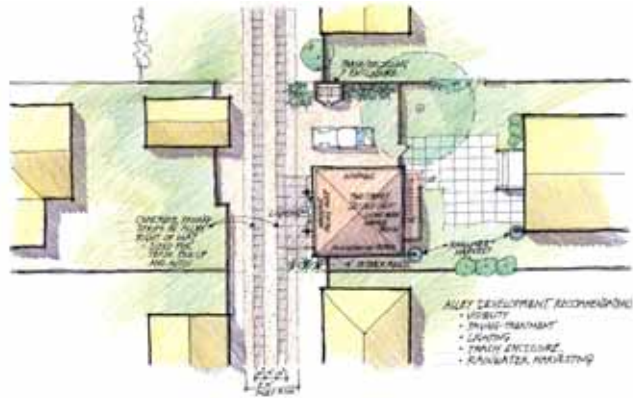


# SOUTHWEST CHICO NEIGHBORHOOD IMPROVEMENT PLAN



The City of Chico | December 2008



DESIGN, COMMUNITY & ENVIRONMENT



SOUTHWEST CHICO NEIGHBORHOOD  
IMPROVEMENT PLAN

Adopted by

The City of Chico

Resolution No. 99-08  
December 2, 2008



**DESIGN, COMMUNITY & ENVIRONMENT**

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# ACKNOWLEDGEMENTS

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Southwest Chico Neighborhood Association  
Hegan Lane business community  
California State University, Chico  
Chico Unified School District  
Chico County Day School  
Chico Heritage Association  
Butte County Association of Governments  
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Jesus Center  
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## **CONSULTANT TEAM**

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Bay Area Economics: *Economics*  
Kimley-Horn and Associates: *Transportation*

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*The Southwest Chico Neighborhood Plan sets a long-term vision and provides tools to steer future growth, development, and investment in Southwest Chico. The vision developed here results from the dedicated participation by community members, neighborhood residents, other stakeholders and work of City staff over the course of a year.*

The purposes of this neighborhood plan are: to articulate a clear vision and policy direction for the Southwest Chico Neighborhood Plan Area; to provide guidance for future public improvements and capital projects in the Plan Area; and to serve as a point of focus for neighborhood involvement in improvements activities, programs and projects. This plan is part of a broader neighborhood planning effort sponsored by the City that seeks to provide guidance on public improvement investments and add a neighborhood-specific perspective on issues to be considered in the process to update the General Plan.



### A. SOUTHWEST CHICO PLAN AREA

The Plan Area is roughly 1.2 square miles (654 acres), with a mix of residential, corridor commercial and manufacturing and light industrial uses. The Plan Area contains several distinct features:

- ◆ Park Avenue, the historic southern arterial to Downtown.
- ◆ The former Diamond Match factory site.
- ◆ The Barber Neighborhood, a residential neighborhood built originally as housing for the Diamond Match factory employees.
- ◆ Residential housing between Park Avenue and the edge of the Chapman-Mulberry Neighborhood.



*A home in the Barber Neighborhood.*



*Little Chico Creek.*

- ◆ An industrial park area accessed via Meyers Street.
- ◆ A large triangular industrial block located between the Park Avenue entry corridor and the Butte County fairgrounds, commonly referred to as “The Wedge.”
- ◆ Little Chico Creek, which runs between the Downtown and the Plan Area.
- ◆ Comanche Creek, which runs immediately south of the Meyers industrial area.

The Plan Area is shown in Figure 1-1.



*A section of the upper Park Avenue Corridor.*



*A business in the lower Park Avenue Corridor.*

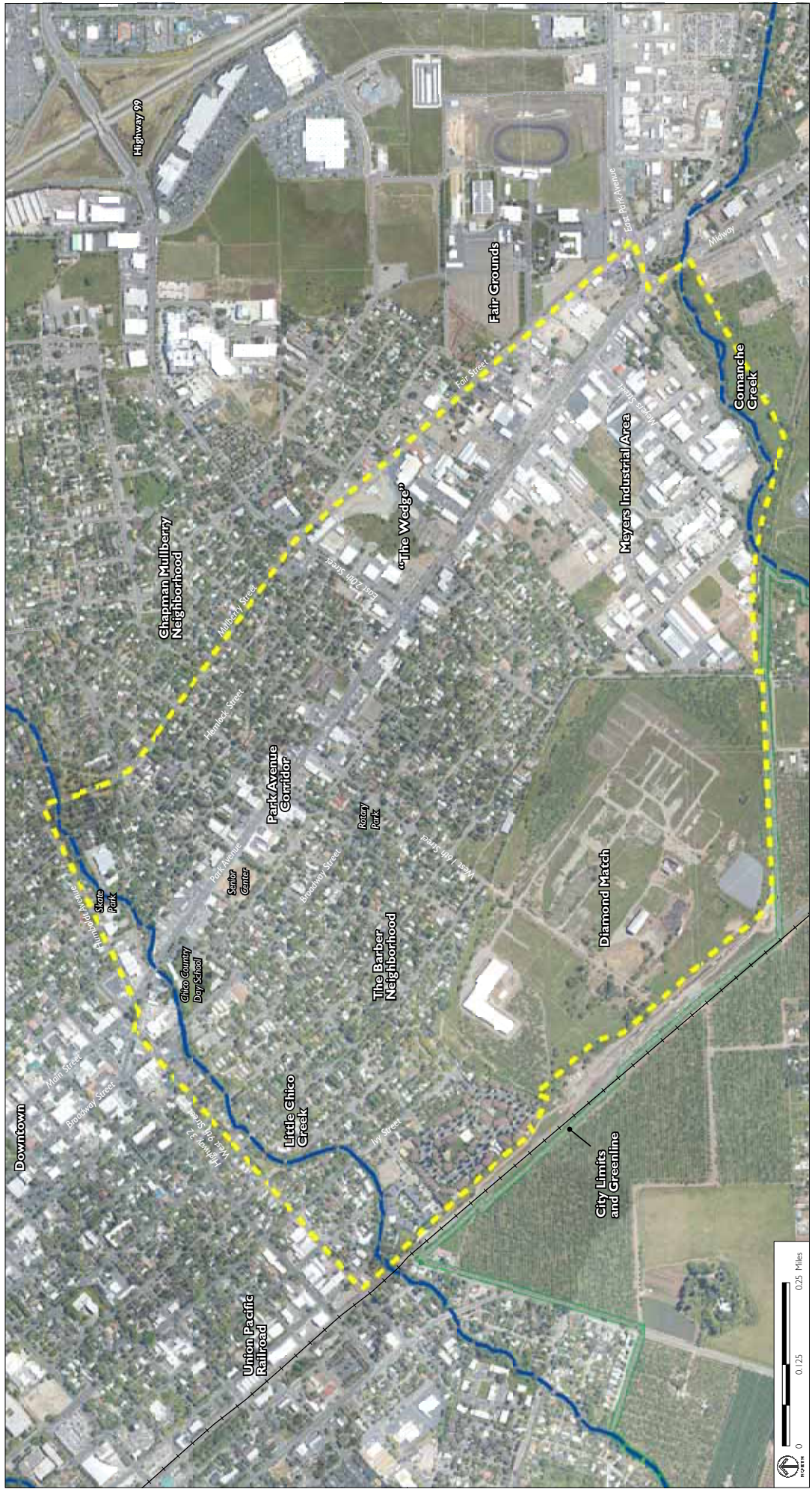


FIGURE I-1  
SOUTHWEST CHICO NEIGHBORHOOD PLAN AREA



**B. PARK AVENUE VISIONING STUDY**

The 1994 Chico General Plan, as amended, identifies Park Avenue as a potential component of a ring transportation corridor. In 2002, the Park Avenue Visioning Study was undertaken for the City of Chico to define a vision for Park Avenue north of East 22<sup>nd</sup> Street and address the future of Park Avenue over a 15- to 20-year horizon. The study imagined future growth along the corridor as being interrelated to the intensification of bus service.

Using input from a community process, the Vision Study identified four component themes for the Park Avenue Corridor:

- ◆ **Community Character:** Ensure that future development makes Park Avenue a unique place.
- ◆ **Pedestrian Scale:** Reintroduce a scale of development that enables Chico residents in the neighborhoods on either side of Park Avenue to safely co-exist with the automobile-oriented nature of Park Avenue.
- ◆ **The Park Avenue Relationship with Chico:** Envision the Park Avenue Corridor as a unique neighborhood that is well integrated into the fabric of the City of Chico.
- ◆ **New Development:** Develop Park Avenue in a manner that implements the Vision. Figure 1-2 illustrates a three-dimensional view of the prototypical building types envisioned for new development along the corridor.



The 1200 Park Avenue Senior Apartments and the street improvements around it were built following the Vision Study's design guidelines.

This Neighborhood Plan expands on the recommendations of the Park Avenue Visioning Study to address how Park Avenue and the surrounding neighborhood can support each other in a socially and economically sustainable manner.

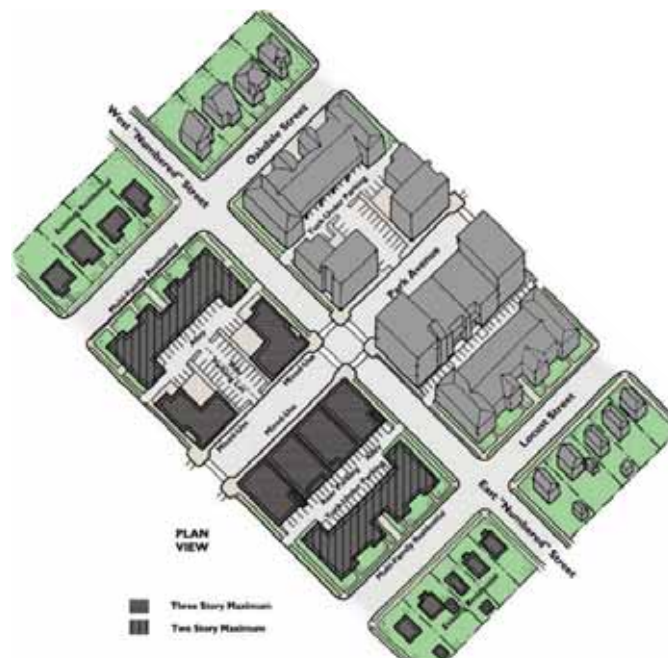


Figure 1-2. Conceptual Building Types from the Park Avenue Visioning Study.

### C. NEIGHBORHOOD PLANNING AND FORM

As a Neighborhood Plan, this document reflects evolving thought about neighborhood planning and neighborhood form. The neighborhood as a planning concept emerged near the end of the 19th century, as a response to the perception of dirty and unsafe urban conditions brought about by the industrial revolution. The neighborhood was a compact place with a mix of land uses where people would work, live, learn, play, and conduct commerce all within a five-minute walkable distance of about a ¼-mile.

The advent of the automobile and development of large tracts of single-family homes redefined acceptable distances, and a new theory of “the neighborhood unit” emerged, where housing was clustered around a node of activity including a school, park, civic institutions and neighborhood-serving commercial businesses. Figure 1-3 shows a conceptual plan of this idea. A neighborhood unit was imagined to support approximately 5,000 people within its “pedestrian shed” which is defined as a ¼-mile radius.



Figure 1-3. Neighborhood unit as drawn by Clarence Perry, 1929.

The neighborhood unit was again redefined in the post-WWII era, as “neighborhood” became any tract of land and houses produced by a single developer. The land uses found in activity nodes in traditional neighborhoods were scattered throughout a town or city, which increased dependence on private automobiles and decreased the viability of mass transit such as buses and street cars.

The Southwest Chico Neighborhood Plan Area reflects a historic development pattern in its smaller, pedestrian-scaled blocks. It also reflects post-WWII planning in that most neighborhood-

serving uses are beyond a convenient walking distance. The Plan Area contains more than five distinct neighborhood units as defined by applying the ¼-mile radius, as shown in Figure 1-4.

Within the Plan Area, however, there are naturally-occurring focal points with the potential of becoming activity nodes; places that are pedestrian accessible and could support multiple neighborhood serving activities. This Plan examines closely the strengths of these focal points, how to build on them--through improved infrastructure, novel development types, and connections to other amenities and the residential neighborhood.

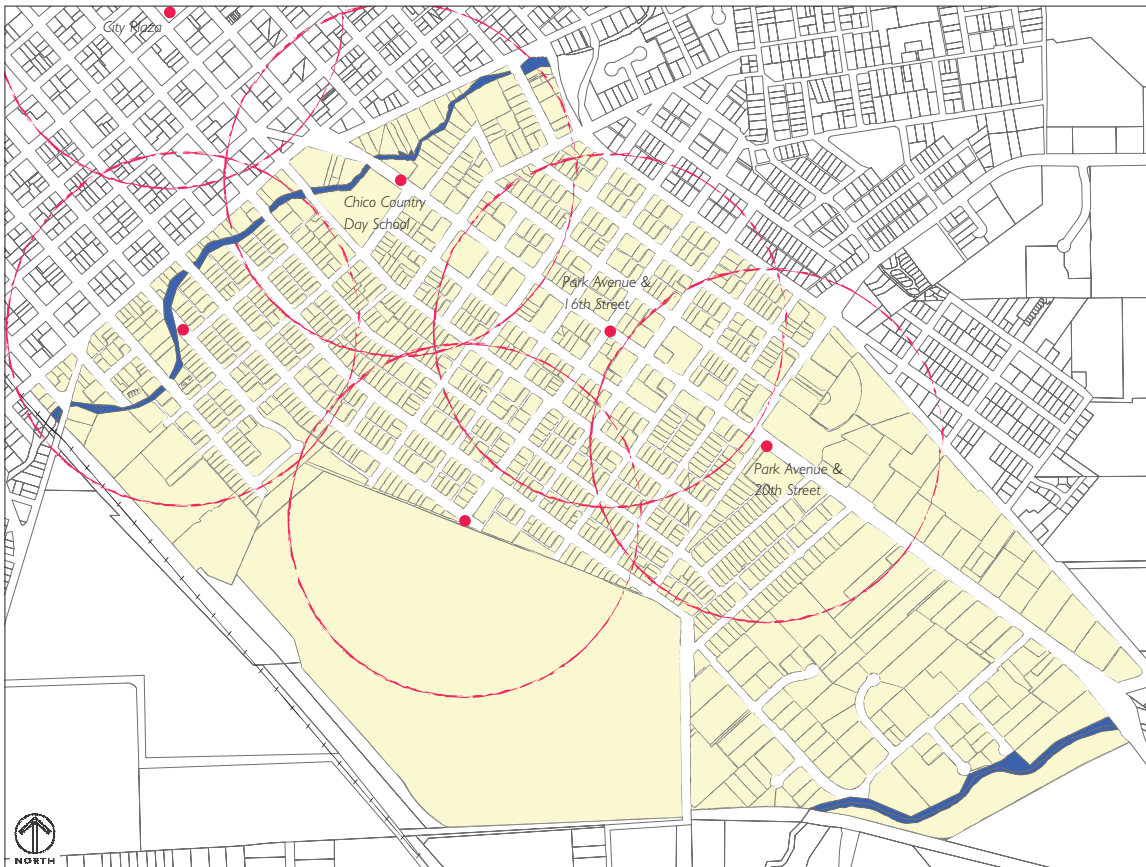


Figure 1-4. ¼-mile radius diagram of Plan Area Neighborhoods.

## D. PLAN DEVELOPMENT PROCESS

### I. Initial Steps

Although the neighborhood began meeting in 2005 to address issues of concern to the neighborhood, work on the Southwest Chico Neighborhood Plan began in March 2007 with a startup meeting to identify concerns and issues and discuss expectations for the process and products. This meeting included City staff and community representatives from the Plan Area. The community representatives comprised the Project Development Team (PDT), a group of neighborhood residents, business owners and land owners who would be engaged through the planning process to offer local knowledge and insight to guide the process and evolving plan.

After the initial meeting, an analysis of the Plan Area's existing conditions—including urban design, market and economic conditions—followed and is summarized in Chapter 2.



Stakeholder Interviews.

### 2. Stakeholder Interviews and Outreach

The City and consultant team identified various interest groups within the Plan Area including residents, business owners, land owners, school officials, parents, and others interested in the future of Southwest Chico. These groups met with the consultant team and City staff to voice their concerns and ideas. Input from these sessions is summarized in Appendix A.

The City printed announcements of the planning process and workshop dates, posted them around the area, and mailed them to all addresses in the Plan Area. Residents also posted signs in front yards announcing the process and workshop.

### 3. Public Workshop and Design Session Series

A six-day series of public meetings and design workshops allowed an iterative process of input from participants with design responses to the issues raised. The designs were presented for feedback, and then refined further based on comments.



Public Workshop.



The workshops and Plan Area tours provided participants opportunities to provide input through several structured formats, including open forums, group exercises, tour workbooks and comment sheets. City staff discussed implementation issues with the consulting team at the team meetings, and groups with specific issues or knowledge met with members of the consulting team. The design studio space was open to the public and visitors could write their comments on notepads which were later read by the consultants.



Public Design Session.

By the final public workshop, participants had seen the core strategies for the Plan several times and were afforded several opportunities to help refine them.

**4. Plan Development**

Following the workshop and design series, the consultants refined strategies, reviewed progress with City staff and prepared a Public Review Draft. This Public Review Draft will be presented at a fourth public workshop for final public comments before the draft Neighborhood Plan is finalized.

**5. Priority Setting Process**

After the draft Neighborhood Plan was presented to the community by the consultant at the fourth planning workshop, City staff led a series of twelve neighborhood priority-setting workshops. Each public workshop focused on the recommendations in the draft Neighborhood Plan pertaining to a particular topic area. Four of the meetings were hosted by local business owners and focused on issues and concerns of the business community.

Southwest Chico Neighborhood Plan – Questionnaire							
Part 1 – RATE THE DRAFT RECOMMENDATIONS: Please indicate how you rate the priority of each recommendation by circling a number (1=low, 5=high).							
RECOMMENDED ACTION	LOW	MED	HIGH	RECOMMENDED ACTION	LOW	MED	HIGH
<b>Infrastructure</b>				<b>Parks</b>			
1) Construct a demonstration streetscape project (walk, bicycle, sidewalk, parking, drainage/landscaping) as a model for future street reconstruction	1	2	3	24) Create new parks within convenient walking distance to all residents	1	2	3
2) Construct parking improvements where necessary neighborhood-wide	1	2	3	25) Explore possibility of acquiring parcel on southeast corner of W. 180 St. & Normal Ave. for a future park site	1	2	3
3) Construct surface drainage infrastructure where necessary neighborhood-wide	1	2	3	<b>Land Use &amp; Neighborhood Character</b>			
4) Install sidewalks where existing or fully damaged	1	2	3	26) Identify existing zoning to encourage small housing on Park Ave. south of 200 St.	1	2	3
5) Start a clean program to assist businesses in repairing falling sewer laterals	1	2	3	27) Change zoning on southeast corner of 200 St. & Park Ave. from industrial to commercial with transit corridor zoning (which allows residential on upper floor)	1	2	3
<b>Public Improvements, Transit, Circulation</b>				28) Create development incentives (i.e. reduced parking requirements) to promote higher intensity mixed uses along Park Ave.	1	2	3
6) Study the feasibility of installing a landscaped median along Park Ave. between 200 and Little Chico Creek	1	2	3	29) Increase visual access to the creek	1	2	3
7) Install gateway features at Little Chico Creek and Park Ave.	1	2	3	30) Target residential rehabilitation funding for low-income neighborhoods	1	2	3
8) Study the feasibility of a short transit loop connecting the SR Neighborhood to downtown	1	2	3	31) Adopt design guidelines for new residential, commercial and industrial development	1	2	3
9) Construct new public roads between Park Ave. and Park St.	1	2	3	<b>Resources</b>			
10) Create 2nd access to Meyer St. area from Park Ave. via Commerce St.	1	2	3	32) Create financial incentives for the use of creative storm water management techniques	1	2	3
11) Reevaluate intersection of Park Ave. and 200 St.	1	2	3	33) Increase funding for trees/urban forest enhancement	1	2	3
<b>Pedestrian &amp; Bicycle Improvements</b>				34) Conduct an historic resources inventory and initiate a signage program to identify historic resources	1	2	3
12) Install pedestrian safety and circulation improvements on W. 200 St. between Park Ave. and Broadway St. at the school	1	2	3	<b>Economic Development</b>			
13) Install high visibility crosswalk on Park Ave. at Little Chico Creek bridge	1	2	3	35) Initiate a program to provide financial assistance for businesses in the downtown neighborhood	1	2	3
14) Plan a regional path along old rail line near Comanche Creek	1	2	3	36) Initiate a program to provide funding for adaptive improvements to commercial buildings on Park Ave.	1	2	3
15) Install bicycle lanes on Chestnut St. from W. 90 St. to W. 180 St.	1	2	3	<b>Health &amp; Safety/Noise</b>			
16) Add crosswalk on south side of 90 St. at Household across Broadway and Park	1	2	3	37) Expand outreach, education and clean-up activities for the creek	1	2	3
17) Widens existing Salem St. bridge to accommodate pedestrians and cyclists or build a separate pedestrian/bike bridge	1	2	3	38) Expand "sweep & dust" clean-up events	1	2	3
18) Improve intersection crossings for cyclists at high traffic locations	1	2	3	39) Consider a party noise ordinance	1	2	3
<b>Creeks &amp; Open Space</b>				<b>Other Ideas</b>			
19) Acquire property along Little Chico Creek as it becomes available	1	2	3	40) <u>bike/pedestrian bridge over Salem + grant at Salem to connect SWChico to downtown.</u>	1	2	3
20) Design future multi-use pathway along south side of Comanche Creek	1	2	3	<u>* or Normal or Chestnut etc.</u>			
21) Design the future extension of existing Little Chico Creek multi-use pathway	1	2	3				
22) Build creek overlooks on the south bank of Little Chico Creek at Normal Ave. and Street St.	1	2	3				
23) Construct a park on City-owned property on north bank of Little Chico Creek at Head St.	1	2	3				

Priority-setting Questionnaire.

Following the priority-setting meetings, a steering committee consisting of business owners, commercial property owners, and residents was formed. The committee oversaw the development of an opinion poll which asked respondents to rate the priority level of the recommendations in the draft Neighborhood Plan. The questionnaire was sent to 2,695 residents, property owners, and business owners within the neighborhood. The response rate was approximately 6 percent, and over 50 percent of respondents to the poll had not attended a meeting related to the planning process.

Based on the results of the priority setting workshops and the opinion poll, the steering committee will develop an annual action plan. The annual action plan will be considered by the City Council and projects incorporated into the Capital Improvement Program as appropriate through the annual budget process.

#### **6. Plan Review and Adoption**

The Neighborhood Plan was presented at a public workshop, refined for review by the City of Chico Planning Commission for General Plan consistency on October 2, 2008, and adopted by the City Council on December 2, 2008.

*This chapter summarizes the existing conditions and the regulatory framework within the Plan Area. It concludes with an analysis of opportunities and constraints.*

## A. EXISTING CONDITIONS

### I. Local Context

The City of Chico, shown in Figure 2-1, has a population over 86,000 and is the largest city in Butte County. It is home to California State University Chico, with an enrollment of more

than 16,000 full-time students. The Southwest Chico Neighborhood Plan Area is just ½-mile south of the University and immediately adjacent to the Downtown.

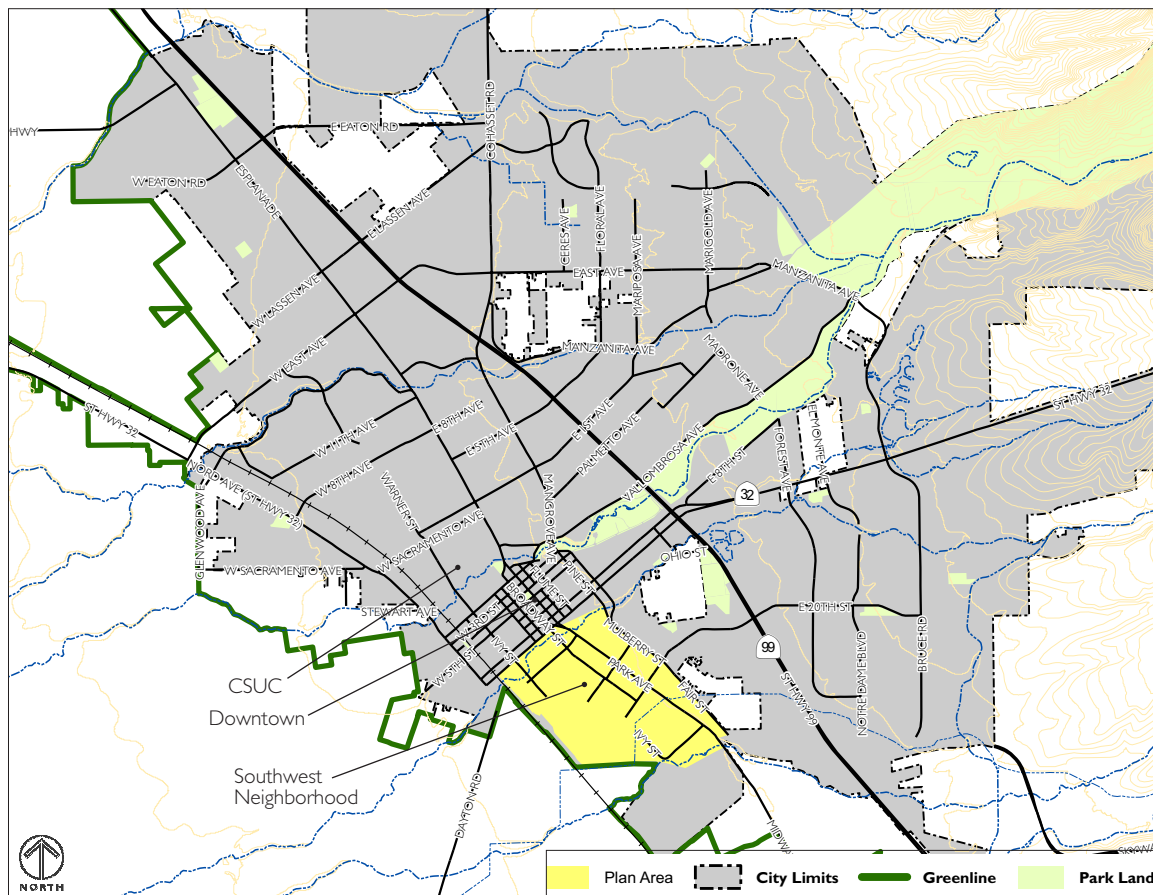


Figure 2-1. The City of Chico, the Greenline, and the Southwest Chico Neighborhood Plan Area.

## 2. Socioeconomic Characteristics

Census 2000 data indicates approximately 4,400 people live within the Southwest Neighborhood. There is a notable Hispanic population accounting for about 16 percent of the total. The median age is 25.5, with 22 percent of the population under age 18. The average household size is 2.1. Approximately 76 percent of households are rented. Average income, which is tabulated to include parts of Downtown outside the Plan Area, is \$25,128 per year.

## 3. Development Patterns

Current land use within the Plan Area is predominately residential, manufacturing and commercial, with very little parks and open space, as shown in Figure 2-2. There are over 1,740 dwelling units, with 46 percent single-family homes and accessory units, and 54 percent multi-family homes.



Typical conditions on Park Avenue north of 20<sup>th</sup> Street.



Streetscape improvements on Park Avenue north of 20<sup>th</sup> Street.

Land use conditions in the Plan Area vary widely. In order to develop a vision that responds to the existing patterns and character, this Plan divides the Plan Area into five Sub-Areas. The Sub-Areas are the Park Avenue Corridor, the Residential Neighborhoods, the Diamond Match site, the Meyers Industrial Area and “The Wedge,” which is the triangular area generally bounded by Park Avenue, Fair Street and East 20<sup>th</sup> Street. The Residential Neighborhoods Sub-Area is composed of two areas divided by Park Avenue, but both share similar development patterns and issues. The Sub-Areas are shown in Figure 2-3 and are discussed individually below.

### *Park Avenue Corridor*

Park Avenue is a four lane major historic arterial that connects to Downtown Chico and also serves as a gateway to the City from State Route 99. North of West 22<sup>nd</sup> Street, Park Avenue is a typical auto-oriented arterial, with a mix of primarily older one-story buildings with small retailers, auto dealerships and a few restaurants which abut residential neighborhoods. In many cases, parking lots front buildings that are deeply setback from the sidewalk. Utilities were recently relocated underground and the sidewalk has consistent street trees along its length. This section of Park Avenue was the subject of the Park Avenue Visioning Study in 2002 described in Chapter 1, which aimed to enhance Park Avenue as a mixed-use transit corridor with higher intensity development.

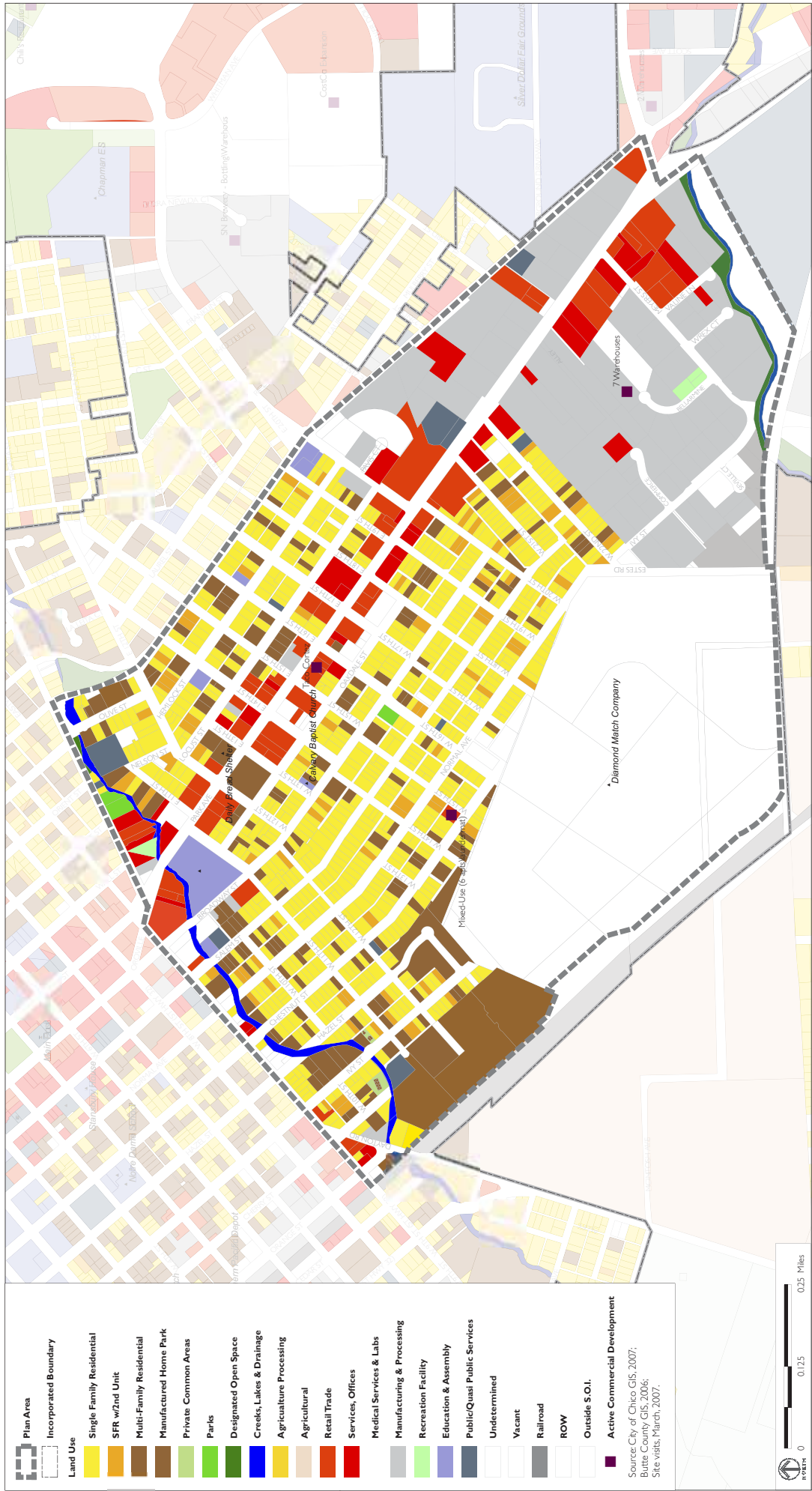


FIGURE 2-2  
CURRENT LAND USES, 2006



The section of Park Avenue from East Park Avenue north to East 22<sup>nd</sup> Street is fronted by older light industrial manufacturing and warehousing buildings on large lots. There are very few vacant buildings. Businesses include automotive dealerships and repair shops, outdoor storage and construction supply dealers. This portion of Park Avenue has a recently improved streetscape, with median plantings, public art, street trees and a multi-use bicycle path.

*The Residential Neighborhoods*

Within the Plan Area, there are two distinct residential areas divided by Park Avenue: the Barber Neighborhood to the west and the residences between Locust Street and Mulberry Street to the east. Historical homes over a century old exist throughout both of these areas. Most development follows the traditional grid block pattern with alley-loaded garages. Single family homes vary in architectural style, but most are fronted with porches and have lush planted front yards. A diverse, dense mature urban forest adds to the character of the neighborhoods.



*Park Avenue streetscape, south of East 20<sup>th</sup> Street.*



*Typical development on Park Avenue, south of East 20<sup>th</sup> Street.*



*A diversity of architectural styles are expressed in the residential areas.*



The streets in the residential areas are tree-lined and often lack sidewalks.

Streets vary greatly in width and character with street right-of-ways as wide as 88 feet and as narrow as 34 feet. Most residential streets east of Park Avenue are generally in good condition, with recent frontage improvements (curb, gutter or sidewalk) and newer pavement. Street conditions and frontage improvements in the Barber Neighborhood range from adequate to poor and vary block-by-block and street-by-street. Speed humps have been installed to deter speeding, but speeding remains a concern for residents.

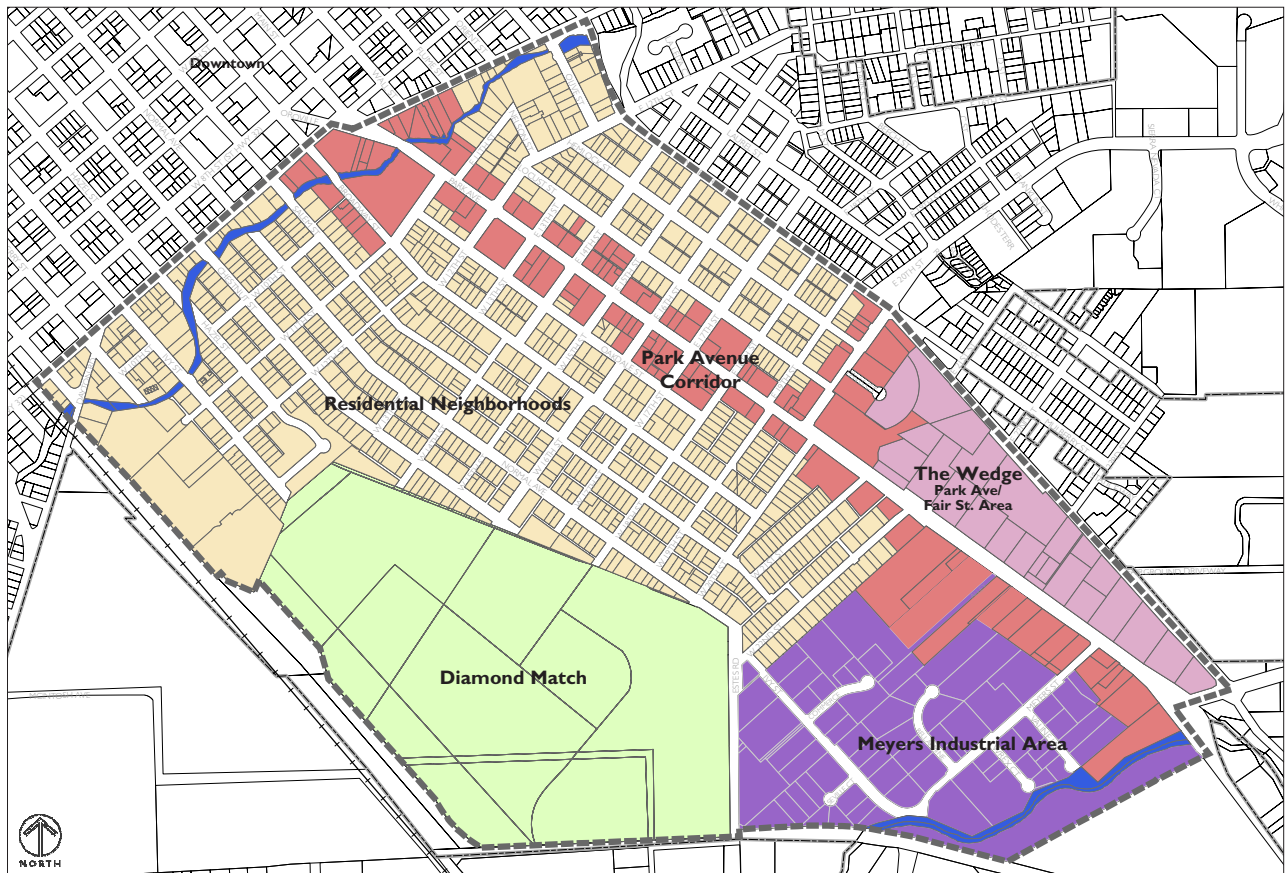


Figure 2-3. Sub-Areas within the Southwest Chico Neighborhood.



*Meyers Industrial Area*

A significant business center in Chico, the Meyers Industrial Area is a cul-de-sac light industrial park that includes some commercial services and offices. Businesses include food wholesalers, architecture firms, propane sales, building supplies, a salon, a coffee shop and a restaurant. There are few vacancies and additional business space is under construction.

*The Wedge - Park Avenue/Fair Street Area*

This large wedge-shaped area bounded by Park Avenue, Fair Street, East Park Avenue and East 20<sup>th</sup> Street is home to primarily light industrial and automotive uses, including: building supplies, recycling facilities, machining and transmission shops, snowboard production and glass blowing and crafts. There are few vacancies, but most businesses are low intensity with low site utilization.

Victor Industries, a former metal tube and can manufacturer, produced flexible tubes and aerosol cans using lead and aluminum on the 4.1-acre site at 365 E. 20<sup>th</sup> Street. From 1958 to 1985, Victor Industries used trichloroethylene to clean their product-line machinery. The resulting solvent waste was then allegedly dumped on the ground outside their manufacturing facility. In 1985, the site was stabilized via a State enforcement action which resulted in the excavation of the top six inches of contaminated soil. This site has been identified as a source of groundwater contamination in the Southwest Chico plume. The California Department of Toxic Substances Control (DTSC) and Victor Trust reached a settlement agreement in 2004, which included payment of funds for remediation, and the State

is now the Responsible Party. At the time of settlement, a covenant restricting land use to industrial or commercial (residential, hospital, school and day care center uses prohibited) was recorded on the entire site. (Source: [www.envirostor.dtsc.ca.gov](http://www.envirostor.dtsc.ca.gov) Envirostor ID No. 04360003, Site Code 100178).



*An industrial building in the Meyers Industrial Area.*



*Orient & Flume Art Glass, is located in the Wedge on Park Avenue.*

### *The Creek Corridors*

The Southwest Neighborhood Plan Area is bounded by two creeks: Little Chico Creek and Comanche Creek. Little Chico Creek at the north of the Plan Area is a densely vegetated creek corridor with residences and businesses backing onto it.



*The creeks are typically hidden from view by buildings or overgrown vegetation, providing spaces for unsavory activities.*

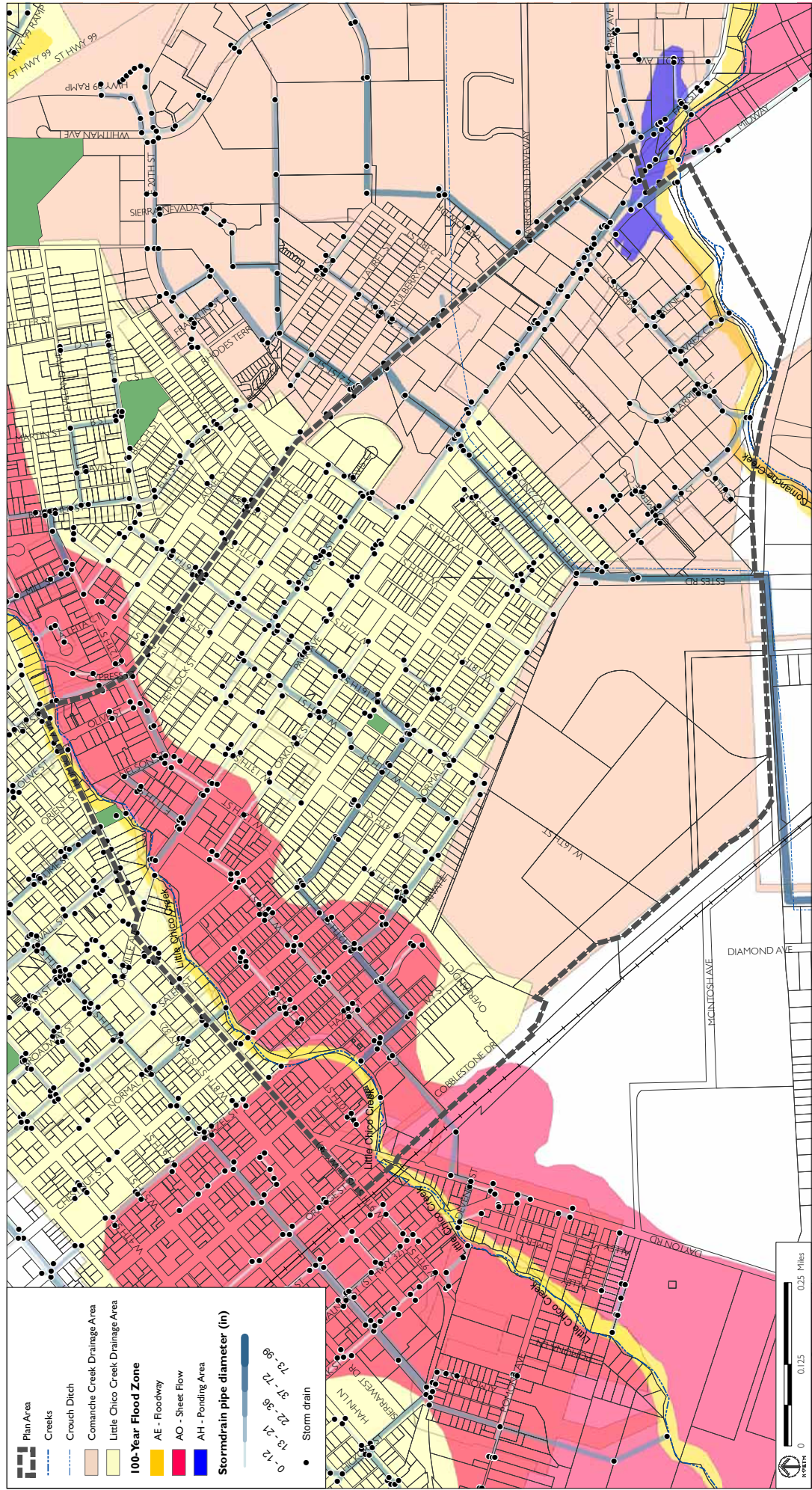


*Both creeks are rich habitats for plants and animals.*

Comanche Creek (also known as Edgar Slough) at the south of the Plan Area is an ephemeral drainage that terminates into a freshwater marsh. Comanche Creek flows year round, however, as it delivers irrigation water diverted from Butte Creek to the M&T Ranch southwesterly of the Plan Area. The Chico Redevelopment Agency owns a large parcel flanking both sides of Comanche Creek. The parcel was acquired to allow preservation of the area as open space and use of the parcel to mitigate the impacts to the Giant Garter Snake, Valley Elderberry Longhorn Beetle and Swainson's Hawk in connection with Redevelopment Agency projects.

Both creeks in the Plan Area are habitat for many animal species and many invasive, non-native plant species. Trash and debris in the creeks are a common sight. Transients set up camps in the dense thickets and under the bridges.

Figure 2-4 shows the flood zones, watersheds and stormwater infrastructure for the area. Although Little Chico Creek is shown as a hazard for the 100-year storm, Federal Emergency Management Agency (FEMA) models may predate some of the upstream controls now in place. If physical circumstances have changed the flood hazard information shown on the effective Flood Insurance Rate Map, an individual or a group of property owners may request a Letter of Map Change from FEMA. Storm drains from the Downtown and several neighborhoods in Chico empty into Little Chico Creek. Storm drains on Park Avenue south of East 20<sup>th</sup> Street and the streets in the Meyers Industrial Area empty in to Comanche Creek. Storm drains on other industrial sites south of the Plan Area, including



Areas draining into Little Chico Creek are shown in light yellow. Areas draining into Comanche Creek are shown in light pink. The 100-year floodways are shown in dark yellow. Areas that would experience sheetflow or ponding during 100-year flood-events are shown in hot pink and dark blue respectively.

FIGURE 2-4  
FLOOD ZONES, WATERSHEDS AND STORMWATER INFRASTRUCTURE



a petro-chemical storage operation, also empty into Comanche Creek. A section of the Crouch Ditch agricultural diversion runs parallel to Normal Avenue (formerly Estes Road) south of West 22<sup>nd</sup> Street and experiences periodic flooding.

#### *Diamond Match Site*

The Diamond Match site is an approximately 138-acre former industrial site bounded by the Barber Neighborhood, the Meyers Industrial Area, the railroad and the City's Greenline. In 1903 the Diamond Match Company located a wood processing and match manufacturing plant at the site which operated in various forms until 1989, when the facilities were closed by then-owner Louisiana-Pacific Corporation (LP). Most of the industrial infrastructure has since been demolished or destroyed. According to DTSC, formaldehyde, pentachlorophenol, heavy metals, solvents and fuel oil wastes were historically released, buried and/or burned on-site. Elevated concentrations of pentachlorophenol were detected in on-site wells. DTSC issued an enforcement order to LP in 1991 and soil was removed and disposed off-site in 1992. In 1995, arsenic contaminated soil was excavated, consolidated and capped on-site and DTSC approved a final Remedial Action Plan. In 1999, DTSC issued a site certification and entered into an Operation & Maintenance Agreement with LP and LP recorded a covenant restricting land use of an approximately 3-acre area capped with asphalt. The covenant restricts use of the capped area to commercial and industrial uses (residential, hospital, and day care uses prohibited) and prohibits removal of groundwater from the site. Any development requires notification of

DTSC. A groundwater pump and treat system was operated until 2003. Groundwater monitoring continues as does the required periodic reporting to the DTSC which is conducted by LP, the Responsible Party under State law. (Source: [www.envirostor.dtsc.ca.gov](http://www.envirostor.dtsc.ca.gov) Envirostor ID No. 04240002, Site Code 100186).

In 2001, the present owner of the site began a visioning process for a mixed-use development on the site consistent with the current General Plan. A conceptual development plan entitled "Barber Yard," was created but no complete application for entitlement has been filed with the City of Chico pursuant to that conceptual plan.

#### **4. Mobility**

The northern portion of the Plan Area has been developed with a compact, pedestrian-scaled block and grid pattern, but the pedestrian infrastructure itself is incomplete and in disrepair. However, a standard curb-gutter-sidewalk layout typical of suburban neighborhoods is in contrast with the eclectic character of the neighborhood, which many in the community would like to preserve.



*A view of Diamond Match with a historic industrial building in the distance.*

Given the distances to daily-needs businesses and the University, bicycles are a very important form of transportation in the Plan Area. Bike routes are well-planned in the Plan Area, with many routes on quiet neighborhood streets running north-south. East-west there are few routes. While routes are good, the pavement conditions on some bicycle routes are significantly deteriorated and in need of attention. There is a multi-use path on Park Avenue south of East 20<sup>th</sup> Street. The bridges at Salem and Ivy Streets are narrow with bicycles forced into vehicular travel

lanes. Park Avenue does not provide any bike lanes. Avid cyclists report avoiding Park Avenue and favor the parallel streets. Bike parking racks on Park Avenue are generally absent.

Vehicular congestion occurs especially at intersections with West 9<sup>th</sup> Street, in front of the Chico Country Day School on West 11<sup>th</sup> Street and in the Meyers Industrial Area. Speed-humps have been added to reduce speeding on streets in the Barber Neighborhood. Figure 2-5 illustrates the traffic controls that exist in the Plan Area.



Figure 2-5. Circulation Controls.

## B. REGULATORY FRAMEWORK

This section reviews relevant 1994 General Plan goals, as amended, and provides information on Chico Redevelopment Agency boundaries and City and State climate initiatives that were considered in the formulation of the Neighborhood Plan.

### I. General Plan

The City's 1994 General Plan, as amended, presents the overall vision for the City and defines many goals and policies. Goals from the Community Design Element relevant to the Southwest Chico Neighborhood Plan include:

- ◆ Establishing linkages for pedestrians and bicyclists as well as automobiles.
- ◆ Reclaiming streets as public spaces.
- ◆ Emulate the positive qualities of traditional Chico neighborhoods in new development.
- ◆ Create positive linkages [from the Diamond Match site] to the surrounding neighborhoods, and encourage a positive connection and orientation to Comanche Creek.

Some relevant specific policies from the General Plan include:

- ◆ **LU-G-2:** Promote infill development.
- ◆ **LU-G-3:** Ensure that new development is at an intensity to ensure long-term compact urban form.
- ◆ **LU-G-6:** Preserve the scale and character of established neighborhoods. With growth, there is a need to ensure that the character of established neighborhoods is not lost.

- ◆ **LU-I-7:** In developing neighborhood plans, guidelines should be prepared to encourage high quality design of residential neighborhoods. These guidelines should address the house and parcel, the street, the block and the mix of densities and uses.
- ◆ **LU-I-32:** "Retrofit" existing neighborhoods that lack convenience retail facilities with small (3-5 acre) neighborhood mixed-use centers, provided suitable sites are available.
- ◆ **LU-I-55:** As a condition of development approval, require preparation of a specific plan for the Diamond Match site.
- ◆ **LU-I-56:** Ensure that the program for the Diamond Match site allows for adaptive reuse of existing buildings and provides a range of housing types (700 to 1,200 units at an average density of 7 to 15 units per gross acre), 4 to 6 acres of retail commercial space, 8 to 10 acres of office space, and 10 to 15 acres of neighborhood parkland. Up to 12 acres of light industrial uses are also permitted provided they do not adjoin residences.
- ◆ **OS-I-22:** Ensure that open space corridors along creeks include protective buffers (non-development setbacks), preserve existing riparian vegetation through the environmental review process, and continue to require a minimum of 25-foot dedication and acquisition of 75 feet for a total of 100-foot setback from top-of bank along creeks. Bicycle and pedestrian paths and low impact recreational uses may be permitted in these open space corridors.

- ◆ **OS-G-15:** Preserve and enhance Chico's creeks and the riparian corridors adjacent to them as open space corridors for the visual amenity, drainage, fisheries, wildlife habitats, flood control and water quality value.
- ◆ **OS-G-16:** Where feasible, integrate creek-side greenways with the City's open space system and encourage public access to creek corridors.
- ◆ **T-I-27:** Explore the feasibility of developing a special design treatment along a "ring transportation corridor" emphasizing public transit and pedestrian improvements. The ring transportation corridor (illustrated in Figure 2-6) could be distinguished by bicycle and pedestrian-friendly wide sidewalks and tree canopies, and mixed-use commercial development with a focus on sidewalk activity and higher density housing along portions of the corridor.

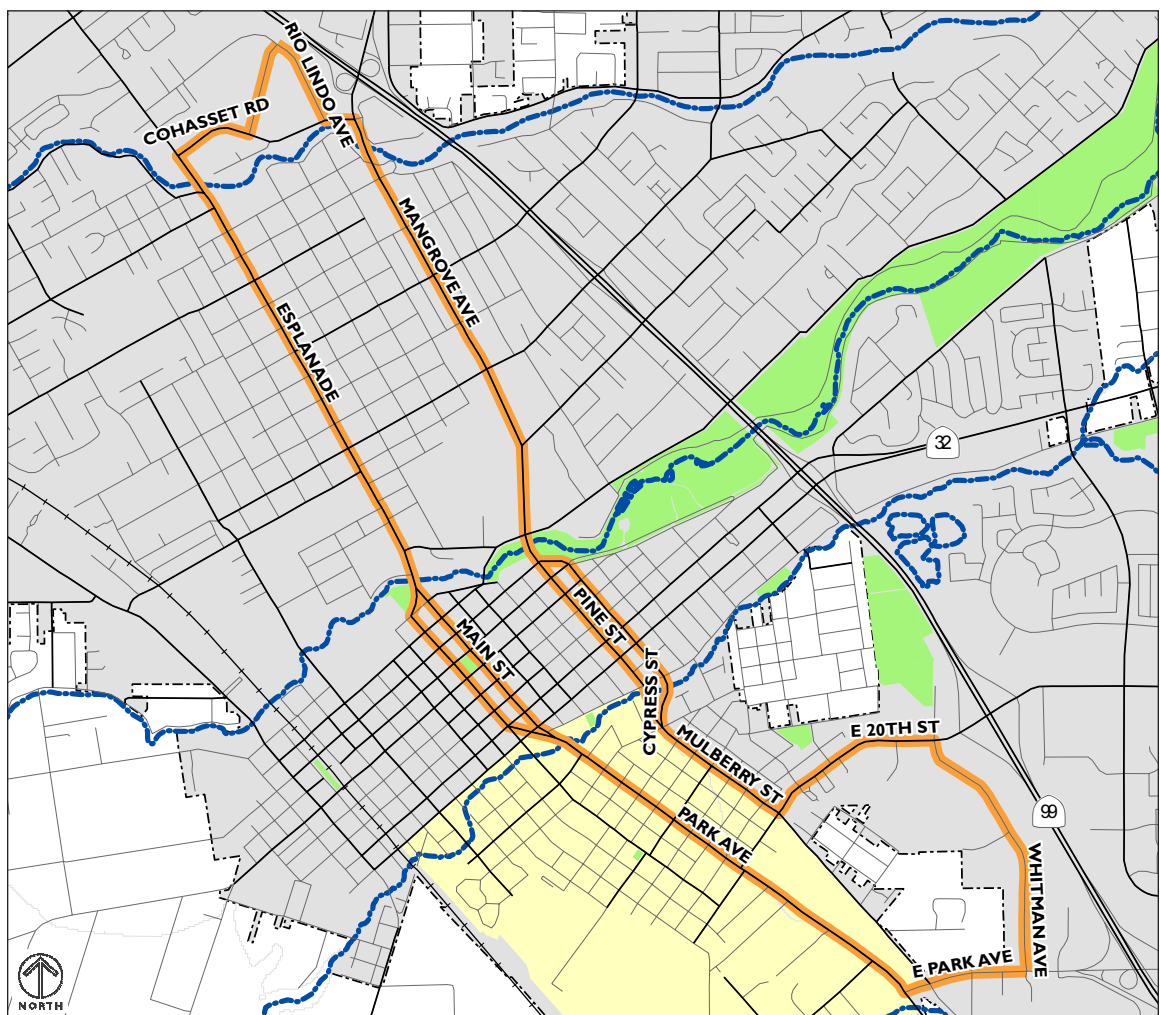


Figure 2-6. The 1994 General Plan ring transportation corridor.



The General Plan also designates allowed land uses for the Plan Area, as shown in Figure 2-8.

## 2. Chico Redevelopment Agency

Most of the Plan Area is included within the Chico Merged Redevelopment Project Area, as shown in Figure 2-7, below. A small portion of the neighborhood is not included in the Project Area because, at the time the Project Area was formed, legal counsel advised the area be excluded because of potential conflicts of interest for City and County officials who owned prop-

erty in that area. Redevelopment project areas exist for the purpose of eliminating blight, and serve to address affordable housing, inadequate public facilities, utilities and infrastructure. The Redevelopment Agency has provided funding for street, pedestrian and bicycle infrastructure projects in the Plan Area, including the Park Avenue reconstruction between East 20<sup>th</sup> Street and Comanche Creek, the Locust Street improvements and the 1200 Park Avenue senior housing development.

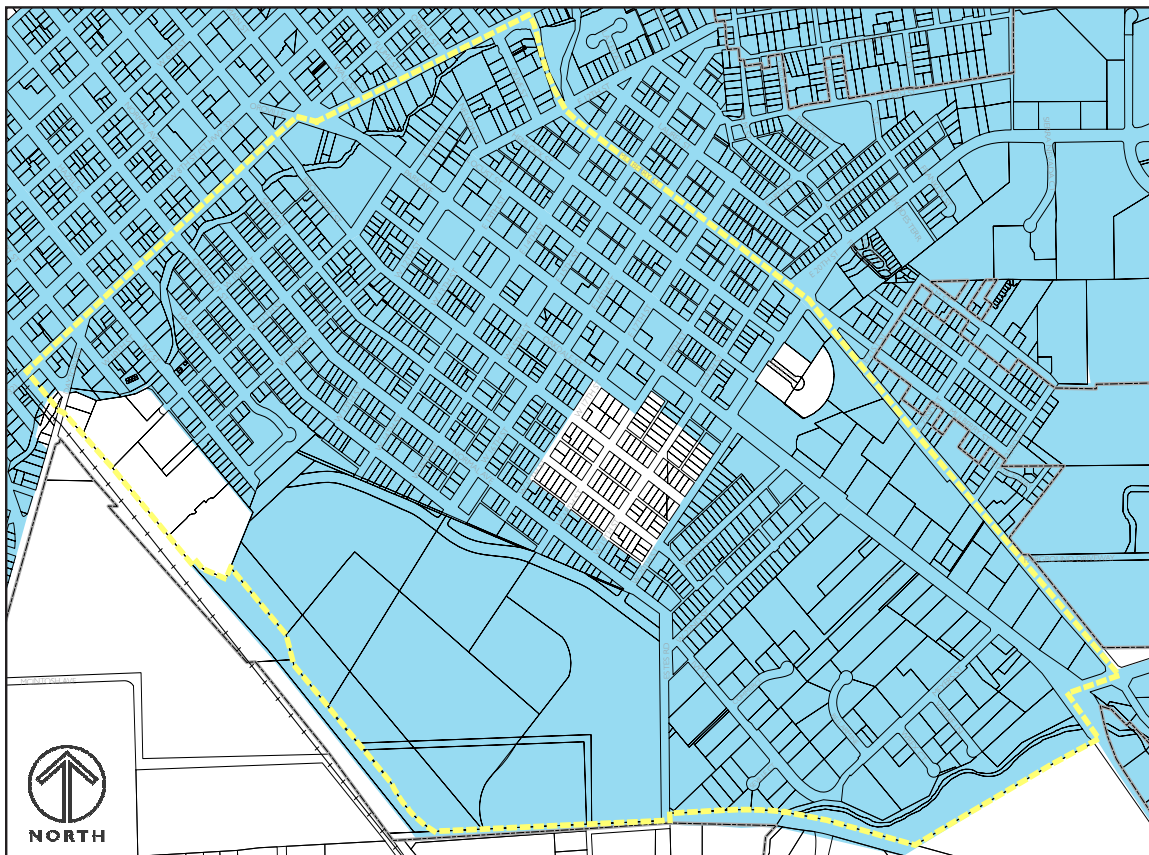


Figure 2-7. The Chico Merged Redevelopment Project Area.

### **3. City and State Climate Initiatives**

The Plan was undertaken shortly after the City of Chico adopted the U.S. Mayors Climate Protection Agreement, which set a goal to reduce green house gas emissions in the City below 1990 levels by 2012. The City also formed a Sustainability Task Force to study local climate change and propose recommendations to implement this policy. The State of California adopted a similar landmark bill AB 32: Global Warming Solutions Act. This calls for an 80 percent reduction below 1990 emissions levels by 2050.

While development standards and regulatory plans for these initiatives do not yet exist, these long-term goals can be aided by thoughtful planning. This Neighborhood Plan provides a number of recommendations which fit the goals of these initiatives, including: creating more compact urban form that enables people to drive less, locating jobs closer to the workforce, making walking and biking more viable alternatives, providing a range of housing opportunities and reducing urban heat island effects.

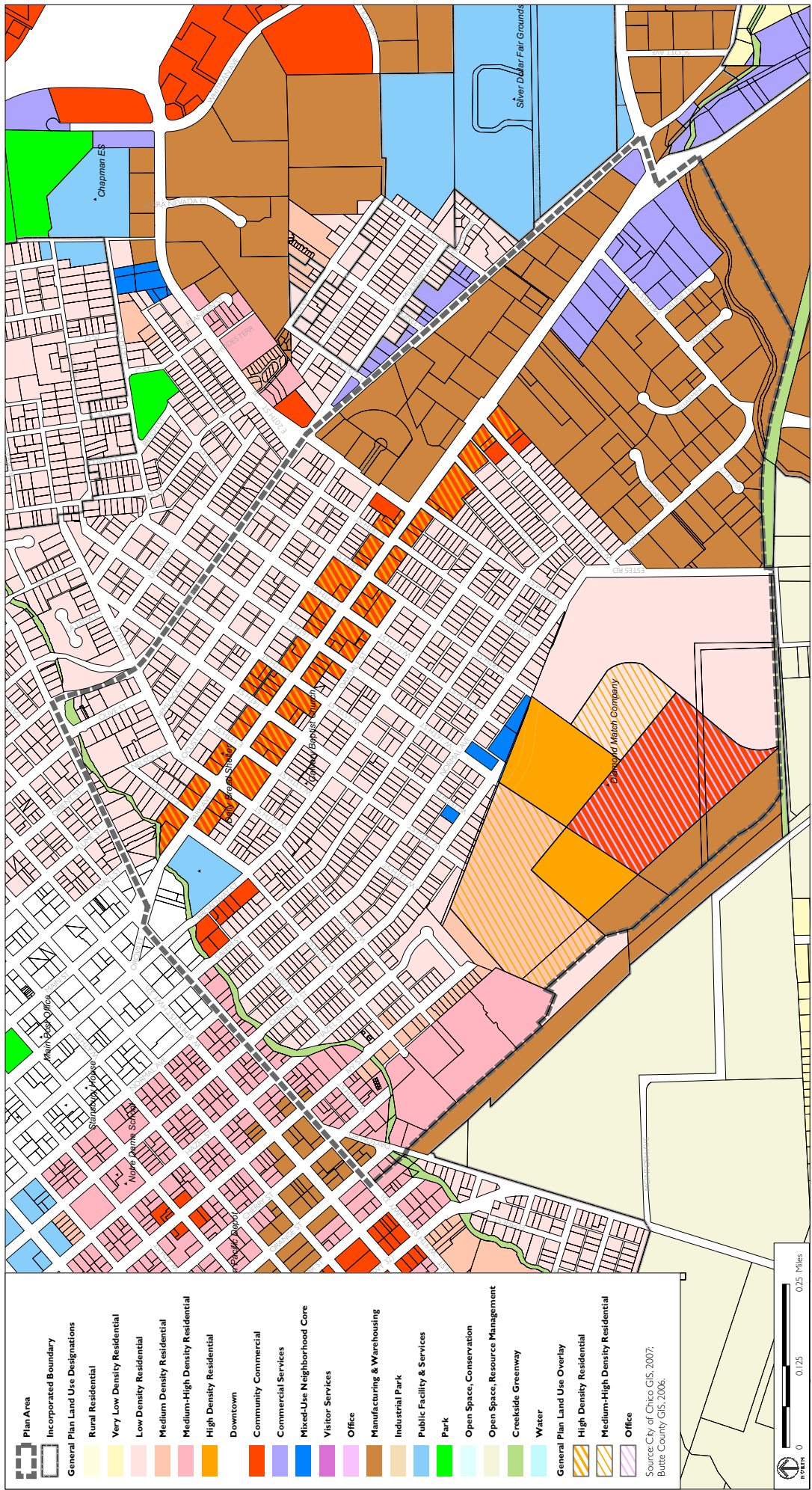


FIGURE 2-8  
1999 GENERAL PLAN LAND USE DESIGNATIONS



**C. OPPORTUNITIES AND CONSTRAINTS**

This section examines opportunities and constraints in the Plan Area through the land utilization patterns and overall neighborhood vitality. An economic analysis of the neighborhood conducted as part of this plan is included in Appendix B.

**I. Land Utilization**

Land utilization refers to how well a parcel is used physically, the level of investment in the parcel, the degree of improvement to the parcel and how well its uses fit with market conditions. Figures 2-9 and 2-10 illustrate utilization calcu-

lated as a ratio of physical improvements to land value (I/L ratio) for commercial and industrial uses. Properties with a low I/L ratio are likely underdeveloped.

The Park Avenue Corridor and the Wedge are especially underutilized. Aerial imagery depicts small building footprints set on large lots and few ancillary activities. Many of the older industrial and heavy commercial businesses provide few jobs and generate little revenue proportionate to the site area, and do not necessarily benefit from the highly visible location on Park Avenue. Businesses that are less reliant on high visibility could more appropriately be located in a business park setting, such as the Meyers Industrial Area.



Figure 2-9. Commercial Improvement/Land Value Ratio. Red properties indicate properties with limited improvements and investments. Green properties have significant improvements. Orange and yellow properties have moderate improvements.

Other areas with untapped potential by virtue of their strategic location in the Plan Area include: the East 20<sup>th</sup> Street and Park Avenue intersection, the area around Broadway Street at Little Chico Creek, the Humboldt Avenue and Park Avenue intersection, and the vacant parcels at Normal Avenue and West 16<sup>th</sup> Street.

## 2. Economic Vitality

Park Avenue and East 20<sup>th</sup> Street are the primary commercial corridors in the Plan Area, and they contain many established neighborhood serving businesses, including eateries, drinking establishments, a hardware store, a furniture store and convenience stores. Additionally, there are many auto-oriented businesses such as auto-parts, body shops and repair.

Based on stakeholder interviews and public workshops, there is a strong desire for additional neighborhood serving retail and services. Demographic analysis indicates the existing population size is a constraining factor, in that it will support a limited amount of retail. To maximize new retail's viability, it should be strategically located near other businesses.

There is community interest in bringing a grocery store to the neighborhood. The potential "supermarket shed" indicates the existing population is too small to support a conventional full size supermarket. However, alternative markets requiring 35,000 square feet or less may find opportunity here.

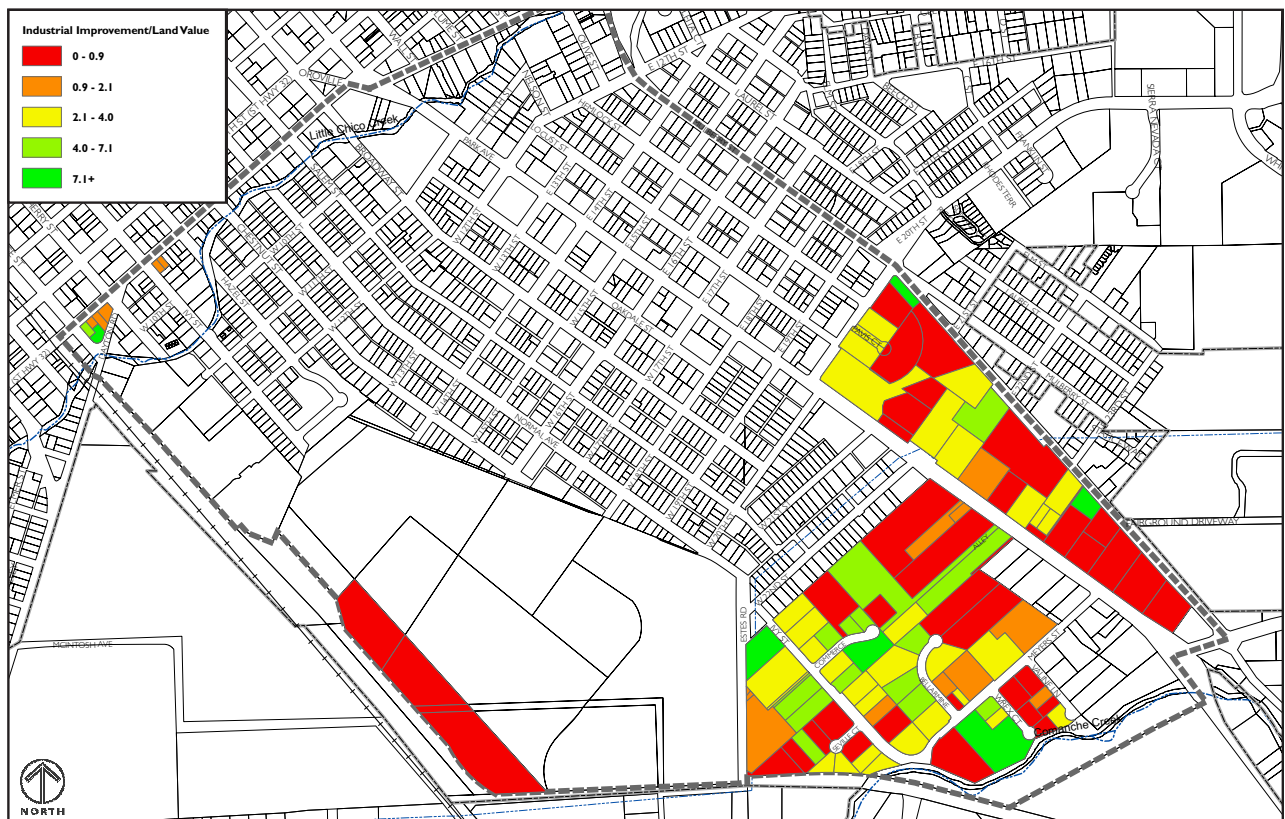


Figure 2-10. Industrial Improvement/Land Value Ratio. Red properties indicate properties with limited improvements and investments. Green properties have significant improvements. Orange and yellow properties have moderate improvements.

There does not appear to be any distinct concentration of office space in the Plan Area, possibly due to lack of suitable space. The Economics Background Memo (see Appendix C) notes that given Park Avenue's proximity to Downtown, the Park Avenue Corridor may be a good location for small- to mid-sized office users who cannot be accommodated Downtown. Current limitations of the use of ground floor space for office uses along Park Avenue, however, constrain that potential.

Individuals contacted as part of the economic analysis indicated that the industrial areas within the Southwest Neighborhood are stable, with limited turnover of tenants. Vacancies in functional spaces are limited; however, older sub-standard structures or structures configured for specific user types may require longer periods of time to fill. Generally, the demand for industrial space in the Chico market was characterized as modest but steady, with small- to mid-sized spaces in highest demand.

The south end of Park Avenue, below 20<sup>th</sup> Street and Fair Street, offers a unique niche: heavy commercial/industrial uses with a highly visible street frontage. On these streets, manufacturing/industrial uses not dependent on street visibility may be more appropriately located in Meyers Street area or other business/industrial park areas.

The Southwest Neighborhood residential market attracts many families with a preference for "Chico Charmers:" older, classic homes in the traditional neighborhood setting. The neighborhood character lends authenticity many find lacking in other areas. The Plan Area also provides more affordable housing opportunities, both for rent and for sale, than many other parts of Chico.

### 3. Open Space

Access to parks and open space is an important issue as only two small parks, Rotary Park and the Humboldt Avenue skate park, exist within the Plan Area. While a greenway is envisioned at Comanche Creek, its location is distant from the residential areas. Opportunities for additional open space include: the City-owned parcels north of Little Chico Creek and open space that would be required if the Diamond Match site is developed in the future. Many in the community would also like to explore the currently vacant site at Normal and 16<sup>th</sup> as a park site.



Humboldt Neighborhood Park.





*The Southwest Neighborhood contains many elements of a vibrant and attractive neighborhood, including residential areas with historic architecture and charming streets, a lively vehicular and transit corridor and commercial and industrial centers. In addition, the neighborhood is bounded by the natural beauty of Little Chico and Comanche Creeks. The vision forwarded by the community builds on these elements, as expressed through the following five goals.*

### **Goal 1: Enhance Community Character**

The residential areas in Southwest Chico are characterized by a unique sense of place, where people take pride in stewardship of their neighborhood. This character stems from the historic pedestrian-scaled grid; the modest cottages, handsome bungalows and stately manors; the expressive gardens and front yards; the dense, mature urban forest; and the semi-rural quality of many of the streets. This Plan looks to maintain and extend this character. Private improvements and development in residential areas should observe design guidelines stemming from the existing context. Street improvements should be designed to improve but not replace. The existing urban forest should be well cared for to enhance what is there, and new trees as necessary.

The commercial areas of Park Avenue, the Wedge, and the Meyers Industrial Area have an informal appeal, with locally-owned businesses reflecting the eclectic charm of the neighborhood. These businesses are distributed with large gaps in the urban fabric, and therein is an opportunity to create new retail energy, residential types and other commercial uses that can invigorate the areas and transform them into vibrant places. The Plan provides architectural strategies to give

shape and texture to the street edge which will increase the presence and visibility of buildings, thus projecting a stronger identity and enhancing the value of the existing businesses. The Plan identifies entry points or gateways marking the threshold of the Southwest Neighborhood, places that will project the distinct identity of the area.



*Garden art in front of a cottage in the Barber Neighborhood.*



*Commercial use in historical building on Park Avenue.*



Active neighborhood retail street, Martinez, California.



Orient & Flume Art Glass on Park Avenue.



Teens heading to the Humboldt Neighborhood Park.

### **Goal 2: Foster a Healthy Local Economy**

The fact that one could easily live, work, and shop for most daily needs all within a small discreet area contributes to the high quality of life in the Southwest Neighborhood. Local businesses provide the economic foundation and informal social forums for Southwest Chico's community. Retaining, fostering, and expanding businesses in the area is critical to the future of the neighborhood. This plan considers mechanisms based on current economic and market conditions such as: supporting existing businesses with programs such as façade improvement programs; creating synergy between existing and new businesses at strategic points on Park Avenue; public investment in streetscape improvements to attract local shoppers; attracting private investment by encouraging new uses in underutilized areas such as the Wedge; and supporting high-visibility commercial activities along Park Avenue.

### **Goal 3: Ensure a Neighborly Environment**

The existing residential areas in Southwest Chico are places where neighbors look after each other, children are safe to play in public areas, residents stroll past porches, and homes and businesses present their best face to the street. These are places where people gather, socialize, recreate, work and conduct their daily business. A community goal is to ensure that this neighborly quality is embodied in any new development. Through improvements to the public realm that encourage public interaction, the creation of new opportunities for neighborhood serving retail, and the development of design guidelines to keep the traditional "not-suburban" character, this environment can be ensured.

**Goal 4: Encourage a Diverse and Sustainable Community**

All of the elements that make up a diverse and sustainable neighborhood exist in Southwest Chico, including residences, employment, retail, a school and recreation opportunities. Moreover, the neighborhood is home to an ethnically and socially diverse community with thriving Hispanic, arts, and student communities. Moreover, there is a strong desire to cultivate this diversity. Providing a range of housing types—live-work, multi-family, and accessory dwellings—that support differing needs and socio-economic levels is key to encouraging further diversity.

Environmental sustainability is a growing concern for the neighborhood, region and larger world. Future development envisioned in this Plan makes proactive steps toward promoting sustainability and lowering greenhouse gas emissions, steps such as: locating housing close to transit and alternative transportation options; providing local commercial services and jobs that encourage shorter and fewer car trips; using native landscaping and integrated stormwater management tools to reducing water usage and run-off; and promoting healthier creeks through stewardship programs and integrating them into public open spaces.

**Goal 5: Provide Safe, Cohesive Networks for Pedestrians, Cyclists, Transit and Motorists**

This Plan provides recommendations for incremental improvements to calm traffic, increase circulation options, alleviate congestion, create new and safer routes and crossings for pedestri-



*Live/Work housing in Santa Cruz, California.*



*Transit-oriented design, Chico, California.*



*Integrated stormwater management.*



*Bicycle and pedestrian route, Chico, California.*

ans and cyclists, and encourage urban patterns that support transit. Along with supporting a variety of modes of transportation, the Plan considers different scales of transportation, from local to citywide, and supports the concept of the ring transportation corridor suggested by the General Plan.

The following chapters provide recommendations to enable these goals to be realized. A conceptual illustrative that summarizes major components of the recommendations is shown in Figure 3-1.



FIGURE 3-1  
SOUTHWEST CHICO CONCEPTUAL ILLUSTRATIVE



*This chapter discusses and illustrates the primary recommendations of the Southwest Chico Neighborhood Improvement Plan. The chapter commences with a discussion of the creek corridors and then discusses each of the neighborhood’s five Sub-Areas, as defined in Chapter 2. Each section identifies overarching goals and objectives and then describes specific recommendations. Recommendations for Mobility and Circulation are addressed in Chapter 5. Additional tools and implementation steps required are detailed in Chapter 7.*

**A. THE CREEK CORRIDORS**

The Plan Area is “bookended” on the north by Little Chico Creek and on the south by Comanche Creek. The relatively flat plain between the creeks contains the streets, homes and businesses that make up the Southwest Neighborhood. These watercourses are important visual and natural resources that also reflect on the health and safety of the surrounding neighborhood. The City’s General Plan includes policies designed to create a healthy biotic environment in the creeks and incorporate them into a green network of open space, but does not address the localized issues of flooding, transients, and general neglect.

*Goals and Objectives*

- ◆ Transform the creeks into assets for the Southwest Neighborhood.
- ◆ Ensure “eyes on the creeks” by neighboring uses to alleviate safety problems at Little Chico and Comanche Creeks.
- ◆ Incorporate the creeks into a network of open space.
- ◆ Reduce the human impact of development on creeks and habitat.

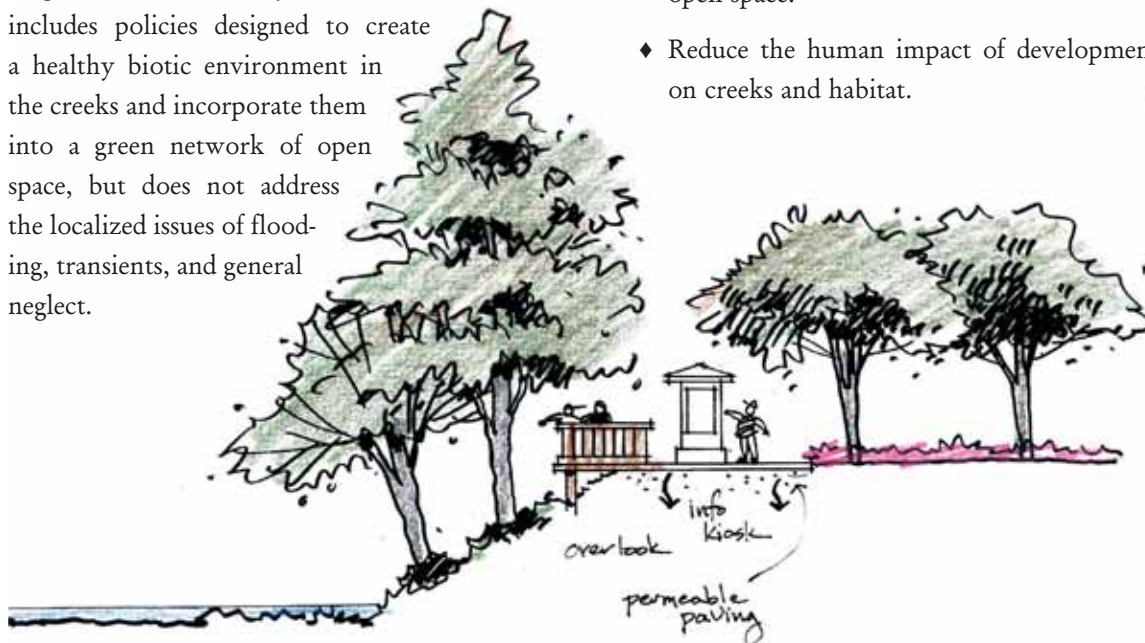


Figure 4-1. Overlook at the end of Hazel Street on the south bank of Little Chico Creek.

## *Recommendations*

### **I. Increase Visual Access**

Creeks can be beautiful and inspiring. Unfortunately, when neglected in an urban environment, they become secluded or ignored spaces. Opening them to the public by increasing visual access is the first step to turning a liability into a community asset.

Street dead-ends at both creeks present an opportunity to bring people closer to the creeks. The dead-ends could be pocket parks with benches, overlooks, and signage to educate visitors about

the creeks and increase stewardship. A conceptual overlook on the south bank of Little Chico Creek at Hazel Street is shown in Figure 4-1. Other possible locations include Normal Avenue and Cherry Street at Little Chico Creek, and Wrex Court, Valine Lane, and the corner of Meyers Street and Ivy Street on Comanche Creek.

At these locations, removing invasive species and dense brush and replacing them with low natives, such as California Blackberry, Wild Rose, and California Mugwort will further increase visual access while stabilizing the banks.



Figure 4-2. Conceptual mixed-use development on the bank of Little Chico Creek.



**2. Orient Development toward the Creeks**

Businesses that build blank walls on the creeks create unsupervised areas and lost opportunities. Buildings should face windows toward the creeks and outdoor seating areas or patios should be provided facing the creeks as shown in Figure 4-2. Section 19.60.030 of the City’s Municipal Code specifies the requirements, standards and limitations that apply to creekside development. Current standards prohibit structures, paving, grading and filling within the required 25-foot setback from top of bank. It is a high priority of the neighborhood to maintain and enforce this limitation on development and to rely on vegetation removal and management to increase surveillance along and into the creeks.

Residential development should also address the creeks. Multi-family development should consider unit entries from the creek side as well as gathering spaces and barbeque areas. Fencing should be low enough to see over easily or, if taller, should be at least 50 percent transparent.

**3. Incorporate the Creeks into a Open Space Network**

The 1994 General Plan, as amended, sets a goal to integrate creekside greenways with the City’s open space, and provide for habitat and, potentially, walking and biking paths. Providing recreational uses along the creeks will also serve to increase surveillance. The discussion below provides strategies for creating active paths and greenways along Little Chico Creek and Comanche Creek.

Comanche Creek warrants a gentle approach and riparian and oak woodland areas should be treated as managed wild area. Invasive plant species are a problem and should be replaced by native riparian plantings. Natural materials, minimal intervention and passive use should guide the design of open space and pathways. For example, a pathway should run well away from the creek rather than along its bank. Figure 4-3 shows how a pathway could connect to existing or envisioned regional bike networks.



Figure 4-3. Potential Comanche Creek Concept.

At Little Chico Creek, a continuous trail could be developed, as illustrated in Figure 4-4. Figure 4-5 shows a design that lays back the top of bank for a trail to delineate the public and private realms and incorporates runoff treatment. Design of elements and materials along Little Chico Creek should reflect an urban context. The Chico Country Day School is an especially important element in this open space network, and any redevelopment there should incorporate public access along the creek, with sensitive design solutions for fencing to allow views toward the public path while maintaining safety for children at the school.

The City owns several parcels north of Little Chico Creek, as well as a sizable property west of Midway along Comanche Creek. Consistent with current City policy through Budget Policy

G(2)(b), the City should make it a priority to consider purchasing other properties bordering the creek as they become available in the future. This existing policy should be reviewed and consideration should be given to clarifying and strengthening it. Larger parcels along Little Chico Creek are well suited for active urban parks: the skatepark on Humboldt Avenue is an excellent example. There is also potential for a park at Hazel Street on the north bank of Little Chico Creek. A conceptual design in Figure 4-6 shows a park where the north bank of the creek has been laid back, thus separating users from traffic on West 9<sup>th</sup> Street and serving to control flooding during high-water events. Physical characteristics in this area as well as the cost to develop and maintain a park at this location would be chief considerations in the design.



Figure 4-4. Potential Little Chico Creek Concept.

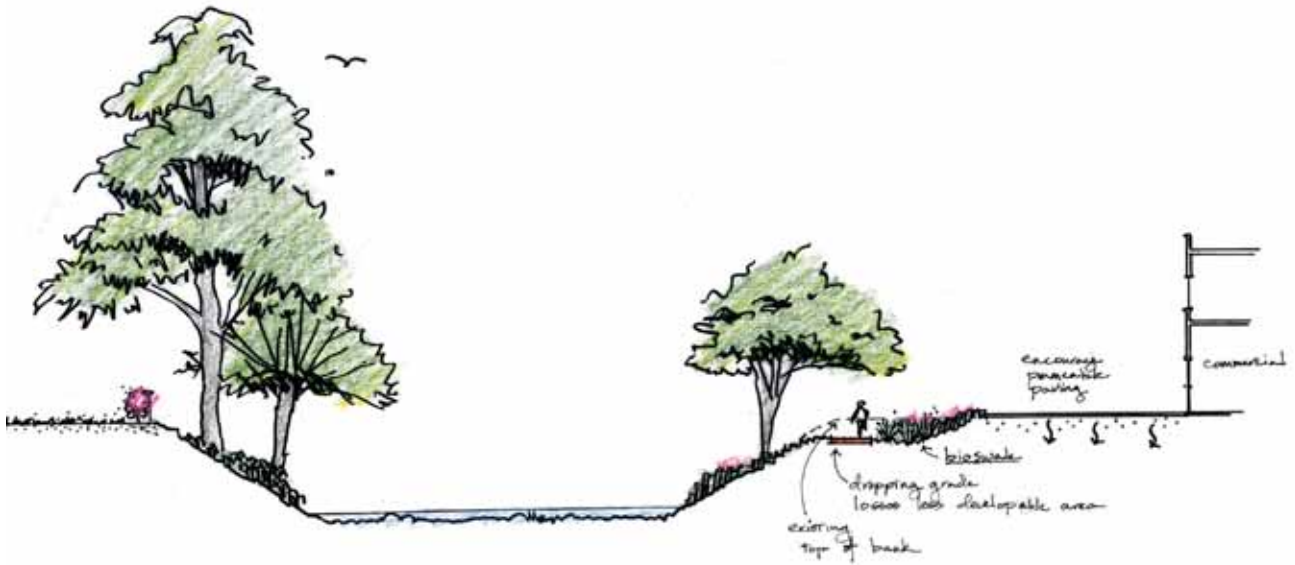


Figure 4-5. Creekside trail next to commercial uses.

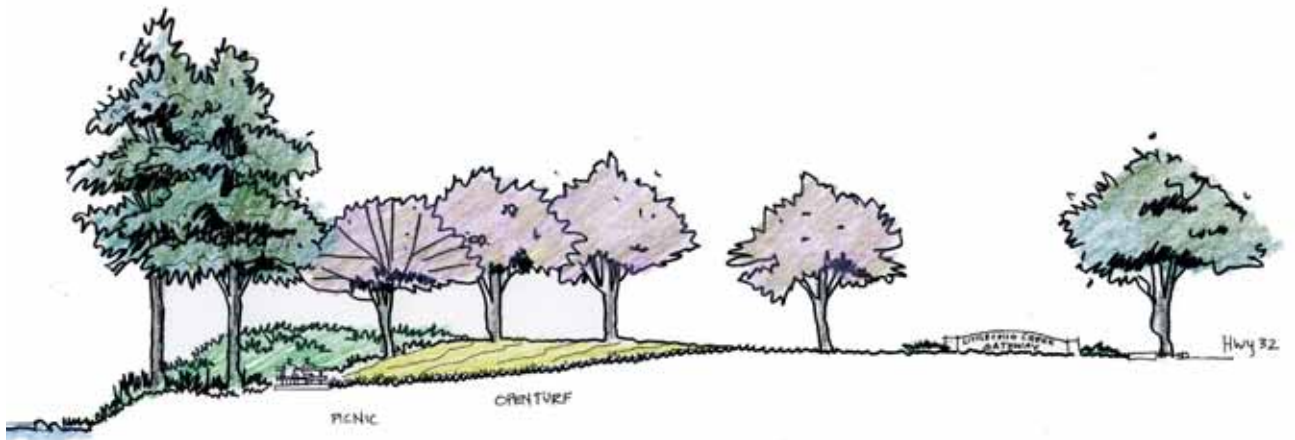


Figure 4-6. Passive park concept for Hazel Street on the north bank of Little Chico Creek.

#### 4. Manage Stormwater Runoff before it enters the Creeks

The City, the Regional Water Quality Control Board and other regional agencies work together to manage stormwater run-off, and the City provides a Best Practices Manual that includes tools to manage stormwater.

Given the Southwest Neighborhood's proximity to Little Chico and Comache Creeks, the City's interest in active and healthy urban creeks, issues with flooding within the neighborhood and the growing capital costs of conveying stormwater, future improvements and development should aim to minimize runoff to the largest extent possible. The following discussion provides recommended management tools appropriate to each of the Sub-Areas.

Within the Residential Neighborhoods, new street improvements could include perforated curbs where there is eight feet or greater of landscaping between curb and sidewalks to capture street runoff. The landscaping could be designed as a gently sloping grassy bio-swale. New and existing residences could be encouraged to divert roof runoff into infiltration areas.



*Grassy bio-swale with perforated curb.*

Within the Meyers Industrial Area and Wedge, it is preferable for runoff to be captured and treated by individual parcels onsite before entering storm drains to avoid introducing pollutants into the creeks and prevent increases in peak discharges causing erosion. Permeable pavement, infiltration trenches and vegetated bio-filtering swales are especially applicable here.

Park Avenue requires a more structured response to addressing stormwater retention and filtration, given the limited right of way and widened sidewalks for pedestrian activity. Suitable approaches in this setting include infiltration planters that are architecturally designed to be part of an adjacent building, permeable paving, greenroofs, cisterns and prefabricated stormwater media filters. Permeable paving could be designed in response to soil type and with a base suitable for traffic loading as well as temporary water catchment. Structural soils, when combined with permeable paving and/or tree wells, can increase water storage capacity while also providing sufficient area for street trees to develop a substantial root system, which in turn will better ensure long-lived and large canopied trees.



*Residential planting to increase the permeability of the sidewalk.*

In the design of all projects throughout the Plan Area, these basic principles could be considered to manage stormwater runoff:

- ◆ **Maximize opportunities to capture runoff and increase natural infiltration and evaporation.** New developments shall comply with current City stormwater management policies. Submit stormwater management plans that demonstrate no net increase in runoff discharge from pre-development conditions and mitigate water quality to the maximum extent practicable.
- ◆ **Minimize impervious surfaces.** Any reduction in pervious surfacing will result an increase in runoff, reduction in water quality and will contribute to the degradation of Little Chico and Comanche Creeks. New development throughout Southwest Chico could consider paving alternatives including permeable concrete and pavers, reduce street widths and increase landscaped areas wherever possible.
- ◆ **Encourage a multi-objective approach to stormwater management.** Denser urban development places a premium on available open space. Stormwater management and creek protection should be approached with multiple goals in mind, such as a greenroof that captures and filters stormwater while also providing a usable outdoor space for building occupants. A turf field within a neighborhood park can be designed to retain floodwaters during a large storm event.
- ◆ **Employ sustainable practices throughout the landscape to limit introduction of pollutants into the watershed.** Conventional landscapes often utilize synthetics fertilizers,

pesticides and herbicides that could enter Chico’s creeks and watersheds. Utilizing a more environmentally friendly approach to installing and maintaining the landscaped areas, such as the holistic approach outlined in the River-Friendly Landscape (RFL) guidelines, not only protects water quality, but also conserves resources, reduces air pollution and water demand, protects soils from erosion and increases wildlife habitat. Encourage the use of Integrated Pest Management (IPM) for landscape design and maintenance.



*Green Street project in Portland, OR designed to capture street runoff.*



*Residential grassy bio-swale to increase water infiltration.*

## B. THE PARK AVENUE CORRIDOR

The Park Avenue Corridor is the spine of the Southwest Chico Area, the common interface between all the Sub-Areas and a key section of Chico's Ring Transportation Corridor. As envisioned in the General Plan, the Ring Transportation Corridor would encourage transit ridership and "could be distinguished by bicycle and pedestrian-friendly wide sidewalks and tree canopies, and mixed-use commercial development with a focus on sidewalk activity and higher density housing." Better transit follows rooftops, and additional housing within one-half mile walking distance of Park Avenue will potentially increase the transit ridership available to make the Ring Corridor a reality.

The 2002 Park Avenue Visioning Study discussed in Chapter 1 defined a vision for Park Avenue north of West 22<sup>nd</sup> Street. The Southwest Chico Neighborhood Improvement Plan recommends specific strategies that address the length of Park Avenue to facilitate its transformation into a transit- and pedestrian-oriented, increasingly economically vibrant focus of neighborhood identity and activity.



Figure 4-7. Park Avenue as imagined in the Park Avenue Visioning Study.

There are many existing healthy businesses on Park Avenue. As discussed in Chapter 2, however, many properties on the corridor are underutilized, indicating that the corridor’s potential is still untapped. Higher intensity development on the corridor will increase activity in the area, offer more amenities to draw people to the area and allow them to “park-once” to meet several needs in one place. Enhancing existing businesses will also help catalyze this transformation.

*Goals and Objectives*

- ◆ Support and enhance existing businesses.
- ◆ Foster high-intensity nodes along Park Avenue that support transit.
- ◆ Encourage new context-sensitive development.
- ◆ Create a pedestrian friendly environment.

*Recommendations*

**I. Focus New Development at Nodes along Upper Park Avenue**

Transformation of Park Avenue should be strategic. However, due to its physical length, changing character and uses, and the constraints on retail need discussed in Chapter 2, Park Avenue should be thought of as a series of nodes—focal points of activity—rather than a single continuous condition. These nodes aim to concentrate energy and investment to create unique, lively and economically sustainable places. Figure 4-8 identifies three natural nodes, or focal points of activity that exist along the corridor and that can be built upon.



Figure 4-8. Park Avenue Node Locations.

#### *Gateway to the Southwest Neighborhood*

Park Avenue between 12<sup>th</sup> Street and 9<sup>th</sup> Street is a threshold, marking passage between the Downtown and the Southwest Chico Neighborhood. Figure 4-9 shows a conceptual site plan for development in this area. This area should convey a sense of arrival and identity through strong architectural forms. A gateway feature at Park Avenue where it crosses Little Chico Creek would accentuate this transition. An arching structure as proposed in the Downtown Streetscape Master Plan or simple gateway markers on either side would be appropriate here and help to solidify an identity for the neighborhood. In this area, a pedestrian crossing should be integrated into the gateway design or nearby.

The pedestrian crossing is considered in further detail in Chapter 5, Circulation. An attractive architectural edge along Park Avenue between Humboldt and 12<sup>th</sup> Street would enhance this experience of transition. Several underutilized properties facing Park Avenue are possibilities for new mixed-use development, extending the retail energy of downtown. The Chico Country Day School has the opportunity to become an important part of the gateway sequence as well. The side of the school along Park Avenue could be considered the “public face” of the school. If renovations to the school take place, the Park Avenue edge should be carefully considered for building improvements, landscaping and art projects such as murals.

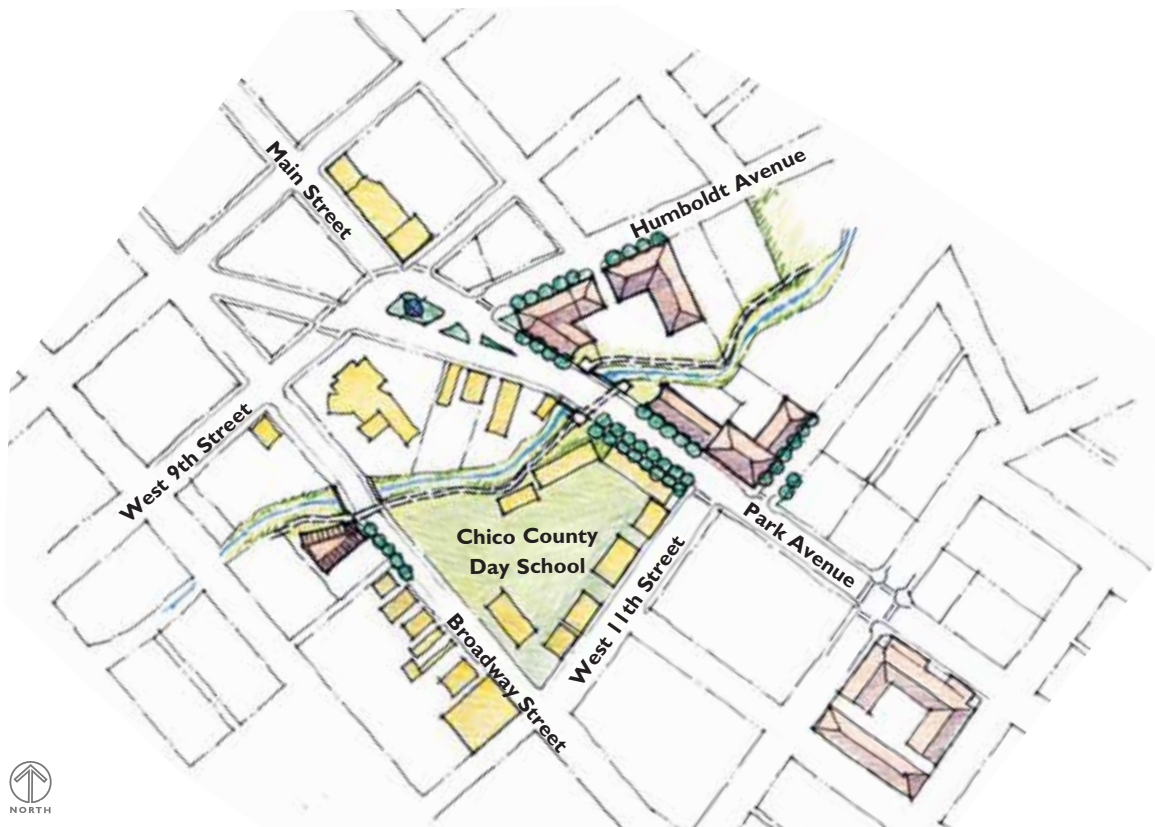


Figure 4-9. Potential gateway to the Neighborhood Concept.



The bridges crossing over Little Chico Creek could present elegant entries to the neighborhood with buildings that engage the creek and present a welcoming public face to streets entering the neighborhood. Figures 4-10 and 4-11 illustrate how a landmark building on the creek could engage the creek with a small plaza containing tables and chairs as part of a restaurant. Residences or offices above would help activate the space. A building of this type could also help activate other nodes described in this plan.



Figure 4-10. Potential mixed-use development.



Figure 4-11. Potential development on bank of Little Chico Creek.

### A Neighborhood Core

The blocks surrounding the intersection of Park Avenue and 16<sup>th</sup> Street are a central location to support smaller neighborhood-serving businesses that fill daily needs for area residents such as a small grocery, cafés and eateries, salons, gift-shops, laundry and dry cleaning services. Should new residential development on the Diamond Match site take place, the intersection of 16<sup>th</sup> Street and Park will likely gain prominence as a primary connection. Creating a hub of activity is critical for businesses to gain synergy, as people can easily walk between many businesses in a pleasant environment. Other possible uses

to encourage a vibrant gathering place are a public library, post office, a plaza or pocket park space and farmer's markets. Figure 4-12 shows a potential site plan for this neighborhood core concept.

While traffic volumes on Park Avenue present challenges to creating a pedestrian- and bike-friendly environment, the quieter numbered side-streets are very pleasant: retail businesses should wrap the corner from Park Avenue to present attractive frontages and facades along them. The wide right-of-ways would allow wide sidewalks with ample diagonal parking. Multi-



Figure 4-12. Potential Neighborhood Core Concept.

story buildings should scale down along Oakdale and Locust Streets to integrate with homes across those streets. New development should include human-scale details such as transparent windows and pedestrian scaled signage. Existing buildings with engaging architectural character such as the historical Stagecoach Antiques building should be preserved, and new development should respect and relate to existing buildings. Figure 4-13 illustrates the kind of development which would activate Park Avenue and wrap around the side streets. Figure 4-14 shows a conceptual street section along a perpendicular street such as 16<sup>th</sup> Street.



Figure 4-13. Conceptual development along Park Avenue.

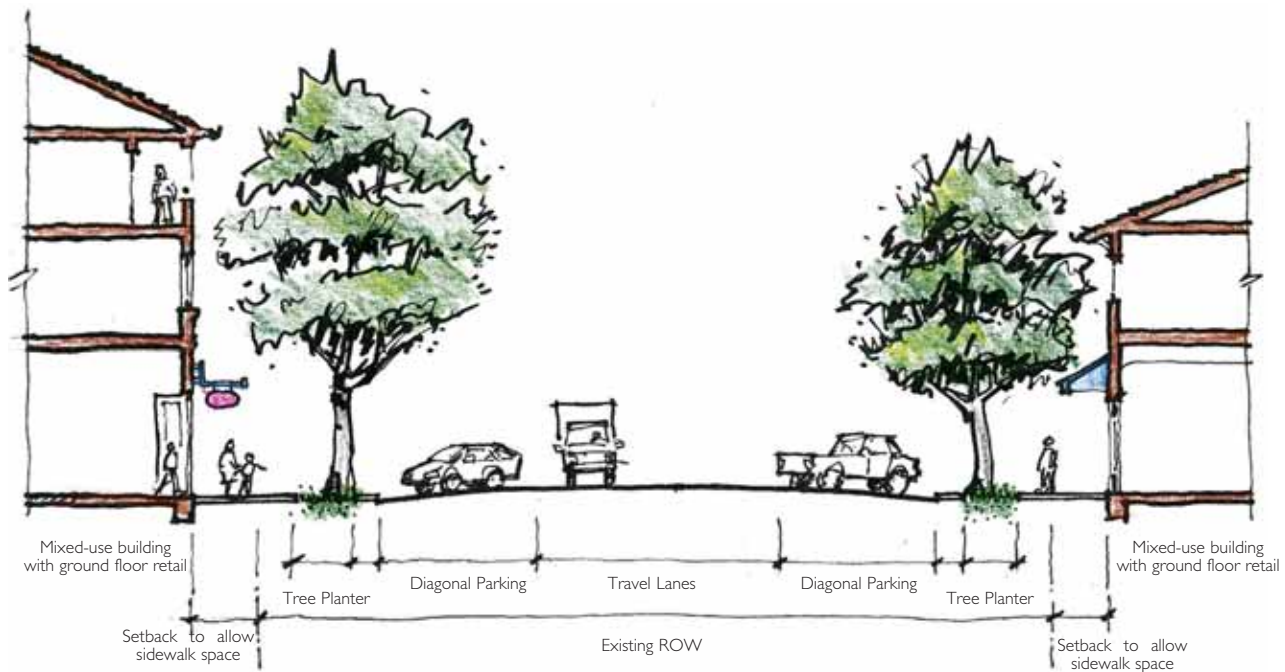


Figure 4-14. A Neighborhood Core "Numbered" side-street.

To enhance the pedestrian environment, Park Avenue and the side-streets should be narrowed at corners with bulb-outs, also helping to link businesses together. The bulb-outs shorten walking distances across streets and offer opportunities for public art, seating, and bicycle racks. Street trees should be planted where they do not already exist and existing mature trees should be preserved with street and sidewalk improvement that provide ample space for healthy trees. Stormwater and runoff management techniques should be integrated in the streetscape design, as discussed in the Creek Corridors recommendations.

Centrally located and architecturally designed bus shelters on both sides of Park Avenue should connect the neighborhood core to the city. One possibility to consider is locating a small civic plaza in the node. A corner location fronting Park Avenue and a number street is preferred, with adjacent uses and entries facing onto the plaza. Integrating the bus shelters with such a civic plaza space should be explored with the Butte County Association of Governments as this area develops.

If the Diamond Match site is developed, West 16<sup>th</sup> Street should be enhanced with additional landscaping, streetscape improvements and traffic calming improvements as applicable. See Chapter 5: Circulation, for further discussion of traffic calming related to development of the Diamond Match site.

#### *Park Avenue at East 20<sup>th</sup> Street*

Located at the southern edge of the residential neighborhoods, East 20<sup>th</sup> Street and Park Avenue is a busy link to Highway 99 and an important crossroads. The plan reviewed two options to improve vehicle flow at this intersection: a two-lane roundabout, or a “dog-leg” in East 20<sup>th</sup> Street to align it with West 20<sup>th</sup> Street across Park Avenue. The roundabout would help to smooth traffic flow and also create a landmark gateway to the area. Further information about the two options can be found in Chapter 5, Circulation.

New development on Park Avenue and 20<sup>th</sup> Street in this area could extend the retail-oriented pedestrian environment of Park Avenue up East 20<sup>th</sup> Street, with buildings approaching the street edge and parking set behind the major street frontage. There are several underutilized properties, including the County Corp Yard, which could be redeveloped. Supporting this type environment would involve zoning changes. A conceptual plan with the roundabout and mixed-use development along East 20<sup>th</sup> Street is shown in Figure 4-15.

Historically, this was the location of the “Mulberry Shops” train building and servicing yard for the Sacramento Northern interurban line that connected Chico and Oakland. The history of this location could be acknowledged through thoughtful references in the architecture and site planning for new development.

**2. Promote Additional Uses in the Upper Park Avenue Corridor**

The length of Upper Park Avenue not included within the three nodes should also be considered as a pedestrian-friendly main street type environment, but without continuous ground floor retail frontage. Existing businesses should be encouraged to improve façades and signage. Area residents are interested in seeing more local businesses open in this area and suggested a farmer’s market would be a good addition. Office or even residential use on the ground floors of new development in this area would add to the vitality of this area. As discussed in the economic analysis done as part of this plan, the City could explore an economic development strategy to encourage office use in the southern part of downtown Chico and the Park Avenue corridor.

Another use that could be located between the three nodes along Park Avenue is a grocery store. Many in the community support the idea of a new grocery, although others feel that the existing stores are sufficient. The economic analysis indicates that there is not enough demand to support a full size supermarket, but a new type of small-format market may be supported, particularly if new residential development occurs at the Diamond Match site and other nearby sites. The UK-based grocery retailer Tesco is aggressively entering the California market, and is opening small-format stores of approximately 10,000 square feet in size. Branded as “Fresh and Easy,” the stores offer a range of household goods and fresh and prepared foods, with locations opening in Sacramento and Yolo County in 2008. Such a market could be considered on Park Avenue or East 20<sup>th</sup> Street. An approximate footprint for a small grocery is shown in Figure 4-16.



Figure 4-15. Park Avenue at East 20<sup>th</sup> Street conceptual plan showing new mixed-use development and a roundabout.



Figure 4-16. Conceptual site plan for a small grocery on Park Avenue.

### 3. Develop Lower Park Avenue as a Retail-Light Industrial District

Existing industrial uses along Park Avenue south of East 20<sup>th</sup> Street do not need the high visibility Park Avenue provides. As recommended by the economic and financial analysis prepared for this plan, the high visibility sites along Park Avenue should have a retail face, with more industrial or background commercial functions behind or in the Meyers Industrial Area. Such businesses would benefit from the visibility and the City's existing investment in streetscape, bikeway and landscaping improvements along Park Avenue. Existing businesses could maintain their current

operations, while expanding with retail frontages serving for home-improvement and industrial arts and crafts, and possibly with offices above. An illustration of this kind of development is shown in Figure 4-17. If existing businesses have no need for public or retail frontage, relocation within the city to areas should be enabled through a formal relocation program.

The existing frontage road parallel to and east of Park Avenue creates a potential for a district of complementary businesses, building on the successful model that Orient & Flume Art Glass Co. provides at 2161 Park Avenue. Properties



Figure 4-17. Potential Retail-Industrial frontage on Park Avenue and the bikeway.

along the frontage road could build synergy as a “design district” for Chico, offering a range of goods and materials for home improvements, furnishings and art.

A similar approach of moving retail/office building frontages toward the street with industrial operations in the rear is also appropriate for parcels west of Park Avenue and on Fair Street.

#### **4. Support Existing Park Avenue Businesses**

The prior recommendations offer mechanisms for capitalizing on Park Avenue’s unique potential to foster a healthy local economy. Existing businesses will play an important role in creating an attractive area. A local voluntary business district could become an effective advisory group, and work with the City to facilitate public improvements in the Park Avenue corridor and surrounding neighborhoods. For existing structures on Park Avenue showing signs of age or disrepair, funding through the City’s façade improvement program should be made available.

### **C. THE RESIDENTIAL NEIGHBORHOODS**

The residential neighborhoods in Southwest Chico are active livable places rich with character and heritage. It is important to maintain their positive aspects, such as a healthy urban forest, compact scale, a range of housing types and styles, interesting gardens and welcoming frontages. At the same time, energy should be put into improving other aspects such as paving, sidewalks, lighting and alleys, as well as development of much-needed park space.

#### *Goals and Objectives*

- ◆ Retain the “not-suburban” character of the residential neighborhoods.
- ◆ Provide safe networks for pedestrians, cyclists, and motorists.
- ◆ Ensure a safe, clean neighborly environment.
- ◆ Maintain a diversity of housing types.
- ◆ Create park spaces so that all residences are within a 5-minute walk to a park.

#### *Recommendations*

##### **I. Physical Infrastructure**

Many residents voiced concerns about the condition of existing infrastructure in the residential neighborhoods. The most commonly voiced concerns were poor street paving and sidewalk conditions, lack of stormwater drainage, problems with sewer infrastructure and public streetlighting. Following are recommendations for improving the physical infrastructure of the residential neighborhoods. A variety of sources can be used to fund infrastructure improvements including redevelopment, Community Development Block Grant, and enterprise funds. Grants at both the federal and State level become available periodically as well.

##### *Streets and Sidewalks*

Southwest Chico has a pleasant pedestrian environment, but there are many opportunities for improvement. The paving widths on almost all streets in the Plan Area are much wider than necessary. The existing sidewalk conditions range from a full curb, gutter and sidewalk with

a planted landscape strip to no improvements at all. Streets generally fall into three ranges of right-of-way width although available right-of-way is sometimes narrower than actual right-of-way, as fences and gardens or even porches may encroach. Thus, the available right-of-way must be determined by a survey. There are four main goals regarding existing streets in the residential neighborhoods:

- ◆ **Create better pedestrian connections.** To create a comprehensive network, the Plan recommends “key pedestrian routes” which are prioritized for improvement, as described in Chapter 5.



*The Southwest Neighborhood is a creative and charming place.*

- ◆ **Create better bicycle connections.** The Plan recommends connecting bicycle paths and routes throughout the neighborhood.
- ◆ **Calm traffic.** Wide streets can lead to vehicles speeding. The Plan recommends a menu of traffic calming measures including street narrowing, bulb-outs, chicanes and mini-circles.
- ◆ **Collect stormwater.** Streets can serve as stormwater collectors with landscaped planting beds or permeable paving. This is discussed in the next section.

Integrated solutions to all of these issues are recommended by the Plan. Improvements to pedestrian connections provide opportunities for traffic calming and stormwater collection. To maximize benefits, paving width should be narrowed to 32 to 36 feet maximum. This paving width will allow two lanes of travel and parking on both sides of the street. Where feasible, the street width should be narrowed to 32'. In minimizing curb-to-curb width, there are several options for how to configure sidewalks and use the “surplus” space, depending on the amount of available right-of-way. Following are strategies to address the varying right-of-ways.

- ◆ **Wide Streets.** The widest streets with 60+ feet of available right-of-way, such as Normal Avenue, could be narrowed to 32 to 36 feet with a bioswale that is at least eight feet wide for proper grading and filtration capacity, and a wide sidewalk. Figure 4-19 shows one edge remaining as existing and pavement reclaimed on the other side of the street to maximize room for tree planting, runoff



filtration in a wide bioswale, and a wider, meandering pathway. Figure 4-20 shows a more conventional arrangement of curbs, planter beds and sidewalks on both sides of the street.

◆ **Medium Streets.** Most streets in the Plan Area fall in this category, with 45 to 60 feet of available right-of-way. On West 18<sup>th</sup> Street, for instance, the right of way width permits a 32- to 36-foot street with a bioswale on one side and a sidewalk. Figure 4-22 shows a wide bioswale on one side of the street with a sidewalk on the other. With sufficient room for an 8-foot bioswale and two 5-foot sidewalks, sidewalks could be installed on both sides as shown in Figure 4-23.

◆ **Narrow Streets.** Where the available right-of-way narrows to less than 45 feet, as on W 12<sup>th</sup> Street, there is room for a sidewalk and paved parking on only one side of the street. The opposite side can remain unpaved and could function as both parking and infiltration area. This is illustrated in Figure 4-25.

Given the size of the Plan Area, all streets will be not be improved simultaneously, and therefore need to be prioritized. The Plan recommends that pedestrian connection priorities and bicycle connections should determine which streets should be improved as described above. Chapter 5 addresses priorities for key pedestrian routes in the residential neighborhoods.



The typically wide residential streets in the Plan Area present great opportunities.



A pleasant, but incomplete, pedestrian network in the neighborhood.



An example of perforated curbs which allow water to infiltrate in a swale.

Figure 4-18. Typical neighborhood street with wide (60+ feet) right-of-way.

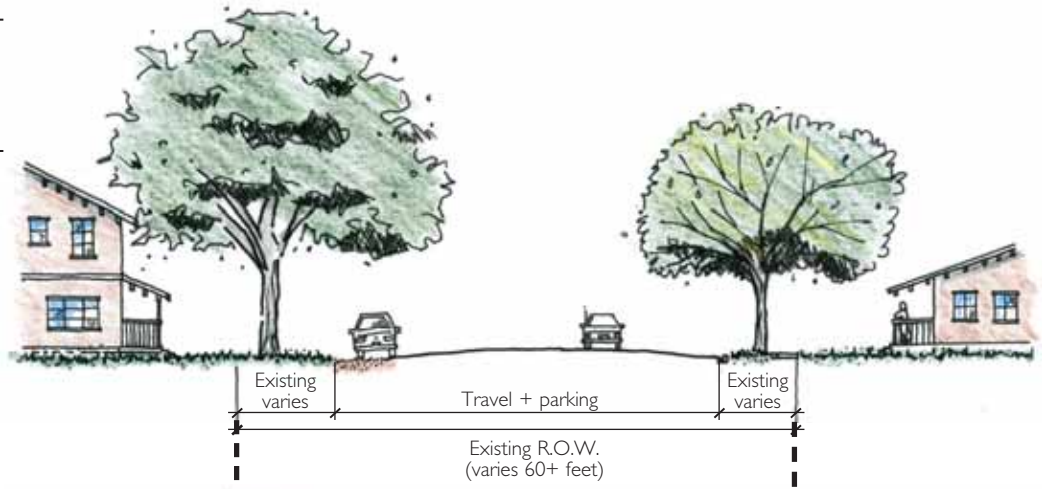


Figure 4-19. Recommended configuration for wide streets, showing a bioswale and a path on one side. The other side is left unimproved.

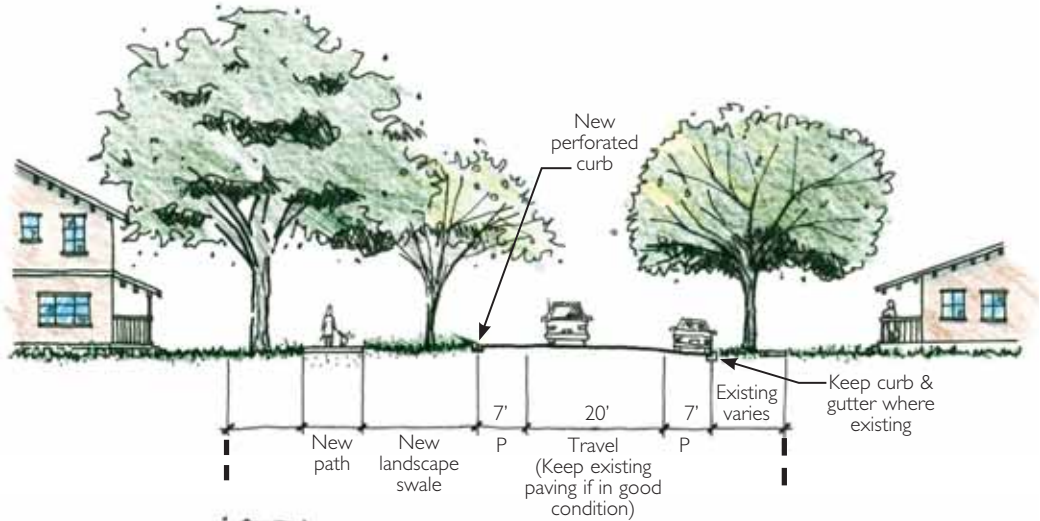
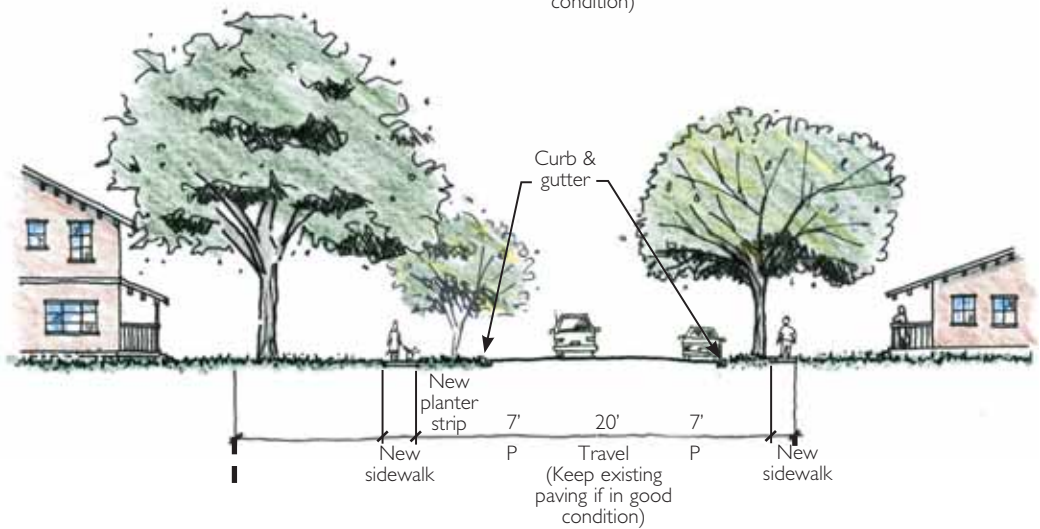


Figure 4-20. Alternative configuration for wide streets, showing sidewalks on both sides.



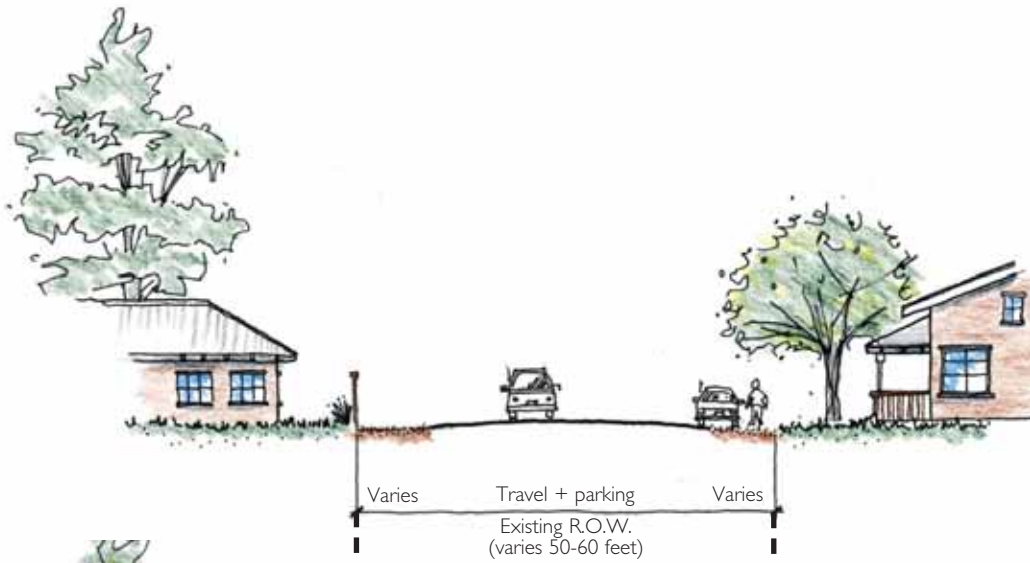


Figure 4-21. Typical neighborhood street with medium (45-60 feet) right-of-way.

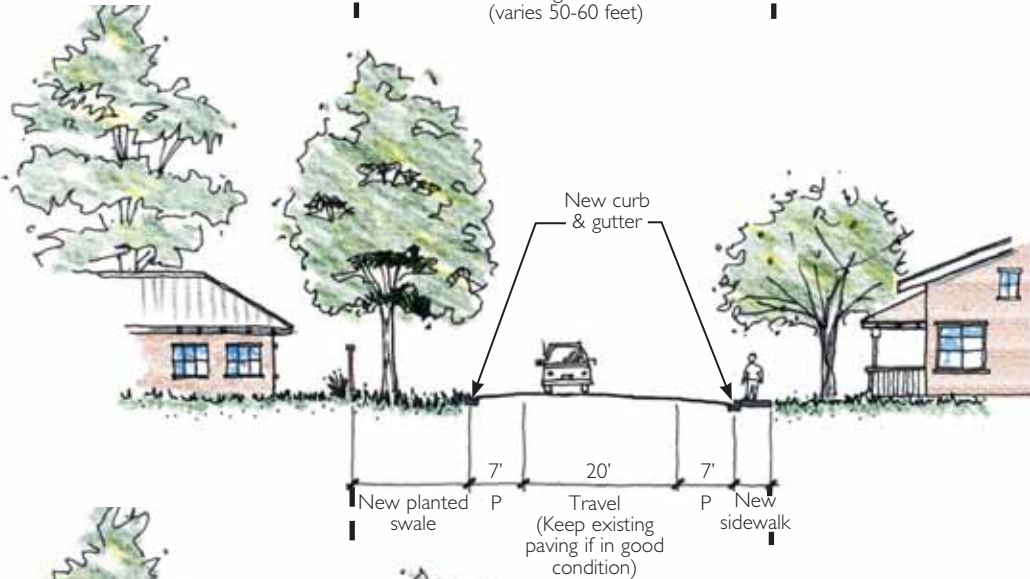


Figure 4-22. Recommended configuration for medium streets, showing a bioswale on one side and a sidewalk on the other.

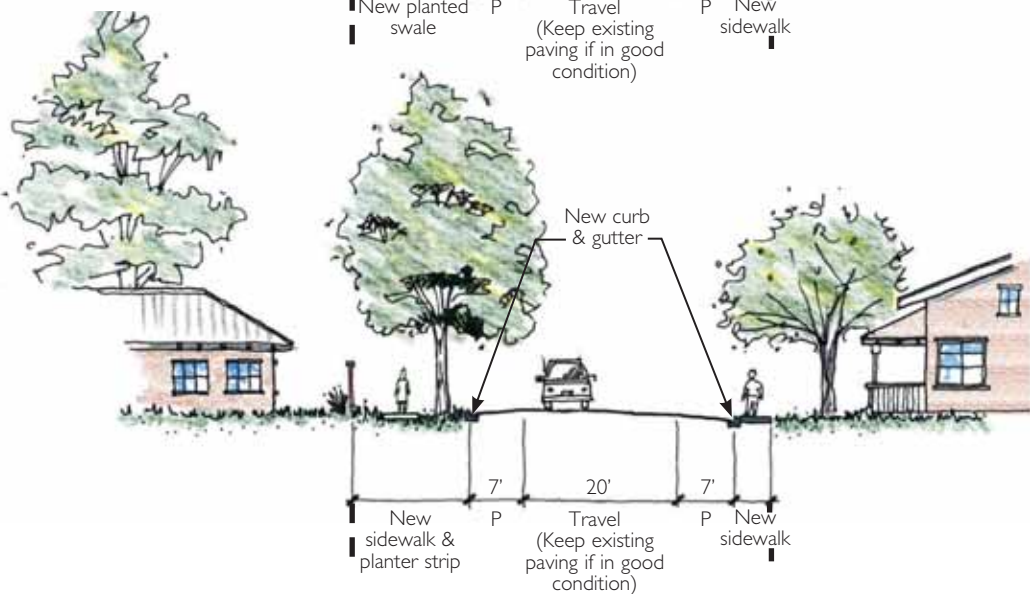


Figure 4-23. Alternative configuration for medium streets, showing sidewalks on both sides.

Figure 4-24. Typical neighborhood street with narrow (< 45 feet) right-of-way.

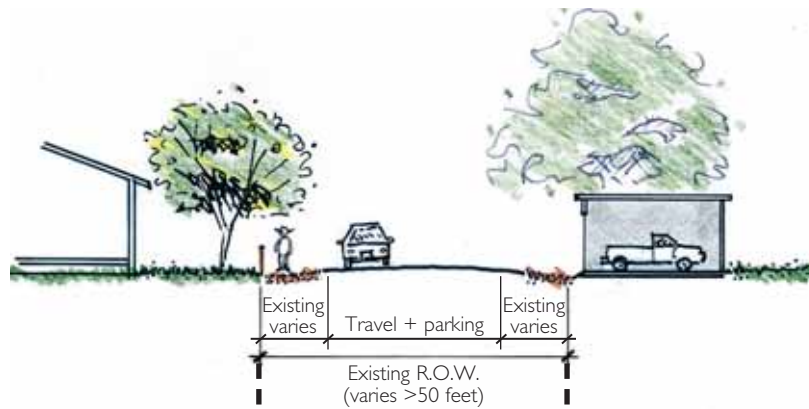
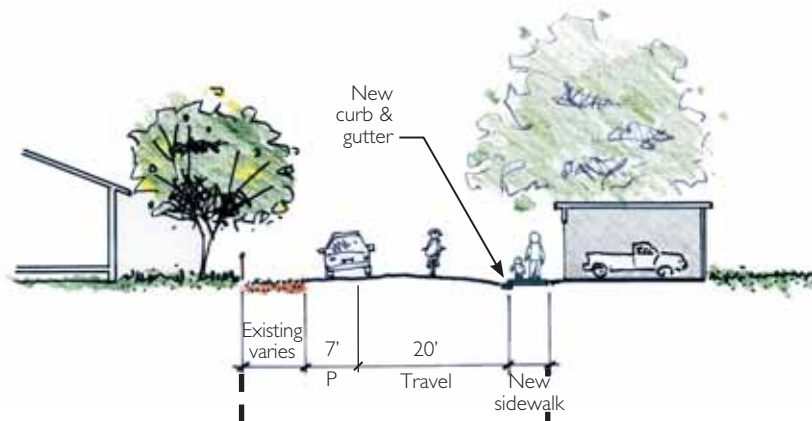


Figure 4-25. Recommended configuration for narrow streets, showing with a sidewalk on one side.



### *Storm Drainage*

Ponding water and poor storm drainage is a perennial problem in the Southwest Neighborhood residential areas, and many streets have no curbs and gutters. Most of the stormwater that falls on residential streets in the Plan Area empties into Little Chico Creek. As described in Section 4.A.4 above, collecting, filtering and allowing stormwater to infiltrate into the ground has many benefits, including reducing overload on the storm drain system. These measures can help to reduce overall flows into the creeks, but localized problems may continue without a program to address them. The Plan recommends that problem areas with persistent ponding be documented and a database created. In some cases storm drainage infrastructure may need to be replaced. It will be important to incorporate these drainage “hot spots” into the priority setting process for deciding which streets and sidewalk improvements should happen first.

### *Sewer Infrastructure*

Many residents of the Southwest Chico residential neighborhoods have concerns regarding the existing sanitary sewer system. To address this issue, a program should be implemented to report, record and map occurrences of sewer problems. Problems with City sewer lines will need to be prioritized and addressed through the capital improvement program. The City of Chico updated the Sanitary Sewer Master Plan in 2003. A copy of the plan is located at

[http://www.chico.ca.us/pubworks/Sanitary\\_Sewer\\_Master\\_Plan.pdf](http://www.chico.ca.us/pubworks/Sanitary_Sewer_Master_Plan.pdf).

Some of the concerns raised by residents are related to the condition of lateral lines leading to residences. Many of these laterals are original to

the age of the residence and therefore quite old. In some cases, settlement may have occurred which prevents efficient functioning. Repair of laterals are the responsibility of property owners. Reference information on potential improvements and funding sources and low income loan programs is found in the Implementation section of this Plan.

### *Street Lighting*

At night in the Southwest Neighborhood soft lights emanate from porches and through windows. It is a pleasant environment that contrasts with the many suburban neighborhoods, glaring with prolific streetlights. To respect the existing character, lamp posts as shown below, should be placed at corners of street intersections rather than overhead lights. Low lighting such as bollards is encouraged along sidewalks. When added, new street lighting should be pedestrian-oriented and below the tree canopy, and be directed to streets and sidewalks with full cut-off, recessed bulbs to avoid light spillage beyond the intended sidewalks and to reduce glare. Particular areas such as the Ivy and Chestnut Street corridors should be evaluated for street lighting needs.



## 2. Ensure a Clean and Safe Environment

Maintenance is an issue in the residential areas as homes in the neighborhood age, and absentee landlords do not address maintenance problems. Maintenance and trash cleanup can be improved with City programs such as low interest loans for façade improvements and dumpster days; a historic property signage program and neighborhood tours can encourage better stewardship. Chapter 7: Implementation discusses these programs further. The Proposed Design Guidelines in Appendix A include guidelines for crime prevention through environmental design.

In particular, the alleys present a maintenance challenge and are often blind-spots that facilitate illegal behavior. The conceptual site plan in Figure 4-26 illustrates improvements to reduce maintenance and safety concerns. Second in-law units that face windows onto the alleys, along with additional lighting and semi-transparent fencing will afford residents greater visibility on them. Trash enclosures would help to address maintenance concerns. Paving strips designed to work for both garbage trucks and automobiles would reduce rutting and still allow stormwater infiltration. Figure 4-27 illustrates how the combination of improvements might appear.

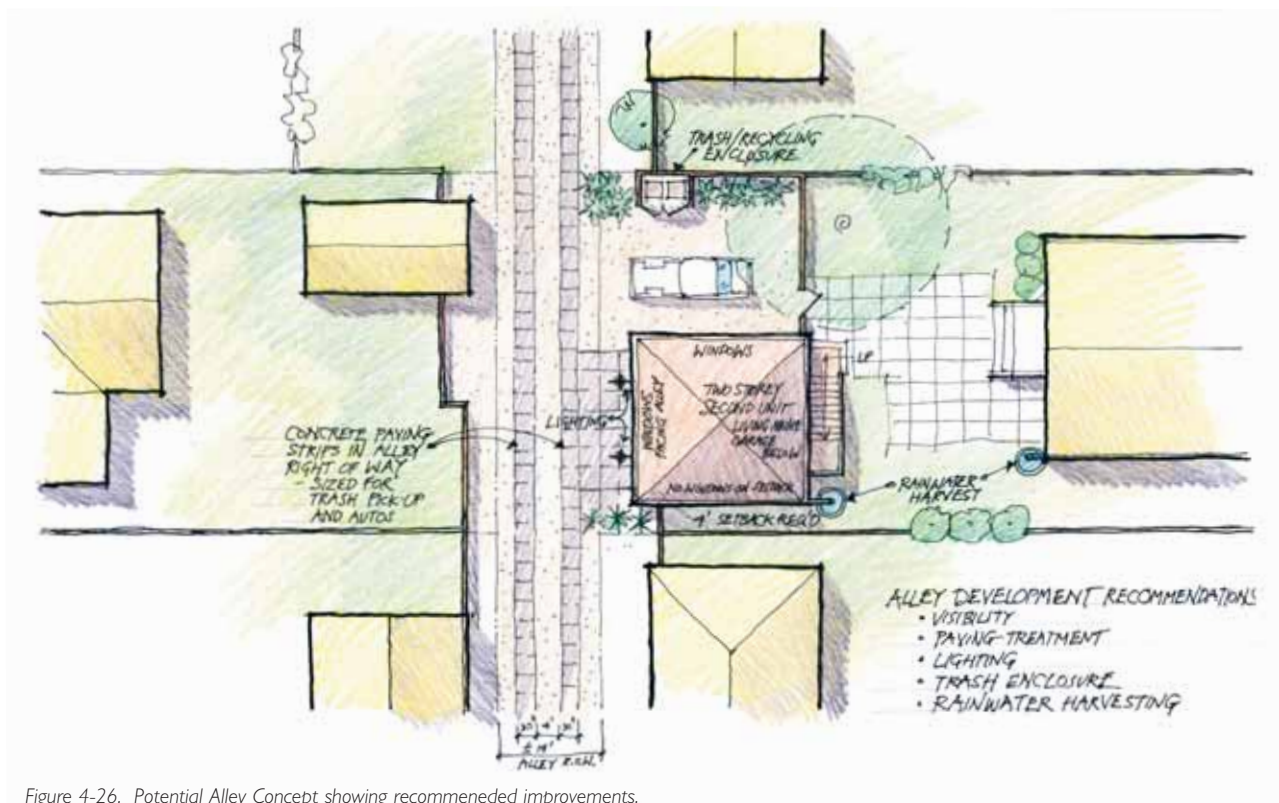


Figure 4-26. Potential Alley Concept showing recommended improvements.

The Plan recommends that one or two alleys be selected to serve as a pilot program to test these guidelines and improvements. Other communities, such as Imperial Beach, California, have established alley assessment districts that help provide funding for maintenance and improvement of alley areas.

Code enforcement efforts, which are typically reactive in nature, can be expanded to identify potential problems in a proactive manner. Modesto, California has a program called Volunteer Assisted Code Enforcement (VACE),

which trains volunteers in the identification of possible code violations. When a VACE volunteer notifies the City of a potential problem, the City Code Enforcement office sends a letter to the property owner indicating a problem may exist and requests corrective action. If the problem still exists upon re-inspection by the VACE volunteer, code enforcement staff can then make a field visit and contact the property owner. Many potential code enforcement issues can be resolved after a courtesy letter is sent and no further action is required. A program similar in nature should be considered.



Figure 4-27. Conceptual Illustration of an improved alley.

### 3. Respect Architectural Patterns

The architecture of the single family homes in the neighborhood offer uncommon diversity and character, while the basic lot development pattern remains consistent. The contrasts between consistent lots and individual expression promote identity and a neighborly environment. Important elements which need to be respected include:

- ◆ A variety of architectural styles and historical periods.
- ◆ A variety of sizes and scales of structures.
- ◆ Front doors and porches facing the street.
- ◆ Front gardens providing character.
- ◆ Alleys that enable friendly streets by removing garage frontages to the rear.

All of these design elements are addressed further in the Proposed Design Guidelines in Appendix A, which establish a basis for evaluating new residential development and upgrades. The Guidelines also define standards for new multi-family developments to ensure their form is contextual to the surrounding single family homes.

### 4. Provide a Diversity of Housing Types

Southwest Chico is an already diverse community both ethnically and socio-economically, in part because of the existing mix of single-family and multi-family homes types provides a range of options for many.

Currently, zoning in the residential areas is almost entirely R-1 (single-family), even where multi-family housing exists. Rebuilding these developments is allowed with significant procedural barriers. This means that there is little incentive to change or improve badly designed or substandard multi-family properties. The Plan recommends procedural changes that are intended to allow existing multi-family dwellings to be rebuilt or redeveloped (within the existing density) in order to preserve the diversity in housing types while allowing improvements to the building stock.

If new multi-family buildings are developed or existing buildings redeveloped, they should follow the Proposed Design Guidelines contained in Appendix A and look for cues and architectural details in the surrounding context; the larger estate homes that have been converted to multi-unit dwellings provide particularly good reference. Figure 4-28 illustrates a conceptual design for multi-family development on the vacant property at Normal Avenue and West 16<sup>th</sup> Street (this property is known to some in the community as the Northern Star Mills property). This site is ideal for a cooperative or co-housing type of development, where smaller, attached housing units and generous common space could provide housing options for younger families unable to afford single family residences.



**5. Preserve the Urban Forest**

Entering the Southwest Chico residential neighborhoods, it is clear they are unique by any standard. A most striking feature is the mature urban forest that dapples streets and gardens under its dense and diverse green canopy. The trees also regulate the climate in the neighborhood and lend comfort to the pedestrian environment. The larger trees especially should be assessed as part of the City’s Urban Forestry program and a prioritized replacement plan developed to address tree protection and management proactively.



Figure 4-28. Potential Concept for multi-family development at West 16<sup>th</sup> Street and Normal Avenue.

## 6. Create New Park Space

Much of the residential development in the Southwest Chico area occurred before park space was required to be reserved and developed. As a result, the residential areas lack park space within a convenient distance. The 1994 Chico General Plan, as amended, sets a standard of .3 mile service area radius around neighborhood parks. Only two developed park sites exist within the plan area, the Skatepark on the north side of Little Chico Creek on Humboldt Avenue at Flume Street, and the 1/3-acre Rotary Park, located on the northwest corner of Broadway and West 16<sup>th</sup> Street. In addition to improving the open space areas and passive recreation areas along the creeks, potential locations for active

park sites should be identified and an inventory of potential park sites should be developed. The community is interested in a network of parks distributed throughout the neighborhood.

As mentioned in the Creeks section of this chapter, one potential park location is the city-owned property at Hazel Street on the north bank of Little Chico Creek. Another potential location is the property located at the southwest corner of Normal Avenue and West 16<sup>th</sup> Street. In addition to being suitable for possible residential development as shown in Figure 4-28, the approximately 2.5-acre site would be an ideal location for a park. Figure 4-29 shows a potential concept which includes a small grass amphitheatre set in a larger



Figure 4-29. Potential Concept for a Community Park at West 16<sup>th</sup> Street and Normal Avenue.

theatre set in a larger lawn for active uses and local performances and gatherings, a community orchard and working garden to provide community members with the ability to grow and harvest their own produce and plants, a community building which could be used by arts groups active in the park and a passive recreation space for more contemplative uses. These are conceptual ideas, and any planning for new park space in the Southwest Neighborhood should include an ample community involvement process.

Figure 4-30 shows a map of the Southwest Neighborhood with .3 mile radii drawn around existing and potential parks. During the priority setting process, residents indicated a willingness to explore the possibility of forming an assessment district for the Southwest Neighborhood to help pay the costs of acquiring, developing and maintaining small, active park sites within the neighborhood.



Figure 4-30. Diagram showing 1/3-mile radius around existing and potential parks.



Figure 4-31. Wedge Concept.

#### D. THE WEDGE – PARK AVENUE/FAIR STREET AREA

The Wedge presents a largely-untapped resource within the neighborhood. Where the existing development pattern is rather low intensity with limited investment, more intense development in this area could offer additional jobs, opportunities for business owners, housing, and revenue to fund the City's capital improvement projects. New development also could help mitigate the presence of hazardous materials in the area.

##### *Goals and Objectives*

- ◆ Develop a range of uses in the Wedge that are appropriate to their immediate context.
- ◆ Provide additional local employment and housing opportunities.
- ◆ Facilitate private investment in the area.
- ◆ Improve local and regional circulation.

##### *Recommendations*

Figure 4-31 provides a conceptual illustrative of the entire Wedge, loosely partitioned into four zones with new conceptual street alignments, intensified development and recommended uses. Generally, live-work and residential uses should be concentrated in the northern section with more industrial uses focused toward the south. Retail uses should front East 20<sup>th</sup> Street and Park Avenue. Development of a retail-industrial district along Park Avenue and the Wedge's frontage on East 20<sup>th</sup> Street has been addressed in the Park Avenue discussion above. The following recommendations apply to other uses in the Wedge.

**I. Encourage Live/Work Housing**

With the exception of the Victor Industries site - where residential use is restricted pursuant to a land use covenant - the portion of the Wedge below East 20<sup>th</sup> Street could be a transitional zone, hosting alternative types of residential development that can coexist near existing light industrial and commercial uses. Two- or three-story live/work housing is an appropriate form of development, with alley loaded parking and ground floor office or workspaces with front doors oriented to the street. This area could be considered for high quality affordable housing projects to catalyze improvements in the area.

Another potentially suitable form of development is “flexhousing.” This is a form of development that allows the interior building spaces to evolve over time as needs change. The first floor layout is designed to accommodate several possible uses, including living, office, or retail space, which occupants can configure for their specific needs, while upper floors serve as residential space. Figure 4-32 shows a concept for mixed-use housing on Fair Street, with lofts above work spaces.



Figure 4-32. Live-work Housing Concept.

## 2. Encourage Commercial/Industrial Flex Condos

Another type of development to be considered in the Wedge is commercial/industrial/flex condo development that provides small shops and workspaces. Economic analyses indicates that these are in demand. This kind of development could upgrade the look of the Wedge over time while providing employment and other economic benefits to the Southwest Neighborhood. Figure 4-33 illustrates an industrial/flex condo development along Fair Street and fronting a new street.

## 3. Create New Connections

To facilitate connections to new development in the Wedge area, the 3,300-foot-long frontage along Park Avenue and Fair Street should be split into smaller parts by providing new streets as shown in the Wedge conceptual plan in Figure 4-31. This could also create new opportunities along the new street frontages and natural transitional spaces with mixed uses in the north and primarily retail-industrial uses to the south.



Figure 4-33. Potential Live-work Housing Concept for the Wedge.

**E. MEYERS INDUSTRIAL AREA**

The Meyers Industrial Area is a significant, long standing business district in Chico that is doing well. The development consists primarily of light industrial buildings, although the recent development of attorney and architects’ offices show the potential for multiple uses. Congestion, circulation and emergency access are concerns for the Meyers Industrial Area.

*Goals and Objectives*

- ◆ Improve circulation.
- ◆ Improve the appearance and landscape.

*Recommendations*

**I. Provide a Secondary Access**

Access into and out of the area is important for the continued economic vitality of the area and currently there is only one access point for the entire area, at Meyers Street. There are several options for providing a secondary access. The



Figure 4-34. Connecting Park Avenue and Commerce Court with a new street along the path of an existing alley to provide a second access to the Meyers Industrial Area.

most logical is to connect Commerce Court to an existing alley which extends to Park Avenue. The alley abuts other light industrial and commercial uses and connects to Park Avenue to an existing signalized intersection. Shown in Figure 4-34, the alley's approximately 34-foot right-of-way can provide bi-directional vehicular access with sidewalks. Further study should be undertaken to explore this option.

Other possible connections raised in the planning process were very controversial. Removing the existing barricade and extending Ivy to the Normal Avenue/West 22<sup>nd</sup> Street intersection as anticipated in the current General Plan was of particular concern to Barber Neighborhood residents, as it could encourage truck traffic to cut through the Barber Neighborhood. A possible bridge over Comanche Creek to connect Otterson Drive to Meyers Street was also discussed. It is a controversial option with a long history of study and debate. One point of view is that this option would have negative impacts on creek habitat and the Southwest Chico Neighborhood. Another point of view is that such a connection is critical to the long-term viability of the Meyers Industrial Area and the adjacent Hegan Lane business park just outside the Plan Area. The issues of circulation and connectivity are acknowledged to be broader than the scope of this neighborhood plan and the City's pending General Plan update will allow for comprehensive examination of these topics.

## **2. Attract High Quality Development**

Ensuring the long-term vitality and attractiveness of the district is a priority. It is notable that recent development in the Meyers Area includes space for architects and other professional offices. Live/work is allowed under the current general plan designation with a use permit and there already exists caretaker housing units in the Meyers Area. Additional opportunities for live/work development may be feasible as well. The diversity of uses indicates vitality within the area that should be underscored. New developments should pay attention to landscape and building detailing. Streets should be planted with street trees to help control heat gain. Natural storm-water management techniques such as swales and permeable surfaces should be considered for parking areas where feasible and the use of native vegetation for landscaping should be considered.

## **F. DIAMOND MATCH SITE**

The future of the Diamond Match site is a source of concern for many residents and presents a significant unknown to the Southwest Neighborhood. This 138-acre infill site has the potential to be developed at some point in the future. Current General Plan policies LU-I-52 through LU-I-56 address various aspects of development on the site including community involvement, preparation of a specific plan, infrastructure, design guidance, and parks & open space. Many of the concerns voiced during the Southwest Chico Neighborhood Plan process will be addressed through the application of existing General Plan policy as well as a required Specific Plan process and California Environmental Quality Act (CEQA) review, should any development pro-



posal for the Diamond Match site be submitted to the City. Issues such as traffic, infrastructure and hazardous contamination (summarized in Chapter 2, Section A.3) would be reviewed in detail as part of a public process at that time. If development is proposed for the Diamond Match site or any other development site with a history of contamination, the City would require a detailed review of remediation actions. This may require supplemental soils testing and a health risk assessment if determined necessary by the City or State of California.

Whatever form development takes on the Diamond Match site, there is strong desire among residents that development should reflect and enhance the historical connection between that site and the Barber Neighborhood. There is also a strong desire to preserve the historic resources remaining on the site.

Many different ideas about the types of development that would be desirable on the Diamond Match site were voiced during the planning process. In addition to the current property owner's conceptual plan for a mixed-use neighborhood called "Barber Yard," ideas expressed ranged widely from developing a solar farm to considering development of a light industrial park on the site. Fundamentally, the development of the site is a function of the goals and desires of the property owner, the land use regulations in effect, and overall economic and market conditions that motivate development of the site. The Southwest Chico Neighborhood Plan community process focused on recommendations which should guide how any future development should interface between the Diamond Match

site and the Barber Neighborhood. The concepts below were discussed extensively through the workshop process.

### *Goals and Objectives*

- ◆ Create multiple automobile, bicycle, and pedestrian linkages between new development in Diamond Match and the Barber Neighborhood.
- ◆ Provide common open space as an amenity for new and existing neighborhoods.
- ◆ Facilitate regional bicycle connections.
- ◆ For residential uses that are proposed, new development should adhere to the Proposed Design Guidelines in Appendix A and should relate to and enhance the diversity of architecture in the existing Barber Neighborhood, consistent with General Plan policy LU-I-55.
- ◆ If industrial or light industrial uses are proposed, avoid intermixing industrial and residential circulation networks.

### *Recommendations*

#### **I. Maximize Connections**

After much discussion with the community, multiple connections from the Diamond Match site into the Barber Neighborhood was seen as the best way to "share the load" of vehicle trips if new development occurs on the Diamond Match site. Figure 4-35 shows many connections from existing streets into the Diamond Match site, which acts to distribute the traffic generated by development on the site. The internal circulation within the Diamond Match site should be

designed to take advantage of these connections, and balance traffic loads between the connections.

It is important to the community, however, that if a development project on the Diamond Match site includes industrial land uses, the circulation network servicing the industrial areas be kept distinct from other circulation routes to prevent truck traffic from traveling through the existing residential neighborhoods. The concept diagram illustrates this idea by connecting a conceptual industrial area on the Diamond Match site to existing cul-de-sacs in the Meyers Industrial Area.

## 2. Calm Traffic

Currently the Barber Neighborhood enjoys a relatively tranquil roadway environment. With any future development of the Diamond Match site, consideration should be given to incorporating mitigation measures into the project and making physical improvements to streets connecting with the Diamond Match site to ensure vehicular movements are not excessive or unsafe. See Chapter 5: Circulation for more information.



Figure 4-35. Diamond Match Interface Conceptual Plan showing bike and vehicular circulation routes.

*This chapter discusses and illustrates circulation conditions and opportunities within and through the Plan Area. It includes recommendations to develop interconnected and safe pedestrian, bicycle and vehicular networks.*

**A. PEDESTRIAN CIRCULATION**

There are many gaps in the pedestrian network in the Plan Area. Getting from one Sub-Area to another can be challenging and crossing Park Avenue is difficult in places. Crossing 8<sup>th</sup> and 9<sup>th</sup> Streets between the Southwest Neighborhood and Downtown can be difficult as well. Following are recommendations for improvements to the pedestrian network.

**I. The Residential Sub-Area**

The grid of streets in the residential neighborhoods in the Southwest Neighborhood is a fine system for pedestrian connectivity. The block size is typically 300 feet, which is a comfortable length for pedestrians, and there is variety in landscape and architecture which creates interest. However, there is a lack of continuous sidewalks in these areas. Continuous sidewalks will greatly contribute to improved pedestrian connectivity.

To make efficient use of limited capital improvement funds, the Plan recommends that key pedestrian routes be prioritized for improvement. Other routes should be improved as funding allows. Key pedestrian routes were determined by using the following criteria:

- ◆ Ability to connect residents with downtown Chico and Chico State, Park Avenue or open spaces.
- ◆ Incorporation of existing continuous sidewalks where they exist.
- ◆ Proximity to residential areas--residents should be no more than one block from a key route.
- ◆ Alignment with existing crossings of Little Chico Creek or signalized crossings of Park Avenue.

Using these criteria, key pedestrian routes were identified. Figure 5-1 shows these key pedestrian routes.

Along these key routes, existing sidewalks in good or fair condition should be assessed for heaving and repaired appropriately. Streets without sidewalks or with sporadic sidewalks should be improved consistent with the concepts described in the Residential Neighborhoods recommendations in Chapter 4. In doing so, there may be pedestrian routes with a pathway on only one side of the street. For example, the southern portions of Normal Avenue, if narrowed and

improved as shown in Figure 4-19, would not have sidewalks on both sides, but instead have one continuous, generously buffered and tree-lined wide pathway. As a pedestrian circulator and amenity, this could have more value than simple gutter-curb-sidewalk configurations. This would also maintain the “not-suburban” character the neighborhood enjoys.

Streets not identified in Figure 5-1 as a key pedestrian route have lower priority for improvement and should be considered after the key pedestrian route network is improved to good condition.

## 2. Other Sub-Areas

Other pedestrian circulation improvements in the Plan Area are needed to ensure safe and direct pedestrian connections throughout the Southwest Neighborhood. The Plan recommends the following improvements:

- ◆ To cross Park Avenue at 9<sup>th</sup> Street a pedestrian must use the one existing crosswalk at the north side of 9<sup>th</sup> Street. The Plan recommends adding additional crosswalks on the south side of the intersection, touching the triangular park.

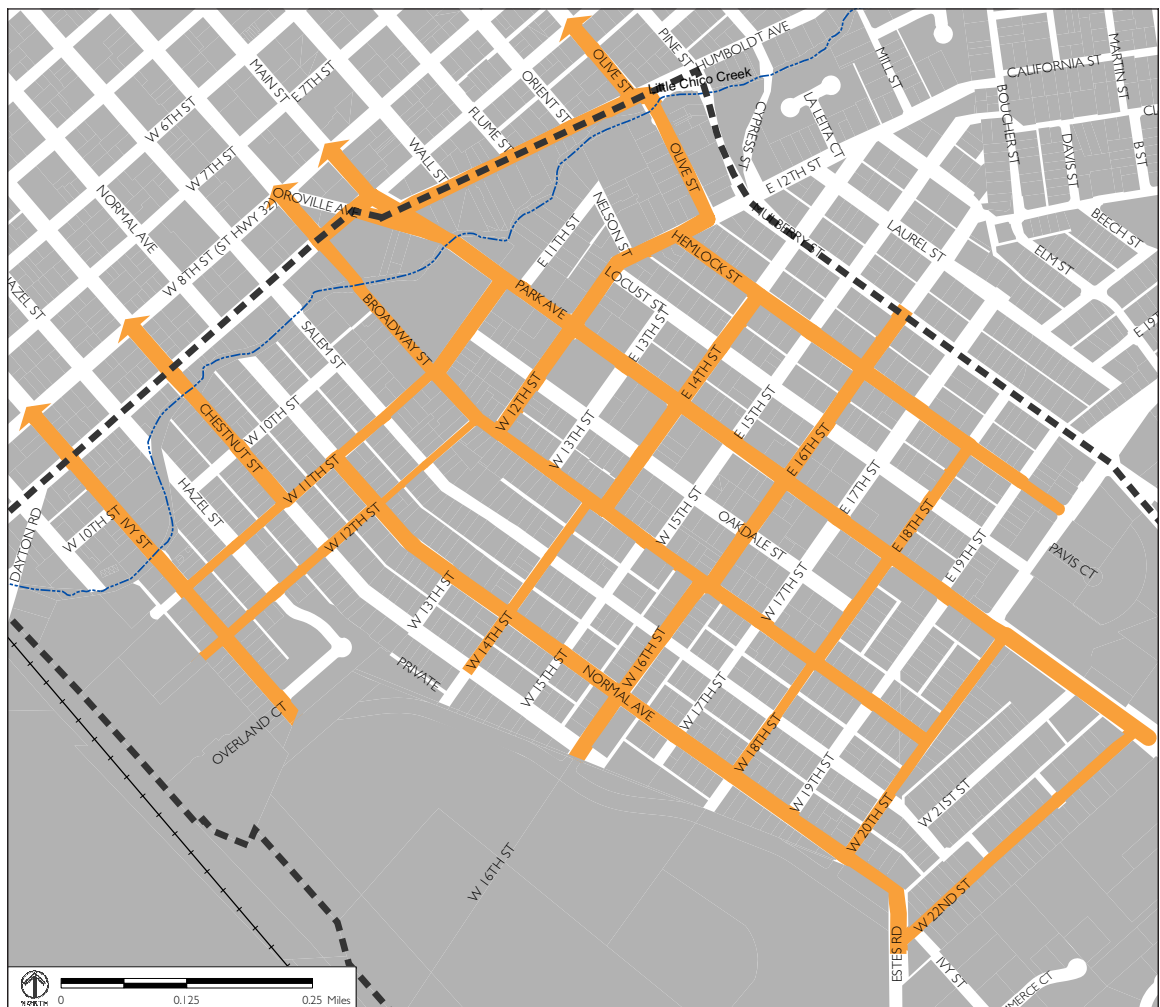


Figure 5-1. Key Pedestrian Routes in the Residential Areas.

- ◆ Due to the distance from the crosswalk across Park Avenue at 9<sup>th</sup> Street to the next crosswalk at 12<sup>th</sup> Street (almost 1,200 feet), the Plan recommends studying an additional crossing of Park Avenue in the vicinity of the Little Chico Creek bridge.
- ◆ The crossing width at Park Avenue makes it difficult for some pedestrians to make it across in the time allowed by the signal. Bulb-outs or curb extensions should be provided at crosswalks along Park Avenue to shorten the crossing distance. Where new development occurs along Park Avenue, this requirement should be part of any entitlement.
- ◆ As discussed in Chapter 4, the Plan proposes new streets in the Wedge Sub-Area between Park Avenue and Fair Street as well as a new connecting street into the Meyers Industrial Area. These new streets will be an important link in the pedestrian network and should be developed with generous sidewalks, landscaping and street trees.
- ◆ Improvements to Fair Street in the Wedge Sub-Area should include a continuous minimum 5-foot-wide sidewalk. Additional pedestrian crossings of Fair Street are recommended, including near the Four Winds Education Center at 2345 Fair Street.
- ◆ As discussed in Chapter 4, well-developed pedestrian pathways along Little Chico Creek will aid in surveillance of the creek and provide additional connections. To meet this goal, depending on the circumstances of each project, new development along the creek could be required to provide public creek access in any setback from Little Chico Creek. A conceptual section showing a public path is shown in Chapter 4, Figure 4-5. When improvements are made to the Chico Country Day School, space should be provided for a public path as well.
- ◆ Comanche Creek will be part of a regional bicycle/pedestrian network and new pathways in this network should be made safe and convenient for pedestrians. A pedestrian/bicycle bridge over Comanche Creek should be considered to connect the Meyers Sub-Area to this future network.
- ◆ The pedestrian network in any future Diamond Match development should be safe and convenient for new residents to connect to the improved key pedestrian routes within the existing residential neighborhood. This will encourage the new residents to walk to neighborhood services along Park Avenue rather than drive.

## B. BICYCLE CIRCULATION

The existing bicycle network in the Southwest neighborhood provides many routes to the north and a multi-use path to the south connecting to regional destinations. Many residents, students at Chico State and employees use bicycles to get to services, school or work. A Class I bike path (separated from vehicular traffic) exists along Park Avenue south of 20<sup>th</sup> Street, and Class II trails (bicycle lanes on streets) exist on East 20<sup>th</sup> Street leading east, Locust Street parallel to Park Avenue leading north and on Salem Street also leading north. Class III (signed bicycle routes) routes exist on Hemlock Street, Mulberry Street and Meyers Street as well as parts of Broadway Street, Salem Street, Chestnut Street and Ivy Street. Linking these bicycle facilities together and providing additional bicycle facilities is a strong desire of the community, as demonstrated by the strong turnout on the bicycle tour during the public workshop process.

### I. Bicycle Network Gaps

There are a few gaps in the bicycle network with room for incremental improvements. Additionally, the Wedge, the Diamond Match site, and the creeks present opportunities for new routes on the network. Figure 5-2 shows proposed improvements to the network. Specific changes include:

- ◆ **Chestnut Street.** The Chestnut Street right-of-way should be evaluated to determine if it can support Class II bike lanes, with the goal of connecting the Barber Neighborhood to an expansive system of bike lanes south of the University. Any new lane striping would also serve to calm traffic.
- ◆ **Salem and Ivy Bridges at Little Chico Creek.** These bridges are currently very narrow. Any major retrofit or improvement should better accommodate Class II bike lanes, signage and pedestrian facilities.
- ◆ **West 20<sup>th</sup> Street.** This street should be designated a Class III route to facilitate East-West bicycle circulation.
- ◆ **East 12<sup>th</sup> Street and Olive Street.** The Olive Street Bridge is an important connection to Downtown for cyclists. East 12<sup>th</sup> Street from Park Avenue and Olive Street itself should be designated as Class III routes.
- ◆ **Little Chico Creek.** A Class I multi-use pathway should be constructed along Little Chico Creek as described in Chapter 4. This pathway could generally run within the development setback on the north side of the creek upstream of the Park Avenue bridge, and on the south side of the creek downstream of the bridge. This is a long-term vision and will require public and private investment to achieve.
- ◆ **Comanche Creek.** A Class I multi-use pathway should be constructed along Comanche Creek as described in Chapter 4, and connect to the envisioned regional rails-to-trails system. A pedestrian and bicycle bridge over Comanche creek accessed from Ivy Street should be considered along with the path alignment.
- ◆ **New Wedge Streets.** The Wedge currently presents a major east-west barrier. Bicycle routes along new streets, as discussed in Chapter 4, should be designated Class III routes.

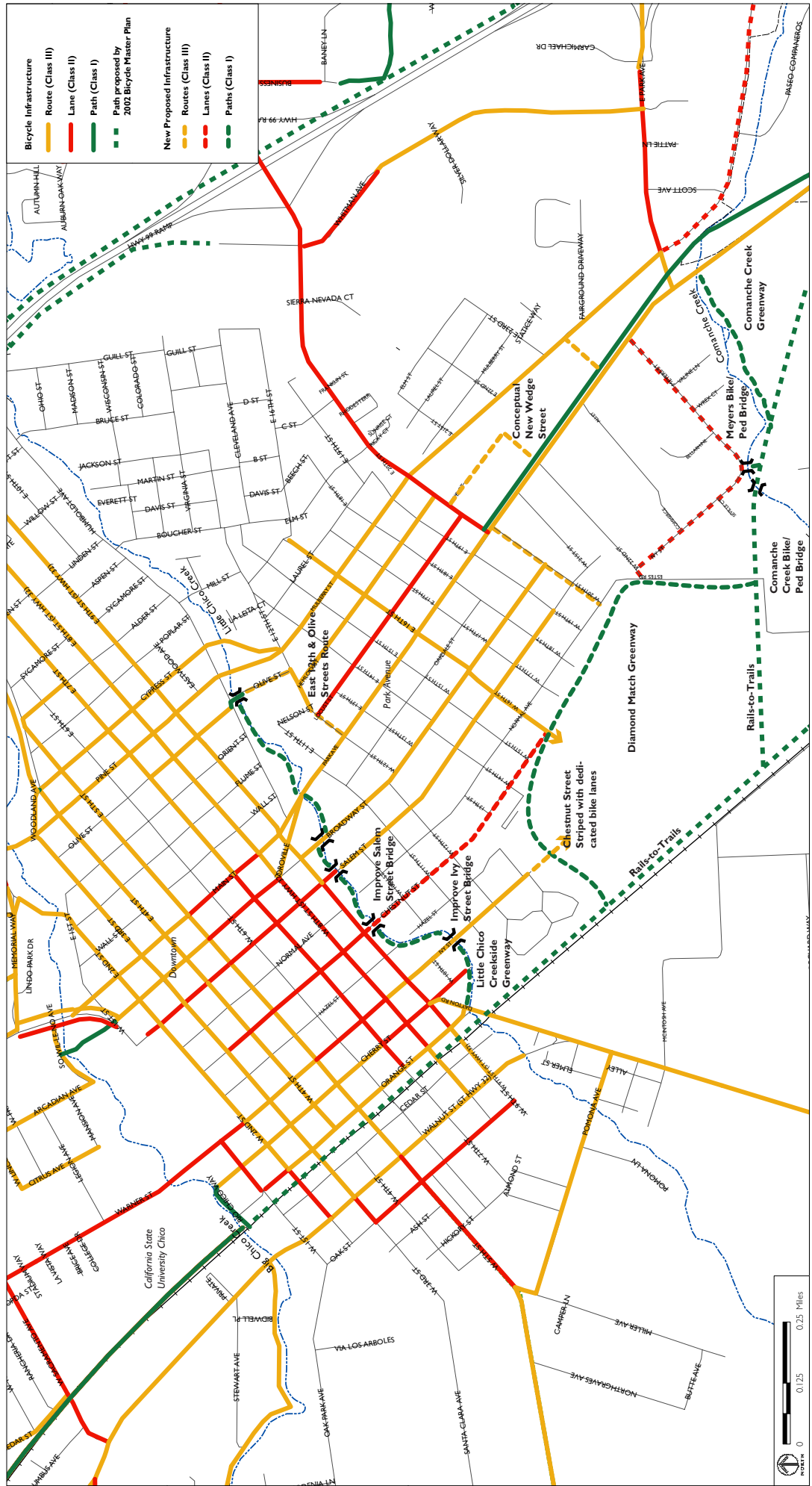


FIGURE 5-2  
BICYCLE INFRASTRUCTURE





- ◆ **Diamond Match Site.** A Class I bicycle path should be considered at the perimeter of new development in the Diamond Match site. Internal bicycle circulators can connect to this path. Class III routes from Ivy and West 16<sup>th</sup> Street can extend into Diamond Match. Upgrading Ivy Street with Class II bike lanes should be considered along with any plan for development on the Diamond Match site.

In addition to enhancing connections and resolving the gaps in the bicycle circulation network, improving deteriorated pavement conditions on the network should be a priority. The pavement on Salem Street, in particular, is in poor condition in some locations and is dangerous for cyclists, especially at night.

It should be noted that where diagonal parking is added on side-streets off of Park Avenue, consideration should be given to back-in diagonal parking as it can reduce automobile and bicycle conflicts. The strategy chosen should consider familiarity, and aim for consistency: the Park Avenue node at 16<sup>th</sup> Street should maintain a uniform strategy of back-in or head-in diagonal parking.

## 2. Bicycle Crossings at Intersections

Consideration should be given to improving bicycle crossings at several intersections in the Southwest Neighborhood. These include Ivy and Chestnut crossing 8<sup>th</sup> and 9<sup>th</sup> streets; Park Avenue at 20<sup>th</sup>, and Park Avenue at East Park. The conventional treatment of bicycles at intersections, whether controlled with stop signs or traffic signals, is to allow the bicyclists to share the travel lane with automobiles. This can lead to bicycle/automobile conflicts. The best solution to these conflicts is to provide striped bike lanes up to and through the intersection. This treatment divides the intersection into discrete lanes for vehicles and bicyclists and clearly defines the right-of-way. Where bicyclists need to turn left at an intersection, a left-turn bike lane may be provided adjacent to the vehicle lane to guide the bicyclists safely through the intersection.

Other potential intersection solutions to improve bicyclist safety and convenience include: advanced stop line and “bicycle box” at signalized intersections, which set the vehicle stop bar back from the intersection and leave a marked “box” for bicyclists in front of the vehicles; bicycle detection sensors that detect bicycles and trigger a change in traffic signals; push-buttons positioned for easy reach by bicyclists to trigger a green light; and separate “bicycle signals” to provide “bike-only” traffic signal phasing to separate conflicts between autos and bicyclists.

### C. VEHICULAR CIRCULATION

Vehicular circulation in the Plan area is generally good. There is a well connected network of streets, and new connections recommended in the Park Avenue/Fair Street Wedge and Meyers Industrial Area concepts will address problems specific to those areas.

The intersection at 9<sup>th</sup> Street/Humboldt Avenue/Park Avenue was brought up in public meetings as being congested at peak times. Some suggested that signal timing could be adjusted. After review by the consultant team it was determined that the intersection signalization and timing works appropriately given the conditions in the intersection. There are, however, two recommendations for improvements to this

intersection, as shown in Figure 5-3. First, a new pedestrian crosswalk crossing Oroville Avenue/Park Avenue on the south side of 9<sup>th</sup> Street could be added to aid pedestrian connectivity without impacting vehicle operations. Second, restriping and signage for vehicles turning left from Humboldt Avenue onto southbound Park Avenue could be provided to reduce conflicts while merging.

West 11<sup>th</sup> Street at Broadway Street next to Chico Country Day School can be improved to address congestion during school drop-off times. A conceptual redesign to increase safety and reduce congestion is illustrated in Figure 5-4. The concept features three travel lanes in the existing width of 11<sup>th</sup> Street, a pedestrian island on W. 11<sup>th</sup> Street, and two bulbouts within the



Figure 5-3. Potential pedestrian and circulation improvements to Park Avenue at 9<sup>th</sup> Street and Humboldt Avenue.

existing curb-to-curb dimension. It was also suggested during the planning process that flex-time scheduling at the school might help reduce congestion by spreading pick up and drop off times throughout the day.

The intersection at Park Avenue and 20<sup>th</sup> Street will become increasingly congested, especially if new development proceeds on the Diamond Match site. Currently, drivers on West 20<sup>th</sup> Street can not go straight or turn left onto Park Avenue. The Plan recommends exploring two options for this intersection. The first creates a dog-leg shape at East 20<sup>th</sup> Street to align with West 20<sup>th</sup> Street, as shown in Figure 5-5. The

second, shown in Figure 5-6, handles flow with a roundabout which could become a distinctive entry point and marker for the neighborhood.

Both options have advantages and disadvantages. Each would require the acquisition of right-of-way, but the roundabout would require more and likely be more expensive. Roundabouts often outperform traditional signalized intersection in terms of vehicular throughput and reduce accidents and fatalities dramatically. A dog-leg may be preferred by cyclists, as a roundabout could pose more risk to them. Cyclist and pedestrian safety would need to be considered in detail in the detailed design of both configurations.

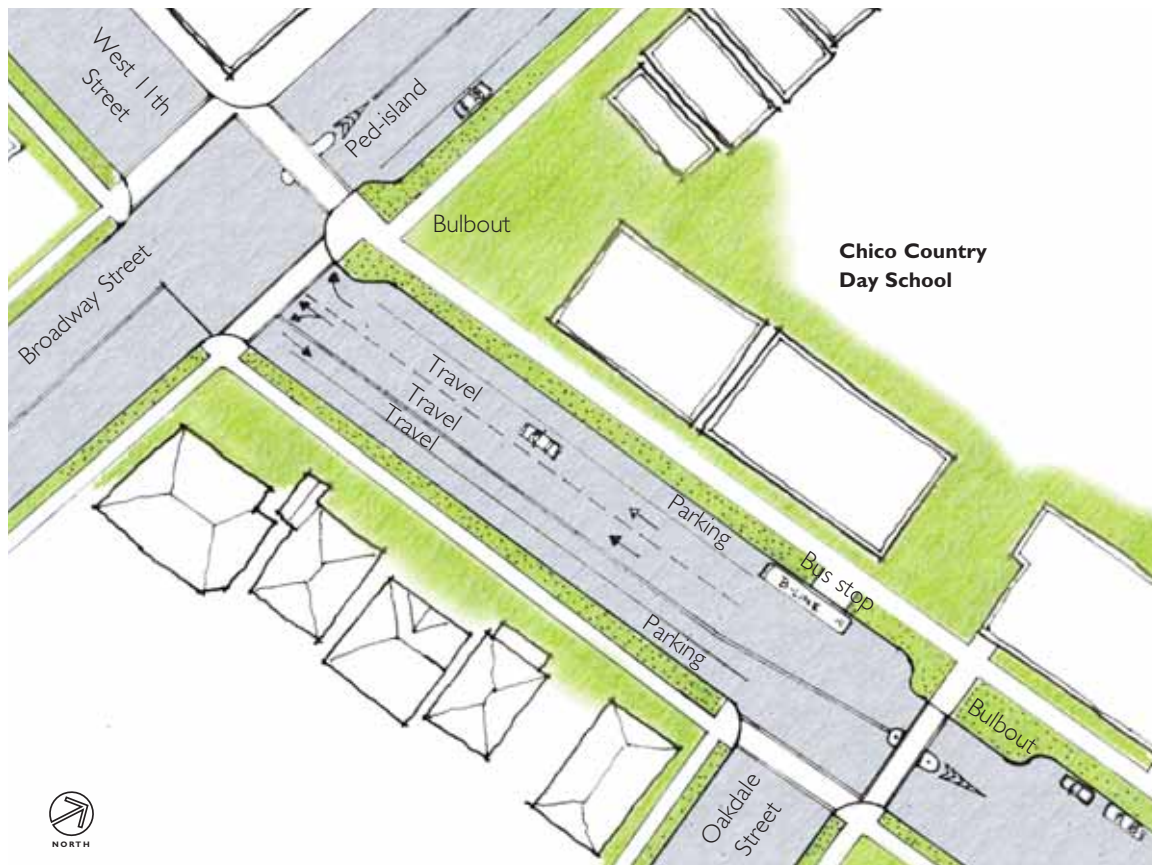


Figure 5-4. Potential circulation improvements in front of Chico Country Day School.



Figure 5-5. Potential "dog-leg" at Park Avenue and 20<sup>th</sup> Street.



Figure 5-6. Potential roundabout at Park Avenue and 20<sup>th</sup> Street.

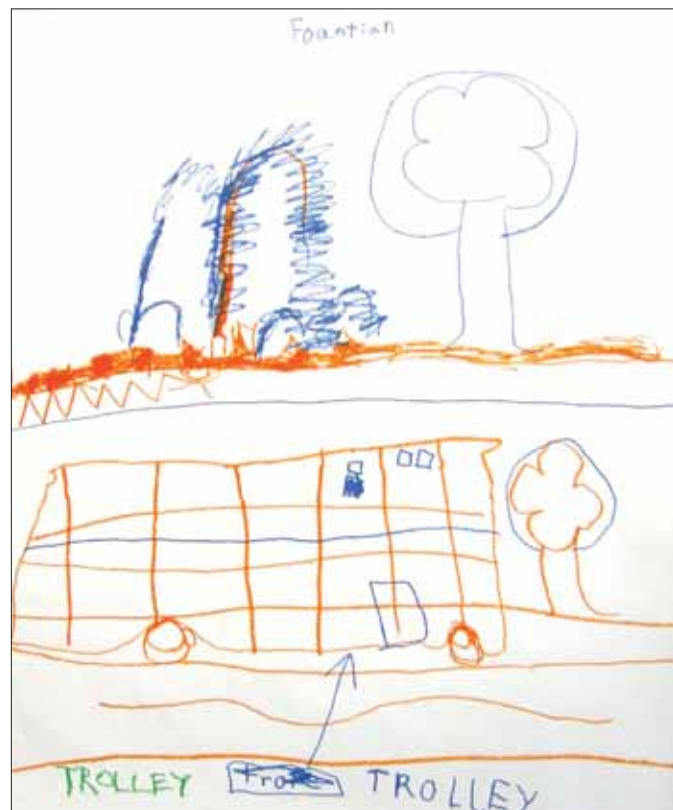
#### D. TRANSIT

The proposed Ring Transportation Corridor (which includes Park Avenue in the Southwest Neighborhood) serves a diverse range of land uses and densities as it traverses the city. Research indicates that land use structure, density, mix and design all influence the use of transit by a community, with land use density and compactness most influencing transit use and the type of transit technology. With the exception of the downtown Central Business District and Chico State University, most of the Ring Transportation Corridor serves relatively low density single family residential neighborhoods, commercial arterials, and industrial areas.

Given the overall density, it appears that the existing densities of the Ring Corridor are best served by fixed-route bus service that may vary in operating characteristics from conventional local bus service (with stops every tenth of a mile and 15-30 minute frequencies) to a Bus Rapid Transit (BRT) System of express service with stops every one to two miles and 5-10 minute frequencies or, most likely, a combination of the two types of systems. Streetcar, trolley, and light rail technologies typically require much higher densities to support both the capital expenditure and operating costs of fixed guideway systems – in the range of 10 housing units per acre combined with major employment centers of upwards of 40 million square feet of commercial uses. Therefore, fixed guideway systems would generally not be feasible to serve the Ring Transportation Corridor given the level of growth anticipated in Chico. Bus Rapid Transit, on the other hand, is generally feasible

with housing densities of 7-8 units per acre and about 8 million square feet of commercial development, similar to Chico today.

Considering a smaller scale of transit service, many in the community felt that reviving a trolley running from downtown Chico down Park Avenue to 20<sup>th</sup> Street would be a substantial benefit to the Southwest Chico neighborhood. The Avenues Neighborhood Plan has also identified the desire for a “short-loop transit system” connecting their neighborhood to Downtown. The City should consider examining these issues in parallel in a single study. If a roundabout were put in place at Park Avenue and 20<sup>th</sup> Street, it would be an ideal turn-around location.



A 6-year-old workshop-participant's vision for a plaza and trolley on Park Avenue.

### E. TRAFFIC CALMING

For the most part, the Southwest Neighborhood has a very well-connected network and this Plan recommends additional connections to make it even better. However, there are places in this network with histories of circulation conflict, such as vehicle speeding where pedestrians are present. This section identifies locations for specific improvements that could calm traffic and discusses the impact of potential development at the Diamond Match site.

### I. Traffic Calming in the Residential Neighborhoods

The following specific locations were identified as having speeding issues. Solutions are recommended as follows:

- ◆ Narrowing the intersection at East 12<sup>th</sup> Street and Locust Street would force traffic using 12<sup>th</sup> Street to slow at this curve. Figure 5-7 illustrates a possible configuration, where Nelson, Locust and E. 12<sup>th</sup> are narrowed with bulb-outs, and medians are added to require cars to make a careful approach to the intersection.



Figure 5-7. Potential Traffic Calming for the East 12<sup>th</sup>, Locust, and Nelson Streets Intersection.

- ◆ Ivy Street was identified as having particular problems with speeders. Adding bulb-outs or a mini-circle at West 12<sup>th</sup> Street and Ivy should be considered.
- ◆ Estes Road/Normal Avenue makes a sharp turn just north of West 22<sup>nd</sup> Street, and speeders rounding the corner have endangered residents nearby. A chicane or choker should be added to Normal Avenue south of 20<sup>th</sup> Street to slow southbound traffic as it approaches the sharp turn. For northbound traffic on Estes Road, two options are proposed. One is a system of islands

and narrowing shown in Figure 5-8, which would force northbound traffic through a diversion. The second is a half round-about that should be considered if Barber Yard is developed with a connection to West 22<sup>nd</sup> Street. This is illustrated in Figure 5-9.

There are also broader, Area-wide concerns that the wide residential streets in the Plan Area are conducive to speeding. There are many possible solutions to calming traffic on these streets. A menu of potential horizontal calming measures is shown in Figure 5-10.

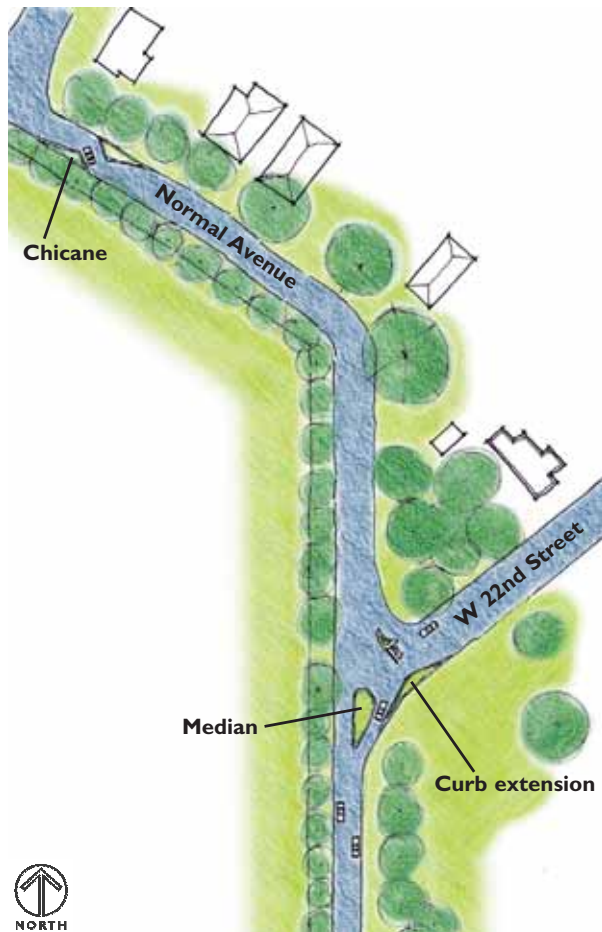


Figure 5-8. Potential Traffic Calming using medians and chicanes on Normal Avenue and West 22<sup>nd</sup> Street.



Figure 5-9. Potential Traffic Calming with a half roundabout on Normal Avenue and West 22<sup>nd</sup> Street.

Currently there are several locations with speed bumps or speed humps in the residential areas. The Chico Fire Department would prefer not to have vertical calming measures such as speed bumps as they can slow response time or damage emergency vehicles. These streets can be narrowed strategically to 20 feet wide with mid-block chokers, as shown in Figure 5-10-G. Where choker islands are installed, high-branching trees and other landscaping could be planted. The design of traffic calming devices should also consider incorporating methods for capturing or diverting stormwater runoff for detention or infiltration.

## **2. Potential Diamond Match Development**

Allowing for several access points from the Diamond Match site will improve the dispersion of traffic throughout the roadway network and reduce the impacts of traffic on individual streets. Conversely, limiting access would concentrate traffic volumes away from the site thereby increasing impacts to traffic and safety. Some considerations in evaluating the existing street network and proposed access to Diamond Match would include which streets might serve as the primary access routes, and what types of traffic calming measures might be implemented to ensure minimal impacts.

Auto traffic generated by the Diamond Match site to points outside the neighborhood will generally use the most direct routes toward the destinations. Depending on the direction of travel the most direct routes would include Ivy and Chestnut Streets to the north, and 16<sup>th</sup>, 18<sup>th</sup>, 20<sup>th</sup>, and 22<sup>nd</sup> Streets to Park Avenue and the freeway.

These streets might experience somewhat higher volumes than other access streets and may be candidates for some form of traffic calming. The conceptual access plan for Diamond Match includes direct connections between the commercial elements and the Meyers Industrial area. This connection would eliminate commercial traffic that would otherwise travel through the neighborhood.

Two types of traffic calming measures may be considered to address potential impacts of development of the Diamond Match site. The first is minimizing traffic volume on individual streets and the second is reducing the speed of autos. Traffic volume is best addressed by providing multiple connections, routes and access points to effectively disperse traffic. The SW Neighborhood Plan includes a menu of traffic calming measures that effectively reduce speed, all of which are centered on roadway narrowing through the use of curb bulbouts, center raised medians, traffic circles and striping solutions.





Figure 5-10-A. Mini-Circle.



Figure 5-10-B. Chicane.

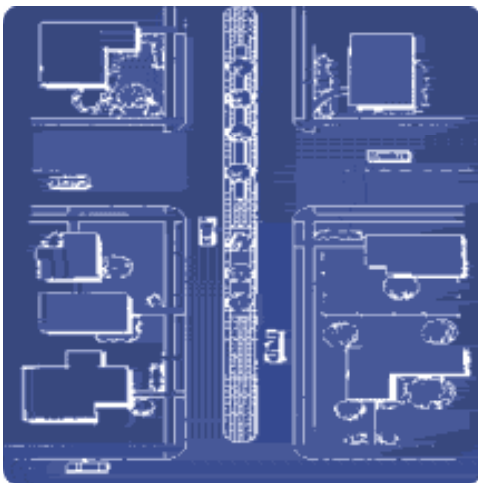


Figure 5-10-C. Median Barrier/Narrowing.

Figure 5-10-D. Slow Point.

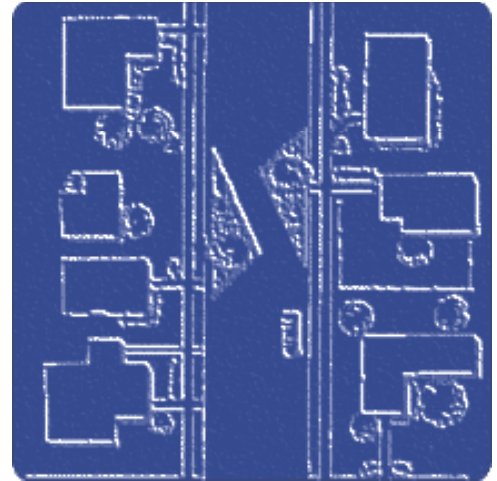


Figure 5-10-E. Intersection Bulbouts.

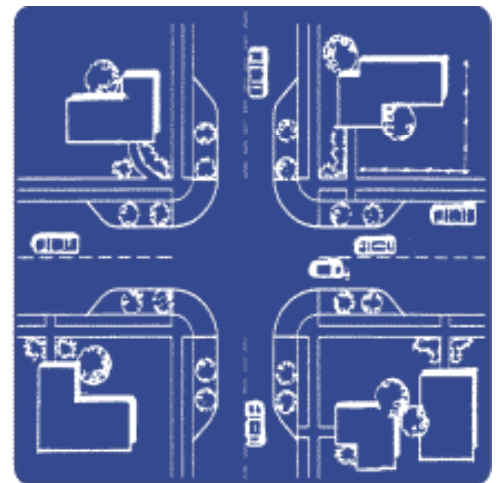


Figure 5-10-F. Roundabout.

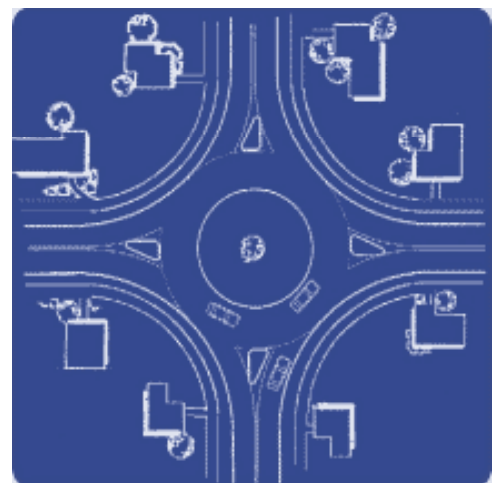




Figure 5-10-G. Choker.

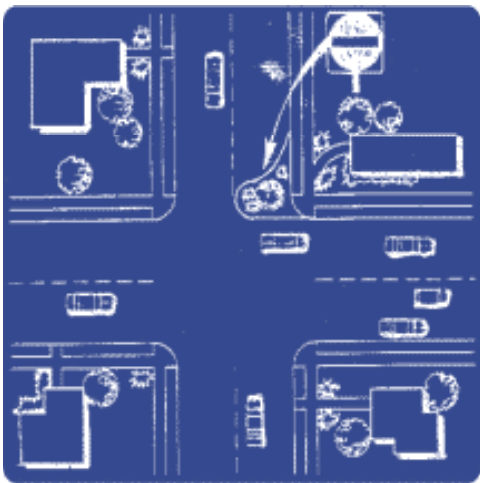


Figure 5-10-H. Half Closure.

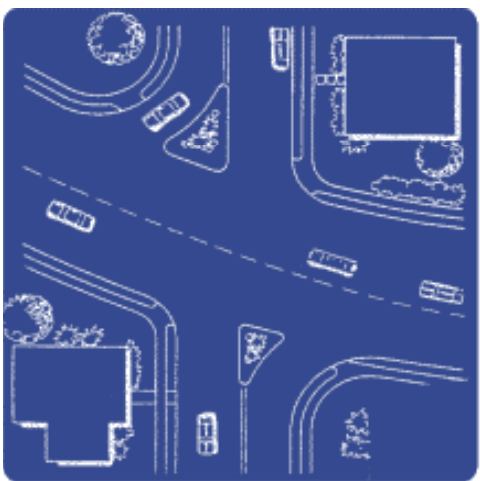


Figure 5-10-I. Turn Restrictions.

Figure 5-10-J. Speed Platform (Raised Intersection).

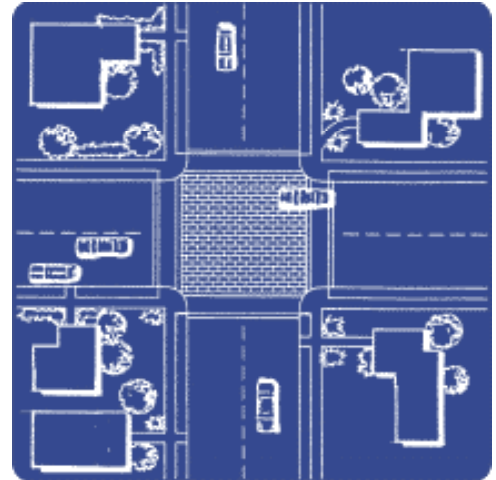
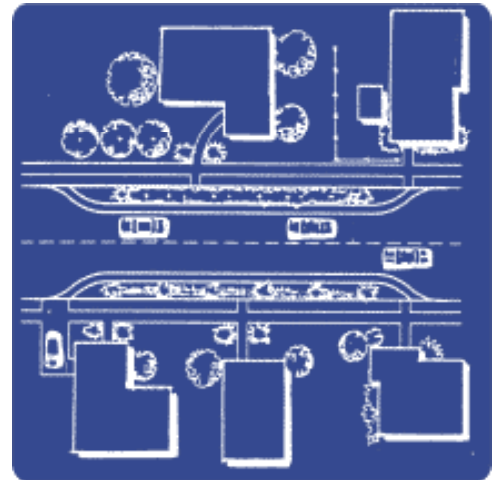


Figure 5-10-K. Neckdown.



## IMPLEMENTATION | 6

*This chapter summarizes the actions and tools necessary to successfully implement the recommendations described in this Neighborhood Plan.*

On the following pages, this Plan's recommendations are distilled into a matrix of area-wide recommendations, recommendations for each Sub-Area, recommendations for circulation, and finally issues identified in the planning process that should be considered in future studies. Each implementation subject presents a description and recommended actions, along with potential sponsors and partners. Implementing the recommendations of the plan will require the focused

efforts of many, including neighbors, business owners, property owners, developers, volunteer organizations and the City. Sponsors can lead the effort and collaborate to implement the recommendations.

Priorities to implement the recommendations will be determined through a series of collaborative community meetings facilitated by City staff.



*The Southwest Chico Neighborhood Plan Conceptual Illustrative.*

Figures 6-1 and 6-2, shown below, are discussed in the tables on the following pages.



Figure 6-1. Parcels identified for possible land use designation and zoning changes.

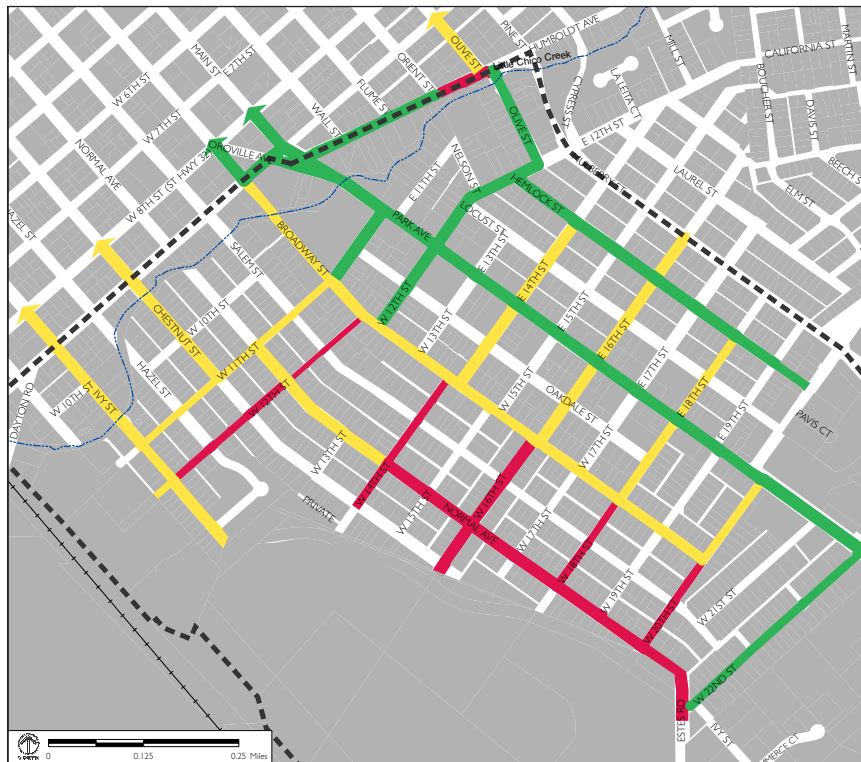


Figure 6-2. Key pedestrian route infrastructure conditions. Green streets have sidewalks in generally good condition and need little or no repair. Yellow streets need to be assessed for heaving and cracking. Red streets have nonexistent or missing sidewalk segments.

TABLE A AREA-WIDE RECOMMENDATIONS

Subject	Description	Actions	Sponsor & Partners	Cost	Timeframe	Priority*
<i>Economic Development</i>						
Land Assembly	The Redevelopment Agency can facilitate land assembly of smaller parcels to enable larger-scale projects and improve circulation. Efforts should focus on Park Avenue nodes and the northern portion of the Wedge in particular.	<ul style="list-style-type: none"> <li>◆ Assist developers with land assembly as opportunities arise.</li> </ul>	Redevelopment Agency, Planning Services Department, Local Property Owners	Variable	Long	III
Partnerships	Revitalization and redevelopment will require a partnership of government, property and business owners, neighborhood residents and lending institutions. Small-footprint grocery operators, for example, could be engaged along with property owners of suitable sites on Park Avenue. Efforts should be focused on strategic, limited areas to maximize potential synergy with adjacent properties and businesses.	<ul style="list-style-type: none"> <li>◆ Facilitate redevelopment efforts by connecting property owners, entrepreneurs, developers and lenders.</li> </ul>	Planning Services Department, Local Property and Business Owners, Developers	Administrative	Short	II
Development Incentives	Incentives to developers can catalyze desired development. The following incentives should be considered: <ol style="list-style-type: none"> <li>1. On Park Avenue and in the Wedge, provide additional parking credits. Parking requirements are frequently impediments to higher density development. On transit corridors, credits upwards of 30 percent of the normal requirement for new development may be appropriate when accounting for on-street parking. It may also be appropriate to consider allowing credit for new on-street parking where diagonal parking is provided.</li> <li>2. Publicly funded intersection improvements with comprehensively designed curb bulb-outs and highlighted pedestrian walkways can add value to private development efforts.</li> <li>3. The City could consider permitting buildings up to four-stories in height at nodes along Park Avenue.</li> </ol>	<ul style="list-style-type: none"> <li>◆ Provide parking credits for quality projects on Park Avenue and in the Wedge.</li> <li>◆ Coordinate City investment in intersection improvements to support/encourage redevelopment.</li> <li>◆ Offer height incentives on Park Avenue.</li> </ul>	Planning Services Department, Capital Project Services Department	Administrative	Short	II
Voluntary Business Associations	Creation of a voluntary business association to work with the City and Barber Neighborhood Association will facilitate decision making and execution.	<ul style="list-style-type: none"> <li>◆ Form a voluntary business association district.</li> </ul>	Local Business and Property Owners	None	Short	II
Business Relocation Program	Maintaining jobs and supporting existing businesses is a priority for the City. Transitioning Park Avenue to support increased transit, retail and housing is also a goal for the City. A business relocation assistance program could help businesses that do not need Park Avenue's exposure move to a more suitable location, such as the Meyers Industrial Area, and make way for new development on Park Avenue.	<ul style="list-style-type: none"> <li>◆ Develop a business relocation assistance program.</li> </ul>	Economic Development Manager, Local Business and Property Owners	Variable	Medium	III
<i>Urban Form</i>						
Design Guidelines	Design guidelines provide a tool to ensure new development fosters a high quality environment and takes into consideration the unique qualities of the Southwest Chico Neighborhood. Additional staff resources to ensure proper review may be required.	<ul style="list-style-type: none"> <li>◆ Adopt Design Guidelines.</li> <li>◆ Ensure adequate staffing.</li> </ul>	Planning Services Department, Local Property Owners	Administrative	Short	II

\* I = Highest Priority, II = Medium Priority, III = Lower Priority

Cost note: Administrative = primarily staff costs





TABLE B CREEK CORRIDORS RECOMMENDATIONS

Subject	Description	Actions	Sponsor & Partners	Cost	Timeframe	Priority
<i>Health and Safety</i>						
Outreach, Education & Clean Up	Encourage and expand opportunities for ongoing education programs and activities, including annual creek clean-ups. Increased community interest and participation in the protection and enhancement of Little Chico Creek and Comanche Creek will greatly enhance ongoing support and stewardship. Local organizations are actively involved in urban creek restoration within Chico and neighbors along both creeks monitor the creek conditions and regularly inform the City of undesirable activities, such as dumping. CSUC organizations sponsor "Scour and Devour," a city-wide cleanup day which also includes the creeks. The city can continue to collaborate with interested citizens and groups by supporting their public outreach and education efforts. Interested citizens and groups should develop programs and engage others to assist. Business owners can provide lighting and lead cleanup vegetation management near their businesses.	<ul style="list-style-type: none"> <li>◆ Continue to support "Scour and Devour".</li> <li>◆ Support activities of Streamrinders and other creek advocacy groups.</li> <li>◆ Engage staff, students, and parents of Chico Country Day School in creek cleanup efforts.</li> </ul>	Barber Neighborhood Association, Housing and Neighborhood Services, General Services Department - Parks Division, CSUC, Streamrinders, Business Owners	Administrative	Ongoing	I
<i>Creeks and Open Space</i>						
Open Space Acquisition	The City has an existing policy which prioritizes the purchase of land bordering creeks. This policy should be enacted whenever possible to provide open space along creeks.	<ul style="list-style-type: none"> <li>◆ Review and clarify City Budget Policy G2(b).</li> </ul>		High Cost	Long	I
Creek Overlooks	Consider providing seating areas, benches and/or overlooks and develop a signage program along the banks of Little Chico Creek at Normal Ave, Cherry Street and Hazel Street. At Comanche Creek consider overlooks at Wrex Court, Valine Lane and the corner of Meyers Street and Ivy Street.	<ul style="list-style-type: none"> <li>◆ Clear vegetation to improve visual access where creek overlooks are identified.</li> <li>◆ Replace with native riparian species.</li> <li>◆ Provide benches and signage to educate and allow users to enjoy the creek.</li> <li>◆ Complete feasibility study.</li> </ul>	Barber Neighborhood Association, Creek Advocacy groups, General Services Department - Parks Division	Moderate Cost	Medium	III
Hazel Street Park	The City-owned parcels at Hazel Street on the north bank of Little Chico Creek at Hazel Street could be developed as a neighborhood pocket park. This should be studied further.	<ul style="list-style-type: none"> <li>◆ Complete feasibility study.</li> </ul>	Planning Services Department, General Services Department - Parks Division	Moderate Cost	Medium	II
Little Chico Creek Trail	Creating a trail along Little Chico Creek is an ambitious and long-term vision, and must be completed incrementally. Ultimately, the City should secure a 25-foot creekside greenway along top of bank along Little Chico Creek and build segments of the trail as land and opportunities arise.	<ul style="list-style-type: none"> <li>◆ Secure a 25-foot creekside greenway along top of bank along Little Chico Creek</li> <li>◆ Build incremental segments of the trail.</li> </ul>	Planning Services Department, General Services Department - Parks Division, Chico Country Day School, Property Owners, Barber Neighborhood Association	High Cost	Long	II
Comanche Creek Trail	A pathway could be provided south of Comanche creek on City-owned property, which could make connections to envisioned trails-to-trails routes on the former railroad right-of-ways. A pedestrian and bicycle bridge accessible from the Meyers Industrial Area at Ivy Street could connect to this pathway.	<ul style="list-style-type: none"> <li>◆ Study a pathway south of Comanche creek.</li> </ul>	Planning Services Department, General Services Department - Parks Division	Moderate Cost	Long	I
<i>Land Use</i>						
Orient Development toward the Creeks	New development and renovations of existing buildings should orient some windows, doors and usable outdoor space toward the creeks to increase watchfulness and surveillance.	<ul style="list-style-type: none"> <li>◆ Adopt provisions in the Design Guidelines which encourage creekside orientation.</li> </ul>	Planning Services Department, Local Property Owners	Administrative	Short	II
<i>Resources</i>						
Green Building Incentives Program	The City should explore potential incentives to implement creative stormwater measures. The increased cost to a developer for installing pervious paving, greenroofs and cisterns, for example, can be greatly offset with a green building incentive program.	<ul style="list-style-type: none"> <li>◆ Study incentives to implement creative stormwater measures.</li> </ul>	Planning Services Department	Administrative	Short	II

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TABLE C PARK AVENUE RECOMMENDATIONS

Subject	Description	Actions	Sponsor & Partners	Cost	Timeframe	Priority
<i>Economic Development</i>						
Commercial Rehabilitation	<p>Actively promote the Façade Improvement Program to business and commercial property owners in the Park Avenue Corridor. All expenses related to the following items should be eligible under this program:</p> <ul style="list-style-type: none"> <li>◆ Renovation of exterior/interior.</li> <li>◆ Landscaping and lighting.</li> <li>◆ Bringing the structure into compliance with building codes.</li> <li>◆ Earthquake retrofitting the structure.</li> </ul> <p>The program should also fund improvements to signage and removal of outdated signs. In addition to actual construction costs, eligible expenses should also include permit fees related to the project and loan preparation costs (including credit checks, preliminary title reports, and title insurance). Construction management assistance should also be available.</p>	<ul style="list-style-type: none"> <li>◆ Promote Façade Improvement Program.</li> <li>◆ Offer construction management assistance.</li> </ul>	Economic Development Manager, Local Businesses and Property Owners	Moderate	Short	I
Lower Park Avenue	Promote a retail-industrial district along lower Park Avenue to encourage expansion and relocation of manufacturing businesses that can benefit from a retail presence along Park Avenue.		Economic Development Manager, Local Businesses and Property Owners	Low Cost	Short	II
<i>Land Use</i>						
Promote Additional Uses in the Upper Park Avenue Corridor	Mixing residential or office uses with commercial uses in buildings facing Park Avenue will provide a more pedestrian friendly environment and should be encouraged. Another use to be explored is a small-format grocery store, particularly if the Diamond Match site and other sites are developed with new residences.	<ul style="list-style-type: none"> <li>◆ Adopt provisions in the Design Guidelines which encourage Mixed-use.</li> <li>◆ Explore the possibility of a new format grocery store along Park Avenue</li> </ul>	Planning Services Department Economic Development Manager, Local Property Owners	Administrative	Medium	II
Encourage Development of Nodes along Park Avenue	Focusing development along Park Avenue into Nodes as discussed in Chapter 4 will concentrate energy and investment into economically sustainable places. One way to achieve this could be to modify the zoning code to allow four story buildings up to 50 feet in height at these Nodes. Current zoning allows buildings up to 45 feet, which constrains high quality four story building types. Building heights should step down to two and three stories on Locust Street and Oakdale Street, on either side of Park Avenue. Buildings fronting on Fair Street in the Wedge could be allowed to be three stories in height.	<ul style="list-style-type: none"> <li>◆ Consider modifications to existing zoning code to allow buildings up to 50 feet at Park Avenue Nodes. Such development would need to adhere to Proposed Design Guidelines in Appendix A.</li> </ul>	Planning Services Department	Administrative	Medium	III
East 20 <sup>th</sup> Street	The land use designation for parcels on both the north and south sides of 20 <sup>th</sup> Street between Park Avenue and Fair Street could be considered for change to a Community Commercial designation, with a High Density Residential overlay, similar to the designation on Park Avenue north of 20 <sup>th</sup> Street. A key parcel that may benefit from this change is the Butte County corporation yard site located at the northeast corner of 20 <sup>th</sup> and Locust streets. Another key site is the parcel at the southeast corner of 20 <sup>th</sup> Street and Park Avenue. This parcel is strategically located between the City's recent improvements on Park Avenue to the south, the Park Avenue corridor to the north and the 20 <sup>th</sup> Street connection to Highway 99 and other parts of Chico east of the Plan Area. These parcels are identified in Figure 7-1.	<ul style="list-style-type: none"> <li>◆ Consider redesignating strategic parcels between Fair Street and Park Avenue to Community Commercial with a High Density overlay.</li> </ul>	Planning Services Department	Administrative	General Plan Update	II
<i>Public Improvements</i>						
Gateway over Little Chico Creek	Design and construct a gateway element that announces the Downtown and Southwest Neighborhood. Involve local artists and designers to evoke the unique character of the neighborhood. The Park Avenue edge of Chico Country Day School should contribute to this gateway, and should be considered carefully in any future renovation of the School.	<ul style="list-style-type: none"> <li>◆ Engage the local arts community.</li> <li>◆ Design and construct gateway features.</li> <li>◆ Incorporate a highly visible pedestrian crossing if possible.</li> <li>◆ Coordinate planned improvements with Chico Country Day School.</li> </ul>	Capital Project Services Department, Local Arts Community, Chico Country Day School	Moderate Cost	Medium	III

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TABLE 1 THE RESIDENTIAL NEIGHBORHOODS RECOMMENDATIONS

Subject	Description	Action	Sponsor & Partners	Cost	Timeframe	Priority
<i>Infrastructure</i>						
Street and Sidewalk Improvements	Street and sidewalk improvements are needed in many locations in the residential neighborhoods. Priorities for improvement should be set as described in the Circulation Section in Table H. As the Plan discusses, street improvements should include better pedestrian connections, better bicycle connections, methods to calm traffic, stormwater management and street lighting.	<ul style="list-style-type: none"> <li>◆ Prioritize connections, select pilot projects, develop cost estimates</li> </ul>	Barber Neighborhood Association, City of Chico Public Works, Capital Project Services Department	Moderate Cost	Medium	I
Local Storm Drainage deficiencies	The community should collect data on problem spots of ponding and lack of drainage. From this data an action list should be developed and prioritized.	<ul style="list-style-type: none"> <li>◆ Create a neighborhood community database</li> </ul>	Barber Neighborhood Association, Capital Project Services Department	Administrative	Short	I
Sewer Improvements	As above, the community should collect data on problem areas of sewage backups and slow flows. An action list should be developed and prioritized. Where private laterals are concerned, funding resources for improvements should be coordinated with property owners.	<ul style="list-style-type: none"> <li>◆ Create a neighborhood community database</li> <li>◆ Develop resource list for funding of improvements</li> </ul>	Barber Neighborhood Association, Capital Project Services Department	Administrative	Short	II
Street Lighting	Street lighting should be included with Street and Sidewalk Improvement projects. Ensure that lighting is appropriate to the character of the neighborhood. For new development, Design Guidelines concerning lighting should be followed.	<ul style="list-style-type: none"> <li>◆ Create street lighting standards for the Southwest Neighborhood.</li> <li>◆ Adopt provisions in the Design Guidelines</li> </ul>	Capital Project Services Department, Planning Services Department, Property Owners, Developers	Moderate Cost	Medium	II
<i>Health and Safety</i>						
Annual/Semi-annual Dumpster Days	"Drop & Dash," a City dumpster-day program coordinated with CSUC, should be continued and supported. The dates should be publicized and follow shortly after Chico State graduation or end-of-term. Dumpsters should be monitored by staff or volunteers to facilitate the process and to prevent illegal dumping. Consider expanding the program to include an end-of-fall-term drop-off day.	<ul style="list-style-type: none"> <li>◆ Support current "Drop &amp; Dash" program.</li> <li>◆ Consider expanding program to include an end-of-fall-term drop-off day(s).</li> <li>◆ Locate drop-off locations in the neighborhood.</li> </ul>	Housing and Neighborhood Services Department, Barber Neighborhood Association, CSUC	Low Cost	Ongoing	I
<i>Land Use</i>						
Non-conforming Multi-family Reconstruction	Owners of multi-family residential buildings in the Southwest Neighborhood should be able to reconstruct or redevelop (at the same density) with out procedural roadblocks. The municipal code states that "For a continuous period of one year, all rights to continue the nonconforming use shall terminate regardless of the owner's intention to recommence the use (Section 19.08.040)." This timeframe may prevent property owners from renovating or reconstructing multi-family structures to a higher quality. The City could consider giving the Planning Director the authority to extend this period by an additional year for rehabilitation or reconstruction of multi-family buildings occupied by nonconforming uses. The City could also encourage rehabilitation by allowing the Director to approve reconstruction permits through a mechanism such as a minor use or administrative permit.	<ul style="list-style-type: none"> <li>◆ Empower the Planning Director to extend the non-conforming use status for one year.</li> <li>◆ Empower the Planning Director to approve reconstruction of non-conforming uses through a minor use or administrative permit.</li> </ul>	Planning Services Department, Planning Commission, City Council	Administrative	Short	III
Vacant Parcels at 16th & Normal	Currently designated Mixed-Use Neighborhood Core which requires all residential units to be above the ground floor, these parcels at the southwest corner of Normal Avenue and 16th Street should be redesignated to allow for residential on the ground floor as well as upper stories. Any new designation should continue to allow retail at the ground floor. These parcels are identified in Figures 4-28 and 4-29. Alternatively, this site could become a new neighborhood park – see Parks and Open Space at the end of this Table for information.	<ul style="list-style-type: none"> <li>◆ Consider redesignating to allow ground-floor residential and retail.</li> </ul>	Planning Services Department	Administrative	General Plan Update	III
<i>Nuisances</i>						
Party Noise	The City should explore adopting a Party Ordinance designed to address problematic party noise. Other university towns such as Santa Cruz have adopted similar measures.	<ul style="list-style-type: none"> <li>◆ Consider adopting a Party Noise Ordinance.</li> </ul>	Housing and Neighborhood Services Department	Administrative	Short	III
Code Enforcement	The Code Enforcement Unit of the Housing and Neighborhood Services Department is responsible for the preservation of neighborhoods through the enforcement of City codes and ordinances. Proactive code enforcement efforts, including a Volunteers Assisting Code Enforcement (VACE) program, should be stepped up in the City.	<ul style="list-style-type: none"> <li>◆ Continue complaint-based enforcement.</li> <li>◆ Implement VACE program.</li> </ul>	Housing and Neighborhood Services Department	Administrative	Ongoing	I

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TABLE 1 THE RESIDENTIAL NEIGHBORHOODS RECOMMENDATIONS

Subject	Description	Action	Sponsor & Partners	Cost	Timeframe	Priority
<i>Public Improvements</i>						
Redevelopment Funding	A portion of the Barber Neighborhood south of West 17 <sup>th</sup> Street that lacks infrastructure improvements (sidewalks, storm drains, etc.) is excluded from the Chico Merged Redevelopment Project Area, as shown in Figure 2-7. This area should be assessed and included in the Project Area if blight conditions are found.	<ul style="list-style-type: none"> <li>Assess the southern Barber Neighborhood.</li> <li>Extend the Merged Redevelopment Project Area as appropriate.</li> </ul>	Redevelopment Agency	Administrative	Short	I
<i>Resources</i>						
Historic Signage Program	Community members expressed interest in promoting Southwest Chico's heritage and a historic inventory of the area identified many notable structures. This history could be promoted by establishing a historic sign program whereby owners of documented historic homes would affix a specially designed plaque dating the structure and describing relevant historical details. Signed homes could become part of heritage walks lead by volunteers in the neighborhood.	<ul style="list-style-type: none"> <li>Develop a program for home-owners to apply for, and receive a plaque describing the historic significance of the structure.</li> <li>Create heritage walks lead by neighborhood volunteers.</li> </ul>	Barber Neighborhood Association, Housing and Neighborhood Services Department	Low Cost	Medium	I
Urban Forest Management	The existing mature urban forest is a community asset that requires management. An inventory and health assessment of all trees in the Plan Area with an 18-inch diameter at breast height (DBH) or larger should be conducted. Neighbors and volunteers could be complete elements of the survey. Based on the survey, establish a maintenance and replacement schedule for falling trees.	<ul style="list-style-type: none"> <li>Inventory trees in the plan area larger than 18-inch diameter-at-breast-height.</li> <li>Establish a maintenance and replacement schedule.</li> </ul>	General Services Department - Urban Forestry, Barber Neighborhood Association	Low Cost	Short	II
<i>Urban Form</i>						
Residential Rehabilitation	The City's Housing Rehabilitation Program should be actively promoted to all eligible households in the Plan Area. Promotion will encourage households to remain by making the neighborhood a more attractive, cared-for area.  Housing conditions should be assessed; housing units that receive ratings of "Poor," "Substandard," or "Blighted" during the assessment survey should require active code enforcement. By first making the Housing Rehabilitation Programs available for both owner-occupied and rental properties in the neighborhood and then enforcing the city's codes, the city could assist local owners with code compliance. The City should also consider modifications to existing programs that create incentive programs for both owner occupants and landlords which might include: <ul style="list-style-type: none"> <li>For landlords: Matching grant funds for needed public improvements associated with the property, including street trees, curb, gutter, and sidewalk when combined with a rehabilitation loan;</li> <li>For homeowners: A straight grant for the above public improvements when combined with a rehabilitation loan;</li> <li>Forgivable interest for homeowners if property is maintained and title does not change for a set period; and</li> <li>Standard below-market rate loans.</li> </ul> Additionally, the program should be expanded to include: street tree planting, motion-detect lighting on alleys, enclosed trash areas in alleys, semi-transparent fencing on alleys.  The multi-family component of the targeted rehabilitation effort could be funded in part by the Redevelopment Agency (RDA) tax increment stream.	<ul style="list-style-type: none"> <li>Promote the Housing Rehabilitation Program.</li> <li>Assess existing housing conditions, and begin active code enforcement.</li> <li>Expand eligible projects to cover specific alley improvements.</li> <li>Expand eligibility to include rental properties.</li> <li>Add incentives to utilize the program.</li> </ul>	Housing and Neighborhood Services Department, Barber Neighborhood Association	Variable	Ongoing	II
Alley maintenance and safety	Programs for funding of maintenance and improvements of alleys should be considered.	<ul style="list-style-type: none"> <li>Consider establishment of an alley assessment district</li> </ul>	Barber Neighborhood Association, Housing and Neighborhood Services Department	Administrative	Short	II
<i>Parks and Open Space</i>						
Create New Parks Convenient to Residents	Two potential park sites are shown in the Plan: Hazel Street and 16 <sup>th</sup> and Normal. The City and residents should investigate the practicability of purchase and maintenance of these sites.	<ul style="list-style-type: none"> <li>Consider an assessment district on residential properties in the Southwest Neighborhood</li> </ul>	Barber Neighborhood Association, Housing and Neighborhood Services Department	High Cost	Medium	I

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TABLE E THE WEDGE RECOMMENDATIONS

Subject	Description	Actions	Sponsor & Partners	Cost	Timeframe	Priority
<i>Economic Development</i>						
New Roads	New roads through the Wedge could provide improved circulation and facilitate new development along them. See Table H Circulation for more information.		Capital Project Services Department, Property Owners	High Cost	Long	II
<i>Land Use</i>						
Northern Wedge	Zoning changes that enable additional residential and office uses in the northern portion of the Wedge would encourage the development of live/work housing, townhomes, and office uses in the Wedge that can complement existing manufacturing uses in this area, particularly those that can benefit from the visibility afforded by locating on Park Avenue and recent streetscape improvements. Proactively changing the zoning can eliminate uncertainty for investors and property owners interested in developing these uses.	<ul style="list-style-type: none"> <li>Consider relaxing the permitted uses to allow more office and live-work development.</li> </ul>	Planning Services Department	Administrative	Short	III

TABLE F MEYERS INDUSTRIAL RECOMMENDATIONS

Subject	Description	Actions	Sponsor & Partners	Cost	Timeframe	Priority
<i>Economic Development</i>						
Attract High Quality Development	A new connection to the Meyers Industrial Area from Commerce Court could provide improved circulation and encourage new development opportunities. Live/work development would increase the 24 hour presence of inhabitants. Ensure appropriate street trees and landscaping for attractiveness and to control heat gain.	<ul style="list-style-type: none"> <li>Coordinate efforts with interested developers and property owners.</li> </ul>	Planning Services Department, Local Property Owners, Developers	Moderate Cost	Medium	I

TABLE G DIAMOND MATCH RECOMMENDATIONS

Subject	Description	Actions	Sponsor & Partners	Cost	Timeframe	Priority
<i>Urban Form</i>						
Diamond Match development-driven traffic calming	If and when Diamond Match development proposals come forward, as part of the Specific Plan process, review connections and provide traffic calming measures as needed.	<ul style="list-style-type: none"> <li>Ensure community review of planning for connections to Diamond Match</li> </ul>	Planning Services Department, Barber Neighborhood Association, Local Property Owners and Developers	Moderate Cost	Unknown	I

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TABLE H CIRCULATION RECOMMENDATIONS

Subject	Description	Actions	Sponsor & Partners	Cost	Timeframe	Priority
<i>Area Wide Improvements</i>						
Street Improvement Pilot	The street and alley design ideas presented for unimproved streets in the residential neighborhoods concept should be tested in a pilot program to allow for further refinement before wider construction. The street segments listed below represent a representative range of right-of-way widths. Neighbors and the City should evaluate these segments and determine if they, or other segments, are more appropriate pilot locations. <ul style="list-style-type: none"> <li>◆ Normal Avenue between West 22<sup>nd</sup> Street and West 16<sup>th</sup> Street.</li> <li>◆ West 20<sup>th</sup> Street between Normal Avenue and Park Avenue.</li> <li>◆ West 12<sup>th</sup> Street between Ivy Street and Broadway Street.</li> <li>◆ The alley between Hemlock Street, Locust Street, East 12<sup>th</sup> Street and East 11<sup>th</sup> Street.</li> </ul>	<ul style="list-style-type: none"> <li>◆ Complete pilot improvements.</li> </ul>	Capital Project Services Department	Moderate Cost	Short	III
<i>Pedestrian/Bicycle Improvements</i>						
Ivy Street Bridge	Consider widening the existing bridge with 5-foot minimum sidewalks and sufficient room for bikes on the shoulder or Class II bike lanes, or provide a separate bicycle/pedestrian bridge.	<ul style="list-style-type: none"> <li>◆ Include pedestrian/bicycle improvements in any retrofit or redesign.</li> </ul>	Capital Project Services Department	High Cost	Long	II
Salem Street Bridge	Consider widening the existing bridge with 5-foot minimum sidewalks and sufficient room for bikes on the shoulder or Class II bike lanes, or provide a separate bicycle/pedestrian bridge.	<ul style="list-style-type: none"> <li>◆ Include pedestrian/bicycle improvements in any retrofit or redesign.</li> </ul>	Capital Project Services Department	High Cost	Long	II
Park Avenue Bulbouts	Consider improving the following intersections along Park Avenue with bulbouts narrowing Park Avenue and the cross street: 12 <sup>th</sup> Street, 15 <sup>th</sup> Street, 16 <sup>th</sup> Street, and 17 <sup>th</sup> Street. Bulbout designs should provide opportunities for public sculpture or other art.	<ul style="list-style-type: none"> <li>◆ Construct bulbouts on Park Avenue, narrowing both Park Avenue and perpendicular streets.</li> <li>◆ Coordinate improvements with private investments.</li> </ul>	Capital Project Services Department, Local Arts Community	Per Project	Medium	I
Fair Street Crossing	Identify locations for and consider installing additional pedestrian crossings along Fair Street, including one near the Four Winds Education Center located at 2345 Fair Street.	<ul style="list-style-type: none"> <li>◆ Install crossings.</li> </ul>	Capital Project Services Department	Low Cost	Short	III
Park Avenue at 9 <sup>th</sup> and Humboldt Avenue	Consider providing pedestrian crossings on the south side of 9 <sup>th</sup> Street at Park Avenue, and improve the striping and signage of the Humboldt Avenue and Park Avenue intersection.	<ul style="list-style-type: none"> <li>◆ Provide pedestrian crossings on the south side of 9<sup>th</sup> Street at Park Avenue.</li> <li>◆ Improve the striping and signage of the Humboldt Avenue and Park Avenue intersection.</li> </ul>	Capital Project Services Department	Low Cost	Short	II
Park Avenue near Little Chico Creek	Consider a pedestrian/bicycle crossing of Park Avenue in the vicinity of the Little Chico Creek bridge.	<ul style="list-style-type: none"> <li>◆ Study and install crossing.</li> </ul>	Capital Project Services Department	Moderate Cost	Medium	II
Neighborhood Sidewalk Connectivity	Figure 6-2 diagrams the amount of infrastructure improvements required on the “key pedestrian routes” recommended in Chapter 5. Streets labeled in green have sidewalks in generally good condition and need little or no repair currently. Streets labeled in yellow need to be assessed for heaving and cracking and repaired appropriately. Streets labeled in red have nonexistent or missing sidewalk segments. The red labeled streets should be prioritized for full street improvement, and should be designed consistent with recommendations found in the Residential Neighborhoods recommendations of Chapter 4.	<ul style="list-style-type: none"> <li>◆ Prioritize improving streets and sidewalks on pedestrian routes and as warranted by existing conditions.</li> <li>◆ Assess existing sidewalks on pedestrian routes for condition and repair as needed.</li> </ul>	Capital Project Services Department, Barber Neighborhood Association	Per Project	Medium	I
Hazel Street Bridge	Consider providing a bicycle/pedestrian bridge at the terminus of Hazel Street at Little Chico Creek.	<ul style="list-style-type: none"> <li>◆ Include improvements in design for Hazel Street Park.</li> </ul>	Capital Project Services Department	High Cost	Medium	II

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TABLE H CIRCULATION RECOMMENDATIONS

Subject	Description	Actions	Sponsor & Partners	Cost	Timeframe	Priority
<b>Bicycle Routes</b>						
Chestnut Street	Consider providing Class II bicycle lanes from West 22 <sup>nd</sup> Street to West 16 <sup>th</sup> Street.	◆ Stripe and sign accordingly.	Capital Project Services Department	Low Cost	Short	I
West 20 <sup>th</sup> Street	Designate and sign as Class III bicycle route.	◆ Designate and sign.	Capital Project Services Department	Low Cost	Short	II
East 12 <sup>th</sup> Street and Olive Street	Designate East 12 <sup>th</sup> between Park Avenue and Olive Street, and Olive Street to the bridge as Class III bicycle route.	◆ Designate and sign.	Capital Project Services Department	Low Cost	Short	II
Rails-to-Trails	Although outside the boundary of the Plan Area, a regional bicycle route along the rail line would be a significant amenity for the Southwest Neighborhood. Residents, businesses and the City should support trail planning efforts.	◆ Participate in Rails-to-Trails planning.	Barber Neighborhood Association, Business Owners, Capital Project Services Department	Unknown	Long	II
Bicycle Intersection crossings	Improvements to intersection crossings for bicycles should be considered at 8th and Ivy, 9th and Ivy, 8th and Chestnut, 9th and Chestnut, 20th and Park Ave, and Park Ave and East Park Ave.	◆ Update bicycle crossing signage.		Low Cost	Short	I
<b>Vehicular Circulation</b>						
Commerce Court at Park Avenue	Commerce Court could be connected to Park Avenue via the existing alley. The alley should be improved with striping and sidewalks.	◆ Extend and upgrade.	Planning Services Department, Capital Project Services Department	Moderate Cost	Medium	I
New Connections through the Wedge	New roads through the Wedge could provide improved circulation and facilitate new development along them. Conceptual alignments are shown in Figure 4-31, aligning with existing streets where possible. Other alignments may be more appropriate or feasible. Securing right-of-ways should be collaborative process between Planning Services, Developers and Property Owners.	◆ Secure new right-of-ways through the Wedge. ◆ Coordinate efforts with interested developers and property owners.	Planning Services Department, Local Property Owners, Developers	Unknown	Long	II
Park Avenue and 20 <sup>th</sup> Street	A roundabout or dog-leg could smooth traffic circulation at 20 <sup>th</sup> Street and Park Avenue. These are long-term proposals that will likely be realized only after Barber Yard develops. However, a feasibility study should be undertaken and coordinated with property owners at this intersection to ensure that new development accommodates future improvements.	◆ Complete feasibility study.	Capital Project Services Department	High Cost	Long	III
Diamond Match Connections	Provide vehicular and pedestrian connections to potential future Diamond Match development at locations shown in Figure 4-36.	◆ Review development application for compliance.	Planning Services Department	Moderate Cost	Unknown	II
<b>Traffic Calming</b>						
East 12 <sup>th</sup> Street between Nelson and Locust	Improve the intersections of East 12 <sup>th</sup> Street at Nelson Street and Locust Street as shown in Figure 5-7 to improve the safety of this intersection.	◆ Install improvements.	Capital Project Services Department	Moderate Cost	Medium	II
Ivy Street and West 12 <sup>th</sup> Street	Narrow Ivy Street with bulb-outs or bulb-outs and medians.	◆ Install improvements.	Capital Project Services Department	Moderate Cost	Medium	II
Normal Avenue at West 22 <sup>nd</sup> Street	Improve the intersection with a median or half roundabout as described in Chapter 5.	◆ Install improvements.	Capital Project Services Department, Barber Land Co.	Moderate Cost	Medium	III
Normal Avenue/Estes Road Corner	Build a chicane or other calming device to slow vehicles traveling southbound on Normal Avenue past West 20 <sup>th</sup> Street.	◆ Install improvements.	Capital Project Services Department	Moderate Cost	Medium	III
West 11 <sup>th</sup> Street at Chico Country Day School	Improve West 11 <sup>th</sup> Street between Oakdale and Broadway, as described in Chapter 5. Improvements include: restriping, pedestrian islands, bulbouts, and curb extensions.	◆ Install improvements.	Chico Unified School District, Chico Country Day School, Capital Project Services Department	Moderate Cost	Medium	I

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TABLE | RECOMMENDATIONS FOR FUTURE STUDY

Subject	Description	Actions	Sponsor & Partners	Cost	Timeframe	Priority
<i>Circulation</i>						
City-wide Circulation	During the planning process a number of roadway network improvements with city-wide significance were discussed. These include: connect the Hegan Lane industrial area to East Park Avenue (Otterson Drive extension); a potential southern connection to Barber Yard across the rail-road tracks; and permanently closing Ivy Street to West 22nd and Normal Avenue and maintaining separation between the Barber Neighborhood and the Meyers Industrial Area. These circulation issues and any potential changes require further study of their benefits and impacts to the city-wide circulation network and will be evaluated as part of the General Plan update.	<ul style="list-style-type: none"> <li>◆ Consider these issues in the General Plan Update process.</li> </ul>	Planning Services Department Capital Project Services Department	Administrative	General Plan Update	II
<i>Land Use</i>						
Affordable Housing	During the planning process, many stakeholders expressed the opinion that housing affordability is a continuing issue and that new development should reflect the Plan Area's diversity. This is a larger issue for the City to explore.	<ul style="list-style-type: none"> <li>◆ Consider modifying the zoning code to add an inclusionary zoning ordinance.</li> </ul>	Capital Project Services Department	Administrative	Medium	II
<i>Public Improvements</i>						
Park Avenue Streetscape Plan	This Plan supports the General Plan suggestion for improvements in the public streets in the "Ring Transportation Corridor". The City should prepare and adopt a design manual for the Park Avenue segment of the ring transportation corridor as an implementation of this visioning plan and provide guidelines for future improvements to public streets and new development projects. Guidelines should cover street improvements, street furniture, bus stops/shelters, public signage and landscaping.	<ul style="list-style-type: none"> <li>◆ Consider developing a streetscape plan for the ring transportation corridor.</li> </ul>	Planning Services Department	Low Cost	Medium	III
<i>Stormwater</i>						
Comprehensive Watershed and Creek Protection Program	Adoption of a city-wide program could further protect and enhance Comanche and Little Chico Creeks, and other riparian corridors. The program could include a vegetation management plan to identify invasive exotics and appropriate eradication and replacement with native species. The plan could also examine the existing City infrastructure. Utilizing "green infrastructure" to capture, treat and convey stormwater onsite will greatly reduce the volume of water entering this system, particularly during storm events, which can in turn help reduce flooding impacts.	<ul style="list-style-type: none"> <li>◆ Consider collaborating with other agencies on a comprehensive protection program.</li> </ul>	Capital Project Services Department, Regional Water Quality Control Board, Creek and watershed interest groups	Low Cost (w/partnerships)	Short	II
Urban Stormwater Toolkit	The City provides a handful of Best Management Practices (BMPs) for addressing stormwater runoff and water quality issues onsite. The City and County could consider expanding these based on tools from other locales, such as the Bay Area C-3 and Portland's Green Streets. These tools would provide developers and property owners easily understandable measures to incorporate into site and architectural designs. A toolkit should include a comparison of cost and feasibility of traditional stormwater infrastructure versus "green street" solutions.	<ul style="list-style-type: none"> <li>◆ Consider expanding the stormwater BMPs.</li> <li>◆ Encourage residential projects to incorporate stormwater BMPs.</li> </ul>	Capital Project Services Department	Administrative	Short	II
<i>Transit</i>						
Short-Loop Downtown Shuttle Service	There was interest expressed during the planning process in a trolley service to connect the Southwest Neighborhood, Downtown, and Chico State. Detailed study is required on this issue. A similar interest was expressed by the Avenues Neighborhood, and a combined study may be appropriate.	<ul style="list-style-type: none"> <li>◆ Consider studying an Avenues/Downtown Chico/Southwest Neighborhood loop shuttle service.</li> </ul>	Capital Project Services Department	Low cost	Medium	II
<i>Urban Form</i>						
Sustainability in Public Works	Neighborhood residents expressed interest in alternative materials such as recycled rubber sidewalks for the residential neighborhood. The City's Sustainability Task Force could extend its investigations to include sustainable materials and practices for public works construction projects.	<ul style="list-style-type: none"> <li>◆ Consider evaluating sustainable materials and practices for incorporation into standard public works construction projects.</li> </ul>	Sustainability Task Force, Capital Project Services Department	Administrative	Short	III

\* I = Highest Priority, II = Medium Priority, III = Lower Priority

Cost note: Administrative = primarily staff costs

