497 | |
| | | | IV | \$4,955,982 | Project | \$0 | | | | |
| | | | V | \$0 | | | | | | |
| | | | Total | \$6,578,723 | | | | | | |
| | | | Project | \$0 | | | | | | |
| 2017 | 6% | \$7,000,000 | II | \$204,771 | Non-Project | \$218,814 | \$255 | \$68,860,594 | Funded | \$200,931 |
| | | | III | \$1,437,644 | | | | | Unmet | \$0 |
| | | | IV | \$4,937,378 | Project | \$0 | | | | |
| | | | V | \$0 | | | | | | |
| | | | Total | \$6,579,793 | | | | | | |
| | | | Project | \$0 | | | | | | |
| 2018 | 6% | \$7,000,000 | II | \$2,571,665 | Non-Project | \$294,881 | \$0 | \$77,230,969 | Funded | \$127,274 |
| | | | III | \$708,729 | | | | | Unmet | \$0 |
| | | | IV | \$3,297,535 | Project | \$0 | | | | |
| | | | V | \$0 | | | | | | |
| | | | Total | \$6,577,929 | | | | | | |
| | | | Project | \$0 | | | | | | |
| 2019 | 6% | \$7,000,000 | II | \$1,868,417 | Non-Project | \$294,782 | \$0 | \$88,476,456 | Funded | \$126,220 |
| | | | III | \$50,572 | | | | | Unmet | \$0 |
| | | | IV | \$4,660,072 | Project | \$0 | | | | |
| | | | V | \$0 | | | | | | |
| | | | Total | \$6,579,061 | | | | | | |
| | | | Project | \$0 | | | | | | |
| 2020 | 6% | \$7,000,000 | II | \$1,689,518 | Non-Project | \$333,599 | \$0 | \$92,174,659 | Funded | \$91,764 |
| | | | III | \$126,688 | | | | | Unmet | \$0 |
| | | | IV | \$2,164,310 | Project | \$0 | | | | |
| | | | V | \$2,593,818 | | | | | | |
| | | | Total | \$6,574,334 | | | | | | |
| | | | Project | \$0 | | | | | | |

| Summary | | | | |
|---------------------|---------------------|--------------------|------------------|------------------|
| Functional Class | Rehabilitation | Prev. Maint. | Funded Stop Gap | Unmet Stop Gap |
| Arterial | \$1,740,563 | \$213,302 | \$37,725 | \$20,489 |
| Collector | \$12,325,782 | \$1,729 | \$206,581 | \$187,080 |
| Residential/Local | \$18,823,495 | \$927,045 | \$723,177 | \$103,928 |
| Grand Total: | \$32,889,840 | \$1,142,076 | \$967,483 | \$311,497 |

Scenarios - Network Condition Summary

Interest: 3% Inflation: 3% Printed: 04/01/2016

Scenario: 2015 PMP - Maintain PCI of 61 - \$7M

| Year | Budget | PM | Year | Budget | PM | Year | Budget | PM |
|------|-------------|----|------|-------------|----|------|-------------|----|
| 2016 | \$7,000,000 | 6% | 2018 | \$7,000,000 | 6% | 2020 | \$7,000,000 | 6% |
| 2017 | \$7,000,000 | 6% | 2019 | \$7,000,000 | 6% | | | |

Projected Network Average PCI by year

| Year | Never Treated | With Selected Treatment | Treated Centerline Miles | Treated Lane Miles |
|------|---------------|-------------------------|--------------------------|--------------------|
| 2016 | 60 | 63 | 14.08 | 28.16 |
| 2017 | 58 | 62 | 17.87 | 35.74 |
| 2018 | 55 | 62 | 27.15 | 54.31 |
| 2019 | 53 | 61 | 23.20 | 46.40 |
| 2020 | 50 | 61 | 37.51 | 75.02 |

Percent Network Area by Functional Class and Condition Category

Condition in base year 2016, prior to applying treatments.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 1.5% | 12.4% | 35.1% | 0.0% | 49.0% |
| II / III | 1.1% | 6.5% | 11.2% | 0.0% | 18.8% |
| IV | 0.9% | 5.6% | 11.5% | 0.0% | 18.0% |
| V | 0.5% | 4.1% | 9.5% | 0.0% | 14.1% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Condition in year 2016 after schedulable treatments applied.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 1.8% | 15.3% | 36.9% | 0.0% | 54.0% |
| II / III | 1.1% | 5.4% | 10.9% | 0.0% | 17.4% |
| IV | 0.7% | 3.7% | 10.0% | 0.0% | 14.4% |
| V | 0.5% | 4.1% | 9.5% | 0.0% | 14.1% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Condition in year 2020 after schedulable treatments applied.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 2.8% | 18.8% | 47.7% | 0.0% | 69.2% |
| II / III | 0.1% | 2.0% | 3.2% | 0.0% | 5.3% |
| IV | 0.0% | 0.0% | 1.5% | 0.0% | 1.5% |
| V | 1.1% | 7.8% | 15.1% | 0.0% | 24.0% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Interest: 3.00%

Inflation: 3.00%

Printed: 04/01/2016

Scenario: 2015 PMP - 5pt Increase in PCI - \$9.8M

| Year | PM | Budget | Rehabilitation | Preventative Maintenance | Surplus PM | Deferred | Stop Gap | | |
|------|----|-------------|----------------|--------------------------|-------------|-----------|--------------|--------|-----------|
| 2016 | 6% | \$9,800,000 | II | \$158,202 | Non-Project | \$0 | \$54,099,126 | Funded | \$588,788 |
| | | | III | \$1,942,180 | Project | \$0 | | Unmet | \$119,430 |
| | | | IV | \$7,110,861 | | | | | |
| | | | V | \$0 | | | | | |
| | | | Total | \$9,211,243 | | | | | |
| | | | Project | \$0 | | | | | |
| 2017 | 6% | \$9,800,000 | II | \$3,836,016 | Non-Project | \$409,779 | \$63,329,012 | Funded | \$180,556 |
| | | | III | \$1,763,194 | Project | \$0 | | Unmet | \$0 |
| | | | IV | \$2,717,850 | | | | | |
| | | | V | \$891,925 | | | | | |
| | | | Total | \$9,208,985 | | | | | |
| | | | Project | \$0 | | | | | |
| 2018 | 6% | \$9,800,000 | II | \$1,747,101 | Non-Project | \$483,642 | \$69,877,768 | Funded | \$112,154 |
| | | | III | \$27,567 | Project | \$0 | | Unmet | \$0 |
| | | | IV | \$3,297,535 | | | | | |
| | | | V | \$4,131,689 | | | | | |
| | | | Total | \$9,203,892 | | | | | |
| | | | Project | \$0 | | | | | |
| 2019 | 6% | \$9,800,000 | II | \$1,512,626 | Non-Project | \$469,313 | \$78,928,940 | Funded | \$120,809 |
| | | | III | \$0 | Project | \$0 | | Unmet | \$0 |
| | | | IV | \$4,660,072 | | | | | |
| | | | V | \$3,037,263 | | | | | |
| | | | Total | \$9,209,961 | | | | | |
| | | | Project | \$0 | | | | | |
| 2020 | 6% | \$9,800,000 | II | \$1,804,142 | Non-Project | \$522,781 | \$80,750,398 | Funded | \$66,310 |
| | | | III | \$126,688 | Project | \$0 | | Unmet | \$0 |
| | | | IV | \$2,164,310 | | | | | |
| | | | V | \$5,115,260 | | | | | |
| | | | Total | \$9,210,400 | | | | | |
| | | | Project | \$0 | | | | | |

| Summary | | | | |
|---------------------|---------------------|--------------------|--------------------|------------------|
| Functional Class | Rehabilitation | Prev. Maint. | Funded Stop Gap | Unmet Stop Gap |
| Arterial | \$1,780,974 | \$213,844 | \$55,511 | \$0 |
| Collector | \$14,015,653 | \$1,729 | \$265,986 | \$119,430 |
| Residential/Local | \$30,247,854 | \$1,669,942 | \$747,120 | \$0 |
| Grand Total: | \$46,044,481 | \$1,885,515 | \$1,068,617 | \$119,430 |

Scenarios - Network Condition Summary

Interest: 3%

Inflation: 3%

Printed: 04/01/2016

Scenario: 2015 PMP - 5pt Increase in PCI - \$9.8M

| Year | Budget | PM | Year | Budget | PM | Year | Budget | PM |
|------|-------------|----|------|-------------|----|------|-------------|----|
| 2016 | \$9,800,000 | 6% | 2018 | \$9,800,000 | 6% | 2020 | \$9,800,000 | 6% |
| 2017 | \$9,800,000 | 6% | 2019 | \$9,800,000 | 6% | | | |

Projected Network Average PCI by year

| Year | Never Treated | With Selected Treatment | Treated Centerline Miles | Treated Lane Miles |
|------|---------------|-------------------------|--------------------------|--------------------|
| 2016 | 60 | 64 | 20.87 | 41.73 |
| 2017 | 58 | 64 | 38.98 | 77.96 |
| 2018 | 55 | 64 | 29.03 | 58.07 |
| 2019 | 53 | 65 | 27.52 | 55.05 |
| 2020 | 50 | 66 | 49.27 | 98.55 |

Percent Network Area by Functional Class and Condition Category

Condition in base year 2016, prior to applying treatments.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 1.5% | 12.4% | 35.1% | 0.0% | 49.0% |
| II / III | 1.1% | 6.5% | 11.2% | 0.0% | 18.8% |
| IV | 0.9% | 5.6% | 11.5% | 0.0% | 18.0% |
| V | 0.5% | 4.1% | 9.5% | 0.0% | 14.1% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Condition in year 2016 after schedulable treatments applied.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 2.2% | 15.3% | 38.9% | 0.0% | 56.4% |
| II / III | 0.7% | 5.4% | 10.6% | 0.0% | 16.8% |
| IV | 0.7% | 3.7% | 8.4% | 0.0% | 12.8% |
| V | 0.5% | 4.1% | 9.5% | 0.0% | 14.1% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Condition in year 2020 after schedulable treatments applied.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 2.9% | 20.8% | 54.0% | 0.0% | 77.6% |
| II / III | 0.0% | 0.0% | 1.1% | 0.0% | 1.1% |
| IV | 0.0% | 0.0% | 1.5% | 0.0% | 1.5% |
| V | 1.1% | 7.8% | 10.9% | 0.0% | 19.8% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Interest: 3.00%

Inflation: 3.00%

Printed: 04/01/2016

Scenario: 2015 PMP - Budget Needs Average
\$24M

| Year | PM | Budget | Rehabilitation | Preventative Maintenance | Surplus PM | Deferred | Stop Gap | | | | |
|------|----|--------------|----------------|--------------------------|-------------|-------------|----------|--------------|--------|-----------|-----|
| 2016 | 6% | \$24,000,000 | II | \$3,279,012 | Non-Project | \$974,003 | \$0 | \$39,786,015 | Funded | \$475,223 | |
| | | | III | \$3,181,326 | | | | | Unmet | \$0 | |
| | | | IV | \$7,604,948 | Project | \$0 | \$0 | \$0 | \$0 | Funded | \$0 |
| | | | V | \$8,485,158 | | | | | | | |
| | | | Total | \$22,550,444 | | | | | | | |
| | | | Project | \$0 | | | | | | | |
| 2017 | 6% | \$24,000,000 | II | \$1,445,763 | Non-Project | \$1,387,924 | \$0 | \$35,112,556 | Funded | \$82,880 | |
| | | | III | \$486,866 | | | | | Unmet | \$0 | |
| | | | IV | \$2,208,940 | Project | \$0 | \$0 | \$0 | \$0 | Funded | \$0 |
| | | | V | \$18,387,485 | | | | | | | |
| | | | Total | \$22,529,054 | | | | | | | |
| | | | Project | \$0 | | | | | | | |
| 2018 | 6% | \$24,000,000 | II | \$1,862,005 | Non-Project | \$1,425,401 | \$0 | \$26,638,682 | Funded | \$21,354 | |
| | | | III | \$27,567 | | | | | Unmet | \$0 | |
| | | | IV | \$3,297,535 | Project | \$0 | \$0 | \$0 | \$0 | Funded | \$0 |
| | | | V | \$17,366,155 | | | | | | | |
| | | | Total | \$22,553,262 | | | | | | | |
| | | | Project | \$0 | | | | | | | |
| 2019 | 6% | \$24,000,000 | II | \$1,204,103 | Non-Project | \$1,434,220 | \$0 | \$19,050,137 | Funded | \$19,115 | |
| | | | III | \$0 | | | | | Unmet | \$0 | |
| | | | IV | \$3,837,411 | Project | \$0 | \$0 | \$0 | \$0 | Funded | \$0 |
| | | | V | \$17,504,813 | | | | | | | |
| | | | Total | \$22,546,327 | | | | | | | |
| | | | Project | \$0 | | | | | | | |
| 2020 | 6% | \$24,000,000 | II | \$1,942,833 | Non-Project | \$1,504,036 | \$0 | \$4,493,723 | Funded | \$0 | |
| | | | III | \$126,688 | | | | | Unmet | \$0 | |
| | | | IV | \$1,689,626 | Project | \$0 | \$0 | \$0 | \$0 | Funded | \$0 |
| | | | V | \$18,737,166 | | | | | | | |
| | | | Total | \$22,496,313 | | | | | | | |
| | | | Project | \$0 | | | | | | | |

Summary

| Functional Class | Rehabilitation | Prev. Maint. | Funded Stop Gap | Unmet Stop Gap |
|---------------------|----------------------|--------------------|------------------|----------------|
| Arterial | \$7,506,397 | \$271,441 | \$20,489 | \$0 |
| Collector | \$49,396,199 | \$1,649,963 | \$309,246 | \$0 |
| Residential/Local | \$55,772,804 | \$4,804,180 | \$268,837 | \$0 |
| Grand Total: | \$112,675,400 | \$6,725,584 | \$598,572 | \$0 |

Scenarios - Network Condition Summary

Interest: 3%

Inflation: 3%

Printed: 04/01/2016

Scenario: 2015 PMP - Budget Needs Average
\$24M

| Year | Budget | PM | Year | Budget | PM | Year | Budget | PM |
|------|--------------|----|------|--------------|----|------|--------------|----|
| 2016 | \$24,000,000 | 6% | 2018 | \$24,000,000 | 6% | 2020 | \$24,000,000 | 6% |
| 2017 | \$24,000,000 | 6% | 2019 | \$24,000,000 | 6% | | | |

Projected Network Average PCI by year

| Year | Never Treated | With Selected Treatment | Treated Centerline Miles | Treated Lane Miles |
|------|---------------|-------------------------|--------------------------|--------------------|
| 2016 | 60 | 69 | 72.93 | 145.87 |
| 2017 | 58 | 74 | 59.80 | 119.60 |
| 2018 | 55 | 77 | 55.00 | 109.99 |
| 2019 | 53 | 80 | 46.42 | 92.84 |
| 2020 | 50 | 84 | 108.11 | 216.98 |

Percent Network Area by Functional Class and Condition Category

Condition in base year 2016, prior to applying treatments.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 1.5% | 12.4% | 35.1% | 0.0% | 49.0% |
| II / III | 1.1% | 6.5% | 11.2% | 0.0% | 18.8% |
| IV | 0.9% | 5.6% | 11.5% | 0.0% | 18.0% |
| V | 0.5% | 4.1% | 9.5% | 0.0% | 14.1% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Condition in year 2016 after schedulable treatments applied.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 2.3% | 17.9% | 48.6% | 0.0% | 68.7% |
| II / III | 0.6% | 2.9% | 4.9% | 0.0% | 8.4% |
| IV | 0.7% | 3.7% | 8.0% | 0.0% | 12.4% |
| V | 0.5% | 4.1% | 5.9% | 0.0% | 10.5% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

Condition in year 2020 after schedulable treatments applied.

| Condition | Arterial | Collector | Res/Loc | Other | Total |
|-----------|----------|-----------|---------|-------|--------|
| I | 4.0% | 27.8% | 65.0% | 0.0% | 96.8% |
| II / III | 0.0% | 0.0% | 0.9% | 0.0% | 0.9% |
| IV | 0.0% | 0.0% | 1.5% | 0.0% | 1.5% |
| V | 0.0% | 0.8% | 0.0% | 0.0% | 0.8% |
| Total | 4.0% | 28.6% | 67.4% | 0.0% | 100.0% |

California City Revenues¹

| Revenue Type | Authority | Description |
|---|--|---|
| TAXES | | |
| Property Tax | California Constitution Article XIII and XIII A; Revenue and Taxation Code 95, 97 | An ad valorem tax imposed on real property (land and permanently attached improvements) and tangible personal property (movable property). |
| Sales and Use Tax | California Constitution Article XIII 25.5(a)(2), 29; Revenue and Taxation Code 7200 et seq. | A tax imposed on the total retail price of any tangible personal property and the use or storage of such property when sales tax is not paid. |
| Transactions and Use Tax | California Constitution Article XIII 25.5(a)(6); Revenue and Taxation Code 7251 et seq. | A tax imposed on the total retail price of any tangible personal property and the use or storage of such property when sales tax is not paid. |
| Business License Tax | California Constitution Article XI 5 | Tax on businesses for the privilege of conducting business within the city. |
| Utility User Tax | California Constitution Article XI 5 | Tax imposed on use of utility services. |
| Transient Occupancy Tax | Revenue and Taxation Code 7280 and 7281 | General tax imposed on occupants for privilege of occupying room(s) in hotel, motel, inn, etc. |
| Documentary Transfer and Real Property Transfer Taxes | Revenue and Taxation Code 11901-11935 | Tax imposed on documents recorded in the transfer of ownership in real estate. |
| Admissions Tax | California Constitution Article XI 5 | Tax on the consumer for the privilege of attending a show, performance, display or exhibition. |
| Parking Tax | California Constitution Article XI 5 | A tax imposed on occupant of off-street parking space for privilege of renting the space within the city. |
| Construction/Development Tax | California Constitution Article XI 5 | Excise tax imposed on the privilege or activity of development and/or the availability or use of municipal services. |
| Mello-Roos Community Facilities Tax | Government Code 53311 et seq. 53368.3 | Special non-ad valorem tax imposed by a local agency to finance public capital facilities and services in connection with new development. |
| Parcel Tax | California Constitution Article XI 5 | Special non-ad valorem tax on parcels of property generally based on either a flat per-parcel rate or a variable rate depending on the size, use and/or number of unites on the parcel. |
| Local Vehicle Registration Taxes | Abandoned Vehicle Abatement CVC 22710, 9250.7 Transportation projects GC 65089.20, CVC 9250.4 Service Authority for Freeway Emergencies S&H 2550 et seq. CVC 2421.5, 9250.10 Auto theft/DUI CVC 9250.14 Fingerprint Identification CVC 9250.19 | A special tax on vehicle registration imposed countywide for specific purposes authorized by state law. <u>Abandoned Vehicle Abatement</u> – Abatement, removal, or disposal as a public nuisance of abandoned, wrecked, dismantled, or inoperative vehicles or parts from public or private property. <u>Transportation Projects</u> – Transportation-related programs and projects that have a relationship or benefit to the payees. <u>Service Authority for Freeway Emergencies</u> – Implementation, maintenance, and operation of a motorist aid system of call boxes on state and county roads and additional motorist aid services. <u>Auto Theft/DUI</u> – Programs that deter, investigate, and prosecute vehicle theft crimes. <u>Fingerprint Identification</u> – Purchase and operation of Cal-ID Fingerprint systems. |
| BENEFIT ASSESSMENTS | | |
| Assessments on Property | California Constitution Article XIID 4 – Proposition 218; GC 53750 et seq. – proposition 218 Omnibus Implementation Act; GC 54703 et seq. – The Benefit Assessment Act of 1982 S&H 2800 et seq. – The special Assessment Investigation, Limitation and Majority Protest Act of 1931; S&H 3110 et seq., | A charge levied on real property for a local public improvement or service that specially benefits that property. |

¹ League of California City – 2014 Municipal Guide

| Revenue Type | Authority | Description |
|--|---|---|
| | <p>S&H 5000 et seq. – Improvement Act of 1911</p> <p>S&H 10000 et seq. – The Municipal Improvement Act of 1913</p> <p>S&H 8500 et seq. – The Improvement Bond Act of 1915</p> <p>S&H 22500 et seq. – The Landscaping and Lighting Act of 1972</p> <p>S&H 36600 et seq. – The Property and Business Improvement District Law of 1994</p> | |
| Assessments on Businesses or Persons | S&H 36500 et seq. – The Parking and Business Improvement Area Law of 1989 | Assessments imposed in connection with some business improvement districts are levied upon businesses, not real property. |
| FEES, CHARGES, AND RATES | | |
| User and Enterprise Fees | California Constitution Article XI Section 7 | Fees paid to a municipality by a person to receive a particular public service including fees paid for municipal utility services. |
| Property-Related Fees | California Constitution Article XI Section 7 California Constitution Article XIII D Section 7 | A levy imposed on a parcel or upon a person as an incident of property ownership for property-related service. |
| Development Impact Fees, Dedications and Exactions | California Constitution Article XI Section 7 Government Code Section 66000-66025 | Dedications of property or fees, other than taxes or special assessments, charged to compensate for new demands on public resources resulting from the development of land and property and imposed as a condition of development approval. |
| Regulatory Fees | California Constitution Article XI Section 7 | A charge imposed on a regulated action to pay for the cost of public programs or facilities necessary to regulate a business or other activity. A regulatory fee does not include a charge on a property or a property owner solely due to property ownership. |
| OTHER REVENUES RAISED LOCALLY | | |
| Cable and Video Franchises | Public Utilities Code Section 440 et. Seq. Section 5800 et seq. | Payment to a municipality from a cable or video franchisee as “rent” or “toll” for the use of the streets and rights of way of a municipality. |
| Solid Waste Franchises | California Constitution Article XI Section 7 Public Resources Code Section 49300 | Payment to a municipality from a solid waste management franchisee as “rent” or “toll” for the use of the streets and rights of way of a municipality. |
| Electric, Gas, Water and Oil Franchises | California Constitution Article XI Section 7 California Constitution Article XI Section 5 | Payment to a municipality from a franchisee as “rent” or “toll” for the use of the streets and rights of way of a municipality. |
| Licenses and Permits | California Constitution Article XI Section 7 | A license or permit is issued to regulate activities or allow the use of public property. Typically, a fee is charged for a license or permit. Sometimes the fee for use of public property or facilities takes the form of rent. A license or permit issued for revenue generation purposes beyond the limitations of a regulatory fee or public property rent is a tax. |
| Fines, Forfeitures and Penalties | Penal Code 1463 Government Code 36900 Proposition 26 | Revenues received and/or bail monies forfeited upon conviction of a misdemeanor or municipal infraction. |
| Rents, Royalties and Concessions | California Constitution Article XI Section 7 California Constitution Article XI Section 9 | Revenues from rental or use of city property and/or resources. Franchise fees, which are sometimes considered rent for the use of public property are discussed in the first three items of this section. |
| Investment Earnings | California Constitution Article XI Section 5 | Revenue earned from the investment of idle public funds. |
| Gifts | Government Code 37354 | Contributions to the city for a public purpose. |
| STATE SUBVENTIONS | | |
| Motor Vehicle License Fee | California Constitution Article XI Section 15 Revenue and Tax Code 10751, 11005 | Based on the market value of a vehicle, VLF is a state-imposed tax for the privilege of operating the vehicle on public streets. |
| Motor Vehicle Fuel Tax | California Constitution Article XI Section 1 Revenue and Tax Code 7301-8404, 8601-9355 Streets and Highways Code 2100 et seq. | A per gallon excise tax on fuel used to propel a motor vehicle or aircraft. |
| Citizens Option for Public Safety (COPS) | Government Code 30061-30065 | Revenue granted from the state General Fund for supplemental public safety services. |
| Proposition 172: Public Safety Sales Tax | California Constitution Article XI Section 35 Government Code 30051 et seq. | A state voter approved half-cent sales and use tax imposed on the total retail price of any tangible personal property as |

| Revenue Type | Authority | Description |
|--|---|--|
| | | partial mitigation for ERAF property tax shifts from cities and counties. |
| Homeowners Property Tax Relief Reimbursement | California Constitution Article XIII Section 3(k) Revenue and Tax Code 218 Government Code 16120-16123, 29100.6 | Revenue to offset city loss of property tax for state imposed \$7,000 per dwelling homeowner exemption. |
| Williamson Act Subvention | Government Code 16140 | The Williamson Act fosters the preservation of open space by lowering property tax on restricted land. State subventions partially reimburse cities and counties for property tax losses under the program. The reimbursement was suspended in 2009. |
| State Mandate Reimbursement | California Constitution Article XIII B Section 6 Revenue and Tax Code 2201 et seq. | State reimbursement to cities and counties for programs and services mandated by the state. |