



DATE: September 27, 2016
TO: PLANNING COMMISSION
FROM: Mike Sawley, Associate Planner (879-6812, mike.sawley@chicoca.gov)
RE: Chico Walmart Expansion Project, 2044 Forest Avenue
AP Nos: 002-370-057, 002-370-055, and 002-170-004

Files: PM 15-02 UP 15-10 UP 15-12 AR 15-16

SUMMARY

The applicant proposes to subdivide a 27-acre site located at 2044 Forest Avenue into three parcels to facilitate:

- Expansion of an existing retail store from 125,889 square feet (sf) to 190,275 sf, including a merchandise/pharmacy drive-through use and related site improvements on Parcel 1;
- Construction of a new 8-pump fuel station (16 fueling positions) with a 1,500 sf convenience store and related site improvements on Parcel 1; and
- Creation of Parcel 2 and Parcel 3, each approximately 2.6 acres in size, for which no development is currently proposed.

Development applications for the proposed project include:

- A parcel map (PM 15-02) to subdivide the site into three parcels and relocate various utility easements;
- Site design and architectural review (AR 15-16) for the design of buildings, parking areas, pedestrian/bicycle circulation, landscaping, and exterior lighting;
- A use permit (UP 15-10) to authorize a drive-through use and permanent outdoor retail sales associated with the retail store; and
- A use permit (UP 15-12) to authorize a new fuel station at the corner of Forest Avenue and Wittmeier Drive.

A planned development permit application (PDP 15-01) to authorize more sign area than allowed by the Chico Municipal Code (CMC) has been withdrawn.

To satisfy the requirements of the California Environmental Quality Act (CEQA), the City prepared an Environmental Impact Report (EIR) to analyze and disclose the potential environmental impacts associated with approving the project. The EIR identified various potential impacts and included mitigation measures to reduce those impacts to less than significant. In one instance, however, the EIR concluded that approving the project might result in a significant and unavoidable traffic impact for which no feasible and sufficient mitigation is available. The impact involves the potential that operations of State Route 99 (SR-99), between SR-32 and East 20th Street, could decrease during the p.m. peak hour to an unacceptable Level of Service (LOS) under the 2035 cumulative plus project scenario.

Pursuant to State law, special findings known as a “Statement of Overriding Considerations” (SOC) are required to be made prior to approving a project with one or more significant and

unavoidable impacts. An SOC sets forth specific overriding economic, legal, technological, social, or other benefits of the project that outweigh the significant effects on the environment. CEQA compliance is discussed further in the Environmental Review section, below.

Approval of the project applications must be preceded by certification of the EIR and adoption of a Statement of Overriding Considerations. Therefore, three separate resolutions have been prepared to facilitate an orderly decision-making process:

- Planning Commission Resolution No. 16-11 would certify the EIR. It includes findings that the EIR was completed in compliance with CEQA, reflects a good faith effort to disclose and mitigate potential environmental impacts that may result from the project, and represents the City's independent judgment and determinations.
- Planning Commission Resolution No. 16-12 would adopt a Statement of Overriding Considerations in compliance with state law as well as a mitigation monitoring program.
- Planning Commission Resolution No. 16-13 would approve the project entitlements, including the parcel map, use permits, and site design and architectural review applications.

Recommendation:

The Community Development Director recommends that the Planning Commission hold a public hearing and, under separate motions:

- 1) Adopt Resolution No. 16-11 certifying the adequacy of the Final EIR (**Attachment A**);
- 2) Adopt Resolution No. 16-12 making certain findings regarding environmental effects and mitigation measures, adopting a Statement of Overriding Considerations, and adopting the Mitigation Monitoring Program (**Attachment B**); and
- 3) Adopt Resolution No. 16-13 approving Parcel Map 15-02, Use Permit 15-10, Use Permit 15-12, and Site Design and Architectural Review 15-16 (**Attachment C**).

Proposed Motions:

- 1) I move that the Planning Commission adopt Resolution No. 16-11, certifying the adequacy of the Final EIR.
- 2) I move that the Planning Commission adopt Resolution No. 16-12, making certain findings regarding environmental effects and mitigation measures, adopting a Statement of Overriding Considerations, and adopting the Mitigation Monitoring Program.
- 3) I move that the Planning Commission adopt Resolution No. 16-13, conditionally approving Parcel Map 15-02, Use Permit 15-10, Use Permit 15-12, and Site Design and Architectural Review 15-16.

EXISTING CONDITIONS

The project site consists of two parcels: (1) a 16.5-acre parcel containing a discount retail store (approximately 126,000 sf in size) and 630-space parking lot with associated landscaped areas (Assessor's Parcel Nos. 002-370-057 and -055); and (2) approximately 10.6-acres of undeveloped land located immediately south of the existing store and parking area (Assessor's Parcel No. 002-170-004), (see Location Map, **Attachment D**).

The project site is designated Regional Commercial on the General Plan Land Use Diagram and zoned CR (Regional Commercial) on the City's Zoning Map. The site is also located within Regional Center Opportunity Site #10 (E. 20th Street), as established in the General Plan. Opportunity Sites are introduced in the General Plan under the heading "*Areas of Potential Change,*" and described as "*strategic infill and redevelopment areas [that] include underutilized transportation corridors, regional retail centers, areas in the City's core, and other residential, light industrial, and mixed use areas that can accommodate growth. To support increased density and intensification of uses at these locations, the City will need to invest resources, particularly to ensure that infrastructure can adequately support growth.*"

Surrounding properties include a hotel use and undeveloped land (north), office and retail uses (east), the Wittmeier Auto Center (south), and SR-99 (west).

The existing Walmart store opened in 1994. It retails general merchandise, including food and beverage items, and operates between 6 a.m. and 12 a.m. (midnight), 7 days a week. The architectural design is mostly comprised of split-faced concrete masonry units (CMU), with a central tower element and repetitive post-and-lintel wall patterns around all elevations. The parking lot is landscaped with small mature trees, mostly honey locust, London planetree and Chinese pistache. The existing store currently has a rooftop solar array.

Vehicular access is taken from three driveways on Baney Lane (two of which allow full access and the third which allows only left-in, right-in, and right-out turning movements); a right-in, right-out driveway on Forest Avenue; and a service driveway from the Business Lane cul-de-sac. A B-Line bus stop is located on Forest Avenue near the driveway entrance.

The undeveloped parcel contains two wooden freeway billboards facing SR-99, weedy vegetation, some ornamental landscaping, and a small depressional seasonal wetland (0.02 acres) located near the SR-99 frontage in the southwestern corner of the site.

A Class I bicycle/pedestrian path exists around the perimeter of the store and parking lot. The path begins at the Business Lane cul-de-sac and continues around the perimeter of the Walmart parking area to the intersection of Forest Avenue/Baney Lane. A seating area with picnic tables is located adjacent to the path near the Walmart store. The path is part of a larger trail network that is planned to extend along the east side of SR-99 ("Bikeway 99").

Two existing, overhead Pacific Gas and Electric Company (PG&E) electrical lines traverse the site east-to-west, over a portion of the Class I bike. The lines support high voltage transmission (115 kilovolts (kv)) and distribution (15 kv), however, they do not provide electrical service to the existing Walmart store which is served via underground lines located behind the store.

PROJECT DESCRIPTION AND ANALYSIS

The proposed project involves the following development applications:

- 1) A tentative parcel map (PM 15-02), to subdivide the site into three parcels and relocate various utility easements (see **Attachment E**);
- 2) Site Design and Architectural Review (AR 15-16) for the design of a proposed retail store expansion, a new fuel station convenience store and canopy, and related site improvements (see **Attachments F through K**);

- 3) A use permit to authorize a drive-through use and outdoor retail sales associated with the retail store (UP 15-10) (see Exhibit IV of **Attachment C**); and
- 4) A use permit to authorize a new fuel station use at the corner of Forest Avenue and Wittmeier Drive (UP 15-12) (see Exhibit V of **Attachment C**).

1. Parcel Map PM 15-02

The proposed parcel map would subdivide the 27-acre site into three parcels (see **Attachment E**). Parcel 1 would be approximately 22 acres, and Parcels 2 and 3 would each be approximately 2.6 acres in size. Parcel 1 would support the expanded store and parking area, as well as a new fuel station with convenience store. No development is proposed on Parcels 2 and 3. The EIR assumes that these smaller parcels will be developed in the future with retail and/or restaurant uses consistent with CR zoning.

The map includes modifications to Wittmeier Drive and Baney Lane, including their respective intersections with Forest Avenue. Baney Lane modifications would include:

- Relocating the easterly (primary) Walmart access driveway approximately 180 feet west of its current location;
- Extending left-turn lanes for eastbound traffic on Baney Lane to increase vehicle storage capacity in advance of the Forest Avenue intersection;
- Extending the center left-turn lane for westbound traffic on Baney Lane heading into the primary Walmart driveway; and
- Adding raised islands at the primary and central Walmart driveways to prohibit outbound left turns onto Baney Lane (the westerly driveway already has such a raised island).

Proposed modifications on Wittmeier Drive include:

- Adding a traffic signal at its intersection with Forest Avenue, with related improvements on the Forest Avenue and Talbert Drive approaches;
- Widening the street to accommodate one westbound lane and three eastbound lanes, including two dedicated left-turn lanes and one through/right-turn lane;
- Adding a new driveway to primarily serve the new fuel station; and
- Adding a new driveway at the end of the existing cul-de-sac to serve the Walmart store and Parcels 2 and 3 (with a shared cross-access easement between the three proposed parcels).

The parcel map also proposes abandonment and relocation of a number of public utility and public access easements, including water, sewer, storm drainage, PG&E, and a public bicycle path. Each of these easements would be relocated south of the store expansion as indicated on the parcel map. The map also depicts existing fire hydrants that would be relocated with the project.

Sheet 4 of 5 of the parcel map shows the existing trees at the site, and lists 42 trees proposed for removal. One of the trees to be removed, a 13-inch cork oak (Tree #13), requires replacement of two trees pursuant to the City's Tree Preservation Regulations. Landscape plans for the project, described in more detail below, include more than 20 new trees that would qualify toward the onsite replacement requirement of two trees.

No modifications to the City's Design Criteria and Improvement Standards contained in CMC Title 18R have been requested.

Staff Analysis and Required Findings

The proposed land division to accommodate subsequent commercial development on the Regional Commercial site is consistent with CMC requirements for minimum lot area and legal access, and the resultant parcels will be served by all necessary utilities. Items requiring discussion and conditions of approval include vesting rights, public bicycle paths and relocation of the overhead power lines. Each of these issues is discussed briefly below.

Vesting Rights: The map title includes a "vesting" designation, which the CMC limits to tentative maps for residential subdivisions pursuant to CMC section 18.22.020. Since the proposed map is not a residential subdivision, a condition is recommended to remove the "vesting" designator from the map title and clarify that the provisions of CMC 18.22 do not apply to the parcel map.

Class I Bicycle Path Relocation: The parcel map would result in the relocation of a public bikeway easement and physical Class I bike path around the southern perimeter of the site. To ensure project consistency with planned future bikeway facilities shown on the City's Bicycle and Pedestrian System Map (Figure CIRC-2 in the General Plan), the Draft EIR identified mitigation requiring the final map to include an easement to the southwesterly corner of the site for a potential bikeway connection to points south. Recommended map conditions specify that the easement along the SR-99 frontage be 20 feet in width to accommodate potential design constraints, and final alignment of the relocated bike path be subject to review and approval by the Public Works Director. It is anticipated that minor changes to the bike path alignment shown on the map may be necessary to properly integrate the path with Wittmeier Drive, and minimize bike path encroachments into Parcels 2 and 3.

Relocation of Overhead Power Lines: The parcel map includes relocating a 50-foot wide PG&E easement and re-routing the associated overhead, high-voltage power lines (one 15 kv, the other 115 kv) around the southern perimeter of the site. The existing easement and overhead lines cross the site just south of the existing Walmart store, and relocating these facilities outside of the footprint of the store expansion is a necessary component of the project.

The proposed new alignment would add new support poles such that the lines are routed south, onto Parcel 2 and Parcel 3, then back to the existing poles that carry the lines off site.

Chico Municipal Code Section 19.60.120 states: "*All proposed electric and telephone facilities, fire alarm conduits, street lighting wiring, cable television and other wiring conduits, and similar facilities shall be placed underground by the developer, unless such placement has been determined to be either infeasible or unsafe by either the public works director or the appropriate review authority. The Council may grant a modification, including a complete waiver of the undergrounding requirement, after considering the general purposes and nature of the proposed development.*"

Title 19 of the CMC provides the following definition for the word feasible: "*Capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, social, and technological factors.*"

The applicant's engineer responded to this code section with the following assertions: (1) the code section does not apply to the project because the overhead lines already exist and do not serve the existing store and, (2) even if the section did apply, placing the high-voltage lines underground is infeasible due to jurisdictional, technological and cost issues.

Based on staff discussions with the City Attorney's office, the code section does apply to the project. Although the overhead lines already exist on the site and do not provide electrical service to the existing or planned development, it is nonetheless a feature of the project to propose new overhead electrical facilities to the extent that new poles and line would be installed along a different alignment through the site.

Staff concurs, however, that placing the high-voltage lines underground would be infeasible due to cost. Correspondence from PG&E indicates that the cost of undergrounding the 115 kv transmission line would range from \$2.5 million to \$10 million per 500 feet of transmission line undergrounded (email from D. Barrios to M. Sawley, 11/18/15). The applicant's engineer used that cost estimate, and the project's need to re-route nearly 1,500 feet of transmission line, to arrive at an overall undergrounding estimate of between \$7.5 and \$30 million. This cost is between half and double the total cost of the proposed expansion project, including all site work improvements. Further, undergrounding the 15 kv distribution line would entail additional costs.

In this case, staff believes that undergrounding the lines as a condition of the project could not be accomplished in a successful manner within a reasonable period of time because such a requirement would cause the applicant to abandon the project due to economic considerations.

Further, in considering the potential social benefits of undergrounding the power lines, the City has no plans to underground any other portions of the high-voltage lines in the vicinity and has approved projects along other portions of the facility without requiring undergrounding (e.g., Meriam Park, Parkway Village and Huntington Village Apartments). There is limited public benefit to undergrounding the re-routed segment of overhead lines at the site.

The Community Development Director and Public Works Director both recommend finding that placing the relocated 115kv/15kv overhead lines underground with the project is not required due to infeasibility.

Although placing the overhead lines underground is not recommended in this case, the proposed route for the relocated lines substantially encroaches upon Parcels 2 and 3, limiting flexibility for future development of those parcels. To minimize limitations on the future development of Parcels 2 and 3, a condition is recommended that would require, to the extent feasible, the relocated PG&E easement for the overhead lines to evenly straddle the shared east-west property line between Parcel 1 and Parcel 2, and between Parcel 1 and Parcel 3, thereby minimizing encroachments into areas reserved for future development.

Required Findings: The Subdivision Map Act requires a denial of a parcel map if any of the following findings are made:

- 1) *The proposed map is not consistent with applicable general and specific plans.*
- 2) *The design or improvement of the proposed subdivision is not consistent with applicable general and specific plans.*

- 3) *The site is not physically suitable for the type of development.*
- 4) *The site is not physically suitable for the proposed density of development.*
- 5) *The design of the subdivision or the proposed improvements are likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.*
- 6) *The design if the subdivision or type of improvements is likely to cause serious public health problems.*
- 7) *The design of the subdivision or the type of improvements will conflict with easements, acquired by the public at large, for access through or use of, property within the subdivision.*

Staff has not identified any elements of the proposed parcel map that would require the Commission to make any of the required findings for denial. Staff has reviewed the proposed project, including the parcel map, for overall consistency with the General Plan (see **Attachment L**). The analysis concludes that, while not consistent with every policy contained in the Plan, the project is consistent with the General Plan as a whole.

The project would result in compatible infill development/redevelopment within a Regional Commercial Opportunity Site, consistent with goals LU-4, LU-5, CD-5, and policies LU-1.3, LU-4.2 and LU-5.1, in that the proposed layout would not cause significant conflicts with existing or anticipated Regional Commercial uses in the area. The project is also consistent with the General Plan's emphasis on intensifying and revitalizing commercial uses within the East 20th Street Opportunity Site. The proposed parcels are of sufficient size to accommodate development in compliance with the City's regulations and all necessary utilities and services are available to serve the parcels, lending further support for a finding of infill compatibility for the project. No specific plans apply to the project.

The site is suitable for the type and density of the proposed development in that it is relatively level, surrounded by compatible commercial land uses and, as conditioned, sufficient areas are provided to meet applicable development regulations pertaining to buildings, off-street parking and landscaping. Based on evidence and mitigation provided by the EIR, the design and improvements associated with the subdivision are not likely to cause substantial environmental damage, substantially injure fish or wildlife or their habitat, or cause serious public health problems. The design of the subdivision would not conflict with public easements for access through the subdivision because all abandoned public access easements would be replaced with new easements that convey substantially the same public benefit.

As supported by the Mitigation Monitoring Program, Conditions of Approval, Subdivision Report (Exhibits I, II and III to **Attachment C**), the project EIR, and this staff report, the proposed parcel map and its design conform with the requirements of Title 18 and Title 19 of the Chico Municipal Code, and would be consistent with the Chico General Plan.

2. Site Design and Architectural Review AR 15-16

Materials for this application include the proposed site plan, landscape plans, architectural elevations, a colors/materials board, exterior lighting details, and applicant's project description. These materials are provided under **Attachments F through K**, respectively.

Site Plan

The main features of the site plan are as follows:

- Expansion of the existing Walmart store by extending the building approximately 150 feet to the south;
- Addition of a drive-through merchandise and pharmacy pick-up area at the southern end of the expanded building;
- An additional 134 vehicle parking spaces located south of the existing parking area (separated by a dedicated pedestrian path linking the store front to Forest Avenue);
- Modified parking area circulation resulting from the relocated primary driveway;
- Reconfigured receiving area behind the store with two new loading docks;
- Outdoor retail display and sales areas depicted in front of the expanded store, and in the northern portion of the parking area;
- A new 8-pump fuel station (16 fueling positions), and a 1,500 sf convenience store with associated parking;
- Relocated Class I public bicycle path near the southern perimeter of the overall site;
- Relocated overhead power lines near the southern perimeter of the overall site;
- A new pedestrian walkway through the central portion of the parking lot to provide pedestrian access between Forest Avenue and the store;
- Covered and uncovered bicycle parking clustered at several locations across the front of the store, with additional bike lockers located near the Tire and Lube Express area; and
- Two outparcels for future development.

Building Expansion and Drive-Through

The existing Walmart store would be expanded by approximately 64,386 square feet for a total size of approximately 190,275 square feet. All new square footage would be for indoor uses. The expanded store would provide 51,944 square feet for grocery sales and grocery stockroom area. The remaining square footage would be used for general merchandise sales, storage and ancillary uses. The store would continue to operate between 6 a.m. and 12 a.m. (midnight), 7 days a week.

The proposal includes requests for a pharmacy and merchandise drive-through at the south side of the building, and permanent outdoor display areas designated on the sidewalk in front of the store and within the parking lot. These uses are detailed later in this report under Section #3, *Use Permit 15-10*.

The following table summarizes existing and proposed/potential development for the project:

Parcel	Acres	Characteristics	Existing/Proposed Square Footage
1	21.88	Walmart Store* : General merchandise and grocery sales; pharmacy/merchandise drive-through; open 6 a.m. to 12 a.m. daily	Existing: 125,889 sf Proposed: <u>64,386 sf</u> Total: 190,275 sf
		Fuel Station : Canopy with 24-hour fueling; kiosk open 5 a.m. to 11 p.m. daily	Proposed: 8 pumps 1,500 square feet
2	2.57	Retail or restaurant	Potential for: 25,700 square feet
3	2.63	Retail or restaurant	Potential for: 26,300 square feet
Total	27.08	—	243,775 square feet
<i>Net New</i>	<i>27.08</i>	—	<i>117,886 square feet</i> <i>8 pumps</i>
<p>*Existing store square footage does not include the outdoor Garden Center area (5,413 sf), or outdoor display/sales areas (5,100 sf).</p> <p>Source: RSC Engineering, 2015.</p>			

Landscape Plans

Landscape plans are included as **Attachment G**. The plans identify all proposed landscaping for the site and provides shading calculations for the expanded portion of the parking lot. The plans indicate that shading of the new parking areas is projected to achieve 53 percent coverage in 15 years. The plans also calculate internal parking lot landscaping at 29 percent of the off-street parking area, exclusive of frontage landscape. Municipal Code requirements for parking lot shading and internal landscaping are 50 percent and 5 percent, respectively. Parking lot shade would primarily be provided by a combination of Drake elm, London planetree, and October Glory maple trees.

The landscape plans include a note stating that *“an inventory of existing shade trees will be performed and any tree found to be missing or nearly dead will be replaced as part of the improvement plans.”*

Architecture

The proposed store expansion would retain a contemporary commercial architectural style for the structure, using mainly tan, beige and brown colors with white trim (see **Attachment H**). The majority of the building exterior would be painted CMU block with architectural columns proposed on all sides of the building. A new front wall assembly would be constructed with distinct entry features for the "Market" and "Home and Pharmacy" entrances. Main building elements would be accentuated by storefront windows, awnings, cultured stone veneer and white cornice. The drawings show a smaller white cornice between the three main building elements on the front elevation with painted metal coping for the rest of the elevation.

Other elevations would have main elements similarly distinguished by cultured stone and cornices. Additional horizontal features, (awnings, bands of stucco, and paint) are included on the front to break up the perceived height of the building. Drive-through elements would be bright orange, highlighting this aspect of the project.

The height of the building would be 25-27 feet, with taller elements up to approximately 35 feet. Roof mounted HVAC units would be screened by parapet walls. As noted above, project signage will be processed by separate permit.

The new fuel station convenience store would also primarily consist of brown and tan building elements, with storefront windows and an awning to announce the main entrance. The awning would be painted blue, to match the valance of the canopy cover for the fuel islands. The brown portion of the building would be constructed with stucco walls over a wainscot of split-faced CMU. The tan portion of the building would be smooth-face CMU. Brown composite fencing is proposed at the rear of the building to screen utilities and appurtenant outdoor operations. The convenience store would be 14-15 feet in height, and the fuel island canopy would be 14-18 feet in height.

Materials board reproductions for the Walmart expansion and fuel station convenience store are provided as **Attachment I**. Physical material samples will be available at the meeting.

The proposal includes new and modified exterior lighting for the parking area and store expansion. Changes include replacing light fixtures in the existing parking area (re-using or replacing the 39-foot tall light poles and 3-foot pole bases), and adding new fixtures in the expanded parking area mounted at 42 feet in height (39-foot poles atop 3-foot bases). Building mounted wall-pack lighting would be added to side and rear store elevations, mounted at 16 feet above grade. No wall-mounted lights are proposed on the front of the store. All new lighting would be full cutoff and directed downward.

The project includes a number of sustainable features aimed at reducing energy consumption. The features are identified, along with many other construction and operational details, in the applicant's project description (see **Attachment K**).

Staff Analysis and Required Findings

The proposed site design and architecture is of appropriate scale and intensity for the Regional Commercial Opportunity Site, and is consistent with CMC requirements for site coverage, building setbacks, maximum building height, landscape buffers, and minimum off-street vehicle and bicycle parking. The store expansion, with its new tower elements and variety of textures would rejuvenate the aging building exterior. The new convenience store would relate to the larger store through use of similar colors and textures, and its unadorned side elevations would be adequately screened over time with landscaping.

Project consistency with the General Plan and Design Guidelines is discussed under the findings section, below. Since both documents provide guidance regarding use of exterior lighting and providing adequate shade tree coverage of parking lots, these issues are addressed prior to the findings section, as follows:

Exterior Lighting: The proposed new and modified 42-foot tall parking lot lights are not consistent with policy OS-1.3, or Design Guidelines 1.5.13 and 1.5.16, which encourage reducing excessive nighttime light and glare by using pedestrian-scale lighting that is no more than 18 feet in height. A condition is therefore recommended to limit new parking lot lighting to 18-feet in height, and to replace the existing taller light poles with poles that result in an overall light height of no more than 18 feet.

Parking Lot Shading: The project is challenged by General Plan Action OS-6.1.1 and Design Guideline 2.1.28, in that trees in the existing parking area are proposed to remain unless determined to be “nearly dead,” and shade tree coverage in the existing parking area is well below 50 percent. Based on the original landscape plan and recent information provided in an arborist’s report, shade tree coverage in the existing parking area is currently approximately 14 percent. The Action and Guideline call for maintaining/expanding the urban forest per City code standards and by “*working with commercial parking lot owners to improve the shade canopy.*”

Fifty percent shading of parking areas has been a minimum Municipal Code requirement since 1987. The 1993 landscape plans for the existing store indicate that 97.2 percent of the paved area would be shaded by the landscape design. In other words, the approved plans indicate approximately 7 acres of shading in 15 years, however, approximately one acre of shade exists after 22 years. Existing shade provides less than one-third of the City’s 50 percent standard.

To achieve consistency with the Action and Guideline above, a condition is recommended that would require replacement of existing trees in the parking area that have not grown to at least roughly one-half of their projected diameter. Most of the existing shade trees were represented with 40-foot diameters on the 1993 landscape plans. The recommended condition would apply to specific (supposedly larger) species and use a threshold of 20 feet in canopy diameter for requiring replacement. This condition would result in the replacement of approximately 124 of the roughly 200 existing parking lot trees.

Replacing the underperforming trees in the parking area would complement the new materials and colors planned for the store expansion, as well as the recommended condition for lower-level parking lot light standards.

With these conditions, and as supported by the consistency findings below, the proposed project would be consistent with the General Plan.

Required Findings: According to CMC 19.18.060, the review authority shall determine whether or not the project adequately meets adopted City standards and design guidelines, based upon the following findings:

- A. *The proposed development is consistent with the General Plan, any applicable specific plan, and any applicable neighborhood or area plans.*

Relevant General Plan policy language, including the policies mentioned below as well as a General Plan consistency analysis, are provided in **Attachment L**.

Expanding and adding additional commercial uses at the partially-developed project site represents compatible infill development/redevelopment within a Regional Commercial Opportunity Site, consistent with goals LU-4, LU-5, CD-5, and policies LU-1.3, LU-4.2 and

LU-5.1, in that the proposed layout would not cause significant conflicts with existing or anticipated Regional Commercial uses in the area. The project is also consistent with the General Plan's emphasis on intensifying and revitalizing commercial uses within the East 20th Street Opportunity Site.

As explained in the EIR, approving the project could result in an unacceptable Level of Service (LOS) for a southbound segment of SR-99 between SR-32 and East 20th Street under the 2035 cumulative plus project modeling scenario, during the p.m. peak hour. Because an acceptable level of service may not be maintained for this facility, there is a potential inconsistency with Policy CIRC-1.4.

However, the proposed project design, as modified by mitigation measures identified in the EIR, would achieve consistency with the remaining policies in the Circulation Element. The proposal is consistent with policies CIRC-1.2, CIRC-2.2.3 and CIRC-3.6, in that it would construct necessary internal and adjacent roadway circulation improvements, use traffic calming measures to reduce cut-through traffic, and include safe and secure bicycle parking facilities. Mitigation measures and existing City processes would ensure project consistency with CIRC-1.3, CIRC-3.1.1, CIRC 5.3, CIRC-9.1, CIRC-9.1.2, CIRC-9.1.3 and CIRC-9.3, by requiring payment of fair share costs for citywide street facility improvements (MM TRANS-3b), incorporating a planned bikeway connection through the site (MM TRANS-7), supporting public transit (MM AIR-2h), and reducing vehicle trips through the use of travel demand management strategies (MM AIR-2f), respectively.

Approval of the project is consistent with General Plan goals and policies to enhance Chico's long-term prosperity (ED-1), and ensure that regulations and permitting processes for the conduct of commerce do not unreasonably inhibit local business activity (ED-1.3).

Although a specific future potential traffic impact is projected to be inconsistent with Policy 1.4, the overall project, as conditioned, implements many other policies and is consistent with the General Plan when considering the policy framework as a whole. No Neighborhood Plans or area plans apply to the project.

- B. The proposed development, including the character, scale, and quality of design are consistent with the purpose/intent of this chapter and any adopted design guidelines.*

The project promotes orderly development and higher use of land by expanding into undeveloped land within an existing regional-serving shopping area, and including circulation improvements that will address existing and future projected needs at the site. Updating the store's appearance and installing landscape improvements in the parking area will increase desirability of investment in the area and achieve a higher visual aesthetic, consistent with the stated purposes of CMC 19.18.

The project is consistent with Design Guidelines that call for commercial buildings to incorporate varied building depths, materials and rooflines (DGs 2.2.22, 2.2.25, 2.2.31), and create a sense of focus toward main entrances through distinguished tower elements, awnings, and storefront windows (DG 2.2.23). Conditions would ensure consistency with DGs that encourage integral parapet roof designs that avoid a "stuck on" appearance (DGs 2.2.24, 2.2.26).

The store expansion is appropriately scaled for this Regional Commercial shopping area comprising the Chico Mall and other large format retailers (DG 1.2.13). Bicycle and pedestrian paths are well marked and in many cases lined with landscaping, which enhances safety (DGs 2.1.23, 2.1.33, 2.1.34). Design Guideline consistency is further enhanced by screening HVAC units with roof parapets, parking lot and drive-through areas with landscaping, and wall-mounted utilities with other building elements, as called-for by DGs 2.1.25, 2.1.36 and 2.2.28. Store elevations reflect superior design solutions over the initial project submittals, and eschew large, bright-blue trademark elements seen in other communities (DGs 1.8.11, 1.8.12, 2.2.13).

The project relates to the greater area and conveys permanence through the use of stone and split-faced CMU, and colors are chosen from a rich palette with white accents that are well placed to complement future signage (DGs 1.2.21, 1.2.32, 2.2.32). The proposed painted and raised wall patterns would promote a pedestrian scale for the main building by breaking up the elevations into single-story sized elements, and reducing the overall perceived height (DG 2.2.11).

Conditions are recommended to ensure project consistency with DG 1.5.13 and 1.5.16, regarding excessive nighttime lighting, and DG 2.1.28, regarding the provision of adequate amounts of parking lot shading.

Text of the specific Design Guidelines mentioned above is reproduced in **Attachment M**. Based on all the above, and subject to conditions as noted, the project is consistent with the City's adopted Design Guidelines.

- C. *The architectural design of structures, including all elevations, materials and colors are visually compatible with surrounding development. Design elements, including screening of equipment, exterior lighting, signs, and awnings, have been incorporated into the project to further ensure its compatibility with the character and uses of adjacent development.*

The design, materials and colors of the proposed new building are visually compatible with the surrounding commercial development, specifically regarding the earth-tone colors, exterior textures, flat parapet roof, and tower elements. Exterior equipment will be properly screened from view by roof parapets, landscaping or by structural walls.

- D. *The location and configuration of structures are compatible with their sites and with surrounding sites and structures, and do not unnecessarily block views from other structures or dominate their surroundings.*

The proposed structures are compatible with the site in that there is ample space provided and the design meets applicable development standards, including building height limitations, setbacks, landscaping and parking requirements. The expanded store and new fuel station will be adequately buffered by landscaping, and the buildings will not unnecessarily block views or unacceptably dominate its surroundings.

- E. *The general landscape design, including the color, location, size, texture, type, and coverage of plant materials, and provisions for irrigation and maintenance, and protection of landscape elements, have been considered to ensure visual relief, to complement structures, and to provide an attractive environment.*

The proposed landscaping includes sufficient variety and would provide visual relief along the Forest Avenue frontage for the fuel station, at new and former driveways along Baney Lane, and throughout the existing and proposed parking areas. Specific attention is paid to screening the drive-through pharmacy/merchandise pick-up area with a variety of trees and shrubs, and to enhance pedestrian orientation by lining the dedicated footpath through the parking area with landscaping on one side. Overall, the landscape design, as conditioned, would provide adequate coverage of plant materials and result in an attractive environment across the entire project site.

3. Use Permit UP 15-10 – Walmart Drive-Through and Outdoor Sales

The pharmacy/merchandise drive-through and permanent outdoor retail sales components of the proposed Walmart store expansion require use permit authorization.

Drive-Through Pharmacy and Merchandise Sales

The drive-through would include three lanes with an entrance gateway feature and check-in kiosks located under a canopy (see **Attachment C, Exhibit IV**). Adequate space would be provided in each lane for queuing three vehicles behind the vehicle at the check-in kiosk for a total stacking capacity of nine vehicles. After checking in, customers would proceed to one of six designated pick-up spaces located under a canopy, or an uncovered queuing position located behind one of the six pick-up spaces. Four typical parking spaces are also provided nearby for potential use by drive-through customers with longer wait periods. Drive-through lanes would be 12 feet wide and provide ample turning radii for vehicle maneuvering.

Permanent Outdoor Display and Sales Area

The expanded store would provide a total of 5,100 square feet of permanent outdoor display and sales of merchandise in designated areas in front of the store and within the parking lot (see **Attachment F**). Items on display and for sale may include home and garden supplies, seasonal merchandise, and propane tanks. The parking lot sales area would occupy 22 parking spaces within the parking lot in front of the garden center. The overall parking count for the store (764 spaces) does not include these 22 spaces that would be used for permanent outdoor sales.

Staff Analysis and Findings:

Chico Municipal Code standards for drive-through facilities and outdoor retail sales are included as **Attachment N**.

Drive-Through: The proposed drive-through use would be on the south side of the store, located well away from the public right-of-way. The drive-through provides good wayfinding, enhanced by distinguishing colors, and is situated such that vehicle circulation will not interfere with general parking lot circulation or emergency vehicle access. A pedestrian walkway that crosses in front of the drive-through entrance would be marked to enhance visibility. No significant issues have been identified with this aspect of the project.

Outdoor Sales: The requested authorization for permanent outdoor sales would essentially sanction the current use of a portion of the parking area in front (easterly) of the Garden Center for outdoor storage. This area has been fenced and used for outdoor sales for several years.

It is a common feature at large format retailers to use portions of their parking area for merchandise displays of bulky items. Loss of parking is not an issue as the parking calculations demonstrate sufficient parking to meet code requirements, without counting the 22 spaces that would be used for outdoor sales. Staff knows of no issues created by the existing outdoor sales.

A relatively small additional area for outdoor sales (approx. 530 sq. ft.) would be designated on the sidewalk in front of the market entrance of the expanded store. This type of outdoor storage is typical for grocery stores. It enlivens the front entrance area, and is not an issue unless the displays block or impede pedestrian movements through the area. Sufficient space for pedestrian movements would be provided around the designated sidewalk sales area in front of the store.

Vending Machines: Groups of vending machines are shown on the elevations and site plans near two of the main store entrances, and on either side of the fuel station convenience store entrance. Pursuant to CMC 19.76.120.B.3.c, the *"area occupied by outdoor vending machines may not exceed 10 percent of the width of the building frontage along which they are located, nor shall the combined area of all vending machines at any one site exceed 32 square feet."* A recommended condition would reinforce this and other code limitations on vending machine areas.

Overall, the outdoor sales areas would occupy fixed locations on private property that would not disrupt the normal function and safety of the site or its circulation, and does not encroach upon required parking spaces, driveways, pedestrian walkways, or required landscaped areas. Staff has not identified any issues associated with this aspect of the proposed outdoor sales.

Required Findings: According to Chico Municipal Code Section 19.24.040, following a public hearing, the review authority may approve a use permit application, with or without conditions, only if all of the following findings can be made:

- A. *The proposed use is allowed within the subject zoning district and complies with all of the applicable provisions of Chapter 19.24 (Use Permits).*

Chico Municipal Code Section 19.44.020 provides for drive-through sales in the CR zoning district, subject to use permit approval, and CMC 19.76.120 provides for permanent outdoor retail sales and activities in the CR zoning district, subject to use permit approval. Use Permit 15-10 has been processed in accordance with the requirements of Chapter 19.24.

- B. *The proposed use would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed use.*

The proposed uses consist of the addition of drive-through sales to an expanded retail store and outdoor retail sales within a partially-developed site. All drive-through improvements will be constructed to City standards, which are intended to promote the health, safety, and general welfare of the public. Drive-through access will be from an off-street parking area, so any off-site affects would be minimal. Adequate vehicle circulation and stacking would be provided, and emergency access to the site would remain unobstructed.

The requested outdoor sales in the parking area would not occupy required parking spaces, and the use has existed for several years without any known issues. It does not block any emergency access routes or otherwise disrupt onsite circulation.

The outdoor sales component requested on the storefront sidewalk area would not impede pedestrian circulation. Outdoor parking lot sales are typical at large-format retailers, particularly in front of Garden Centers, and sidewalk displays/sales are typical in front of grocery stores. No detrimental impacts to the health, safety, or welfare of neighborhood workers or residents have been identified in association with the proposed drive-through or outdoor sales uses.

Based on the above, the proposed drive-through and outdoor sales would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed uses.

- C. *The proposed use will not be detrimental and/or injurious to property and improvements in the neighborhood of the proposed use, as well as the general welfare of the City.*

The proposed drive-through and outdoor retail sales would take access from an off-street parking area where the use would be fully contained on the project site, so any off-site affects to property or improvements would be minimal. Adequate vehicle circulation and stacking would be provided, and emergency access to the site would remain unobstructed. Adding new driveways to access the site from Wittmeier Drive will add options for incoming customers, helping to disperse traffic patterns across the site and minimize any issues that may occur at existing points of access.

The outdoor sales component requested on the storefront sidewalk area would not impede pedestrian circulation. Outdoor parking lot sales are typical at large-format retailers, particularly in front of Garden Centers, and sidewalk displays/sales are typical in front of grocery stores. No detrimental impacts to the health, safety, or welfare of neighborhood workers or residents have been identified in association with the proposed drive-through or outdoor sales uses.

Existing regulations require that any public improvements damaged during the course of construction be repaired or reconstructed by the applicant. No other impacts to property or improvements have been identified. Based on the above, the proposed drive-through and outdoor sales would not be detrimental or injurious to property and improvements in the neighborhood, or the general welfare of the City.

- D. *The proposed use will be consistent with the policies, standards, and land use designations established by the General Plan.*

Staff has reviewed the proposed project for overall consistency with the General Plan (see **Attachment L**). The analysis concludes that, while not consistent with every policy contained in the Plan, the project is consistent with the General Plan as a whole.

The proposed drive-through and outdoor sales uses are consistent with the Regional Commercial designation for the site, which accommodates a wide variety of retail uses, including those which are auto-oriented (such as drive-through uses). The uses are also

consistent with the General Plan's emphasis on intensifying and revitalizing commercial uses within the East 20th Street Opportunity Site.

Approval of the project is also consistent with General Plan goals and policies to enhance Chico's long-term prosperity (ED-1), ensure that regulations and permitting processes for the conduct of commerce do not unreasonably inhibit local business activity (ED-1.3). Based on the above, the uses will be consistent with the General Plan.

- E. *The design, location, size, and operating characteristics of the proposed use are compatible with the existing and future land uses in the vicinity.*

The proposed uses consist of the addition of drive-through sales to an expanded retail store and outdoor retail sales within a partially-developed site. All drive-through improvements will be constructed to City standards. Access to the drive-through would be fully-contained on site and compatible with existing and anticipated future commercial development within the area.

The outdoor sales in the parking area has existing for several years without resulting in incompatibilities with other land uses, and other outdoor sales areas would be limited in size, ensuring that they will not dominate building elevations or the result in incompatibilities with existing or future land uses in the vicinity. Based on the above, the design, location, size, and operating characteristics of the proposed use are compatible with the existing and future land uses in the vicinity.

4. Use Permit UP 15-12 – Fuel Station

The proposed fuel station requires use permit authorization. The fuel station would be located near the intersection of Forest Avenue and Wittmeier Drive/Talbert Drive (see **Attachment C, Exhibit V**). Access would be provided by an existing right-in, right-out driveway on Forest Avenue, and a new driveway on Wittmeier Drive. Wide landscape buffers would be included along Forest Avenue and Wittmeier Drive, flanking either side of the relocated Class I bike path. Proposed exterior lighting is comprised of a combination of recessed can lights in the overhead canopy and six new 42-foot tall parking area lights. However, recommended conditions of approval would limit the height of parking area lights to 18 feet.

Staff Analysis and Findings:

Chico Municipal Code standards for gas stations are included as **Attachment O**. The new fuel station would comply with the requirements set forth under CMC 19.76.090, in that it would:

1. Be the first fuel station at the intersection of Forest Avenue and Wittmeier Drive;
2. Be on a site with approximately 2 acres dedicated to the fuel station use, which exceeds the 15,000 sf minimum area required by Code;
3. Have approximately 250 feet of frontage on Wittmeier Drive and approximately 350 feet of frontage on Forest Avenue, which exceeds the Code minimum of 100 feet of frontage on each street;
4. Be situated such that the pump islands are over 15 feet from any property line (approximately 100 feet provided) with at least 3 feet of landscaping along adjacent right-of-way lines (approximately 38 feet to 125 feet provided) and the cashier

- location provides direct visual access to the pump islands and vehicles parked adjacent to the islands;
5. Have one vehicle access point located off a shared private driveway between Wittmeier Drive and the Walmart parking lot, which is not more than two vehicular access points to or from adjacent public rights-of-way;
 6. Not result in curb cuts with less than 30 feet of separation along any street frontage;
 7. Include landscaped areas outside of required setbacks that exceed 10 percent of the net site area (over 33 percent provided), provide a landscape buffer over five feet in width along the front property line (minimum 38 feet provided), and provide a planter area of at least 200 sf at the intersection (subject to a condition limiting landscaping at that location);
 8. Be constructed with exterior lights that are energy-efficient, stationary, and, as conditioned: (a) shielded to ensure that all light is directed away from adjacent properties and public rights-of-way, (b) fully recessed in the case of canopy lights, and (c) not of such a high intensity so as to cause a traffic hazard, be used as an advertising element, or adversely affect adjacent properties, in compliance with CMC Section 19.60.050 (Exterior lighting);
 9. Not include service bays;
 10. Not adjoin any residential zoning districts;
 11. Entail activities and operations conducted entirely within a structure, except for fuel dispensing and sale of items via vending machines in compliance with CMC 19.76.120;
 12. As conditioned, not result any vehicles parked on sidewalks, parkways, driveways, or alleys;
 13. As conditioned, not result in any vehicles parked on the premises for the purpose of vehicular sales;
 14. Include future signage in compliance with code requirements;
 15. As conditioned, not locate used or discarded vehicle parts or equipment, or disabled, junked, or wrecked vehicles in any open area outside of the convenience store structure;
 16. As conditioned, not include noise from bells, loudspeakers, or tools that may exceed the criteria referenced under CMC 19.60.080 (Noise) and shall not be audible from residentially zoned parcels between the hours of 7:00 p.m. and 7:00 a.m. on weekdays and Saturdays, and before 10:00 a.m. and after 7:00 p.m. on Sundays and nationally recognized holidays; and
 17. As conditioned, not receive used motor oil without prior approval by the City Fire Department.

Required Findings: According to Chico Municipal Code Section 19.24.040, following a public hearing, the review authority may approve a use permit application, with or without conditions, only if all of the following findings can be made:

- A. *The proposed use is allowed within the subject zoning district and complies with all of the applicable provisions of Chapter 19.24 (Use Permits).*

Chico Municipal Code Section 19.44.020 provides for gas station uses in the CR zoning district, subject to use permit approval. Use Permit 15-12 has been processed in accordance with the requirements of Chapter 19.24.

- B. *The proposed use would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed use.*

The proposed fuel station will be compatible with surrounding commercial uses, as circulation and access for the use would be accommodated by a new driveway from Wittmeier Drive as well as through an expanded parking area associated with the main retail store. The installation of proposed and required improvements associated with Wittmeier Drive and other access points to the site would result in adequate vehicle access. No residences are located in close proximity to the site. No detrimental impacts to the health, safety, or welfare of neighborhood workers or residents have been identified in association with the proposed fuel station use.

Based on the above, the proposed fuel station would not be detrimental to the health, safety, and general welfare of persons residing or working in the neighborhood of the proposed uses.

- C. *The proposed use will not be detrimental and/or injurious to property and improvements in the neighborhood of the proposed use, as well as the general welfare of the City.*

The proposed fuel station would take access from a new driveway off Wittmeier Drive and an off-street parking area, and sufficient space is provided near the fuel pumps to ensure that vehicle queues would be fully contained on the project site, thereby minimizing potential for any off-site effects to property or improvements. Existing regulations require that any public improvements damaged during the course of construction be repaired or reconstructed by the applicant. Based on the above, the proposed fuel station would not be detrimental or injurious to property and improvements in the neighborhood, or the general welfare of the City.

- D. *The proposed use will be consistent with the policies, standards, and land use designations established by the General Plan.*

Staff has reviewed the proposed project for overall consistency with the General Plan (see **Attachment L**). The analysis concludes that, while not consistent with every policy contained in the Plan, the project is consistent with the General Plan as a whole.

The proposed fuel station use is consistent with the Regional Commercial designation for the site, which accommodates a wide variety of retail uses, including those which are auto-oriented (such as fuel stations). The project is also consistent with the General Plan's emphasis on intensifying and revitalizing commercial uses within the East 20th Street Opportunity Site.

The fuel station represents compatible infill development/redevelopment within a Regional Commercial Opportunity Site, consistent with goals LU-4, LU-5, CD-5, and policies LU-1.3, LU-4.2 and LU-5.1, in that the proposed layout would make use of undeveloped land within a site surrounded by urban development and would not cause significant conflicts with existing or anticipated Regional Commercial uses in the area.

Approving the fuel station use is also consistent with General Plan goals and policies to enhance Chico's long-term prosperity (ED-1), ensure that regulations and permitting processes for the conduct of commerce do not unreasonably inhibit local business activity (ED-1.3).

- E. *The design, location, size, and operating characteristics of the proposed use are compatible with the existing and future land uses in the vicinity.*

The proposed fuel station would be set back from the street intersection, with an ample landscape buffer from Forest Avenue and Wittmeier Drive, and constructed with sufficient circulation to result in compatible operations with nearby land uses. Conditions associated with the design review would ensure that the height of new lighting would be limited, minimizing potential offsite affects. Based on the above, the proposed fuel station would be compatible with existing and future land uses in the vicinity.

ENVIRONMENTAL REVIEW

Background

When applications were received for this project on July 2, 2015, the materials included a request for the City to process an EIR to comply with CEQA requirements. Once the project applications were deemed complete on September 21, 2015, a Notice of Preparation and Public Scoping Meeting was published in the Enterprise Record on October 3, 2015. The public scoping meeting was held on October 15, 2015. Approximately 20 persons attended the scoping meeting, and testimony was received from six speakers. Also, seven comment letters were received in response to the Notice of Preparation.

Following the scoping meeting, a Draft EIR was prepared and released for a 45-day public and agency review period that began on June 17, 2016. On July 21, 2016, a noticed public hearing was held by the Planning Commission to receive comments on the Draft EIR. Approximately 30 persons attended the hearing, and testimony was received from 12 speakers.

The public review period ended on August 1, 2016, and all of the comments received during the review period were compiled into a Final EIR. Responses to comments pertaining to environmental issues were also prepared and included in the Final EIR, as well as an errata section that refines and clarifies specific statements in the Draft EIR. The Planning Commission was previously provided copies of both the Draft EIR and the Final EIR.

The Draft EIR identified several potential environmental impacts associated with the project and included mitigation measures to reduce those impacts to less than significant. In one instance, however, the EIR concluded that approving the project would potentially result in a significant and unavoidable traffic impact for which feasible mitigation is not sufficient to reduce the impact to less than significant. This potential traffic impact is discussed in detail below.

Purpose of CEQA

The intent of CEQA centers around the fundamental concepts of informing governmental decision makers and the public about the potential and significance of environmental effects of proposed activities. Further, CEQA strives to identify ways in which environmental damage can be avoided or significantly reduced. Impacts may be reduced through the implementation of feasible project alternatives or mitigation measures. Lastly, CEQA aims to disclose to the public the reasons why a governmental agency might approve a project even if significant environmental effects could result.

While the fundamental purpose of CEQA is to disclose potential impacts and ensure the incorporation of feasible mitigation measures into a project, the ultimate goal of the legislation is to compel government at all levels to make decisions with environmental consequences in mind. To that end, CEQA does not require technical perfection in an EIR, but rather adequacy, completeness, and a good-faith effort at full disclosure.

With this understanding, staff believes that the EIR for the proposed project has been adequately prepared and represents a good-faith effort at disclosure.

Significant Unavoidable Traffic Impact

The EIR found that the project would conflict with the applicable Congestion Management Program in that operations on southbound SR-99 north of E. 20th Street are projected to degrade to an unacceptable LOS F under Cumulative Plus Project conditions during the p.m. peak hour. According to Caltrans, freeway operations are considered acceptable at LOS E or better on SR-99 north of Skyway.

The project proponent would pay all adopted transportation fees (as required by Mitigation Measure TRANS-3b), but the impact would remain potentially significant and unavoidable because the timing of physical improvements cannot be guaranteed to occur prior to the future traffic volumes that would cause the segment of SR-99 to operate at an unacceptable level of service. As such, the impact must be considered "significant and unavoidable." However, it is important to view this traffic impact in its proper context.

The impact was derived from a computer traffic model that projected cumulative traffic volumes out to the year 2035, using a series of conservative assumptions intended to ensure that any potential impacts are not understated. These modeling assumptions are listed below, each followed by a brief staff analysis.

Trip Generation Rates: Trip generation rates used in the EIR to forecast traffic from the store expansion were derived from field counts at the existing Walmart Store, which were found to be higher than reference rates contained in the Institute of Transportation Engineers (ITE) Trip Generation Manual for "Free Standing Discount Store" or "Free Standing Discount Superstore." Trip generation rates for these store uses take the form of vehicles per 1,000 sf.

Staff Analysis: Using the existing, higher, trip generation rates for the expanded store presupposes that every additional 1,000 sf of floor area will increase traffic in a linear fashion, above the amount associated with the existing store. It is not certain, and cannot be known at this time, that the expanded store will attract new trips from additional customers or additional trips from existing customers, such that traffic from the expanded store increases in linear

fashion based on the amount of added square footage.

Internal Trip Reduction: An internal trip reduction rate of five percent was used in the EIR.

Staff Analysis: A higher internal trip reduction rate could have been used, as was the case in the two prior City EIRs for large-format retail stores with a gas station. For example, the 2009 EIR for a previous Walmart Expansion proposal used an internal trip reduction rate of 10 percent, and the Costco Expansion EIR afforded that project internal trip reduction rates of 48.7 percent and 60.8 percent for the weekday p.m. peak hour and Saturday peak hour, respectively.

Pass-By and Diverted Trip Reductions: A pass-by trip reduction rate of 17 percent and a diverted trip reduction rate of 23 percent were used in the EIR, for a total combined reduction rate of 40 percent for these types of trips.

Staff Analysis: Higher pass-by and diverted reduction rates could have been used. For example, the 2009 EIR for a previous Walmart Expansion proposal used a combined reduction rate for pass-by and diverted trips of 46.4 percent during the a.m./p.m. peak hour and 45.8 percent during the Saturday peak hour for the expanded store.

Cumulative Traffic Projections: The future 2035 cumulative traffic volumes include traffic from projects that have not yet been approved, and traffic from large, approved projects with significant amounts of future development yet to be realized. Examples include the following Special Planning Areas (SPAs) as outlined in the General Plan, as well as development of other undeveloped areas within the City's Sphere of Influence:

- SPA-3 Doe Mill/Honey Run (1,287 acres, 2,095 dwelling units, 374,247 sf non-residential)
- SPA-2 Barber Yard (112 acres, 1,096 dwelling units, 403,882 sf non-residential)
- SPA-5 South Entler (238 acres, 949 dwelling units, 1,348,754 sf non-residential)

Staff Analysis: Provisions for future development of the SPAs listed above are made by the General Plan, and it is in this sense that their development is reasonably foreseeable. However, it is altogether possible, and perhaps likely, that full development of these SPAs will not be realized in the next 19 years. For example, a major issue concerning the future development of the South Entler SPA is the timing of a new SR-99 overpass that will be needed at that location to accommodate full build out, but is not anticipated to be constructed by the City until approximately 2035.

As a result of the conservative traffic generation assumptions outlined above, the LOS projections likely overstate the future potential traffic impacts to some degree, and the significant impact identified is potentially avoidable because it is dependent upon future decisions on development projects. Nonetheless, the methods used for the traffic analysis in the EIR reflect industry standards, expert opinions from transportation engineers, and are valid for the purposes of providing an adequate CEQA analysis.

Since the EIR found that the project will result in a significant unavoidable traffic impact, in order to approve the project the Commission is required to adopt a Statement of Overriding Considerations (SOC). The SOC identifies social, economic or other benefits resulting from

the project which outweigh the unavoidable adverse environmental impacts, and sets forth the specific reasons for proceeding with the project based on the EIR and other information in the record (see **Attachment B, Exhibit I**).

PUBLIC CONTACT

A 10-day public hearing notice was mailed to all landowners and residents within 500 feet of the site, and a legal notice was published in the *Chico Enterprise Record*. Comments received during the circulation period for the DEIR (06/17/16 thru 08/01/16) are included in the Final EIR along with City responses to the issues raised within those comments. Comments received between 08/02/16 and 09/27/16 are include under **Attachment P**.

DISTRIBUTION:

PC Distribution

AP Sawley

Sheppard Mullin Richter & Hampton LLP, Attn: Alexander L. Merritt, Four Embarcadero Center, 17th Floor, San Francisco, CA 94111-4109

Gresham Savage Nolan & Tilden, PC, Attn: Miriam Montesinos, 580 California Street, 12th Floor, San Francisco, CA 94104

RSC Engineering, Attn: Tiffany Wilson, 2250 Douglas Blvd., Ste. 150, Roseville, CA 95661
Walmart Stores Inc., Attn: Michael Allan, 2001 SE 10th Street, Bentonville, AR 72716

ATTACHMENTS:

- A. Planning Commission Resolution No. 16-11
- B. Planning Commission Resolution No. 16-12
 - Exhibit I Statement of Overriding Considerations
 - Exhibit II Mitigation Monitoring and Reporting Program
- C. Planning Commission Resolution No. 16-13
 - Exhibit I Mitigation Monitoring and Reporting Program
 - Exhibit II Parcel Map Conditions of Approval
 - Exhibit III Subdivision Report
 - Exhibit IV Plats to Accompany Use Permit 15-10
 - Exhibit V Plat to Accompany Use Permit 15-12
- D. Location/Notification Map
- E. Vesting Tentative Parcel Map (PM 15-02) (5 sheets)
- F. Site Plan
- G. Landscape Plans (6 sheets)
- H. Architectural Elevations
- I. Materials Board
- J. Exterior Lighting
- K. Applicant's Project Description
- L. General Plan Consistency Analysis
- M. Design Guideline Consistency Analysis
- N. Drive-Through and Outdoor Sales Standards
- O. Gas Station Standards
- P. Public Comments Received 08/02/16 through 09/27/16

1 Final EIR in accordance with CEQA, including City responses to all comments submitted during
2 the Draft EIR public review period, and to fully address all potential effects of implementation of
3 the Project; and

4 WHEREAS, the Final EIR was forwarded to all commenting agencies on or about
5 September 9, 2016, which was not less than ten (10) days prior to the date established for a
6 Planning Commission hearing to consider the proposed Project; and

7 WHEREAS, the Planning Commission of the City of Chico held a duly noticed public
8 hearing to consider the Project.

9 NOW THEREFORE, BE IT RESOLVED by the City of Chico Planning Commission as
10 follows:

11 1. The Planning Commission makes the following findings based upon the entire record
12 before it, including but not limited to the Draft EIR, all documents incorporated by reference
13 therein, all comments received and responses provided, the Findings of Fact and Statement of
14 Overriding Considerations (as provided in Exhibit I to Resolution No. 16-12), the Mitigation
15 Monitoring and Reporting Program (as provided in Exhibit II to Resolution No. 16-12), and all
16 other evidence in the record of these proceedings:

17 A. The recitals set forth above are true and correct.

18 B. The NOP and Draft EIR were duly prepared, noticed, and properly circulated in accordance
19 with the provisions of CEQA.

20 C. All comments received during the period of public review have been duly considered and
21 incorporated into the Final EIR, and when necessary, replied to, all in accordance with
22 CEQA.

23 D. The City provided written responses to all public agency comments received on the Draft
24 EIR at least ten (10) days before certification of the Final EIR, pursuant to the provisions
25 of CEQA.

26 E. A good faith effort has been made to identify potentially feasible mitigation measures and
27 alternatives to the extent necessary to avoid or substantially lessen the significant adverse
28 effects of the project, and such mitigation measures and alternatives were considered in the

1 review process in accordance with the provisions of CEQA.

2 F. The EIR for the proposed Project has been properly completed and has identified all
3 significant environmental effects of the proposed Project, and there are no known potential
4 significant environmental effects that are not addressed in the EIR.

5 G. A good faith effort has been made to seek out and incorporate all points of view in the
6 preparation of the Draft EIR and the Final EIR.

7 H. The Planning Commission has reviewed and considered the information in the EIR.

8 I. The EIR for the Project reflects the City's independent judgment and analysis.

9 2. Certification of the EIR.

10 Having independently considered the EIR, the Planning Commission hereby certifies that
11 the EIR has been prepared, circulated for agency and public review, and completed in
12 compliance with the requirements of CEQA and fully and adequately discloses and addresses
13 all environmental issues known to be associated with the Project.

14 3. The Planning Commission hereby specifies that the materials and documents which
15 constitute the record of proceedings upon which its decision is based are located at and under
16 the custody of the City of Chico Community Development Department.

17 THE FOREGOING RESOLUTION WAS ADOPTED by the Planning Commission of the
18 City of Chico at its meeting held on October 6, 2016, by the following vote:

19 AYES:

20 NOES:

21 ABSENT:

22 ABSTAINED:

23 DISQUALIFIED:

24 ATTEST:

APPROVED AS TO FORM:



25
26 _____
27 Mark Wolfe
28 Planning Commission Secretary

Vincent C. Ewing
City Attorney*

*Pursuant to The Charter of the City of Chico, Section 906(E)

1 Project, CEQA requires the City to adopt a Statement of Overriding Considerations in the event
2 the Planning Commission approves the Project; and

3 WHEREAS, the City is required by Public Resources Code section 21081.6 (a) to adopt a
4 mitigation monitoring and reporting program to ensure that the mitigation measures adopted by
5 the City are actually carried out; and

6 WHEREAS, a Mitigation Monitoring and Reporting Program for the Project has been
7 prepared.

8 NOW, THEREFORE, BE IT RESOLVED by the City of Chico Planning Commission as
9 follows:

- 10 1. The Commission adopts the CEQA Findings of Fact attached at Exhibit I to this resolution,
11 as required by Public Resources Code section 21081, subdivision (a);
- 12 2. The Commission adopts the Statement of Overriding Considerations, included within
13 Exhibit I to this resolution, as required by Public Resources Code section 21081,
14 subdivision (b);
- 15 3. The Commission adopts the Mitigation Monitoring and Reporting Program attached as
16 Exhibit II to this resolution, as required by Public Resources Code section 21081.6,
17 subdivision (a).

18 THE FOREGOING RESOLUTION WAS ADOPTED by the Planning Commission of the
19 City of Chico at its meeting held on October 6, 2016, by the following vote:

20 AYES:

21 NOES:

22 ABSENT:

23 ABSTAINED:

24 DISQUALIFIED:

25 ATTEST:

APPROVED AS TO FORM:

26 _____
27 Mark Wolfe
28 Planning Commission Secretary

Vincent C. Ewing
City Attorney*

*Pursuant to The Charter of the City of Chico, Section 906(E)

**CEQA FINDINGS OF FACT
and
STATEMENT OF OVERRIDING
CONSIDERATIONS**

**I.
INTRODUCTION**

These findings, as well as the accompanying statement of overriding considerations have been prepared in accordance with the California Environmental Quality Act (“CEQA”) the CEQA Guidelines (14 CCR § 15000 et seq.), and the local procedures adopted by the City of Chico (“City”). The City is the lead agency for the environmental review of the project and has the principal responsibility for its approval. The project covered by these findings and the relevant CEQA documents is known as the Chico Walmart Expansion Project (“Project”).

**II.
STATEMENT OF FINDINGS.**

The findings and determinations contained herein are based on the competent and substantial evidence, both verbal and written, contained in the entire record relating to the Project and the EIR. The findings and determinations constitute the independent findings and determinations by the Planning Commission in all respects and are fully and completely supported by substantial evidence in the record as a whole.

Although the findings below identify specific pages within the Draft and Final EIRs in support of various conclusions reached below, the Planning Commission hereby incorporates by reference and adopts as its own, the reasoning set forth in both environmental documents, and thus relies on that reasoning, even where not specifically mentioned or cited herein, in reaching the conclusions set forth below, except where additional evidence is specifically mentioned. This is especially true with respect to the Planning Commission’s approval of mitigation measures recommended in the EIR, and the reasoning set forth in responses to comments in the Final EIR. The Planning Commission further intends that if these findings fail to cross-reference or incorporate by reference any other part of these findings, any finding required or permitted to be made by this Planning Commission with respect to any particular subject matter of the Project must be deemed made if it appears in any portion of these findings or findings elsewhere in the record.

**III.
DEFINITIONS AND ACRONYMS**

“BCAQMD” means Butte County Air Quality Management District

“CDFW” means California Department of Fish and Wildlife

“CEQA” means California Environmental Quality Act.

“City” means City of Chico.

“Commission” or “Planning Commission” means the Planning Commission of the City of Chico.

“Council” or “City Council” means the City Council of the City of Chico.

“DEIR” or “Draft EIR” means the Draft Environmental Impact Report for the Chico Walmart Expansion Project, dated June 17, 2016.

“EIR” means Environmental Impact Report, including both the DEIR and FEIR.
“FEIR” or “Final EIR” means the Final Environmental Impact Report for the Chico Walmart Expansion Project, dated September 9, 2016.
“LOS” means level of service.
“MM” means mitigation measure.
“MMRP” means Mitigation Monitoring and Reporting Program.
“NO_x” means nitrogen oxide.
“NOP” means Notice of Preparation.
“NOP/IS” means Notice of Preparation and Initial Study.
“PM₁₀” means particulate matter equal to or less than 10 microns in diameter.
“ROG” means reactive organic gases.
“RWQCB” means Regional Water Quality Control Board
“SCH” means State Clearinghouse.
“USACE” means U.S. Army Corps of Engineers
“VOC” means Volatile Organic Compounds

IV. **PROJECT DESCRIPTION**

A. PROJECT DESCRIPTION

The City is evaluating the environmental effects of (1) expanding the existing Walmart store located at 2044 Forest Avenue by up to 66,500 square feet; (2) developing an eight-pump fuel station (16 vehicle fueling positions) with a 1,500-square-foot convenience market; and (3) create two outlot parcels that would have a combined development potential of 52,000 square feet of commercial uses. Following the subdivision, the Walmart/fuel station/parking lot parcel (Parcel 1) would total 21.88 acres, Parcel 2 would be 2.63 acres, and Parcel 3 would be 2.57 acres.

Following expansion, the Walmart would be approximately 195,688 square feet. All new square footage would be for indoor uses; no changes to outdoor garden center square footage or outdoor display/sales areas are proposed. The expanded store would provide 55,729 square feet for grocery sales and grocery stockroom area. The remaining square footage would be used for general merchandise sales and storage. In addition to the store expansion, the following uses are anticipated as part of the proposed project:

- Expanded grocery sales;
- Pharmacy/merchandise drive-through;
- Fueling station; Other retail and service tenants; and
- Permanent outdoor display and sales area

New landscaping, parking and updated signage will also be included as part of the Project. End uses for Parcels 2 and 3 have not yet been identified. Based on the allowable uses of the “CR—Regional Commercial” zoning district, it was assumed for purposes of the EIR that these parcels can support retail or restaurant uses at approximately 10,000 square feet per acre. This would yield a building of approximately 26,300 square feet on Parcel 2 and a building of approximately 25,700 square feet on Parcel 3. Future development proposals on these parcels may require additional environmental review depending on the nature of the approvals required.

Discretionary approvals for the Project include approval of a Tentative Parcel Map, Use Permit, Planned Development Permit, and Site Design and Architectural Review. Draft EIR Section 2, Project Description provides a complete description of the Project.

B. PROJECT OBJECTIVES.

The objectives of the proposed Project are to:

1. Positively contribute to the local economy through new capital investment, creation of new employment opportunities, expansion of the tax base, and increased retail offerings.
2. Reinforce Chico's status as a regional retail node by increasing commercial retail and service offerings within an established regional and highway-oriented commercial area.
3. Expand an existing regional-serving retail use close to SR-99 in order to better serve the retail demands of the Market Area, while also minimizing the need for infrastructure improvements.
4. Promote economic growth in accordance with the goals and policies set forth in the City of Chico General Plan.
5. Facilitate the development of undeveloped and underutilized land on an infill site zoned for commercial use in the Chico city limits.
6. Develop complementary fuel station, retail, and restaurant uses that are compatible with surrounding land uses and which provide consumers with additional convenient and competitive options.
7. Design a site plan to minimize overall access and circulation conflicts, such as facilitation of the circulation between the store, service station and future development on the adjacent parcel.
8. Enhance bicycle and pedestrian circulation by relocating the existing Class I bicycle/pedestrian path around the perimeter of the site in order to minimize conflicts with motor vehicles.
9. Improve the overall visual appearance of the area by removing two outdated and unsightly billboards and developing new commercial uses that employ high-quality contemporary architecture and landscaping.

C. PROJECT APPROVALS.

In order to implement the Project, the Project proponent will need the following approvals from the City:

- Tentative Parcel Map
- Use Permit (for drive-in/drive-through sales and gas station)
- Planned Development Permit
- Site Design and Architectural Review
- Final Map Approval or Recordation
- Building and grading permits

V.
ENVIRONMENTAL REVIEW PROCESS

In accordance with Section 15082 of the California Environmental Quality Act (CEQA) Guidelines, the City issued a Notice of Preparation (NOP) of an EIR on October 6, 2015 (SCH# 2015102017). This notice was circulated to the public, local, State, and Federal agencies, and other interested parties to solicit comments on the proposed Project.

Pursuant to CEQA Guidelines Section 15082(c)(1), the City of Chico held a public scoping meeting for the proposed Project on Thursday, October 15, 2015 in the Chico City Council Chambers, 421 Main Street, Chico, CA 95927. The meeting was duly noticed in the NOP that was posted on the City's website and directly mailed to public agencies and private parties, as well as in a public notice printed in the Chico Enterprise-Record. Approximately 20 persons attended the meeting and six persons provided oral testimony.

The EIR includes an analysis of the following issue areas:

- Aesthetics, Light and Glare
- Air Quality/Greenhouse Gas Emissions
- Biological Resources
- Cultural Resources
- Geology, Soils and Seismicity
- Hazards and Hazardous Materials
- Hydrology and Water Quality
- Land Use
- Noise
- Public Services and Utilities
- Transportation
- Urban Decay

The Draft EIR determined that the Project would have no impact on Agriculture and Forest Resources, Mineral Resources, Population and Housing or Recreation. Therefore, impacts to these areas were not further studied in the EIR.

The City published the DEIR for public and agency review. The public review period was 45 days, beginning June 17, 2016, and ending on August 1, 2016. The City received a number of comment letters from agencies and the public regarding the DEIR. In September 2016, the City published a Final EIR for the Project.

VI.
RECORD OF PROCEEDINGS

The record of proceedings for the decision on the Project consists of the following documents, at a minimum:

- The Notice of Preparation dated October 6, 2015, and all other public notices issued by the City in conjunction with the Project;
- Oral testimony received at the October 15, 2015 public scoping meeting;
- All applications for approvals and development entitlements related to the Project and submitted to the City;
- Comments received on the Notice of Preparation issued by the City;
- The DEIR and all appendices to the DEIR for the Project;
- Notices of Completion and of Availability, providing notice that the DEIR had been completed and was available for public review and comment;
- All comments submitted by agencies or members of the public during the comment period on the DEIR;
- All comments and correspondence submitted to the City with respect to the Project, in addition to timely comments on the DEIR;
- The Final EIR for the Project dated September 2016, including all documents referred to or relied upon therein, and documents relied upon or referenced in these findings, which include, but are not limited to the following:
 - All timely comments received on the DEIR and responses to those comments;
 - All Technical appendices to the EIR;
 - Letters and correspondence submitted to the City following the release of the FEIR;
 - The Mitigation Monitoring and Reporting Program for the Project;
- The Notices of Public Hearing issued in connection with Planning Commission hearings on the Project.
- All findings and resolutions adopted by the City in connection with the Project approvals, and all documents cited or referred to therein;
- All reports, studies, memoranda (including internal memoranda not protected by the attorney-client privilege), maps, staff reports, or other planning documents relating to the Project prepared by the City, consultants to the City, or responsible or trustee agencies with

respect to the City's compliance with the requirements of CEQA and with respect to the City's action on the Project;

- All reports, studies, memoranda, maps, staff reports, or other planning documents related to the Project cited or referenced in the preparation of the DEIR or FEIR;
- All documents submitted to the City by other public agencies or members of the public in connection with the Project, up through the close of the public hearing.
- Any documentary or other evidence submitted to the City at any other information sessions, public meeting or public hearing;
- The relevant files of the City of Chico Planning Services Department for the Project;
- The relevant City files and the materials submitted by the Project applicant;
- The City of Chico General Plan and Chico Municipal Code;
- Matters of common knowledge to the City including, but not limited to Federal, State, and local laws and regulations;
- Any documents expressly cited in these findings, in addition to those cited above; and
- Any other materials required for the record of proceedings by Public Resources Code Section 21167.6(e).

The official custodian of the record is the Planning Services Director of the City of Chico, located at 411 Main Street, Chico, CA 95928.

VII. **FINDINGS REQUIRED UNDER CEQA**

Public Resources Code section 21002 provides that “public agencies should not approve projects as proposed if there are feasible alternatives or feasible mitigation measures available which would substantially lessen the significant environmental effects of such projects[.]” The same statute states that the procedures required by CEQA “are intended to assist public agencies in systematically identifying both the significant effects of proposed projects and the feasible alternatives or feasible mitigation measures which will avoid or substantially lessen such significant effects.” Section 21002 goes on to state that “in the event [that] specific economic, social, or other conditions make infeasible such project alternatives or such mitigation measures, individual projects may be approved in spite of one or more significant effects thereof.”

The mandate and principles announced in Public Resources Code section 21002 are implemented, in part, through the requirement that agencies must adopt findings before approving projects for which EIRs are required. (See Pub. Resources Code, § 21081, subd. (a); CEQA Guidelines, § 15091, subd. (a).) For each significant environmental effect identified in an EIR for a proposed project, the approving agency must issue a written finding reaching one or more of three permissible conclusions. The first such finding is that “[c]hanges or alterations have been required

in, or incorporated into, the project which avoid or substantially lessen the significant environmental effect as identified in the final EIR.” (CEQA Guidelines, § 15091, subd. (a)(1)).

The second permissible finding is that “[s]uch changes or alterations are within the responsibility and jurisdiction of another public agency and not the agency making the finding. Such changes have been adopted by such other agency or can and should be adopted by such other agency.” (CEQA Guidelines, § 15091, subd. (a)(2))

The third potential conclusion is that “[s]pecific economic, legal, social, technological, or other considerations, including provision of employment opportunities for highly trained workers, make infeasible the mitigation measures or project alternatives identified in the final EIR.” (CEQA Guidelines, § 15091, subd. (a)(3).) Public Resources Code section 21061.1 defines “feasible” to mean “capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, social and technological factors.”

The CEQA Guidelines do not define the difference between “avoiding” a significant environmental effect and merely “substantially lessening” such an effect. The City must therefore glean the meaning of these terms from the other contexts in which the terms are used. Public Resources Code section 21081, on which CEQA Guidelines section 15091 is based, uses the term “mitigate” rather than “substantially lessen.” The CEQA Guidelines therefore equate “mitigating” with “substantially lessening.” Such an understanding of the statutory term is consistent with the policies underlying CEQA, which include the policy that “public agencies should not approve projects as proposed if there are feasible alternatives or feasible mitigation measures available which would substantially lessen the significant environmental effects of such projects.” (Pub. Resources Code, § 21002.)

For purposes of these findings, the term “avoid” refers to the effectiveness of one or more mitigation measures to reduce an otherwise significant effect to a less than significant level. In contrast, the term “substantially lessen” refers to the effectiveness of such measure or measures to substantially reduce the severity of a significant effect, but not to reduce that effect to a less than significant level.

Although CEQA Guidelines section 15091 requires only that approving agencies specify that a particular significant effect is “avoid[ed] or substantially lessen[ed],” these findings, for purposes of clarity, will specify whether the effect in question has been reduced to a less than significant level, or has been substantially lessened but remains significant. Moreover, although section 15091, read literally, does not require findings to address environmental effects that an EIR identifies as merely “potentially significant,” these findings will nevertheless fully account for all such effects identified in the FEIR.

CEQA requires that the lead agency adopt mitigation measures or alternatives, where feasible, to substantially lessen or avoid significant environmental impacts that would otherwise occur. Project modification or alternatives are not required, however, where such changes are infeasible or where the responsibility for modifying the project lies with some other agency. (CEQA Guidelines, § 15091, subd. (a)) With respect to a project for which significant impacts are not avoided or substantially lessened, a public agency, after adopting proper findings, may nevertheless approve the project if the agency first adopts a statement of overriding considerations setting forth the specific reasons why the agency found that the project’s “benefits” rendered “acceptable” its

“unavoidable adverse environmental effects.” (CEQA Guidelines, §§ 15093, 15043, subd. (b); see also Pub. Resources Code, § 21081, subd. (b))

These findings constitute the City’s best efforts to set forth the evidentiary and policy bases for its decision to approve the Project in a manner consistent with the requirements of CEQA. To the extent that these findings conclude that various proposed mitigation measures outlined in the EIR are feasible and have not been modified, superseded or withdrawn, the City hereby binds itself to require implementation of these measures. These findings, in other words, are not merely informational, but rather constitute a binding set of obligations that will come into effect when the City adopts a resolution approving the Project.

VIII. **MITIGATION MONITORING AND REPORTING PROGRAM**

A Mitigation Monitoring and Reporting Program (MMRP) has been prepared for the Project, and is being approved by the Planning Commission by the same resolution that adopts these findings. The City will use the MMRP to track compliance with Project mitigation measures. The MMRP will remain available for public review during the compliance period. The MMRP is a separate document from the EIR.

IX. **FINDINGS REGARDING ENVIRONMENTAL EFFECTS AND MITIGATION MEASURES**

The DEIR identified a number of significant and potentially significant environmental effects (or impacts) that the Project may cause. Some of these significant impacts can be reduced to a level of less than significant through the adoption of feasible mitigation measures. Others cannot be reduced to a less than significant level and will be significant and unavoidable. For the reasons set forth in Section XI, *infra*, however, the City has determined that overriding economic, social or other considerations outweigh the significant, unavoidable effects of the Project.

The City finds that all impacts related to Aesthetics, Light and Glare, Hazards and Hazardous Materials, Land Use, Public Services and Utilities and Urban Decay were all determined to be less than significant without the need for mitigation.

The City’s findings with respect to Project impacts requiring mitigation within the following topical areas are made below. With the exception of these identified impacts, the City finds that other impacts within these topical areas do not require mitigation and are less than significant. Likewise, unless otherwise specifically identified below, all cumulative impacts within these impact areas were determined to be less than significant.

A. Air Quality/Greenhouse Gas Emissions

- 1. Impact AIR-1:** The DEIR found that the Project could potentially conflict with or obstruct implementation of the applicable air quality plan. The Butte County portion of the Sacramento Air Basin is currently designated non-attainment for the state and federal ozone and PM_{2.5} standards and the PM₁₀ state standards. The current applicable air quality plan for the BCAQMD is the Northern Sacramento Valley Planning Area 2015 Triennial Air Quality Plan (**DEIR at 3.2-21 to 3.2-23**).

The DEIR found that compliance with Butte County Air Quality Management District Rules 205, 230 and 231, along with best management practices to minimize fugitive dust and diesel emissions, would reduce impacts to less than significant. These requirements are contained in Mitigation Measure AIR-1.

- a. Finding Regarding Impact AIR-1 and Mitigation Measure AIR-1:** The incorporation of Mitigation Measure AIR-1 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

- 2. Impact AIR-2:** The DEIR found that the Project could potentially violate an air quality standard or contribute substantially to an existing or projected air quality violation; specifically, with regard to the Project's anticipated construction and operational emissions of ROG and NO_x. Mitigation Measure AIR-2a has been provided, that requires the applicant to use low VOC paint for the interior of all proposed structures, and Mitigation Measure AIR-2b has been provided that requires the applicant to utilize off-road diesel equipment that meets the Tier 3 emission standards for all off-road equipment that is greater than 50 horsepower. Mitigation Measures AIR-2c through AIR-2j have been provided to reduce operational ROG and NO_x emissions (such as through preparation of a Transportation Demand Management Program), and Mitigation Measure AIR-2k would require the Project applicant to participate in an Off-site Mitigation Program in order to reduce ROG and NO_x operational emissions to less than significant levels, consistent with the BCAQMD's CEQA Handbook and current practices. As a result, the DEIR found that incorporation of Mitigation Measures AIR-2a through AIR-2k would reduce these impacts to less than significant. **(DEIR at 3.2-23 to 3.2-39).**
 - a. Finding Regarding Impact AIR-2 and Mitigation Measures AIR-2a through AIR-2k:** The incorporation of Mitigation Measures AIR-2a through AIR-2k into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that these mitigation measures be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

- 3. Impact AIR-3:** The DEIR found that the Project may result in a cumulatively considerable net increase of criteria pollutants for which the Project region is in nonattainment under an applicable federal or state ambient air quality standard; specifically, due to the Project's PM_{2.5} and PM₁₀ emissions. However, the DEIR found that incorporation of Mitigation Measures AIR-2a through AIR-2k (discussed above) would reduce these impacts to less than significant. **(DEIR at 3.2-40).**

- a. Finding Regarding Impact AIR-3 and Mitigation Measures AIR-2a through AIR-2k:** The incorporation of Mitigation Measures AIR-2a through AIR-2k into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that these mitigation measures be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.
- 4. Impact AIR-6:** The DEIR concluded that the Project would generate direct and indirect greenhouse gas emissions; but that after implementation of Mitigation Measures AIR-2c through AIR-2j, the Project would be within the Climate Action Plan's target of a 44.5 percent reduction in greenhouse gas emissions over BAU by 2020, and impacts would be less than significant. **(DEIR at 3.2-52 to 3.2-57).**

 - a. Finding Regarding Impact AIR-6 and Mitigation Measures AIR-2c through AIR-2j:** The incorporation of Mitigation Measures AIR-2a through AIR-2j into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that these mitigation measures be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.
- 5. Impact AIR-7:** The DEIR found that the Project could potentially conflict with an applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gases, but that implementation of Mitigation Measures AIR-2a through AIR-2k would ensure compliance with the City of Chico 2020 Climate Action Plan and reduce operational greenhouse gas and criteria pollutant emissions to less than significant levels, consistent with the BCAQM CEQA Handbook and City's Climate Action Plan **(DEIR at 3.2-57 to 3.2-60).**

 - a. Finding Regarding Impact AIR-7 and Mitigation Measures AIR-2a through AIR-2k:** The incorporation of Mitigation Measures AIR-2a through AIR-2k into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that these mitigation measures be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

B. Biological Resources

1. **Impact BIO-1:** The DEIR found that the Project could potentially adversely impact special-status plant species, due to the presence of non-native annual grassland which provides habitat for a number of such species. Mitigation Measure BIO-1 will require the preparation of focused botanical surveys prior to the issuance of any grading or construction permits. If any species are identified, avoidance strategies will be implemented, but if avoidance is not possible then mitigation will take place in consultation with the California Department of Fish and Wildlife (CDFW). CDFW would need to approve the mitigation plan prior to transplantation and commencement of construction activities. The DEIR found that incorporation of Mitigation Measure BIO-1 would reduce these impacts to less than significant. **(DEIR at 3.3-24 to 3.3-26).**

a. **Finding Regarding Impact BIO-1 and Mitigation Measure BIO-1:** The incorporation of Mitigation Measure BIO-1 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

2. **Impact BIO-2:** The DEIR found that the Project could potentially impact special-status wildlife species including the western spadefoot, burrowing owl, Swainson's hawk, bats, migratory birds, and vernal pool shrimp species. Mitigation Measures BIO-2a to BIO-2d require the preparation of protocol-level surveys and preconstruction surveys by a qualified biologist, as well as avoidance and relocation procedures if special-status species are found, in coordination with CDFW. Mitigation Measure BIO-2e requires monitoring by a qualified biologist for any activities that may impact seasonal wetland. **(DEIR at 3.3-26 to 3.3-35).** These DEIR found that incorporation of Mitigation Measures BIO-2a to BIO-2e would reduce impacts to special status wildlife species to less than significant.

a. **Finding Regarding Impact BIO-2 and Mitigation Measures BIO-2a to BIO-2e:** The incorporation of Mitigation Measures BIO-2a to BIO-2e into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that these mitigation measures be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

3. **Impact BIO-3:** The DEIR found that the Project could potentially impact 0.02 acres of depression seasonal wetland. No direct impacts to the wetland are proposed, and indirect impacts would be avoided during certain phases of the Project pursuant to Mitigation Measure BIO-2e. Two Project features are proposed within 50 feet of the seasonal wetland: one of the poles associated with the relocated high-voltage overhead power lines, and the relocated Class I bicycle/ pedestrian path. Mitigation Measure BIO-3 is provided, which requires relocating the power line pole and bike

path a minimum of 50 feet from the wetland such that the indirect impact measures of Mitigation Measure BIO-2e can be implemented. With these changes to the Project, no direct or indirect impacts to the seasonal wetland would occur during construction of all Project features currently proposed for construction. For any future Project phases that necessitate work within 50 feet of the seasonal wetland, such as development of Parcel 2 or other Project feature, the developer shall implement Mitigation Measure BIO-4, which requires the applicant to obtain the requisite approvals from USACE and RWQCB. The DEIR found the implementation of Mitigation Measures BIO-2e, BIO-3, and BIO-4 would reduce this potential impact to a level of less than significant. (DEIR at 3.3-35 to 3.3-36).

a. Finding Regarding Impact BIO-3 and Mitigation Measures BIO-2e BIO-3 and BIO-4: The incorporation of Mitigation Measures BIO-2e, BIO-3 and BIO-4 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that these mitigation measures be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

4. Impact BIO-4: The DEIR found that the Project may potentially impact 0.02 acres of federally-protected wetlands. No development is proposed that would directly impact the wetland. Mitigation Measures BIO-2e and BIO-3 would require relocating all known Project features at least 50 feet away from the wetland such that indirect impact avoidance measures can be implemented during construction. However, the future development plans for Parcel 2 are not known at this time and could result in direct and/or indirect impacts to the wetland. If construction is necessary within 50 feet of the wetland, then there is potential for direct or indirect impacts to the federally protected depression seasonal wetland, Mitigation Measure BIO-4 requires the Project applicant to obtain a Section 404 Clean Water Act Permit from the USACE and a Section 401 Water Quality Certification from the RWQCB prior to the issuance of any construction, grading, or demolition permits that entail work within 50 feet of the wetland. If the USACE or RWQCB determine that no permit from their agency is required for a particular phase of the Project, then Mitigation Measure BIO-4 provides that letters to that effect from each agency would satisfy the mitigation for the Project phase in question. Regarding any direct impacts to the wetland, the mitigation requires that any Waters of the U.S. or jurisdictional wetlands that would be lost or disturbed shall be replaced or rehabilitated on a "no-net loss" basis in accordance with the USACE mitigation guidelines or permit conditions. The DEIR found the implementation of Mitigation Measure BIO-4 would reduce impacts to a level of less than significant. (DEIR at 3.3-36 to 3.3-37).

a. Finding Regarding Impact BIO-4 and Mitigation Measure BIO-4: The incorporation of Mitigation Measure BIO-4 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have been

required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

C. Cultural Resources

1. **Impact CUL-1:** The Cultural Resources Assessment indicated that there were no records of historic resources or evidence of such resources in the Project area. Nonetheless, there is always the possibility that previously unknown historic resources exist below the ground surface within the Project site. The DEIR found that subsurface construction activities associated with the proposed Project may damage or destroy previously undiscovered historic resources. The DEIR found the implementation of standard cultural resources construction mitigation as required under Mitigation Measure CUL-1 would reduce this impact to less than significant. **(DEIR at 3.4-9).**

a. **Finding Regarding Impact CUL-1 and Mitigation Measure CUL-1:** The incorporation of Mitigation Measure CUL-1 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

2. **Impact CUL-2:** The Cultural Resources Assessment indicated that there were no records of archaeological resources or evidence of such resources in the Project area. Nonetheless, there is always the possibility that previously unknown archaeological resources exist below the ground surface within the Project site. The DEIR found that subsurface construction activities associated with the proposed Project may damage or destroy previously undiscovered archaeological resources. However, the DEIR found the implementation of standard cultural resources construction mitigation as required under Mitigation Measure CUL-1 would reduce this impact to less than significant. **(DEIR at 3.4-10).**

a. **Finding Regarding Impact CUL-2 and Mitigation Measure CUL-1:** The incorporation of Mitigation Measure CUL-1 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

3. **Impact CUL-3:** The Cultural Resources Assessment indicated that the northwest corner of the Project area may extend onto the Modesto Formation, which is a potentially fossiliferous unit that may be impacted by Project-related excavations. However, this portion of the Project area is built with the existing Walmart store and infrastructure, and, thus, the upper soil layers were previously disturbed by grading and earthwork activities. The Cultural Resources Assessment concluded that it is highly unlikely that any significant paleontological resources would be encountered. However, if significant paleontological resources are discovered, the

DEIR found the implementation of Mitigation Measure CUL-3 will reduce impacts to a level of less than significant. (DEIR at 3.4-10 to 3.4-11).

- a. Finding Regarding Impact CUL-3 and Mitigation Measure CUL-3:** The incorporation of Mitigation Measure CUL-1 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.
- 4. Impact CUL-4:** The Cultural Resources Assessment indicated that there were no records of Native American burial sites or evidence that human remains are known to exist within the Project area. However, there is always the possibility that subsurface construction activities associated with the Project, such as trenching and grading, could potentially damage or destroy previously undiscovered human remains. Accordingly, this is a potentially significant impact. However, if human remains are discovered, the DEIR found the implementation of Mitigation Measure CUL-4 would reduce impacts to a level of less than significant, through contact with the Butte County Coroner and the Native American Heritage Commission, as well as compliance with State law. (DEIR at 3.4-11 to 3.4-12).
 - a. Finding Regarding Impact CUL-4 and Mitigation Measure CUL-4:** The incorporation of Mitigation Measure CUL-4 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

D. Geology, Soils and Seismicity

- 1. Impact GEO-2:** The Project would involve grading, building construction, and paving activities that could result in erosion and sedimentation. Left unabated, the accumulation of sediment in downstream waterways could result in the blockage of flows, potentially causing increased localized ponding or flooding. However, the Project will be subject to existing city regulations requiring implementation of stormwater quality control measures during construction activities. These pollution prevention practices include erosion control measures such as sediment traps and vegetating disturbed areas, which would prevent soil and sediment from entering downstream waterways. The DEIR found the incorporation of Mitigation Measure HYD-1a would reduce impacts to less than significant. (DEIR at 3.5-8).
 - a. Finding Regarding Impact GEO-2 and Mitigation Measure HYD-1:** The incorporation of Mitigation Measure HYD-1 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into

the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

E. Hydrology and Water Quality

- 1. Impact HYD-1:** The DEIR found that construction and operational activities associated with the Project have the potential to degrade water quality in downstream water bodies. Mitigation Measure HYD-1a is proposed, which would require the Project applicant to prepare and implement a Stormwater Pollution Prevention Plan (SWPPP). The implementation of this mitigation measure would ensure that potential, short-term, construction water quality impacts are reduced to a level of less than significant. The City will also require the Project applicant to prepare a Stormwater Mitigation Plan (SWMP) for review and approval that identifies BMPs necessary to control stormwater pollution from operational activities and facilities, and provide for appropriate maintenance over time. The SWMP would include design concepts that are intended to accomplish a "first flush" objective that would remove contaminants from the first 2 inches of stormwater before it enters area waterways. To ensure that stormwater quality measures are implemented, Mitigation Measure HYD-1b is proposed, which would require the Project applicant to prepare and submit a SWMP to the City of Chico for review and approval. The implementation of this mitigation measure would ensure that potential, long-term, operational water quality impacts are reduced to a level of less than significant. **(DEIR at 3.7-8 to 3.7-11).**
 - a. Finding Regarding Impact HYD-1 and Mitigation Measures HYD-1a and HYD-1b:** The incorporation of Mitigation Measure HYD-1a and HYD-1b into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that these mitigation measures be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

F. Noise

- 1. Impact NOI-1:** The DEIR found that the Project could result in exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance during construction activities. Mitigation Measure NOI-1 would require noise attenuation measures such as limitations on construction hours, equipment specifications and temporary noise barriers. The DEIR found the implementation of Mitigation Measure NOI-1 would reduce impacts to less than significant. **(DEIR at 3.9-28 to 3.9-36).**
 - a. Finding Regarding Impact NOI-1 and Mitigation Measure NOI-1:** The incorporation of Mitigation Measure NOI-1 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have

been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

2. **Impact NOI-4:** The DEIR found that the Project would result in temporary increases in noise levels above ambient noise levels, due to the transport of workers and movement of construction materials to and from the Project site, or from the noise generated on-site during site preparation, grading, building construction, paving, and painting activities. The DEIR found that compliance with the City's permissible time periods for construction activities, as well as implementing the best management noise reduction techniques and practices outlined in Mitigation Measure NOI-1, would ensure that potential short-term construction noise impacts on sensitive receptors in the Project vicinity would be reduced to less than significant. (DEIR at 3.9-47 to 3.9-48).

- a. **Finding Regarding Impact NOI-4 and Mitigation Measure NOI-1:** The incorporation of Mitigation Measure NOI-1 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

G. Transportation

1. **Impact TRANS-3:** The DEIR concluded that the intersection of E. 20th St./Forest Avenue would be degraded from an acceptable LOS E under Cumulative No Project conditions to an unacceptable LOS F under Cumulative Plus Project conditions. The City is currently contemplating modifying plans to include roundabouts instead of roadway widening along E. 20th Street, however, the roundabout alternative has not been formally incorporated into the City's Capital Improvement Program. Mitigation Measure TRANS-3b requires the Project applicant to pay fair-share costs of off-site circulation improvements at E. 20th Street/Forest Avenue. Under either improvement option (restriping or roundabout), the intersection would operate at acceptable levels after mitigation. Therefore, the residual significance of this impact would be less than significant. To mitigate the impact, the applicant shall provide the City of Chico with the requisite funds for improving the intersection of E 20th Street/Forest Avenue. The improvements shall consist of: a) restriping the northbound approach (Forest Avenue) to provide a left-turn lane, a shared left-turn/ through lane, a through lane, and a right-turn lane, and b) modifying the signal operation to provide split phasing on the northbound and southbound approaches. The City of Chico shall implement the improvements when monitoring determines that the intersection operation is approaching unacceptable levels. This improvement is reflected in Mitigation Measure TRANS-3a. (DEIR at 3.11-57 to 3.11-65).

- a. **Finding Regarding Impact TRANS-3 and Mitigation Measures TRANS-3a and TRANS-3b:** The incorporation of Mitigation Measures TRANS-3a and TRANS-3b into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that these mitigation measures be required in or incorporated into the

Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

2. **Impact TRANS-4:** The DEIR found that the Project would conflict with the applicable Congestion Management Program. Operations on southbound SR-99 north of E. 20th Street are projected to degrade from an acceptable LOS E under Cumulative No Project conditions to an unacceptable LOS F under Cumulative Plus Project conditions during the PM peak hour. The proposed Project would pay all adopted transportation fees (as required by Mitigation Measure TRANS-3b), but the impact would remain potentially significant and unavoidable because the timing of physical intersection improvements cannot be guaranteed to occur prior to the future traffic volumes that would cause the segment of SR-99 to operate at an unacceptable level of service. *As such, the residual significance of this impact would be significant and unavoidable.* (DEIR at 3.11-66 to 3.11-81).

- a. **Finding Regarding Impact TRANS-4 and Mitigation Measure TRANS-3b:** The Project's cumulative contribution to the impacts on southbound SR-99 north of E. 20th Street can be minimized through Mitigation Measure TRANS-3b. The City hereby directs that this mitigation measure be required in or incorporated into the Project. However, implementation of that mitigation will not reduce those impacts to less than significant.

3. **Impact TRANS-5:** The DEIR found that the Project could potentially increase hazards due to a design feature; specifically, proposed roadway modifications on Wittmeier Drive could potentially affect access to the Project site and Wittmeier Auto Center while increasing hazards along the roadway. The current lane configuration may impede vehicular access and truck loading from Wittmeier Drive into the Wittmeier Auto Center south of the roadway, particularly at its easternmost driveway. Vehicular conflicts may also occur as drivers attempt to navigate the cul-de-sac in tandem with vehicles arriving and departing from the westernmost Walmart driveway. Also, the westernmost driveway being directly aligned with the Wittmeier Drive travel lanes may cause vehicular conflicts for vehicles turning in and out of the Wittmeier Auto Center driveways. Mitigation Measure TRANS-5 provides two potential options for addressing these potential conflicts, subject to review and approval by the City of Chico. The DEIR found that with implementation of one of the specified options, impacts would be reduced to less than significant. (DEIR at 3.11-82 to 3.11-84).

- a. **Finding Regarding Impact TRANS-5 and Mitigation Measure TRANS-5:** The incorporation of Mitigation Measure TRANS-5 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact's significant effects on the environment.

4. **Impact TRANS-7:** The DEIR found that the Project may conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities. The existing Class I path would be relocated to near the edge of the Project site, paralleling Forest Avenue, Wittmeier Drive, and SR-99. While the proposed Project includes a portion of the planned SR-99 Bikeway, approximately 75 feet of the bikeway located between the southwest corner of the proposed bicycle path and the southern edge of the Project site are not included in the proposed Project. Without an accommodation of a potential future extension of the City’s planned SR-99 Bikeway to the southern edge of the Project site, this is considered a potentially significant impact. To mitigate this impact, the Project shall be modified to include an easement for a potential future Class I bikeway connection that would extend south from the southwesterly corner of the Project site. With the ability reserved by the City to establish a future connection to this planned bikeway facility, this potential impact would be reduced to a level of less than significant. This recommendation is reflected in Mitigation Measure TRANS-7. The DEIR found the implementation of Mitigation Measures TRANS-7 would reduce this potential impact to less than significant. **(DEIR at 3.11-85 to 3.11-87).**

a. **Finding Regarding Impact TRANS-7 and Mitigation Measure TRANS-7:** The incorporation of Mitigation Measure TRANS-7 into the Project will ensure that these impacts are reduced to less than significant. The City hereby directs that this mitigation measure be required in or incorporated into the Project. The City therefore finds that changes or alterations have been required in, or incorporated into the Project that substantially lessen or avoid this impact’s significant effects on the environment.

X. PROJECT ALTERNATIVES

A. ALTERNATIVES ANALYSIS

The CEQA Guidelines require that an EIR describe a reasonable range of alternatives to a project that would feasibly attain the basic project objectives but would avoid or substantially lessen one or more of the project’s significant effects (CEQA Guidelines Section 15126.6(a)).

Section 15126.6 of the CEQA Guidelines requires the consideration of a reasonable range of potentially feasible alternatives that could reduce or eliminate any significant adverse environmental effects of the proposed project, including alternatives that may, to some degree, impede the project’s objectives.

Public Resources Code section 21002 provides that “public agencies should not approve projects as proposed if there are feasible alternatives or feasible mitigation measures available which would substantially lessen the significant environmental effects of such projects[.]” The procedures required by CEQA “are intended to assist public agencies in systematically identifying both the significant effects of proposed projects and the feasible alternatives or feasible mitigation measures which will avoid or substantially lessen such significant effects.”

“[I]n the event [that] specific economic, social, or other conditions make infeasible such project alternatives or such mitigation measures, individual projects may be approved in spite of one or more significant effects.”

Among the factors that may be taken into account when addressing the feasibility of alternatives are site suitability, economic viability, availability of infrastructure, general plan consistency, other plans or regulatory limitations, jurisdictional boundaries, and whether the proponent can reasonably acquire, control or otherwise have access to the alternative site. (CEQA Guidelines, § 15126.6, subd. (f)(1)) The concept of “feasibility” also encompasses the question of whether a particular alternative or mitigation measure promotes the underlying goals and objectives of a project.

Where a significant impact can be substantially lessened (i.e., mitigated to an “acceptable level”) solely by the adoption of mitigation measures, the lead agency, in drafting its findings, has no obligation to consider the feasibility of alternatives with respect to that impact, even if the alternative would mitigate the impact to a greater degree than the project. (Pub. Resources Code, § 21002) In short, CEQA requires that the lead agency adopt mitigation measures or alternatives, where feasible, to substantially lessen or avoid significant environmental impacts that would otherwise occur. Project modification or alternatives are not required, however, where such changes are infeasible or where the responsibility of modifying the project lies with some other agency. (CEQA Guidelines, § 15091, subds. (a), (b))

With respect to a project for which significant impacts are not avoided or substantially lessened, a public agency, after adopting proper findings, may nevertheless approve the project if the agency first adopts a statement of overriding considerations setting forth the specific reasons why the agency found the project’s “benefits” rendered “acceptable” its “unavoidable adverse environmental effects.” (CEQA Guidelines, §§ 15093, 15043, subd. (b); see also Pub. Resources Code, § 21081, subd. (b))

The discussion regarding Project impacts in Section IX, above, reveals that most significant effects identified in the EIR will be reduced to less than significant through the incorporation of mitigation measures. There remain, however, some effects which cannot be substantially lessened and will remain significant and unavoidable. Specifically, the Project would have a significant and unavoidable impact to transportation, due to generating new trips to the segment of southbound State Route 99 between State Route 32 and E. 20th Street that would operate at unacceptable levels under Cumulative Plus Project conditions. Thus, the City, in considering alternatives in these findings, need only determine whether any alternatives are environmentally superior with respect to this identified transportation impact. If any alternatives are in fact superior with respect to this impact, the City is then required to determine whether the alternatives are feasible. If the City determines that no alternative is both feasible and environmentally superior with respect to the unavoidable significant impacts identified in the DEIR, the City may approve the Project as mitigated, after adopting a Statement of Overriding Considerations.

The Draft EIR discussed several alternatives to the Project in order to present a reasonable range of options. The alternatives evaluated included:

- (1) No Project Alternative;
- (2) Walmart Expansion Only Alternative; and
- (3) Reduced Intensity Alternative.

Significant and Unavoidable Impacts

The Project-specific significant and unavoidable impacts that would result from Project implementation are:

- **Regional Facilities:** The proposed Project would contribute new trips to the segment of southbound State Route 99 between State Route 32 and E. 20th Street that would operate at unacceptable levels under Cumulative Plus Project conditions. Feasible mitigation measures are proposed to lessen the severity of impacts; however, the residual significance of this impact would be significant and unavoidable.

The EIR examined the Project alternatives in detail, exploring their comparative advantages and disadvantages with respect to the Project to determine whether any of the alternatives could meet most or all of the Project's objectives, while avoiding or substantially lessening its significant, unavoidable impacts. Three alternatives that could potentially meet the Project objectives were considered as part of the environmental review for the Project. The following section provides a summary of the alternatives considered.

Summary of Alternatives Considered

The EIR examined a reasonable range of alternatives to the proposed Project to determine whether any of those alternatives could meet most or all of the Project's objectives while avoiding or substantially lessening its significant impacts. The alternatives in the EIR were selected taking into consideration the following factors:

- **Alternative 1 - No Project.** This alternative assumes that the site remains in its partially developed state with the existing Wal-Mart store continuing in operation and the remainder of the site remaining vacant. **(DEIR at 5-2 to 5-3)**
- **Alternative 2- Walmart Expansion Only Alternative.** Under the Walmart Expansion Only Alternative, the existing Walmart store would be expanded by up to 66,500 square feet as proposed; the fuel station and Parcel 2 and 3 retail/restaurant uses would be eliminated. The expanded Walmart store would total 197,802 square feet. **(DEIR at 5-3 to 5-9).**
- **Alternative 3- Reduced Intensity Alternative.** Under the Reduced Density Alternative, a 25 percent reduction in development potential would be applied to each Project use. The Walmart store would be expanded by 49,875 square feet; the fuel station would consist of six pumps and a 1,125-square-foot convenience market; Parcel 2 would total 19,725 square feet; and Parcel 3 would total 19,275 square feet. The Reduced Density Alternative would develop a total of 90,000 square feet of new commercial uses on-site. **(DEIR at 5-9 to 5-14).**

Alternatives Eliminated from Detailed Consideration

Three alternative locations (Barber Yard, South Entler and the Sunset Hills Golf and Country Club) were considered and rejected based on various operational concerns or environmental constraints. **(DEIR at 5-15 to 5-16).**

B. APPROACH.

For each Project alternative discussed below, the potentially significant environmental impacts of the alternative are identified, as well as impacts of the proposed Project that would be avoided. The same environmental categories presented for the proposed Project in the DEIR, Section 3, Environmental Impact Analysis, have been addressed for each alternative. If a significant Project-related impact would be avoided under the alternative, or if the alternative would cause a significant impact that would not occur under the proposed Project, the impact category is generally discussed below. If a significant impact would not be avoided or created under the alternative, and, therefore, remains similar to that identified for the proposed Project, the impact category is not discussed.

C. FINDINGS REGARDING ALTERNATIVES ANALYZED IN DETAIL

Alternative 1 – No-Project

Characteristics

CEQA Guidelines Section 15126.6(e) requires that a “no-project” alternative be evaluated in an EIR. The “no-project” analysis shall discuss the existing conditions at the time the notice of preparation is published or at the time environmental analysis is commenced. The “no-project” alternative is what would be reasonably expected to occur in the foreseeable future if the project were not approved, based on current plans and consistent with available infrastructure and community services.

The purpose of describing and analyzing a no project alternative is to allow decision-makers to compare the impacts of approving the proposed project with the impacts of not approving the proposed project. CEQA Guidelines Section 15126(e)(2) states that “If the environmentally superior alternative is the “no-project” alternative, the EIR shall also identify an environmentally superior alternative among the other alternatives.”

Under the No Project Alternative, the proposed expansion of the Walmart store, the gas station and the outlot parcels, which would support retail or restaurant uses, would not be built. This alternative assumes that the site remains in its partially developed state with the existing Walmart store continuing in operation and the remainder of the site remaining vacant.

Conclusions

Under the No Project Alternative, the existing Walmart store would remain unchanged and no new development would occur. The Walmart store would continue to retail general merchandise and a limited amount of food and beverage items, and operate between 6 a.m. and 12 a.m. (midnight), 7 days a week. No changes to these characteristics would occur. Accordingly, this alternative would

avoid all of the proposed Project's significant impacts (including significant unavoidable impacts), as well as the need to implement any mitigation measures.

The No Project Alternative would avoid the proposed Project's significant and unavoidable impacts and would have less impact on all environmental topical areas. However, it would not advance any of the Project objectives, including: (1) positively contributing to the local economy and (2) reinforcing Chico's status as a regional retail node. The No Project Alternative would not meet the following Project objectives: (3) expanding an existing regional-serving retail use; (4) promoting economic growth in accordance with the goals and policies set forth in the City of Chico General Plan; (5) facilitating the development of undeveloped and underutilized land on an infill site; (6) developing complementary fuel station, retail, and restaurant uses; (7) designing a site plan to minimize overall access and circulation conflicts; (8) enhancing bicycle and pedestrian circulation; and (9) improving the overall visual appearance of the area by removing two outdated and unsightly billboards.

Based on these considerations, the City finds that the No Project Alternative is less desirable than the proposed Project and is infeasible and, therefore, rejects this alternative.

Alternative 2- Walmart Expansion Only Alternative

Characteristics

The Walmart Expansion Only Alternative consists of the expansion of the Walmart store as contemplated by the proposed Project and the elimination of all other new commercial uses, as well as improvements to Wittmeier Drive. The purpose of this alternative is to evaluate a land use concept that includes the largest component of the proposed Project, while also eliminating the smaller components in order to reduce site disturbance and the amount of new development.

The Walmart store would be expanded by 66,500 square feet to 197,802 square feet. The expanded Walmart store would operate 24 hours a day, 7 days a week, and would retail groceries and general merchandise. The store would have the same visual appearance and occupy the same footprint as contemplated by the proposed Project. The internal circulation and parking layout would be modified to accommodate the store expansion. To offset the loss of parking by the store expansion, new parking would be constructed on approximately 3 acres where the fuel station would be constructed under the proposed Project.

(The Class I bicycle/pedestrian trail would be re-routed around the perimeter of the new parking area, but the balance of the facility would maintain its current alignment.) Additionally, the Baney Lane driveways would be modified to prevent left-out turning movements. No vehicular connections would be provided to Wittmeier Drive, and the cul-de-sac would maintain its current lane configuration and traffic control devices (e.g., side street stop control at Forest Avenue).

Aside from the 3 acres occupied by the new parking area, the undeveloped portion of the Project site would remain in its existing condition for the foreseeable future. This includes the depressional seasonal wetland in the southwest corner of the Project site and the wooden freeway billboards, which would not be removed. (This alternative assumes that no changes to the billboards would occur, as there would be no impetus to terminate the existing outdoor advertising agreement.) This alternative would require the same discretionary approvals as the proposed Project, including

Tentative Parcel Map, Use Permit, Planned Development Permit, and Site Design and Architectural Review.

Conclusions

A full evaluation of the potential environmental impacts of the Walmart Expansion Only Alternative, as compared to those of the proposed Project, is provided at DEIR pages 5-3 to 5-9. The Walmart Expansion Only Alternative would lessen the severity of, but would not avoid, the significant and unavoidable transportation impact (the only significant and unavoidable impact that would occur) associated with the proposed Project.

The Walmart Expansion Only Alternative would advance all of the Project objectives, although several would be advanced to a lesser degree than the proposed Project, primarily because of the 53,500-square-foot reduction in development potential and the \$29.6 million reduction in new sales. This includes objectives related to: (1) positively contributing to the local economy through new capital investment, creation of new employment opportunities, expansion of the tax base, and increased retail offerings; (2) increasing commercial retail and service offerings within an established regional and highway-oriented commercial area; (3) expanding an existing regional-serving retail use close to SR-99 in order to better serve the retail demands of the Market Area; and (4) facilitating the development of undeveloped and underutilized land on an infill site. Additionally, the Walmart Expansion Only Alternative would not advance the objectives associated with (1) developing complementary fuel station, retail, and restaurant uses that are compatible with surrounding land uses and that provide consumers with additional convenient and competitive options; or (2) improving the overall visual appearance of the area by removing two outdated and unsightly billboards.

CEQA requires the identification of an environmentally superior alternative in an EIR. If the “No Project” alternative is the environmentally superior alternative, then the EIR must also identify an environmentally superior alternative from the remaining alternatives.

Public Resources Code section 21061.1 defines “feasible” to mean “capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, social, and technological factors.” CEQA Guidelines section 15364 adds another factor: “legal” considerations. The concept of “feasibility” also encompasses the question of whether a particular alternative or mitigation measure promotes the underlying goals and objectives of a project.

The City has determined that the Walmart Expansion Only Alternative is the environmentally superior alternative. However, differences in impacts between this alternative and the Project are only nominal, as the significant and unavoidable transportation impacts that would occur under the Project would only be reduced, but not avoided by this alternative. In addition, the applicant’s Project objectives are not fully met by this alternative. Based on these considerations, the City finds that the Walmart Expansion Only Alternative is less desirable than the proposed Project, and rejects this alternative.

Alternative 3- Reduced Intensity Alternative

Characteristics

The Reduced Density Alternative consists of a 25 percent reduction to each Project use that would result in a net reduction of two fuel station pumps and 30,000 square feet relative to the proposed Project. In total, this alternative would develop six fuel station pumps and 90,000 square feet of new commercial uses on the Project site. The purpose of this alternative is to evaluate a land use concept which reduces the overall development intensity, while still facilitating the development of similar commercial uses on the Project site.

The Walmart store expansion would occur in the same location, although it would add 49,875 square feet to the store instead of 66,500 square feet. The expanded store would total 181,177 square feet. All other operational characteristics would be identical to the proposed Project. The fuel station would have a canopy over six pumps (12 vehicle fueling positions) and a 1,125- square-foot convenience market. All other operational characteristics would be identical to the proposed Project. The development potential on Parcels 2 and 3 would be reduced to 19,275 square feet and 19,725 square feet, respectively. As with the proposed Project, end users would be retail or restaurant.

The Reduced Density Alternative would have the same internal circulation facilities and driveway connections as the proposed Project. The Baney Lane driveways would be modified to prohibit left-out movements, and the Wittmeier Drive cul-de-sac would have two driveway connections. As contemplated by the proposed Project, a signal would be installed at the intersection of Forest Avenue/Wittmeier Drive, and the Class I bicycle/pedestrian trail would be re-routed around the perimeter of the Project site. This alternative would require the same discretionary approvals as the Project, including Tentative Parcel Map, Use Permit, Planned Development Permit, and Site Design and Architectural Review.

Conclusions

A full evaluation of the potential environmental impacts of the Reduced Intensity Alternative, as compared to those of the proposed Project, is provided at DEIR pages 5-9 to 5-13. The Reduced Intensity Alternative would lessen the severity of, but would not avoid, the significant and unavoidable transportation impact (the only significant and unavoidable impact that would occur) associated with the proposed Project.

The Reduced Density Alternative would advance all of the Project objectives, although several would be advanced to a lesser degree than the proposed Project primarily because of the 30,000-square foot reduction in development potential and the \$15.4 million reduction in new sales. This includes objectives related to (1) positively contributing to the local economy through new capital investment, creation of new employment opportunities, expansion of the tax base, and increased retail offerings; (2) increasing commercial retail and service offerings within an established regional and highway-oriented commercial area; and (3) expanding an existing regional-serving retail use close to State Route 99 in order to better serve the retail demands of the Market Area.

Based on these considerations, the City finds that the Reduced Intensity Alternative is less desirable than the proposed Project, and rejects this alternative.

XI.

STATEMENT OF OVERRIDING CONSIDERATIONS

“CEQA recognizes that in determining whether and how a project should be approved, a public agency has an obligation to balance a variety of public objectives, including economic, environmental, and social factors and in particular the goal of providing a decent home and satisfying living environment for every Californian.” (CEQA Guidelines, § 15021, subd. (d))

To reflect the ultimate balancing of competing public objectives when the agency decides to approve a project that will cause one or more significant effects on the environment, an agency must prepare a statement of overriding considerations.” (CEQA Guidelines, § 15021, subd. (d), 15093)

A statement of overriding considerations must set forth the specific reasons why the agency found that the project’s “specific economic, legal, social, technological, or other benefits” rendered “acceptable” its “unavoidable adverse environmental effects.” (CEQA Guidelines, §§ 15093, subd. (a), 15043, subd. (b); see also Pub. Resources Code, § 21081, subd. (b))

A. SIGNIFICANT AND UNAVOIDABLE IMPACTS

As set forth above, the approval of the Project will result in significant adverse environmental effects in relation to transportation that cannot be avoided even with the adoption of all feasible mitigation measures.

B. FINDING OF OVERRIDING CONSIDERATIONS

The following statement identifies the reasons why, in the City’s judgment, the benefits of the Project outweigh its unavoidable significant impacts.

The City finds that each of the overriding considerations expressed as benefits and set forth below constitutes a separate and independent ground for such a finding. The substantial evidence supporting the various benefits can be found in the documents identified for inclusion in the Record of Proceedings.

The City has considered the EIR, the public Record of Proceedings on the proposed Project and other written materials presented to and prepared by the City, as well as verbal and written testimony received, and hereby determines that implementation of the Project would result in the following substantial public benefits:

1. The Project will result in the expansion of an existing building and construction of new complementary retail uses and a fuel station on a currently underutilized site located within an urbanized commercial area, rather than the abandonment of an existing building and the development of an entirely new building on a vacant site. Expanding an existing building and adding complementary retail uses and a fuel station on an underutilized site will result in efficient in-fill and full development of a site which is already designated and zoned for the proposed use and which is

surrounded by other commercial use. Expansion on an existing site will also result in fewer environmental impacts than would be associated with construction of an entirely new store and other retail uses at a different, previously undeveloped location.

2. The Project will strengthen the commercial vitality of the community's regional retail market by upgrading and expanding the existing store in order to retain and increase its competitiveness in the regional market.
3. The Project will reinforce Chico's status as a regional retail node by increasing commercial retail and service offerings within an established regional and highway-oriented commercial area.
4. The Project will positively contribute to the local economy by creating jobs. Using a standard commercial employment rate of 1 employee/500 square feet, the proposed Project is estimated to create as many as 240 new jobs. New employment opportunities would include full-time and part-time positions. The California Employment Development Department estimated that there were 6,600 unemployed persons in Butte County as of May 2016. Of this figure, an estimated 2,600 unemployed persons are in Chico. In addition, the Project will create temporary construction jobs.
5. The Project would expand an existing regional-serving retail use close to SR-99 in order to better serve the retail demands of the market area, while also minimizing the need for new infrastructure improvements.
6. The Project will generate additional property, sales and fuel tax revenue, as the assessed valuation of the Project site will be greatly increased, and additional taxable sales will be generated by the expanded and new retail uses and fuel station.
7. The Project will promote economic growth in accordance with the goals and policies set forth in the City of Chico General Plan; specifically, Goal ED-3, which calls for the City to maintain a redevelopment strategy that encourages successful commercial and employment centers.
8. The Project will enhance bicycle and pedestrian circulation by relocating the existing Class I bicycle/pedestrian path around the perimeter of the site in order to minimize conflicts with motor vehicles.
9. The Project will improve the overall visual appearance of the area by removing two outdated and unsightly billboards and developing new commercial uses that employ high-quality contemporary architecture and landscaping.
10. The Project will provide retail services which are responsive to the purchasing patterns of a large portion of the City's population.

After weighing the economic, legal, social, technological, and other benefits of the proposed Project against the significant unavoidable impacts of the Project identified in the EIR, the City hereby determines that those benefits outweigh the risks and adverse environmental impacts of the Project, and further determines that the Project's significant unavoidable impacts are acceptable.

Accordingly, the City adopts the Statement of Overriding Considerations, recognizing that significant unavoidable impacts will result from implementation of the Project. Having (i) adopted all feasible mitigation measures, as discussed in the Environmental Impact Report and herein; (ii) rejected alternatives to the Project, as discussed in the Environmental Impact Report and herein; and (iii) recognized the significant unavoidable impacts of the Project, the City hereby finds that the benefits of the proposed Project, as stated herein, are determined to be overriding considerations that warrant approval of the Project and outweigh and override its significant unavoidable transportation impacts, and thereby justify the approval of the Chico Walmart Expansion Project.

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**Mitigation Monitoring and Reporting Program
for the
Chico Walmart Expansion Project
Draft Environmental Impact Report
City of Chico, Butte County, California
State Clearinghouse Number 2015102017**

Prepared for:

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Report Date: September 9, 2016

Table 1: Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>Section 3.2—Air Quality/Greenhouse Gas Emissions</p> <p>MM AIR-1: During construction activities, The applicant shall ensure that construction activities:</p> <p>(1) Adhere to the provisions of Butte County Air Quality Management District Rules 205, 230, and 231; and</p> <p>(2) Incorporate specific Best Practices to minimize diesel exhaust and fugitive dust emissions during construction, as specified in Appendix C of the Butte County Air Quality Handbook, October 23, 2014, available at: http://bcaqmd.shasta.com/wp-content/uploads/CEQA-Handbook-Appendices-2014.pdf. Examples of these types of measures include, but are not limited to:</p> <ul style="list-style-type: none"> • Limiting idling of construction vehicles to 5 minutes or less. • Ensuring that all small engines are tuned to the manufacturer’s specifications. • Powering diesel equipment with Air Resources Board-certified motor vehicle diesel fuel. • Utilizing construction equipment that meets ARB’s 2007 certification standard or cleaner. • Using electric powered equipment when feasible. • The applicant shall prepare and submit documentation (e.g., equipment plan or certification) to the City of Chico verifying compliance as part of the grading permit application. <p>Pursuant to the above, the developer shall prepare and submit a dust control plan consistent with Rule 205 prior to or in conjunction with building and/or grading permits, subject to approval by city staff, and shall note and demonstrate on the construction plans that architectural coatings and asphalt to be used for the project shall meet the standards set forth by District Rules 230 and 231. Additionally, the developer shall maintain material safety data sheets or other records at the construction site that document the content of volatile organic compounds (VOCs) of the architectural coatings and asphalt used at the project site, in compliance with District Rules 230 and 231.</p>					
	Submittal of documentation; notes on construction plans; site inspection	Prior to issuance of building permits; during construction activities	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
MM AIR-2a: During the architectural coating phase of construction activities, the project applicant shall require that all construction contractors utilize low volatile organic compound (VOC) paint that does not exceed 150 grams of VOC per liter for interior architectural coatings. The applicant shall keep records documenting the VOC content of paint products and provide them to the City of Chico upon request.	Notes on construction plans; site inspection	During the architectural coating phase of construction activities	City of Chico		
MM AIR-2b: During grading and earthwork activities, the applicant shall require that all off-road diesel-powered construction equipment greater than 50 horsepower meet or exceed United States Environmental Protection Agency (EPA) Tier 3 off-road emissions standards. The applicant shall keep records certifying documenting the emissions rating of all off-road diesel-powered construction equipment and provide them to the City of Chico upon request.	Notes on construction plans; site inspection	During grading and earthwork activities	City of Chico		
MM AIR-2c: To the extent feasible, future development on Parcels 2 and 3 shall provide pedestrian sidewalk connections to both the Walmart store and bike path networks, and new structures shall include roof overhangs that are sufficient to block the high summer sun from windows, but also allow the lower winter sun to enter windows.	Site Design and Architectural Review; review of building plans	Prior to issuance of building permits	City of Chico		
MM AIR-2d: Prior to issuance of building permits, the project applicant shall prepare and submit building plans to the City of Chico that demonstrate that all new structures have outdoor electrical outlets that are accessible to maintenance workers and landscapers to allow the use of electric powered equipment.	Review of building plans	Prior to issuance of building permits	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>MM AIR-2e: Prior to issuance of building permits, the applicant shall revise the landscape plans to include primarily native drought-resistant trees that are low ROG emitting. For example, the sycamores (<i>Platanus</i> spp.) shall be replaced with species such as maples (<i>Acer</i> spp.), pistache (<i>Pistacia</i> spp.) and zelkova species.</p>	<p>Review of building plans</p>	<p>Prior to issuance of building permits</p>	<p>City of Chico</p>		
<p>MM AIR-2f: Prior to issuance of the certificate of occupancy for the expanded Walmart store, the applicant shall retain a qualified transportation consultant to prepare and submit a Transportation Demand Management (TDM) program to the City of Chico for review and approval. The TDM program shall identify measures to reduce daily gasoline-powered and diesel-powered vehicle trips to the project site, with an objective of a minimum 10 percent reduction in gasoline-powered and diesel-powered trips. The approved TDM program shall be implemented in conjunction with the opening of the expanded store. Examples of trip reduction measures may include, but are not limited to, the following:</p> <ul style="list-style-type: none"> • Post transit information (maps, schedules, fares, etc.) in a public area of Walmart that is accessible to employees and patrons; • Provide employer-subsidized transit passes; • Sponsor an employee ride sharing program; • Provide employee lockers for personal items; • Provide employees with an employee only restroom with a shower; • Provide secure indoor bicycle parking (racks or lockers) for employees; • Provide customer bicycle parking (racks) in safe and convenient locations; • Allow flex scheduling or compressed scheduling practices; • Provide preferential parking spaces for clean air vehicles; • Provide a minimum of two charging stations for electric vehicles; and 	<p>Review of TDM program; site inspection</p>	<p>Prior to issuance of the certificate of occupancy for the expanded Walmart store</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<ul style="list-style-type: none"> If home delivery service is provided in the future, it shall be performed using low-emission or alternative-fueled (electric, natural gas, hydrogen, etc.) vehicles. 					
<p>MM AIR-2g: To the extent that such equipment is readily available and can adequately perform all tasks, Walmart shall use electric-, propane-, or natural gas-powered mobile equipment (forklifts, non-street legal street sweepers, etc.) for operational activities within the project site. Existing gasoline- or diesel-powered mobile equipment may continue to be used until its service life is exhausted.</p>	Submittal of documentation; site inspection	Prior to occupancy of the expanded Walmart store	City of Chico		
<p>MM AIR-2h: Prior to occupancy of the expanded Walmart store, the applicant shall enhance the existing Butte Regional Transit stop on Forest Avenue. The enhanced transit stop shall conform to Butte Regional Transit design standards and provide a minimum of a shelter, seating, lighting, transit information, and a pedestrian connection to the Walmart store entrance.</p>	Site inspection	Prior to occupancy of the expanded Walmart store	City of Chico		
<p>MM AIR-2i: Prior to issuance of occupancy permits for the expanded Walmart store, the project applicant shall implement idling restriction program for heavy-duty diesel vehicles. The program shall require that all trucks comply with state regulations limiting idling to no more than 5 minutes. The program shall be implemented through signage in all loading areas and training of store personnel about the idling restrictions.</p>	Submittal of documentation; site inspection	Prior to issuance of occupancy permits for the expanded Walmart store	City of Chico		
<p>MM AIR-2j: Prior to issuance of occupancy permits for the expanded Walmart store, the project applicant shall provide at least of one electrical hookup in each of the two proposed Walmart loading docks that is capable of powering a truck-mounted transport refrigeration unit (TRU) with an electrical hookup option.</p>	Site inspection	Prior to issuance of occupancy permits for the expanded Walmart store	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>MM AIR-2k: Prior to the recordation of the Final Map, the project applicant shall participate in an Offsite Mitigation Program coordinated through the BCAQMD. The project applicant shall utilize a methodology based on the BCAQMD CEQA Handbook with final details to be approved by the BCAQMD and City for calculating the payment to the Offsite Mitigation Program.</p>	<p>Submittal of documentation</p>	<p>Prior to the recordation of the Final Map</p>	<p>City of Chico</p>		
<p>Section 3.3—Biological Resources</p>					
<p>MM BIO-1: Prior to issuance of any grading or construction permits or the first ground-disturbing activities, the applicant shall retain a qualified botanist to conduct two focused botanical surveys for special-status plant species within the undeveloped portion of the project site. The first survey shall occur in April and the second survey shall occur in either May or June. The surveys shall be conducted for the following plant species: Ahart’s paronychia, Butte County meadowfoam, woolly meadowfoam, adobe lily, Bidwell’s knotweed, Butte County golden clover, depauperate milkvetch, Ferris’ milkvetch, hogwallow starfish, pink creamsacs, Red Bluff dwarf rush, round-leaved filaree, Tehama navarretia, and veiny monardella. If no special-status plants are observed, then a letter report documenting the results of the final survey shall be provided to the project applicant and the City of Chico Community Development Department for their records, and no additional measures are required. If any of the non-listed special-status plants occur within the project site, they shall be avoided to the extent feasible. If the plants cannot be avoided, the biologist shall prepare a special-status plant mitigation and monitoring plan in consultation with the California Department of Fish and Wildlife (CDFW). The plan shall be subject to CDFW approval and may include, but is not limited, to the following elements as applicable to the species, based on the project</p>	<p>Submittal of documentation</p>	<p>Prior to issuance of any grading or construction permits or the first ground-disturbing activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<ul style="list-style-type: none"> A salvage/transplanting program for the salvage and transfer of special-status plants populations before the initiation of construction activities to another location either off-site or on-site that will be preserved in perpetuity (via conservation easement, deed restriction, or other appropriate legal means). Permits may be required from the CDFW or USFWS if a listed species is found and may require further mitigation in consultation with the appropriate agency or agencies. The program shall specify appropriate timing of salvage and transplanting efforts based on the specific special-status plant species found and the propagation program shall establish success criteria for the affected special-status plants. In addition to salvaging of special-status plants themselves, salvage efforts shall include topsoil and seedbanks surrounding impacted plants, if doing so will not contribute to the spread of invasive or noxious plant species. Purchasing credits at an off-site mitigation bank approved by USFWS or CDFW, as applicable, based on the listed plant species found. Criteria that must be met within the special-status plant mitigation and monitoring plan prior to commencement of construction or grading within the undeveloped portions of the project site. <p>USFWS or CDFW, as applicable, shall approve the plan prior to transpiration or commencement of grading or construction activities.</p> <p>This mitigation measure shall not apply if the Draft Butte Regional Conservation Plan is adopted at the time of ground-disturbing activities and provides coverage for impacts to these plant species, and all required conservation measures or mitigation required under the Conservation Plan for this project are implemented. If this scenario occurs, mitigation that occurs pursuant to the Conservation Plan shall not be less than the procedures spelled out in this mitigation measure.</p>	<p>Submittal of documentation</p>	<p>Prior to issuance of any grading or construction permits or the first ground-disturbing activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>MM BIO-2a: Prior to any ground-disturbing activities, the applicant shall retain a qualified biologist to conduct a protocol-level survey for the western spadefoot during the appropriate period to identify western spadefoot toads (e.g., following a rain event during the breeding season, identification of other known active breeding populations in the region). If no western spadefoot are observed, then a letter report shall be submitted to the City of Chico Community Development Department and CDFW for their records. Additionally, a preconstruction survey shall be conducted within 14 days prior to the start of any ground disturbance to verify absence of this species from the project area.</p> <p>If western spadefoot are found, ground disturbance shall cease and consultation with CDFW shall occur prior to resumption of ground-disturbing activities to develop an impact minimization plan that may include but is not limited to relocation, timing restrictions, habitat compensation, etc. The plan shall be reviewed and subject to approval by CDFW and implemented prior to ground-disturbing activities.</p> <p>In addition, if western spadefoot are found, the biologist shall be on-site during initial grading. If western spadefoot toad is observed within the project footprint, all construction shall temporarily halt until the biologist relocates the species to suitable habitat within the project site, but outside of the project footprint, or other suitable location in the project vicinity.</p> <p>This mitigation measure shall not apply if the Draft Butte Regional Conservation Plan is adopted at the time of ground-disturbing activities and provides coverage for impacts to the western spadefoot and all required conservation measures or mitigation required under the Butte Regional Conservation</p>	<p>Notes on construction plans; submittal of documentation; site inspection</p>	<p>Prior to any ground-disturbing activities; during initial grading activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>Plan for this project are implemented. If this scenario occurs, mitigation that occurs pursuant to the Conservation Plan shall not be less than the procedures spelled out in this mitigation measure.</p> <p>MM BIO-2b: Prior to the first ground-disturbing activities, the project applicant shall retain a qualified biologist to conduct four protocol-level surveys during the breeding season (one site visit between February 15 and April 15 and three between April 15 and July 15, one of which shall be conducted after June 15), at least three weeks apart, in accordance with the 2012 California Department of Fish and Wildlife Staff Report on Burrowing Owl Mitigation (2012 Staff Report). The survey shall include an approximately 500-foot (150-meter) buffer around the project site, where access is permitted. If the surveys are negative, then a letter report shall be prepared documenting the methodology and results within two weeks following the final survey. If the surveys result in negative findings, the biologist shall still conduct a take avoidance survey between 14 days and 30 days prior to commencement of construction, in accordance with the 2012 Staff Report. A letter report documenting the methodology and results shall be submitted to the City of Chico Community Development Department and CDFW following the results of the final survey for their records.</p> <p>If burrows are observed within the undeveloped portion of the project site or within 500 feet of the project site, an impact assessment shall be prepared and submitted to the CDFW, in accordance with the 2012 Staff Report. If it is determined that project activities may result in impacts to nesting, occupied, and satellite burrows and/or burrowing owl habitat, the project proponent shall consult with CDFW and develop a detailed mitigation plan such that the habitat acreage, number of</p>	<p>Notes on construction plans; submittal of documentation; site inspection</p>	<p>Prior to the first ground-disturbing activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>burrows, and burrowing owls impacted are replaced in a manner acceptable to CDFW. The mitigation plan shall be based on the methods provided in the 2012 Staff Report.</p> <p>This mitigation measure shall not apply if the Draft Butte Regional Conservation Plan is adopted at the time of ground-disturbing activities and provides coverage for impacts to the burrowing owl and all required conservation measures or mitigation required under the Butte Regional Conservation Plan for this project are implemented. If this scenario occurs, mitigation that occurs pursuant to the Conservation Plan shall not be less than the procedures spelled out in this mitigation measure.</p>					
<p>MM BIO-2c: No more than 14 days prior to the start of ground disturbance or tree removal, the applicant shall retain a qualified biologist to conduct a pre-construction surveys for special-status bat species. If no special-status bats are observed roosting, then a letter report documenting the results of the survey shall be provided to the City of Chico Community Development Department, CDFW, and any other applicable resource agency for their records. A subsequent bat survey shall be required if construction does not commence within 14 days of a negative survey, or if construction is planned to resume after halting for more than 14 days.</p> <p>If bats are found, the biologist shall consult with the CDFW prior to ground disturbance, tree removal, or other construction activity to determine specific avoidance measures. Avoidance measures may include establishing a minimum 250-foot buffer around the roosting location until it is no longer occupied. Construction activities, including but not limited to tree removal, shall not proceed within the buffer until the biologist has determined that the roost is no longer</p>	Notes on construction plans; submittal of documentation; site inspection	No more than 14 days prior to the start of ground disturbance or tree removal; during construction activities	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>occupied by the bats and has communicated the determination in writing to the City of Chico Community Development Department and CDFW.</p> <p>MM BIO-2d: No more than 14 days prior to any vegetation removal activities that occur during the nesting season (February 15 and August 31), the applicant shall retain a qualified biologist to conduct a preconstruction survey for active nests. If the preconstruction survey shows that there is no evidence of active nesting bird nests, then a letter report shall be provided to the City of Chico Community Development Department and any applicable resource agency for their records.</p> <p>If any active nests are located within the project site, an appropriate buffer zone shall be established around the nests, as determined by the biologist. The biologist shall mark the buffer zone with construction tape or pin flags and maintain the buffer zone, where construction activities shall be prohibited, until the end of breeding season or until the young have successfully fledged. Buffer zones are typically 100 feet for migratory bird nests and 250 to 500 feet for raptor nests. A qualified biologist shall monitor active nests weekly during construction to evaluate potential nesting disturbance by construction activities. If establishing the typical buffer zone is impractical, the qualified biologist may reduce the buffer depending on the species, and daily monitoring by the biologist is required to ensure that the nest is not disturbed and no forced fledging occurs. Monitoring as described above shall continue until the qualified biologist determines that the nest is no longer occupied and has communicated the determination in writing to the City of Chico Community Development Department and applicable resource agency.</p> <p>This mitigation measure does not apply to vegetation removal</p>	<p>Notes on construction plans; submittal of documentation; site inspection</p>	<p>No more than 14 days prior to any vegetation removal activities that occur during the nesting season (February 15 and August 31); during construction activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>activities that occur outside of the nesting season (September 1 to February 14).</p> <p>MM BIO-2e: For phases of the project that would result in ground disturbance within 250 feet of the seasonal wetland but do not involve any disturbance within 50 feet of the seasonal wetland, the applicant shall implement indirect impact avoidance measures as follows:</p> <ol style="list-style-type: none"> 1. Retain a qualified biologist to direct the installation of silt fencing and high-visibility fencing a minimum of 50 feet from the edge of the seasonal wetland; 2. Conduct a Worker Awareness Training Program prior to the start of construction informing workers of the wetland and advising them to stay clear of the area. The training shall be repeated as necessary for all subsequent sub-contractor crews that are scheduled to begin working on the project; 3. Retain a qualified biologist to monitor on-site vegetation clearing and initial grading activities; and 4. Develop and implement a dust control plan, consistent with Rule 205 of the Butte County Air Quality Management District (Fugitive Dust Emissions), which regards the silt fencing around the seasonal wetland as a property line over which allowing wind-driven fugitive dust shall be minimized. <p>Upon submitting applications to the city for grading, construction, or other site disturbance, the developer shall provide a scaled drawing that depicts the wetland, 50-foot and 250-foot buffers around the wetland, and proposed work area(s), including designated equipment/material staging and worker parking areas. The developer shall arrange a preconstruction inspection of the silt/visibility fencing by city staff prior to commencement of construction.</p>	<p>Notes on construction plans; submittal of documentation; site inspection</p>	<p>Prior to and during construction activities that would occur within 250 feet of the seasonal wetland</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>If future phases of the project necessitate directly impacting the seasonal wetland, such as development of Parcel 2 or other project feature, the developer shall either assume presence or conduct protocol-level surveys for listed vernal pool branchiopods in accordance with the USFWS Survey Guidelines prior to any ground-disturbing activities. Protocol surveys consist of one wet-season survey and one dry-season survey. If the developer decides to conduct protocol-level surveys and the surveys result in negative findings of the federally listed species, then no additional measures are required so long as the USFWS accepts the negative survey results. If listed branchiopods are found during protocol surveys or presence is assumed, then the applicant shall pursue consultation with the USFWS to obtain a Biological Opinion that authorizes the work. The developer shall implement all conditions (aka Reasonable and Prudent measures) identified in the Biological Opinion as well as those contains in any associated permits.</p> <p>If a future phase of the project necessitates work within 50 feet of the seasonal wetland but no direct impacts are anticipated then the developer shall prepare a detailed plan for the work demonstrating that no impacts will occur to the wetland and obtain letter of no effect (or “not likely to adversely affect” determination) from the Army Corps of Engineers.</p> <p>This mitigation measure shall not apply if the Draft Butte Regional Conservation Plan is adopted at the time of grading activities and provides coverage for impacts to the listed vernal pool shrimp species, and all required conservation measures or mitigation required under the Conservation Plan for this project are implemented. If this scenario occurs, mitigation that occurs pursuant to the Conservation Plan shall not be less than the procedures spelled out in this mitigation measure.</p>	<p>Notes on construction plans; submittal of documentation; site inspection</p>	<p>Prior to and during construction activities that would occur within 250 feet of the seasonal wetland</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>MM BIO-3: The applicant shall modify all improvement plans and construction drawings to provide a minimum separation of 50 feet between the seasonal wetland and (1) the poles associated with the relocated high-voltage overhead power lines, and (2) the relocated Class I bicycle/pedestrian path, subject to review and approval by City Public Works and Community Development Department staff. The separation distance of these project features from the wetland shall be sufficient to enable implementation of the indirect impact avoidance measures of Mitigation Measure BIO-2e.</p>	<p>Approval of improvement plans</p>	<p>Prior to construction activities</p>	<p>City of Chico</p>		
<p>MM BIO-4: If construction occurs within 50 feet of the depressional seasonal wetland in the southwestern portion of the project site, the applicant shall obtain all requisite approvals and permits from the USACE and Regional Water Quality Control Board (RWQCB) for impacts to Waters of the United States and Waters of the State prior to issuance of any City permits for construction, grading, or demolition activities that occurs within 100 feet of the resource. If the USACE or RWQCB determine that no permit from their agency is required for a particular phase of the project, then this mitigation measure only requires acknowledgement of such from each agency regarding the project phase in question. If any future phase of the project involves direct impacts to the wetland, then the developer shall mitigate the impact through either (1) creation and preservation of seasonal wetland habitat within the project site, (2) off-site restoration of features of equal or greater value, or (3) purchase of credits at an agency-approved mitigation bank in the region at a ratio to be determined through consultation with the USACE and RWQCB, but no less than a 1:1 ratio.</p> <p>This mitigation measure shall not apply if the Draft Butte</p>	<p>Submittal of documentation</p>	<p>Prior to construction activities that occur within 100 feet of the depressional seasonal wetland</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
Regional Conservation Plan is adopted at the time of grading activities and provides coverage for impacts to the depression seasonal wetland, and all required conservation measures or mitigation required under the Conservation Plan for this project are implemented. If this scenario occurs, mitigation that occurs pursuant to the Conservation Plan shall not be less than the procedures spelled out in this mitigation measure.					
Section 3.4—Cultural Resources					
MM CUL-1: If prehistoric or historic-period archaeological resources are encountered, all earth-disturbing construction activities within 100 feet of the find shall halt and the City of Chico Community Development Department shall be notified at (530) 879-6800. Prehistoric archaeological materials might include obsidian and chert flakedstone tools (e.g., projectile points, knives, scrapers) or toolmaking debris; culturally darkened soil (midden) containing heat-affected rocks, artifacts, or shellfish remains; and stone milling equipment (e.g., mortars, pestles, handstones, or milling slabs); and battered stone tools, such as hammerstones and pitted stones. Historic-period materials might include stone, concrete, or adobe footings and walls; filled wells or privies; and deposits of metal, glass, and/or ceramic refuse. A Secretary of the Interior-qualified archaeologist shall inspect the findings within 72 hours of discovery. If it is determined that the project could damage a historical resource or a unique archaeological resource (as defined pursuant to the CEQA Guidelines) mitigation shall be implemented in accordance with PRC Section 21083.2 and Section 15126.4 of the CEQA Guidelines, with a preference for preservation in place. Consistent with Section 15126.4(b)(3), this may be accomplished through	Notes on construction plans	Prior to construction activities	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>planning construction to avoid the resource; incorporating the resource within open space; capping and covering the resource; or deeding the site into a permanent conservation easement. If avoidance is not feasible, a qualified archaeologist shall prepare and implement a detailed treatment plan in consultation with the City of Chico. Treatment of unique archaeological resources shall follow the applicable requirements of PRC Section 21083.2. Treatment for most resources would consist of (but would not be limited to) sample excavation, artifact collection, site documentation, and historical research, with the aim to target the recovery of important scientific data contained in the portion(s) of the significant resource to be impacted by the Project. The treatment plan shall include provisions for analysis of data in a regional context, reporting of results within a timely manner, curation of artifacts and data at an approved facility, and dissemination of reports to local and state repositories, libraries, and interested professionals.</p>	Notes on construction plans	Prior to construction activities	City of Chico		
<p>MM CUL-3: In the event a fossil is discovered during construction for the proposed project, excavations within 50 feet of the find shall be temporarily halted or delayed until the discovery is examined by a qualified paleontologist, in accordance with Society of Vertebrate Paleontology standards. The applicant shall include a standard inadvertent discovery clause in every construction contract to inform contractors of this requirement. If the paleontological resources are found to be significant, they shall be avoided by project construction activities and recovered by a qualified paleontologist. Upon completion of the recovery, a paleontological assessment shall be conducted by a qualified paleontologist to determine if further monitoring for paleontological resources is required. The assessment shall include (1) the results of any geotechnical</p>					

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>investigation prepared for the project area, (2) specific details of the construction plans for the project area, (3) background research, and (4) limited subsurface investigation within the project area. If a high potential to encounter paleontological resources is confirmed, subsurface construction shall be temporarily halted until a monitoring plan of further project subsurface construction is prepared in conjunction with this assessment. After project subsurface construction has ended, a report documenting monitoring, methods, findings, and further recommendations regarding paleontological resources shall be prepared and submitted to the City of Chico.</p>					
<p>MM CUL-4: In the event of discovery or recognition of any human remains during construction activities, such activities within 100 feet of the find shall cease until the Butte County Coroner has been contacted to determine that no investigation of the cause of death is required. The Native American Heritage Commission (NAHC) shall be contacted within 24 hours if it is determined that the remains are Native American. The NAHC will then identify the person or persons it believes to be the most likely descendant from the deceased Native American (PRC Section 5097.98), who in turn would make recommendations to the City of Chico for the appropriate means of treating the human remains and any associated funerary objects (CEQA Guidelines Section 15064.5(d)).</p>	Notes on construction plans	Prior to construction activities	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>Section 3.7—Hydrology and Water Quality</p> <p>MM HYD-1a: Prior to the issuance of grading permits, the project applicant shall file a Notice of Intent with and obtain a facility identification number from the State Water Resources Control Board. The project applicant shall also submit a Stormwater Pollution Prevention Plan (SWPPP) to the City of Chico that identifies specific actions and Best Management Practices (BMPs) to prevent stormwater pollution during construction activities. The City of Chico shall review and approve the SWPPP. The SWPPP shall identify a practical sequence for BMP implementation, site restoration, contingency measures, responsible parties, and agency contacts. The SWPPP shall include but not be limited to the following elements:</p> <ul style="list-style-type: none"> • Comply with the requirements of the State of California’s most current Construction Stormwater Permit. • Temporary erosion control measures shall be implemented on all disturbed areas. • Disturbed surfaces shall be treated with erosion control measures during the October 15 to April 15 rainy season. • Sediment shall be retained on-site by a system of sediment basins, traps, or other BMPs. • The construction contractor shall prepare Standard Operating Procedures for the handling of hazardous materials on the construction site to eliminate discharge of materials to storm drains. • BMP performance and effectiveness shall be determined either by visual means where applicable (e.g., observation of above-normal sediment release), or by actual water sampling in cases where verification of contaminant reduction or elimination (such as inadvertent petroleum release) is required by the Central Valley Regional Water 	<p>Approval of Stormwater Pollution Prevention Plan; notes on construction plans</p>	<p>Prior to the issuance of grading permits</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>Quality Control Board to determine adequacy of the measure.</p> <ul style="list-style-type: none"> In the event of significant construction delays or delays in final landscape installation, native grasses or other appropriate vegetative cover shall be established on the construction site as soon as possible after disturbance, as an interim erosion control measure throughout the wet season. 					
<p>MM HYD-1b: Prior to the issuance of building permits, the project applicant shall submit a final Storm Water Mitigation Plan (SWMP) to the City of Chico for review and approval. The plan shall be developed using the California Stormwater Quality Association’s “New Development and Redevelopment Handbook.” The SWMP shall identify pollution prevention measures and BMPs necessary to control stormwater pollution from operational activities and facilities, and provide for appropriate maintenance over time. The SWMP shall include design concepts that are intended to accomplish a “first flush” objective that would remove contaminants from the first 2 inches of stormwater before it enters area waterways. The project applicant shall also prepare and submit an Operations and Maintenance Agreement to the City identifying procedures to ensure that stormwater quality control measures work properly during operations.</p>	Approval of Storm Water Mitigation Plan	Prior to the issuance of building permits	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
Section 3.9—Noise					
<p>MM NOI-1: The following noise attenuation measures shall be implemented during construction activities:</p> <ul style="list-style-type: none"> • Construction activities are limited to the hours of 7:00 a.m. to 9:00 p.m. Monday through Saturday, and 10:00 a.m. to 6:00 p.m. on Sundays and federal holidays. The City of Chico shall have discretion to permit construction activities to occur outside of these hours if compelling circumstances warrant an exception (e.g., weather conditions necessary to pour concrete). • All construction equipment shall use noise reduction devices (e.g., mufflers and engine shrouds) that are no less effective than those originally installed by the manufacturer. If no noise reduction devices were installed by the manufacturer, a minimum of a muffler shall be installed for the duration of project-related construction activities. • If stationary equipment (compressors, pumps, generators, etc.) is to be used within 300 feet of any office or residential receptor, a noise barrier shall be erected to attenuate noise. The noise barrier shall consist of an enclosure, temporary noise panels/barriers, or acoustical/sound blankets over fencing. 	Notes on construction plans; site inspection	During construction activities	City of Chico		
Section 3.11—Transportation					
<p>MM TRANS-3a: Prior to issuance of the first building permit for the proposed project, the applicant shall provide the City of Chico with the requisite funds for improving the intersection of E. 20th Street / Forest Avenue. The improvements shall consist of: (1) restriping the northbound approach (Forest Avenue) to provide a left-turn lane, a shared left-turn/through lane, a through lane, and a right-turn lane; and (2) modifying the signal operation to provide split phasing on the</p>	Receipt of fees	Prior to issuance of the first building permit	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
northbound and southbound approaches. The City of Chico shall implement the improvements when monitoring determines that the intersection operation is approaching unacceptable levels.					
MM TRANS-3b: Prior to issuance of building permits for the proposed project, the applicant shall pay the fair-share cost of off-site circulation improvements, as calculated through the City's adopted development impact fee program.	Receipt of fees	Prior to issuance of building permits	City of Chico		
MM TRANS-5: Prior to the approval of any improvement plans for Wittmeier Drive, the project applicant shall retain a qualified traffic engineer to evaluate the following options for addressing (1) conflicts between through and turning vehicles and (2) vehicle movement enhancements on Wittmeier Drive. The engineer shall identify a recommended option or options for each issue, and the applicant shall prepare and submit plans to the City of Chico for review and approval that reflect the recommended options. The approved plans shall be incorporated into the proposed project. To address conflicts between through and turning vehicles, one or both of the following options shall be implemented: <ul style="list-style-type: none"> • Option 1: Modify the eastbound dual left-turn lanes on Wittmeier Drive to provide a single left-turn lane, thereby providing an extra lane to facilitate westbound turning movements and truck loading and unloading. Based on estimated Plus Project peak-hour left-turn movements from Wittmeier Drive to Forest Avenue, it is not anticipated that dual left-turn lanes are necessary to accommodate turning vehicle queues. • Option 2: Install a roundabout at the Wittmeier Drive cul-de-sac to address potential vehicular conflicts between vehicles turning in the cul-de-sac and vehicles entering and exiting 	Review of improvement plans	Prior to the approval of any improvement plans for Wittmeier Drive	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>the westernmost Walmart driveway on Wittmeier Drive. The roundabout would also address potential safety issues arising from vehicles entering and exiting the westernmost Walmart driveway at higher rates of speed.</p> <p>To address vehicle movement enhancements, one of the following options shall be implemented:</p> <ul style="list-style-type: none"> • Option A: One left-turn lane and one through-right lane in the eastbound direction, and one through-left lane and one right-turn pocket in the westbound direction. Truck loading and unloading activity could occur on the curbside between the two driveways on the south side of Wittmeier Drive. • Option B: One left-turn lane and one through-right lane in the eastbound direction, one through-right lane in the westbound direction, and one two-way center left-turn lane with a median extending 50 feet west of the Forest Avenue/Wittmeier Drive intersection. Truck loading and unloading activity could occur in the two-way center left-turn lane. 	Review of final parcel map	Prior to recordation of the final parcel map	City of Chico		
<p>MM TRANS-7: Prior to recordation of the final parcel map, the project applicant shall provide an easement for a potential future Class I bikeway connection that would extend south from the southwesterly corner of the project site to the planned SR-99 Bikeway.</p>					

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1 C. The site is suitable for the type and density of the proposed development in that it is
2 relatively level, surrounded by compatible commercial land uses and, as conditioned,
3 sufficient areas are provided to meet applicable development regulations pertaining to
4 buildings, off-street parking and landscaping. Based on evidence and mitigation provided
5 by the EIR, the design and improvements associated with the subdivision are not likely to
6 cause substantial environmental damage, substantially injure fish or wildlife or their
7 habitat, or cause serious public health problems. The design of the subdivision would not
8 conflict with public easements for access through the subdivision because all abandoned
9 public access easements would be replaced with new easements that convey substantially
10 the same public benefit.

11 D. As supported by the Mitigation Monitoring and Reporting Program, Conditions of
12 Approval, Subdivision Report (attached hereto as Exhibits I, II and III), the EIR, and the
13 staff report dated September 27, 2016, the proposed parcel map and its design conform
14 with the requirements of Title 18 and Title 19 of the Chico Municipal Code, and is
15 consistent with the Chico General Plan.

16 E. No substantial evidence has been presented that would require disapproval of the Project
17 pursuant to Government Code Section 66474.

18 F. The EIR for the Project reflects the City's independent judgment and analysis.

19 2. With regard to Site Design and Architectural Review 15-16 the Planning Commission finds
20 that:

21 A. Expanding and adding additional commercial uses at the partially-developed Project site
22 represents compatible infill development/redevelopment within a Regional Commercial
23 Opportunity Site, consistent with goals LU-4, LU-5, CD-5, and policies LU-1.3, LU-4.2
24 and LU-5.1, in that the proposed layout would not cause significant conflicts with existing
25 or anticipated Regional Commercial uses in the area. The Project is also consistent with
26 the General Plan's emphasis on intensifying and revitalizing commercial uses within the
27 East 20th Street Opportunity Site.

28 As explained in the EIR, approving the Project could result in an unacceptable Level of

1 Service (LOS) for a southbound segment of SR-99 between SR-32 and East 20th Street
2 under the 2035 cumulative plus Project modeling scenario, during the Saturday p.m. peak
3 hour. Because an acceptable level of service may not be maintained for this facility, there
4 is a potential inconsistency with Policy CIRC-1.4. However, the proposed Project design,
5 as modified by mitigation measures identified in the EIR, would achieve consistency with
6 the remaining policies in the Circulation Element. The proposal is consistent with CIRC-
7 1.2, CIRC-2.2.3 and CIRC-3.6, in that it would construct necessary internal and adjacent
8 roadway circulation improvements, use traffic calming measures to reduce cut-through
9 traffic, and include safe and secure bicycle parking facilities. Mitigation measures and
10 existing City processes would ensure Project consistency with CIRC-1.3, CIRC-3.1.1,
11 CIRC 5.3, CIRC-9.1, CIRC-9.1.2, CIRC-9.1.3 and CIRC-9.3, by requiring payment of fair
12 share costs for citywide street facility improvements (MM TRANS-3b), incorporating a
13 planned bikeway connection through the site (MM TRANS-7), supporting public transit
14 (MM AIR-2h), and reducing vehicle trips through the use of travel demand management
15 strategies (MM AIR-2f), respectively.

16 Consistent with policy OS-1.2, mitigation measures also ensure that Project-related
17 impacts to the onsite wetland are avoided, or mitigated by requiring the applicant to obtain
18 all necessary state and federal permits for future development that may affect special status
19 species or their habitat.

20 The energy efficiency measures outlined in the applicant's project description for the store
21 expansion are consistent with policy SUS-5.2, which calls for supporting the inclusion of
22 energy efficient design in private projects.

23 The site design and architectural aspects of the Project are generally consistent with
24 policies CD-3.2, CD-5.3, and LU-4.4, in that the existing public bike path would be
25 replaced in a manner that would facilitate future connectivity to other public bikeways, the
26 store expansion would include higher quality materials (such as multiple storefront
27 windows and decorative stone columns), and a better-articulated façade and roofline
28 relative to the existing building.

1 Approval of the Project is consistent with General Plan goals and policies to enhance
2 Chico's long-term prosperity (ED-1), and ensure that regulations and permitting processes
3 for the conduct of commerce do not unreasonably inhibit local business activity (ED-1.3).
4 Although a specific future potential traffic impact is projected to be inconsistent with
5 Policy 1.4, the overall Project, as conditioned, implements many other policies and is
6 consistent with the General Plan when considering the policy framework as a whole. No
7 Neighborhood Plans or area plans apply to the Project.

8 B. The Project promotes orderly development and higher use of land by expanding into
9 undeveloped land within an existing regional-serving shopping area, and including
10 circulation improvements that will address existing and future projected needs at the site.
11 Updating the store's appearance and installing landscape improvements in the parking area
12 will increase desirability of investment in the area and achieve a higher visual aesthetic,
13 consistent with the stated purposes of CMC 19.18. The Project is consistent with Design
14 Guidelines that call for commercial buildings to incorporate varied building depths,
15 materials and rooflines (DGs 2.2.22, 2.2.25, 2.2.31), and create a sense of focus toward
16 main entrances through distinguished tower elements, awnings, and storefront windows
17 (DG 2.2.23). Conditions would ensure consistency with DGs that encourage integral
18 parapet roof designs that avoid a "stuck on" appearance (DGs 2.2.24, 2.2.26).

19 The store expansion is appropriately scaled for this Regional Commercial shopping area
20 comprising the Chico Mall and other large format retailers (DG 1.2.13). Bicycle and
21 pedestrian paths are well marked and in many cases lined with landscaping, which
22 enhances safety (DGs 2.1.23, 2.1.33, 2.1.34). Design Guideline consistency is further
23 enhanced by screening HVAC units with roof parapets, parking lot and drive-through areas
24 with landscaping, and wall-mounted utilities with other building elements, as called-for by
25 DGs 2.1.25, 2.1.36 and 2.2.28. Store elevations reflect superior design solutions over the
26 initial Project submittals, and eschew large, bright-blue trademark elements seen in other
27 communities (DGs 1.8.11, 1.8.12, 2.2.13).

28 The Project relates to the greater area and conveys permanence through the use of stone

1 and split-faced CMU, and colors are chosen from a rich palette with white accents that are
2 well placed to complement future signage (DGs 1.2.21, 1.2.32, 2.2.32). The proposed
3 painted and raised wall patterns would promote a pedestrian scale for the main building by
4 breaking up the elevations into single-story sized elements, and reducing the overall
5 perceived height (DG 2.2.11). Conditions are recommended to ensure Project consistency
6 with DG 1.5.13 and 1.5.16, regarding excessive nighttime lighting, and DG 2.1.28,
7 regarding the provision of adequate amounts of parking lot shading.

8 Based on all the above, and subject to conditions as noted, the Project is consistent with
9 the City's adopted Design Guidelines.

10 C. The design, materials and colors of the proposed new building are visually compatible with
11 the surrounding commercial development, specifically regarding the earth-tone colors,
12 exterior textures, flat parapet roof, and tower elements. Exterior equipment will be
13 properly screened from view by roof parapets, landscaping or by structural walls.

14 D. The proposed structures are compatible with the site in that there is ample space provided
15 and the design meets applicable development standards, including building height
16 limitations, setbacks, landscaping and parking requirements. The expanded store and new
17 fuel station will be adequately buffered by landscaping, and the buildings will not
18 unnecessarily block views or unacceptably dominate its surroundings.

19 E. The proposed landscaping includes sufficient variety and would provide visual relief along
20 the Forest Avenue frontage for the fuel station, at new and former driveways along Baney
21 Lane, and throughout the existing and proposed parking areas. Specific attention is paid
22 to screening the drive-through pharmacy/merchandise pick-up area with a variety of trees
23 and shrubs, and to enhance pedestrian orientation by lining the dedicated footpath through
24 the parking area with landscaping on one side. Overall, the landscape design, as
25 conditioned, would provide adequate coverage of plant materials and result in an attractive
26 environment across the entire Project site.

27 3. With regard to Use Permit 15-10 the Planning Commission finds that:

28 A. Chico Municipal Code Section 19.44.020 provides for drive-through sales in the CR zoning

1 district, subject to use permit approval, and CMC 19.76.120 provides for permanent
2 outdoor retail sales and activities in the CR zoning district, subject to use permit approval.
3 Use Permit 15-10 has been processed in accordance with the requirements of Chapter
4 19.24.

5 B. The proposed uses consist of the addition of drive-through sales to an expanded retail store
6 and outdoor retail sales within a partially-developed site. All drive-through improvements
7 will be constructed to City standards, which are intended to promote the health, safety, and
8 general welfare of the public. Drive-through access will be from an off-street parking area,
9 so any off-site affects would be minimal. Adequate vehicle circulation and stacking would
10 be provided, and emergency access to the site would remain unobstructed. The requested
11 outdoor sales in the parking area would not occupy required parking spaces, and the use
12 has existed for several years without any known issues. It does not block any emergency
13 access routes or otherwise disrupt onsite circulation. The outdoor sales component
14 requested on the storefront sidewalk area would not impede pedestrian circulation.
15 Outdoor parking lot sales are typical at large-format retailers, particularly in front of
16 Garden Centers, and sidewalk displays/sales are typical in front of grocery stores. No
17 detrimental impacts to the health, safety, or welfare of neighborhood workers or residents
18 have been identified in association with the proposed drive-through or outdoor sales uses.
19 Based on the above, the proposed drive-through and outdoor sales would not be detrimental
20 to the health, safety, and general welfare of persons residing or working in the
21 neighborhood of the proposed uses.

22 C. The proposed drive-through and outdoor retail sales would take access from an off-street
23 parking area where the use would be fully contained on the Project site, so any off-site
24 affects to property or improvements would be minimal. Adequate vehicle circulation and
25 stacking would be provided, and emergency access to the site would remain unobstructed.
26 Adding new driveways to access the site from Wittmeier Drive will add options for
27 incoming customers, helping to disperse traffic patterns across the site and minimize any
28 issues that may occur at existing points of access.

1 The outdoor sales component requested on the storefront sidewalk area would not impede
2 pedestrian circulation. Outdoor parking lot sales are typical at large-format retailers,
3 particularly in front of Garden Centers, and sidewalk displays/sales are typical in front of
4 grocery stores. No detrimental impacts to the health, safety, or welfare of neighborhood
5 workers or residents have been identified in association with the proposed drive-through
6 or outdoor sales uses.

7 Existing regulations require that any public improvements damaged during the course of
8 construction be repaired or reconstructed by the applicant. No other impacts to property
9 or improvements have been identified. Based on the above, the proposed drive-through
10 and outdoor sales would not be detrimental or injurious to property and improvements in
11 the neighborhood, or the general welfare of the City.

12 D. The proposed drive-through and outdoor sales uses are consistent with the Regional
13 Commercial designation for the site, which accommodates a wide variety of retail uses,
14 including those which are auto-oriented (such as drive-through uses). The uses are also
15 consistent with the General Plan's policy framework and the General Plan as a whole, as
16 outlined in Section 2, above.

17 E. The proposed uses consist of the addition of drive-through sales to an expanded retail store
18 and outdoor retail sales within a partially-developed site. All drive-through improvements
19 will be constructed to City standards. Access to the drive-through would be fully-contained
20 on site and compatible with existing and anticipated future commercial development within
21 the area. The outdoor sales in the parking area has existing for several years without
22 resulting in incompatibilities with other land uses, and other outdoor sales areas would be
23 limited in size, ensuring that they will not dominate building elevations or the result in
24 incompatibilities with existing or future land uses in the vicinity. Based on the above, the
25 design, location, size, and operating characteristics of the proposed use are compatible with
26 the existing and future land uses in the vicinity.

27 4. With regard to Use Permit 15-12 the Planning Commission finds that:

28 A. Chico Municipal Code Section 19.44.020 provides for gas station uses in the CR zoning

1 district, subject to use permit approval. Use Permit 15-12 has been processed in accordance
2 with the requirements of Chapter 19.24.

3 B. The proposed fuel station will be compatible with surrounding commercial uses, as
4 circulation and access for the use would be accommodated by a new driveway from
5 Wittmeier Drive as well as through an expanded parking area associated with the main
6 retail store. The installation of proposed and required improvements associated with
7 Wittmeier Drive and other access points to the site would result in adequate vehicle access.
8 No residences are located in close proximity to the site. No detrimental impacts to the
9 health, safety, or welfare of neighborhood workers or residents have been identified in
10 association with the proposed fuel station use. Based on the above, the proposed fuel
11 station would not be detrimental to the health, safety, and general welfare of persons
12 residing or working in the neighborhood of the proposed uses.

13 C. The proposed fuel station would take access from a new driveway off Wittmeier Drive and
14 an off-street parking area, and sufficient space is provided near the fuel pumps to ensure
15 that vehicle queues would be fully contained on the Project site, thereby minimizing
16 potential for any off-site affects to property or improvements. Existing regulations require
17 that any public improvements damaged during the course of construction be repaired or
18 reconstructed by the applicant. Based on the above, the proposed fuel station would not be
19 detrimental or injurious to property and improvements in the neighborhood, or the general
20 welfare of the City.

21 D. The proposed fuel station use is consistent with the Regional Commercial designation for
22 the site, which accommodates a wide variety of retail uses, including those which are auto-
23 oriented (such as fuel stations). The Project is also consistent with the General Plan's
24 policy framework and the General Plan as a whole, as outlined in Section 2, above.

25 E. The proposed fuel station would be set back from the street intersection, with an ample
26 landscape buffer from Forest Avenue and Wittmeier Drive, and constructed with sufficient
27 circulation to result in compatible operations with nearby land uses. Conditions associated
28 with the design review would ensure that the height of new lighting would be limited,

1 minimizing potential offsite affects. Based on the above, the proposed fuel station would
2 be compatible with existing and future land uses in the vicinity.

3 5. The Planning Commission hereby approves Tentative Parcel Map 15-02, Site Design and
4 Architectural Review 15-16, Use Permit 15-10, and Use Permit 15-12, each subject to compliance
5 with: (a) the Mitigation Monitoring and Reporting Program set forth in Exhibit I, (b) the conditions
6 of approval set forth in Exhibit II, and (c) the provisions of the Subdivision Report set forth in
7 Exhibit III.

8 THE FOREGOING RESOLUTION WAS ADOPTED by the Planning Commission of the
9 City of Chico at its meeting held on October 6, 2016, by the following vote:

10 AYES:

11 NOES:

12 ABSENT:

13 ABSTAINED:

14 DISQUALIFIED:

15
16 ATTEST:

17
18 _____
19 Mark Wolfe
20 Planning Commission Secretary

APPROVED AS TO FORM:

21
22 
23 _____
24 Vincent C. Ewing
25 City Attorney*

26
27 *Pursuant to The Charter of
28 the City of Chico, Section 906(E)

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**Mitigation Monitoring and Reporting Program
for the
Chico Walmart Expansion Project
Draft Environmental Impact Report
City of Chico, Butte County, California
State Clearinghouse Number 2015102017**

Prepared for:

City of Chico

Community Development Department
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Contact: Mr. Mike Sawley, Associate Planner

Prepared by:

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Contact: Jason Brandman, Project Director
Grant Gruber, Project Manager

Report Date: September 9, 2016

Table 1: Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>Section 3.2—Air Quality/Greenhouse Gas Emissions</p> <p>MM AIR-1: During construction activities, The applicant shall ensure that construction activities:</p> <ul style="list-style-type: none"> (1) Adhere to the provisions of Butte County Air Quality Management District Rules 205, 230, and 231; and (2) Incorporate specific Best Practices to minimize diesel exhaust and fugitive dust emissions during construction, as specified in Appendix C of the Butte County Air Quality Handbook, October 23, 2014, available at: http://bcaqmd.shasta.com/wp-content/uploads/CEQA-Handbook-Appendices-2014.pdf. Examples of these types of measures include, but are not limited to: <ul style="list-style-type: none"> • Limiting idling of construction vehicles to 5 minutes or less. • Ensuring that all small engines are tuned to the manufacturer’s specifications. • Powering diesel equipment with Air Resources Board-certified motor vehicle diesel fuel. • Utilizing construction equipment that meets ARB’s 2007 certification standard or cleaner. • Using electric powered equipment when feasible. • The applicant shall prepare and submit documentation (e.g., equipment plan or certification) to the City of Chico verifying compliance as part of the grading permit application. <p>Pursuant to the above, the developer shall prepare and submit a dust control plan consistent with Rule 205 prior to or in conjunction with building and/or grading permits, subject to approval by city staff, and shall note and demonstrate on the construction plans that architectural coatings and asphalt to be used for the project shall meet the standards set forth by District Rules 230 and 231. Additionally, the developer shall maintain material safety data sheets or other records at the construction site that document the content of volatile organic compounds (VOCs) of the architectural coatings and asphalt used at the project site, in compliance with District Rules 230 and 231.</p>	<p>Submittal of documentation; notes on construction plans; site inspection</p>	<p>Prior to issuance of building permits; during construction activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
MM AIR-2a: During the architectural coating phase of construction activities, the project applicant shall require that all construction contractors utilize low volatile organic compound (VOC) paint that does not exceed 150 grams of VOC per liter for interior architectural coatings. The applicant shall keep records documenting the VOC content of paint products and provide them to the City of Chico upon request.	Notes on construction plans; site inspection	During the architectural coating phase of construction activities	City of Chico		
MM AIR-2b: During grading and earthwork activities, the applicant shall require that all off-road diesel-powered construction equipment greater than 50 horsepower meet or exceed United States Environmental Protection Agency (EPA) Tier 3 off-road emissions standards. The applicant shall keep records certifies documenting the emissions rating of all off-road diesel-powered construction equipment and provide them to the City of Chico upon request.	Notes on construction plans; site inspection	During grading and earthwork activities	City of Chico		
MM AIR-2c: To the extent feasible, future development on Parcels 2 and 3 shall provide pedestrian sidewalk connections to both the Walmart store and bike path networks, and new structures shall include roof overhangs that are sufficient to block the high summer sun from windows, but also allow the lower winter sun to enter windows.	Site Design and Architectural Review; review of building plans	Prior to issuance of building permits	City of Chico		
MM AIR-2d: Prior to issuance of building permits, the project applicant shall prepare and submit building plans to the City of Chico that demonstrate that all new structures have outdoor electrical outlets that are accessible to maintenance workers and landscapers to allow the use of electric powered equipment.	Review of building plans	Prior to issuance of building permits	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>MM AIR-2e: Prior to issuance of building permits, the applicant shall revise the landscape plans to include primarily native drought-resistant trees that are low ROG emitting. For example, the sycamores (<i>Platanus</i> spp.) shall be replaced with species such as maples (<i>Acer</i> spp.), pistache (<i>Pistacia</i> spp.) and zelkova species.</p>	<p>Review of building plans</p>	<p>Prior to issuance of building permits</p>	<p>City of Chico</p>		
<p>MM AIR-2f: Prior to issuance of the certificate of occupancy for the expanded Walmart store, the applicant shall retain a qualified transportation consultant to prepare and submit a Transportation Demand Management (TDM) program to the City of Chico for review and approval. The TDM program shall identify measures to reduce daily gasoline-powered and diesel-powered vehicle trips to the project site, with an objective of a minimum 10 percent reduction in gasoline-powered and diesel-powered trips. The approved TDM program shall be implemented in conjunction with the opening of the expanded store. Examples of trip reduction measures may include, but are not limited to, the following:</p> <ul style="list-style-type: none"> • Post transit information (maps, schedules, fares, etc.) in a public area of Walmart that is accessible to employees and patrons; • Provide employer-subsidized transit passes; • Sponsor an employee ride sharing program; • Provide employee lockers for personal items; • Provide employees with an employee only restroom with a shower; • Provide secure indoor bicycle parking (racks or lockers) for employees; • Provide customer bicycle parking (racks) in safe and convenient locations; • Allow flex scheduling or compressed scheduling practices; • Provide preferential parking spaces for clean air vehicles; • Provide a minimum of two charging stations for electric vehicles; and 	<p>Review of TDM program; site inspection</p>	<p>Prior to issuance of the certificate of occupancy for the expanded Walmart store</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<ul style="list-style-type: none"> If home delivery service is provided in the future, it shall be performed using low-emission or alternative-fueled (electric, natural gas, hydrogen, etc.) vehicles. 					
<p>MM AIR-2g: To the extent that such equipment is readily available and can adequately perform all tasks, Walmart shall use electric-, propane-, or natural gas-powered mobile equipment (forklifts, non-street legal street sweepers, etc.) for operational activities within the project site. Existing gasoline- or diesel-powered mobile equipment may continue to be used until its service life is exhausted.</p>	Submittal of documentation; site inspection	Prior to occupancy of the expanded Walmart store	City of Chico		
<p>MM AIR-2h: Prior to occupancy of the expanded Walmart store, the applicant shall enhance the existing Butte Regional Transit stop on Forest Avenue. The enhanced transit stop shall conform to Butte Regional Transit design standards and provide a minimum of a shelter, seating, lighting, transit information, and a pedestrian connection to the Walmart store entrance.</p>	Site inspection	Prior to occupancy of the expanded Walmart store	City of Chico		
<p>MM AIR-2i: Prior to issuance of occupancy permits for the expanded Walmart store, the project applicant shall implement idling restriction program for heavy-duty diesel vehicles. The program shall require that all trucks comply with state regulations limiting idling to no more than 5 minutes. The program shall be implemented through signage in all loading areas and training of store personnel about the idling restrictions.</p>	Submittal of documentation; site inspection	Prior to issuance of occupancy permits for the expanded Walmart store	City of Chico		
<p>MM AIR-2j: Prior to issuance of occupancy permits for the expanded Walmart store, the project applicant shall provide at least of one electrical hookup in each of the two proposed Walmart loading docks that is capable of powering a truck-mounted transport refrigeration unit (TRU) with an electrical hookup option.</p>	Site inspection	Prior to issuance of occupancy permits for the expanded Walmart store	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>MM AIR-2k: Prior to the recordation of the Final Map, the project applicant shall participate in an Offsite Mitigation Program coordinated through the BCAQMD. The project applicant shall utilize a methodology based on the BCAQMD CEQA Handbook with final details to be approved by the BCAQMD and City for calculating the payment to the Offsite Mitigation Program.</p>	<p>Submittal of documentation</p>	<p>Prior to the recordation of the Final Map</p>	<p>City of Chico</p>		
<p>Section 3.3—Biological Resources</p>					
<p>MM BIO-1: Prior to issuance of any grading or construction permits or the first ground-disturbing activities, the applicant shall retain a qualified botanist to conduct two focused botanical surveys for special-status plant species within the undeveloped portion of the project site. The first survey shall occur in April and the second survey shall occur in either May or June. The surveys shall be conducted for the following plant species: Ahart’s paronychia, Butte County meadowfoam, woolly meadowfoam, adobe lily, Bidwell’s knotweed, Butte County golden clover, depauperate milkvetch, Ferris’ milkvetch, hogwallow starfish, pink creamsacs, Red Bluff dwarf rush, round-leaved filaree, Tehama navarretia, and veiny monardella. If no special-status plants are observed, then a letter report documenting the results of the final survey shall be provided to the project applicant and the City of Chico Community Development Department for their records, and no additional measures are required. If any of the non-listed special-status plants occur within the project site, they shall be avoided to the extent feasible. If the plants cannot be avoided, the biologist shall prepare a special-status plant mitigation and monitoring plan in consultation with the California Department of Fish and Wildlife (CDFW). The plan shall be subject to CDFW approval and may include, but is not limited, to the following elements as applicable to the species, based on the project</p>	<p>Submittal of documentation</p>	<p>Prior to issuance of any grading or construction permits or the first ground-disturbing activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<ul style="list-style-type: none"> A salvage/transplanting program for the salvage and transfer of special-status plants populations before the initiation of construction activities to another location either off-site or on-site that will be preserved in perpetuity (via conservation easement, deed restriction, or other appropriate legal means). Permits may be required from the CDFW or USFWS if a listed species is found and may require further mitigation in consultation with the appropriate agency or agencies. The program shall specify appropriate timing of salvage and transplanting efforts based on the specific special-status plant species found and the propagation program shall establish success criteria for the affected special-status plants. In addition to salvaging of special-status plants themselves, salvage efforts shall include topsoil and seedbanks surrounding impacted plants, if doing so will not contribute to the spread of invasive or noxious plant species. Purchasing credits at an off-site mitigation bank approved by USFWS or CDFW, as applicable, based on the listed plant species found. Criteria that must be met within the special-status plant mitigation and monitoring plan prior to commencement of construction or grading within the undeveloped portions of the project site. <p>USFWS or CDFW, as applicable, shall approve the plan prior to transpiration or commencement of grading or construction activities.</p> <p>This mitigation measure shall not apply if the Draft Butte Regional Conservation Plan is adopted at the time of ground-disturbing activities and provides coverage for impacts to these plant species, and all required conservation measures or mitigation required under the Conservation Plan for this project are implemented. If this scenario occurs, mitigation that occurs pursuant to the Conservation Plan shall not be less than the procedures spelled out in this mitigation measure.</p>	<p>Submittal of documentation</p>	<p>Prior to issuance of any grading or construction permits or the first ground-disturbing activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>MM BIO-2a: Prior to any ground-disturbing activities, the applicant shall retain a qualified biologist to conduct a protocol-level survey for the western spadefoot during the appropriate period to identify western spadefoot toads (e.g., following a rain event during the breeding season, identification of other known active breeding populations in the region). If no western spadefoot are observed, then a letter report shall be submitted to the City of Chico Community Development Department and CDFW for their records. Additionally, a preconstruction survey shall be conducted within 14 days prior to the start of any ground disturbance to verify absence of this species from the project area.</p> <p>If western spadefoot are found, ground disturbance shall cease and consultation with CDFW shall occur prior to resumption of ground-disturbing activities to develop an impact minimization plan that may include but is not limited to relocation, timing restrictions, habitat compensation, etc. The plan shall be reviewed and subject to approval by CDFW and implemented prior to ground-disturbing activities.</p> <p>In addition, if western spadefoot are found, the biologist shall be on-site during initial grading. If western spadefoot toad is observed within the project footprint, all construction shall temporarily halt until the biologist relocates the species to suitable habitat within the project site, but outside of the project footprint, or other suitable location in the project vicinity.</p> <p>This mitigation measure shall not apply if the Draft Butte Regional Conservation Plan is adopted at the time of ground-disturbing activities and provides coverage for impacts to the western spadefoot and all required conservation measures or mitigation required under the Butte Regional Conservation</p>	<p>Notes on construction plans; submittal of documentation; site inspection</p>	<p>Prior to any ground-disturbing activities; during initial grading activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>Plan for this project are implemented. If this scenario occurs, mitigation that occurs pursuant to the Conservation Plan shall not be less than the procedures spelled out in this mitigation measure.</p> <p>MM BIO-2b: Prior to the first ground-disturbing activities, the project applicant shall retain a qualified biologist to conduct four protocol-level surveys during the breeding season (one site visit between February 15 and April 15 and three between April 15 and July 15, one of which shall be conducted after June 15), at least three weeks apart, in accordance with the 2012 California Department of Fish and Wildlife Staff Report on Burrowing Owl Mitigation (2012 Staff Report). The survey shall include an approximately 500-foot (150-meter) buffer around the project site, where access is permitted. If the surveys are negative, then a letter report shall be prepared documenting the methodology and results within two weeks following the final survey. If the surveys result in negative findings, the biologist shall still conduct a take avoidance survey between 14 days and 30 days prior to commencement of construction, in accordance with the 2012 Staff Report. A letter report documenting the methodology and results shall be submitted to the City of Chico Community Development Department and CDFW following the results of the final survey for their records.</p> <p>If burrows are observed within the undeveloped portion of the project site or within 500 feet of the project site, an impact assessment shall be prepared and submitted to the CDFW, in accordance with the 2012 Staff Report. If it is determined that project activities may result in impacts to nesting, occupied, and satellite burrows and/or burrowing owl habitat, the project proponent shall consult with CDFW and develop a detailed mitigation plan such that the habitat acreage, number of</p>	<p>Notes on construction plans; submittal of documentation; site inspection</p>	<p>Prior to the first ground-disturbing activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>burrows, and burrowing owls impacted are replaced in a manner acceptable to CDFW. The mitigation plan shall be based on the methods provided in the 2012 Staff Report.</p> <p>This mitigation measure shall not apply if the Draft Butte Regional Conservation Plan is adopted at the time of ground-disturbing activities and provides coverage for impacts to the burrowing owl and all required conservation measures or mitigation required under the Butte Regional Conservation Plan for this project are implemented. If this scenario occurs, mitigation that occurs pursuant to the Conservation Plan shall not be less than the procedures spelled out in this mitigation measure.</p>					
<p>MM BIO-2c: No more than 14 days prior to the start of ground disturbance or tree removal, the applicant shall retain a qualified biologist to conduct a pre-construction surveys for special-status bat species. If no special-status bats are observed roosting, then a letter report documenting the results of the survey shall be provided to the City of Chico Community Development Department, CDFW, and any other applicable resource agency for their records. A subsequent bat survey shall be required if construction does not commence within 14 days of a negative survey, or if construction is planned to resume after halting for more than 14 days.</p> <p>If bats are found, the biologist shall consult with the CDFW prior to ground disturbance, tree removal, or other construction activity to determine specific avoidance measures. Avoidance measures may include establishing a minimum 250-foot buffer around the roosting location until it is no longer occupied. Construction activities, including but not limited to tree removal, shall not proceed within the buffer until the biologist has determined that the roost is no longer</p>	Notes on construction plans; submittal of documentation; site inspection	No more than 14 days prior to the start of ground disturbance or tree removal; during construction activities	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>occupied by the bats and has communicated the determination in writing to the City of Chico Community Development Department and CDFW.</p> <p>MM BIO-2d: No more than 14 days prior to any vegetation removal activities that occur during the nesting season (February 15 and August 31), the applicant shall retain a qualified biologist to conduct a preconstruction survey for active nests. If the preconstruction survey shows that there is no evidence of active nesting bird nests, then a letter report shall be provided to the City of Chico Community Development Department and any applicable resource agency for their records.</p> <p>If any active nests are located within the project site, an appropriate buffer zone shall be established around the nests, as determined by the biologist. The biologist shall mark the buffer zone with construction tape or pin flags and maintain the buffer zone, where construction activities shall be prohibited, until the end of breeding season or until the young have successfully fledged. Buffer zones are typically 100 feet for migratory bird nests and 250 to 500 feet for raptor nests. A qualified biologist shall monitor active nests weekly during construction to evaluate potential nesting disturbance by construction activities. If establishing the typical buffer zone is impractical, the qualified biologist may reduce the buffer depending on the species, and daily monitoring by the biologist is required to ensure that the nest is not disturbed and no forced fledging occurs. Monitoring as described above shall continue until the qualified biologist determines that the nest is no longer occupied and has communicated the determination in writing to the City of Chico Community Development Department and applicable resource agency.</p> <p>This mitigation measure does not apply to vegetation removal</p>	<p>Notes on construction plans; submittal of documentation; site inspection</p>	<p>No more than 14 days prior to any vegetation removal activities that occur during the nesting season (February 15 and August 31); during construction activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>activities that occur outside of the nesting season (September 1 to February 14).</p> <p>MM BIO-2e: For phases of the project that would result in ground disturbance within 250 feet of the seasonal wetland but do not involve any disturbance within 50 feet of the seasonal wetland, the applicant shall implement indirect impact avoidance measures as follows:</p> <ol style="list-style-type: none"> 1. Retain a qualified biologist to direct the installation of silt fencing and high-visibility fencing a minimum of 50 feet from the edge of the seasonal wetland; 2. Conduct a Worker Awareness Training Program prior to the start of construction informing workers of the wetland and advising them to stay clear of the area. The training shall be repeated as necessary for all subsequent sub-contractor crews that are scheduled to begin working on the project; 3. Retain a qualified biologist to monitor on-site vegetation clearing and initial grading activities; and 4. Develop and implement a dust control plan, consistent with Rule 205 of the Butte County Air Quality Management District (Fugitive Dust Emissions), which regards the silt fencing around the seasonal wetland as a property line over which allowing wind-driven fugitive dust shall be minimized. <p>Upon submitting applications to the city for grading, construction, or other site disturbance, the developer shall provide a scaled drawing that depicts the wetland, 50-foot and 250-foot buffers around the wetland, and proposed work area(s), including designated equipment/material staging and worker parking areas. The developer shall arrange a preconstruction inspection of the silt/visibility fencing by city staff prior to commencement of construction.</p>	<p>Notes on construction plans; submittal of documentation; site inspection</p>	<p>Prior to and during construction activities that would occur within 250 feet of the seasonal wetland</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>If future phases of the project necessitate directly impacting the seasonal wetland, such as development of Parcel 2 or other project feature, the developer shall either assume presence or conduct protocol-level surveys for listed vernal pool branchiopods in accordance with the USFWS Survey Guidelines prior to any ground-disturbing activities. Protocol surveys consist of one wet-season survey and one dry-season survey. If the developer decides to conduct protocol-level surveys and the surveys result in negative findings of the federally listed species, then no additional measures are required so long as the USFWS accepts the negative survey results. If listed branchiopods are found during protocol surveys or presence is assumed, then the applicant shall pursue consultation with the USFWS to obtain a Biological Opinion that authorizes the work. The developer shall implement all conditions (aka Reasonable and Prudent measures) identified in the Biological Opinion as well as those contains in any associated permits.</p> <p>If a future phase of the project necessitates work within 50 feet of the seasonal wetland but no direct impacts are anticipated then the developer shall prepare a detailed plan for the work demonstrating that no impacts will occur to the wetland and obtain letter of no effect (or “not likely to adversely affect” determination) from the Army Corps of Engineers.</p> <p>This mitigation measure shall not apply if the Draft Butte Regional Conservation Plan is adopted at the time of grading activities and provides coverage for impacts to the listed vernal pool shrimp species, and all required conservation measures or mitigation required under the Conservation Plan for this project are implemented. If this scenario occurs, mitigation that occurs pursuant to the Conservation Plan shall not be less than the procedures spelled out in this mitigation measure.</p>	<p>Notes on construction plans; submittal of documentation; site inspection</p>	<p>Prior to and during construction activities that would occur within 250 feet of the seasonal wetland</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>MM BIO-3: The applicant shall modify all improvement plans and construction drawings to provide a minimum separation of 50 feet between the seasonal wetland and (1) the poles associated with the relocated high-voltage overhead power lines, and (2) the relocated Class I bicycle/pedestrian path, subject to review and approval by City Public Works and Community Development Department staff. The separation distance of these project features from the wetland shall be sufficient to enable implementation of the indirect impact avoidance measures of Mitigation Measure BIO-2e.</p>	<p>Approval of improvement plans</p>	<p>Prior to construction activities</p>	<p>City of Chico</p>		
<p>MM BIO-4: If construction occurs within 50 feet of the depression seasonal wetland in the southwestern portion of the project site, the applicant shall obtain all requisite approvals and permits from the USACE and Regional Water Quality Control Board (RWQCB) for impacts to Waters of the United States and Waters of the State prior to issuance of any City permits for construction, grading, or demolition activities that occurs within 100 feet of the resource. If the USACE or RWQCB determine that no permit from their agency is required for a particular phase of the project, then this mitigation measure only requires acknowledgement of such from each agency regarding the project phase in question. If any future phase of the project involves direct impacts to the wetland, then the developer shall mitigate the impact through either (1) creation and preservation of seasonal wetland habitat within the project site, (2) off-site restoration of features of equal or greater value, or (3) purchase of credits at an agency-approved mitigation bank in the region at a ratio to be determined through consultation with the USACE and RWQCB, but no less than a 1:1 ratio.</p> <p>This mitigation measure shall not apply if the Draft Butte</p>	<p>Submittal of documentation</p>	<p>Prior to construction activities that occur within 100 feet of the depression seasonal wetland</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
Regional Conservation Plan is adopted at the time of grading activities and provides coverage for impacts to the depositional seasonal wetland, and all required conservation measures or mitigation required under the Conservation Plan for this project are implemented. If this scenario occurs, mitigation that occurs pursuant to the Conservation Plan shall not be less than the procedures spelled out in this mitigation measure.					
Section 3.4—Cultural Resources					
MM CUL-1: If prehistoric or historic-period archaeological resources are encountered, all earth-disturbing construction activities within 100 feet of the find shall halt and the City of Chico Community Development Department shall be notified at (530) 879-6800. Prehistoric archaeological materials might include obsidian and chert flakedstone tools (e.g., projectile points, knives, scrapers) or toolmaking debris; culturally darkened soil (midden) containing heat-affected rocks, artifacts, or shellfish remains; and stone milling equipment (e.g., mortars, pestles, handstones, or milling slabs); and battered stone tools, such as hammerstones and pitted stones. Historic-period materials might include stone, concrete, or adobe footings and walls; filled wells or privies; and deposits of metal, glass, and/or ceramic refuse. A Secretary of the Interior-qualified archaeologist shall inspect the findings within 72 hours of discovery. If it is determined that the project could damage a historical resource or a unique archaeological resource (as defined pursuant to the CEQA Guidelines) mitigation shall be implemented in accordance with PRC Section 21083.2 and Section 15126.4 of the CEQA Guidelines, with a preference for preservation in place. Consistent with Section 15126.4(b)(3), this may be accomplished through	Notes on construction plans	Prior to construction activities	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>planning construction to avoid the resource; incorporating the resource within open space; capping and covering the resource; or deeding the site into a permanent conservation easement. If avoidance is not feasible, a qualified archaeologist shall prepare and implement a detailed treatment plan in consultation with the City of Chico. Treatment of unique archaeological resources shall follow the applicable requirements of PRC Section 21083.2. Treatment for most resources would consist of (but would not be limited to) sample excavation, artifact collection, site documentation, and historical research, with the aim to target the recovery of important scientific data contained in the portion(s) of the significant resource to be impacted by the Project. The treatment plan shall include provisions for analysis of data in a regional context, reporting of results within a timely manner, curation of artifacts and data at an approved facility, and dissemination of reports to local and state repositories, libraries, and interested professionals.</p>	Notes on construction plans	Prior to construction activities	City of Chico		
<p>MM CUL-3: In the event a fossil is discovered during construction for the proposed project, excavations within 50 feet of the find shall be temporarily halted or delayed until the discovery is examined by a qualified paleontologist, in accordance with Society of Vertebrate Paleontology standards. The applicant shall include a standard inadvertent discovery clause in every construction contract to inform contractors of this requirement. If the paleontological resources are found to be significant, they shall be avoided by project construction activities and recovered by a qualified paleontologist. Upon completion of the recovery, a paleontological assessment shall be conducted by a qualified paleontologist to determine if further monitoring for paleontological resources is required. The assessment shall include (1) the results of any geotechnical</p>					

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>investigation prepared for the project area, (2) specific details of the construction plans for the project area, (3) background research, and (4) limited subsurface investigation within the project area. If a high potential to encounter paleontological resources is confirmed, subsurface construction shall be temporarily halted until a monitoring plan of further project subsurface construction is prepared in conjunction with this assessment. After project subsurface construction has ended, a report documenting monitoring, methods, findings, and further recommendations regarding paleontological resources shall be prepared and submitted to the City of Chico.</p>					
<p>MM CUL-4: In the event of discovery or recognition of any human remains during construction activities, such activities within 100 feet of the find shall cease until the Butte County Coroner has been contacted to determine that no investigation of the cause of death is required. The Native American Heritage Commission (NAHC) shall be contacted within 24 hours if it is determined that the remains are Native American. The NAHC will then identify the person or persons it believes to be the most likely descendant from the deceased Native American (PRC Section 5097.98), who in turn would make recommendations to the City of Chico for the appropriate means of treating the human remains and any associated funerary objects (CEQA Guidelines Section 15064.5(d)).</p>	<p>Notes on construction plans</p>	<p>Prior to construction activities</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>Section 3.7—Hydrology and Water Quality</p> <p>MM HYD-1a: Prior to the issuance of grading permits, the project applicant shall file a Notice of Intent with and obtain a facility identification number from the State Water Resources Control Board. The project applicant shall also submit a Stormwater Pollution Prevention Plan (SWPPP) to the City of Chico that identifies specific actions and Best Management Practices (BMPs) to prevent stormwater pollution during construction activities. The City of Chico shall review and approve the SWPPP. The SWPPP shall identify a practical sequence for BMP implementation, site restoration, contingency measures, responsible parties, and agency contacts. The SWPPP shall include but not be limited to the following elements:</p> <ul style="list-style-type: none"> • Comply with the requirements of the State of California’s most current Construction Stormwater Permit. • Temporary erosion control measures shall be implemented on all disturbed areas. • Disturbed surfaces shall be treated with erosion control measures during the October 15 to April 15 rainy season. • Sediment shall be retained on-site by a system of sediment basins, traps, or other BMPs. • The construction contractor shall prepare Standard Operating Procedures for the handling of hazardous materials on the construction site to eliminate discharge of materials to storm drains. • BMP performance and effectiveness shall be determined either by visual means where applicable (e.g., observation of above-normal sediment release), or by actual water sampling in cases where verification of contaminant reduction or elimination (such as inadvertent petroleum release) is required by the Central Valley Regional Water 	<p>Approval of Stormwater Pollution Prevention Plan; notes on construction plans</p>	<p>Prior to the issuance of grading permits</p>	<p>City of Chico</p>		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>Quality Control Board to determine adequacy of the measure.</p> <ul style="list-style-type: none"> In the event of significant construction delays or delays in final landscape installation, native grasses or other appropriate vegetative cover shall be established on the construction site as soon as possible after disturbance, as an interim erosion control measure throughout the wet season. 					
<p>MM HYD-1b: Prior to the issuance of building permits, the project applicant shall submit a final Storm Water Mitigation Plan (SWMP) to the City of Chico for review and approval. The plan shall be developed using the California Stormwater Quality Association’s “New Development and Redevelopment Handbook.” The SWMP shall identify pollution prevention measures and BMPs necessary to control stormwater pollution from operational activities and facilities, and provide for appropriate maintenance over time. The SWMP shall include design concepts that are intended to accomplish a “first flush” objective that would remove contaminants from the first 2 inches of stormwater before it enters area waterways. The project applicant shall also prepare and submit an Operations and Maintenance Agreement to the City identifying procedures to ensure that stormwater quality control measures work properly during operations.</p>	Approval of Storm Water Mitigation Plan	Prior to the issuance of building permits	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
Section 3.9—Noise					
<p>MM NOI-1: The following noise attenuation measures shall be implemented during construction activities:</p> <ul style="list-style-type: none"> Construction activities are limited to the hours of 7:00 a.m. to 9:00 p.m. Monday through Saturday, and 10:00 a.m. to 6:00 p.m. on Sundays and federal holidays. The City of Chico shall have discretion to permit construction activities to occur outside of these hours if compelling circumstances warrant an exception (e.g., weather conditions necessary to pour concrete). All construction equipment shall use noise reduction devices (e.g., mufflers and engine shrouds) that are no less effective than those originally installed by the manufacturer. If no noise reduction devices were installed by the manufacturer, a minimum of a muffler shall be installed for the duration of project-related construction activities. If stationary equipment (compressors, pumps, generators, etc.) is to be used within 300 feet of any office or residential receptor, a noise barrier shall be erected to attenuate noise. The noise barrier shall consist of an enclosure, temporary noise panels/barriers, or acoustical/sound blankets over fencing. 	Notes on construction plans; site inspection	During construction activities	City of Chico		
Section 3.11—Transportation					
<p>MM TRANS-3a: Prior to issuance of the first building permit for the proposed project, the applicant shall provide the City of Chico with the requisite funds for improving the intersection of E. 20th Street / Forest Avenue. The improvements shall consist of: (1) restriping the northbound approach (Forest Avenue) to provide a left-turn lane, a shared left-turn/through lane, a through lane, and a right-turn lane; and (2) modifying the signal operation to provide split phasing on the</p>	Receipt of fees	Prior to issuance of the first building permit	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
northbound and southbound approaches. The City of Chico shall implement the improvements when monitoring determines that the intersection operation is approaching unacceptable levels.					
MM TRANS-3b: Prior to issuance of building permits for the proposed project, the applicant shall pay the fair-share cost of off-site circulation improvements, as calculated through the City's adopted development impact fee program.	Receipt of fees	Prior to issuance of building permits	City of Chico		
MM TRANS-5: Prior to the approval of any improvement plans for Wittmeier Drive, the project applicant shall retain a qualified traffic engineer to evaluate the following options for addressing (1) conflicts between through and turning vehicles and (2) vehicle movement enhancements on Wittmeier Drive. The engineer shall identify a recommended option or options for each issue, and the applicant shall prepare and submit plans to the City of Chico for review and approval that reflect the recommended options. The approved plans shall be incorporated into the proposed project. To address conflicts between through and turning vehicles, one or both of the following options shall be implemented: <ul style="list-style-type: none"> • Option 1: Modify the eastbound dual left-turn lanes on Wittmeier Drive to provide a single left-turn lane, thereby providing an extra lane to facilitate westbound turning movements and truck loading and unloading. Based on estimated Plus Project peak-hour left-turn movements from Wittmeier Drive to Forest Avenue, it is not anticipated that dual left-turn lanes are necessary to accommodate turning vehicle queues. • Option 2: Install a roundabout at the Wittmeier Drive cul-de-sac to address potential vehicular conflicts between vehicles turning in the cul-de-sac and vehicles entering and exiting 	Review of improvement plans	Prior to the approval of any improvement plans for Wittmeier Drive	City of Chico		

Table 1 (cont.): Chico Walmart Expansion Project Mitigation Monitoring and Reporting Program

Mitigation Measures	Method of Verification	Timing of Verification	Responsible for Verification	Verification of Completion	
				Date	Initial
<p>the westernmost Walmart driveway on Wittmeier Drive. The roundabout would also address potential safety issues arising from vehicles entering and exiting the westernmost Walmart driveway at higher rates of speed.</p> <p>To address vehicle movement enhancements, one of the following options shall be implemented:</p> <ul style="list-style-type: none"> • Option A: One left-turn lane and one through-right lane in the eastbound direction, and one through-left lane and one right-turn pocket in the westbound direction. Truck loading and unloading activity could occur on the curbside between the two driveways on the south side of Wittmeier Drive. • Option B: One left-turn lane and one through-right lane in the eastbound direction, one through-right lane in the westbound direction, and one two-way center left-turn lane with a median extending 50 feet west of the Forest Avenue/Wittmeier Drive intersection. Truck loading and unloading activity could occur in the two-way center left-turn lane. 	Review of final parcel map	Prior to recordation of the final parcel map	City of Chico		
<p>MM TRANS-7: Prior to recordation of the final parcel map, the project applicant shall provide an easement for a potential future Class I bikeway connection that would extend south from the southwesterly corner of the project site to the planned SR-99 Bikeway.</p>					

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EXHIBIT “II”
CONDITIONS OF APPROVAL
Chico Walmart Expansion Project
(PM 15-02, AR 15-16, UP 15-10 and UP 15-12)

Parcel Map PM 15-02

1. The creation of three lots on an approximately 27-acre site is authorized, as depicted on the “PM 15-02 Vesting Tentative Map, 2044 Forest Avenue, Chico, CA 95928”, date-stamped Sep 21, 2015, by the Planning Services Division, except as revised by any of the following conditions of approval.
2. The subdivision shall be developed in compliance with all other applicable State and local Code provisions, including those of the Public Works Department and the Fire Department. The applicant is responsible for contacting these offices to verify the need for permits.
3. In the event that all fees have not been paid prior to recordation of the final map, the following notation shall be included on the final map:

“In accordance with the provisions of the Chico Municipal Code, a transportation facility fee, park facility fee, and building and equipment fee may be assessed and levied upon the owner of any lot or parcel within this subdivision at the time a new building or structure is constructed on such lot or parcel, at the time an alteration or addition is made to an existing building or structure constructed on such lot or parcel which results in the expansion of building or structure, or at the time of a change in use of an existing building or structure constructed on the lot or parcel. In addition, a storm drainage facility fee may be assessed and levied upon the owner of any lot or parcel within this subdivision at the time such lot or parcel is first used for any residential or nonresidential purpose, at the time the area of the lot or parcel devoted to such residential or nonresidential use is expanded, or at the time of a change in the use of the lot or parcel. Such transportation facility fee, park facility fee, building and equipment fee and storm drainage facility fee will be calculated from the schedule of such fees adopted by resolution of the City Council and in effect on the date of approval of such final map or parcel map, together with any adjustments to such schedules of fees made in accordance with the provisions of the Chico Municipal Code subsequent to the date of approval of the final map or parcel map to account for any changes in the type or extent of transportation facilities, park facilities, buildings and equipment and/or storm drainage facilities which will be required as a result of the development and/or use of real property during the period upon which such fees are based, any change in the estimated cost of the transportation facilities, park facilities, buildings and equipment and/or storm drainage facilities upon which such fees are based, or any change in that portion of the estimated cost of such transportation facilities, park facilities, buildings and equipment and/or storm drainage facilities which cannot be funded from revenue sources available to the City other than such fees.”

4. Prior to recording the final map, any taxes and/or assessments against the property shall be paid.
5. Impacts to school facilities within the Chico Unified School District shall be fully mitigated by payment of school impact fees to the extent permitted by State Law.

6. All qualifying trees, pursuant to CMC 16.66, that are removed with the project shall be replaced as follows:
 - a. On-site. For every six inches in DBH removed, a new 15 gallon tree shall be planted on-site. Replacement trees shall be of similar species, unless otherwise approved by the urban forest manager, and shall be placed in areas dedicated for tree plantings. New plantings' survival shall be ensured for three years after the date of planting and shall be verified by the applicant upon request by the director. If any replacement trees die or fail within the first three years of their planting, then the applicant shall pay an in-lieu fee as established by a fee schedule adopted by the City Council.
 - b. Off-site. If it is not feasible or desirable to plant replacement trees on-site, payment of an in-lieu fee as established by a fee schedule adopted by the City Council shall be required.
7. The "vesting" designation shall be removed from the parcel map title, as the provisions of CMC 18.22.020 only apply to residential subdivisions and the subject map is not a residential subdivision.
8. Final alignment of the public bike path easement and improvement details shall be subject to review and approval by the Public Works Director.
9. The width of the public bike path easement along the State Route 99 frontage shall be 20 feet, to accommodate potential design constraints associated with site topography and physical bike path features (lighting, signage, etc.).
10. The relocated PG&E easement for the overhead electrical lines shall be revised to evenly straddle the shared east-west property line between Parcel 1 and Parcel 2, and between Parcel 1 and Parcel 3. This condition shall not apply if demonstrated to be infeasible to the satisfaction of the Public Works Director.

Architectural Review AR 15-16

11. All approved building plans and permits shall note on the cover sheet that the project shall comply with AR 15-16 (Chico Walmart Expansion Project). No building permits related to this approval shall be finalized without authorization of Planning staff.
12. All approved building plans and permits shall note that wall-mounted utilities and roof or wall penetrations, including vent stacks, utility boxes, exhaust vents, gas meters and similar equipment, shall be screened by appropriate materials and colors. All parapet caps and other metal flashing shall be painted, consistent with the approved building colors. Adequate screening shall be verified by Planning staff prior to issuance of a certificate of occupancy.

13. New tower elements, including cornice, on the Walmart store expansion shall extend back onto the main roof a minimum of six feet to ensure that they appear sufficiently integrated with the structure from views at ground level.
14. All new pole-mounted lights in the parking area shall be limited to 18 feet in height. Existing pole-mounted light assemblies in the parking area shall be replaced, as needed over time, with poles/bases that also meet this 18-foot height limit for parking lot lights.
15. Prior to issuance of building plans for the store expansion, an inventory of existing shade trees in the parking area shall be conducted. Any missing tree shall be replaced with a new shade tree in compliance with City standard planting plans. Any existing honey locust, London plane tree, Chinese Pistache, Chinese hackberry, scarlet oak or pin oak tree that has grown to less than 20 feet in diameter shall be replaced with a new shade tree in compliance with City standard planting plans. The provision of adequate irrigation shall also be confirmed or corrected in conjunction with parking area landscape improvements.

Use Permit 15-10 (Walmart Drive-Through and Outdoor Sales)

16. Use Permit 15-10 authorizes drive-through sales and outdoor sales, in substantial accord with the “Plat to Accompany Use Permit 15-10 (Walmart Drive-Through and Outdoor Sales)” and in compliance with all other conditions of approval.
17. The permittee shall comply with all other State and local Code provisions, including those of the Building Division, Public Works Department, Fire Department, and Butte County Environmental Health. The permittee is responsible for contacting these offices to verify the need for permits.
18. Outdoor vending machines at the site shall meet the code standards contained in CMC 19.76.120.B, including but not limited to:
 - a. Outdoor vending machines are permitted along the building frontage that includes the primary business entrance. Where the primary business entrance is located at the building corner, outdoor vending machines will be permitted on only one side of the building.
 - b. Outdoor vending machines shall not exceed 10 percent of the width of the building frontage along which they are located, nor shall the combined area of all vending machines at any one site exceed 32 square feet.

Use Permit 15-12 (Walmart Fuel Station)

19. Use Permit 15-12 authorizes a gas station use, in substantial accord with the “Plat to Accompany Use Permit 15-12 (Walmart Fuel Station)” and in compliance with all other conditions of approval.

20. The permittee shall comply with all other State and local Code provisions, including those of the Building Division, Public Works Department, Fire Department, and Butte County Environmental Health. The permittee is responsible for contacting these offices to verify the need for permits.

21. Comply with all requirements set forth under Chico Municipal Code Section 19.76.090 (Gas Stations), including but not limited to:
 - a. Landscaping within the sight distance area at the northwest corner of the intersection of Forest Avenue and Wittmeier Drive shall be maintained at a height of 30 inches or less, except for trees with a vertical growth habit that do not cause sight distance issues as determined by the Public Works Director.
 - b. The Russian sage planned for the corner shall be replaced with shrubs or groundcover that typically do not grow to exceed 30 inches in height.
 - c. Canopy lights shall be fully recessed.
 - d. All lighting shall not be of a high intensity so as to cause a traffic hazard, be used as an advertising element, or adversely affect adjacent properties, in compliance with CMC Section 19.60.050 (Exterior lighting).
 - e. Vehicles shall not be parked on sidewalks or driveways.
 - f. Vehicles shall not be parked on the premises for the purpose of vehicular sales.
 - g. Used or discarded vehicle parts or equipment, or disabled, junked, or wrecked vehicles shall not be located in any open area outside of the convenience store structure.
 - h. Any noise from bells, loudspeakers, or tools shall not exceed the criteria referenced under CMC 19.60.080 (Noise), and shall not be audible from residentially zoned parcels between the hours of 7:00 p.m. and 7:00 a.m. on weekdays and Saturdays, and before 10:00 a.m. and after 7:00 p.m. on Sundays and nationally recognized holidays.
 - i. Used motor oil may not be received without prior City Fire Department approval.

Mitigation Measures from the EIR (SCH#2015102017)

22. The developer shall comply with all mitigation measures contained in the Mitigation Monitoring and Reporting Program for the Chico Walmart Expansion Project, hereby incorporated by reference in its entirety.



Subdivision Report

Meeting Date 10/6/16

DATE: September 27, 2016

File: PM 15-02

TO: PLANNING COMMISSION

FROM: Matt Johnson, Senior Development Engineer, 879-6910
Public Works Department

RE: **Vesting Tentative Parcel Map PM 15-02 Walmart Expansion Project**

Exhibit "III"

This office has reviewed the Vesting Tentative Parcel Map PM 15-02 Walmart Expansion Project and herewith submits the following findings and recommendations for same.

A. MODIFICATIONS TO TITLE 18R - DESIGN CRITERIA AND IMPROVEMENT STANDARDS OF THE CHICO MUNICIPAL CODE

The Subdivider has requested no modifications to Titles 18R of the Chico Municipal Code (CMC).

B. ENVIRONMENTAL IMPACT REPORT

To satisfy the requirements of the California Quality Act (CEQA), the City prepared an Environmental Impact Report (EIR) to analyze and disclose the potential environmental impacts associated with approving the project. The EIR identified various potential impacts and included mitigation measures to reduce those impacts to less than significant.

The Subdivider will be required to comply with all engineering related mitigation measures with one exemption as described in Resolution No. 16-12.

C. PUBLIC FACILITY CONSTRUCTION

1. Streets

- a) The Subdivider shall construct City standard streets and appurtenant facilities at the following locations in conformance with the Tentative Map and further described below:
 - 1) Wittmeier Drive - Adding a traffic signal at its intersection with Forest Avenue, with related improvements on the Forest Avenue and Talbert Drive approaches.
 - 2) Wittmeier Drive - Widening the street to accommodate one westbound lane, three eastbound lanes, and parking along both sides, including two dedicated left-turn lanes and one through/right-turn lane.
 - 3) Wittmeier Drive - Adding a new driveway to primarily serve the new fuel station.
 - 4) Wittmeier Drive - Adding a new driveway at the end of the existing cul-de-sac to serve the Walmart store and Parcels 2 and 3 (with a shared cross-access easement between the three proposed parcels).

2. Storm Drainage

a) Facility Construction

The Subdivider shall design and install the following City standard storm drain facilities:

- 1) Onsite - Relocate the existing twin 48-inch storm drains.
- 2) Access to Manholes - Provide access to manholes via an all-weather surface and have sufficient space for maintenance trucks to turn around. Access shall be available 24 hours per day, 365 days per year.

b) NPDES Requirements

Storm drain drop inlets shall be marked with Illustrative Storm Markers to achieve City of Chico NPDES Requirements.

c) Storm Drainage Analysis

The Subdivider shall submit a storm drain analysis to the Public Works Department - Engineering for review and approval. The analysis, including hydraulic grade lines for both the existing and realigned conditions, must demonstrate that the proposed realigned storm drain easement and storm drain lines will not reduce the capacity of the existing storm drain facility.

d) The subdivider shall pay a storm drain fee calculated in accordance with the current fee schedule under the requirements of the Chico Municipal Code, prior to recordation of the Final Map.

3. Sanitary Sewer Fees

The Subdivider shall complete an Application for Sewer Connection.

The Subdivider shall pay a sanitary sewer main fee to the City of Chico prior to recordation of the Final Map, plus applicable trunk line and water pollution control plant capacity fees in conjunction with building permits. All of the aforementioned fees will be subject to the terms and conditions of the Application for Sewer Connection.

4. Post Construction Standards

The Project must comply with the City's Post Construction Standard Plan.

5. Street Signs and Striping

The Subdivider shall install City standard street signs, regulatory signs, pavement striping and pavement markings on all streets, and bicycle facilities that they are required herein to construct.

6. Street Lights

The Subdivider shall install City standard street lights on steel poles with concrete bases on all streets that they are required herein to construct.

7. Bicycle Facilities

The Subdivider shall construct the following bicycle facility:

- a) A relocated Class I bicycle path in conformance with City standards within the project site as depicted on the Tentative Map. Final alignment of the new relocated bike path shall be subject to review and approval of the Public Works Director - Engineering.

8. Street Trees

Street trees shall be planted in accordance with Public Works Department - Maintenance requirements.

9. Landscaping

The Subdivider shall install landscaping and an irrigation system at the following locations:

- a) Within the westerly one-half of the raised median along the project Forest Avenue frontage.

D. PRIVATE FACILITY CONSTRUCTION

1. Streets

- a) The Subdivider shall construct City standard streets and appurtenant facilities at the following locations in conformance with the Tentative Map and further described below:
 - 1) Baney Lane - Relocating the easterly (primary) Walmart access driveway approximately 180 feet west of its current location.
 - 2) Baney Lane - Extending left-turn lanes for eastbound traffic on Baney Lane to increase vehicle storage capacity in advance of the Forest Avenue intersection.
 - 3) Baney Lane - Extending the center left-turn lane for westbound traffic on Baney Lane heading into the primary Walmart driveway.
 - 4) Baney Lane - Adding raised islands at the primary and central Walmart driveways to prohibit outbound left turns onto Baney Lane (the westerly driveway already has such a raised island).

2. Sanitary Sewer

a) Facility Construction

The Subdivider shall design and install the following City standard sanitary sewer facilities:

- 1) Business Lane - Relocate the 8-inch sanitary sewer from Business Lane.
- 2) Access to Manholes - Provide access to manholes via an all-weather surface and have sufficient space for maintenance trucks to turn around. Access shall be available 24 hours per day, 365 days per year.

b) Sanitary Sewer Analysis

The Subdivider shall submit a sanitary sewer analysis to the Public Works Department - Engineering for review and approval. The analysis must demonstrate that the proposed realigned sanitary sewer easement and sanitary sewer line will not reduce the capacity of the existing sanitary sewer facility.

c) Other

- 1) Because the sanitary sewer line is a private facility, the Subdivider shall provide documented evidence that they have the right to re-align the sewer.
- 2) It appears that part of the re-alignment must occur off-site on the Krispy Kreme site, the Subdivider shall provide documented evidence that they have the right to enter onto that property and re-align the sewer.

3. Street Signs and Striping

The Subdivider shall install City standard street signs, regulatory signs, pavement striping and pavement markings on all streets, and bicycle facilities that they are required herein to construct.

E. MAINTENANCE

Prior to filing the Final Map, the Subdivider shall be required to make provisions to fund the maintenance of certain public improvements. The improvements to be covered shall be:

Within the westerly one-half of the raised median along the project Forest Avenue frontage.

The Subdivider shall prepare the necessary documents and provide the required supporting documents. Formation of a maintenance district requires action by the City Council. The district shall be complete and formed prior to recordation of the Final Map.

F. SUBDIVISION GRADING

1. Soils Report

The Subdivider shall submit a Geological and/or Soils Report, prepared by a registered engineer, that includes, but is not limited to, the following:

- a) An investigation of the nature, distribution and strength of existing soils.
- b) A description of site geology.
- c) Conclusions and recommendations covering the adequacy of the site for the proposed development, storm drainage disposal, grading procedures and corrective measures.
- d) Verification that the site is suited to proposed BMPs.

2. Grading Standards

All subdivision grading shall be in conformance with Chapter 16R.22, Grading Standards, of the Chico Municipal Code.

G. PROPERTY CONVEYANCES

1. Dedications

In conjunction with recordation of the Final Map for this subdivision, the Subdivider shall:

- a) Dedicate a 50-foot PG&E Easement as depicted on the Tentative Map.
- b) Dedicate a 15-foot Bicycle Easement as depicted on the Tentative Map.
- c) Dedicate a 52-foot Storm Drain Easement as depicted on the Tentative Map.
- d) Dedicate a 20-foot Sanitary Sewer Easement as depicted on the Tentative Map.
- e) Dedicate a 20-foot Water Easement as depicted on the Tentative Map.
- f) Dedicate a varying width Access Easement as depicted on the Tentative Map.
- g) Dedicate a 10-foot-wide public service easement adjacent to public rights-of-way.

2. Abandonments

The right-of-way and easement abandonments depicted on the Tentative Map are approved. Said abandonments, in accordance with the provisions of the Subdivision Map Act, shall become effective upon Final Map recordation.

- a) Abandon a portion of the 50-foot PG&E Easement as depicted on the Tentative Map.
- b) Abandon a portion of the 15-foot Bicycle Easement as depicted on the Tentative Map.
- c) Abandon a portion of the 52-foot Storm Drain Easement as depicted on the Tentative Map.
- d) Abandon a portion of the 20-foot Sanitary Sewer Easement as depicted on the Tentative Map.
- e) Abandon a portion of the 20-foot Water Easement as depicted on the Tentative Map.

H. OTHER PUBLIC SERVICES

1. Public Utilities

a) Underground Requirements

The issue of undergrounding of utilities will be addressed in the Community Development Department report.

b) Easement Obstructions

All public utility and/or public service easements shall be kept free and clear of any and all obstructions, including but not limited to, structures, longitudinal fencing and/or soundwalls, which may impede the construction, operation and maintenance of public utility facilities within such easements.

c) Utility Company Comments

- 1) AT&T, as of the date of this report, did not respond to a request for comments.
- 2) Pacific Gas and Electric Company has had some communications with the Community Development Department regarding undergrounding the overhead power lines crossing the Walmart site.
- 3) California Water Service Company, as of the date of this report, did not respond to a request for comments.

2. Fire Protection

The Subdivider shall comply with the recommendations of the Fire Department, City of Chico.

I. DESIGN CRITERIA AND IMPROVEMENT STANDARDS

All public and joint-use private improvements shall be designed in accordance with Chapter 18R.08, Design Criteria, of the Chico Municipal Code, except as modified by the conditions of approval for this subdivision.

The Subdivider shall submit improvement plans, profiles, typical sections, details and specifications to the Public Works Department - Engineering for review and approval prior to the start of any construction of public and joint-use private improvements.

All public and joint-use private improvements shall be constructed in conformance with Chapter 18R.12, Improvement Standards, of the Chico Municipal Code and in conformance with the details shown on the approved improvement plans.

J. ADMINISTRATIVE REQUIREMENTS

1. Subdivision Improvement Agreement

If the public and joint-use private improvements required herein are not satisfactorily completed prior to recordation of the Final Map, the Subdivider shall enter into a subdivision improvement agreement in conformance with Chapter 18.36, Subdivision Improvement Requirements, of the Chico Municipal Code.

2. Subdivision Fees

a) Plan Checking Fee

The Subdivider shall pay to the City of Chico a subdivision plan checking fee upon filing the Final Map and/or improvement plans and specifications for checking in the following amount:

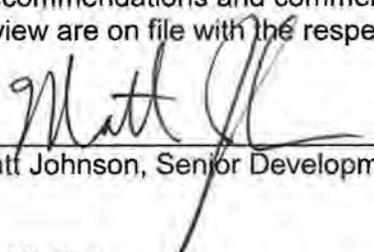
An initial deposit of 1½% of the estimated cost of all public and/or joint use private improvements exclusive of private utility facilities (\$750 minimum). A final fee equal to actual City costs.

b) Inspection Fee

The Subdivider shall pay to the City of Chico an inspection fee prior to commencing construction in the following amount:

An initial deposit of 2% of estimated cost of all public and/or joint use private improvements exclusive of private utility facilities. A final fee equal to actual City costs.

Recommendations and comments of all parties to whom the Tentative Map was circulated for review are on file with the respective parties and in Community Development Department.



Matt Johnson, Senior Development Engineer

Distribution:

Original - Planning PM 15-02 File
Development Engineering Parcel Map File

RECEIVED
Oct 09, 2015
City of Chico
Planning Services

Plat to Accompany Use
Permit 15-10
Page 1 of 2



VICINITY MAP



NO.	DESCRIPTION



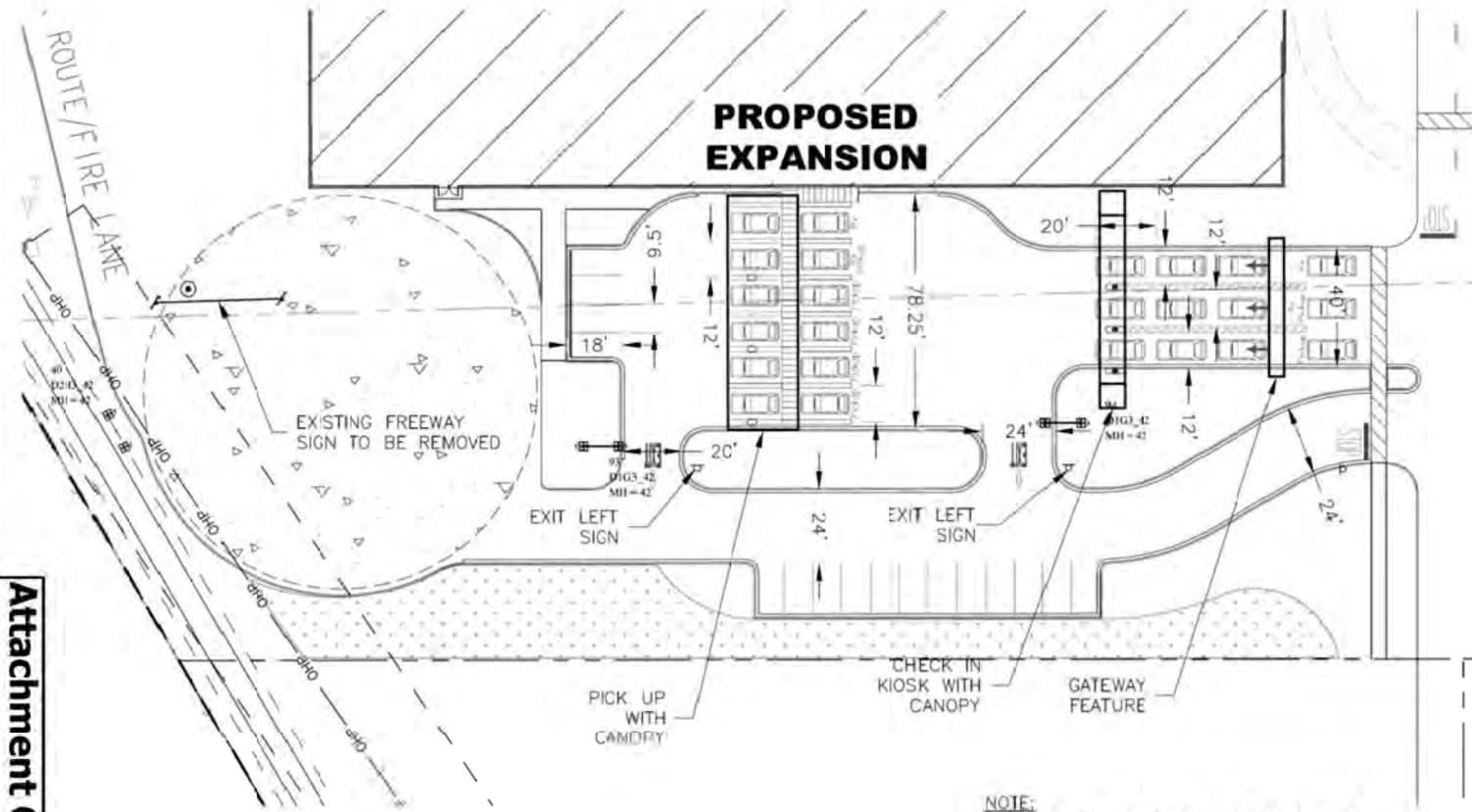
PROJECT NO.	15-10-046
DRAWN BY	PSJ/EP
CHECKED BY	PSJ/EP
DESIGNED BY	RSC/EP

PRELIMINARY SITE PLAN
WALMART STORE NO. 2044-09
EXPANSION
 2044 FOREST AVENUE
 CHICO, CA 95928

SHEET TITLE
SITE PLAN

Exhibit IV

DATE: 09/08/15



NOTE:
 SEE PICK-UP DRIVE THRU ELEVATION FOR
 DETAILS OF GATEWAY FEATURE AND PICK UP
 KIOSK WITH CANOPY



Attachment C, Exhibit IV

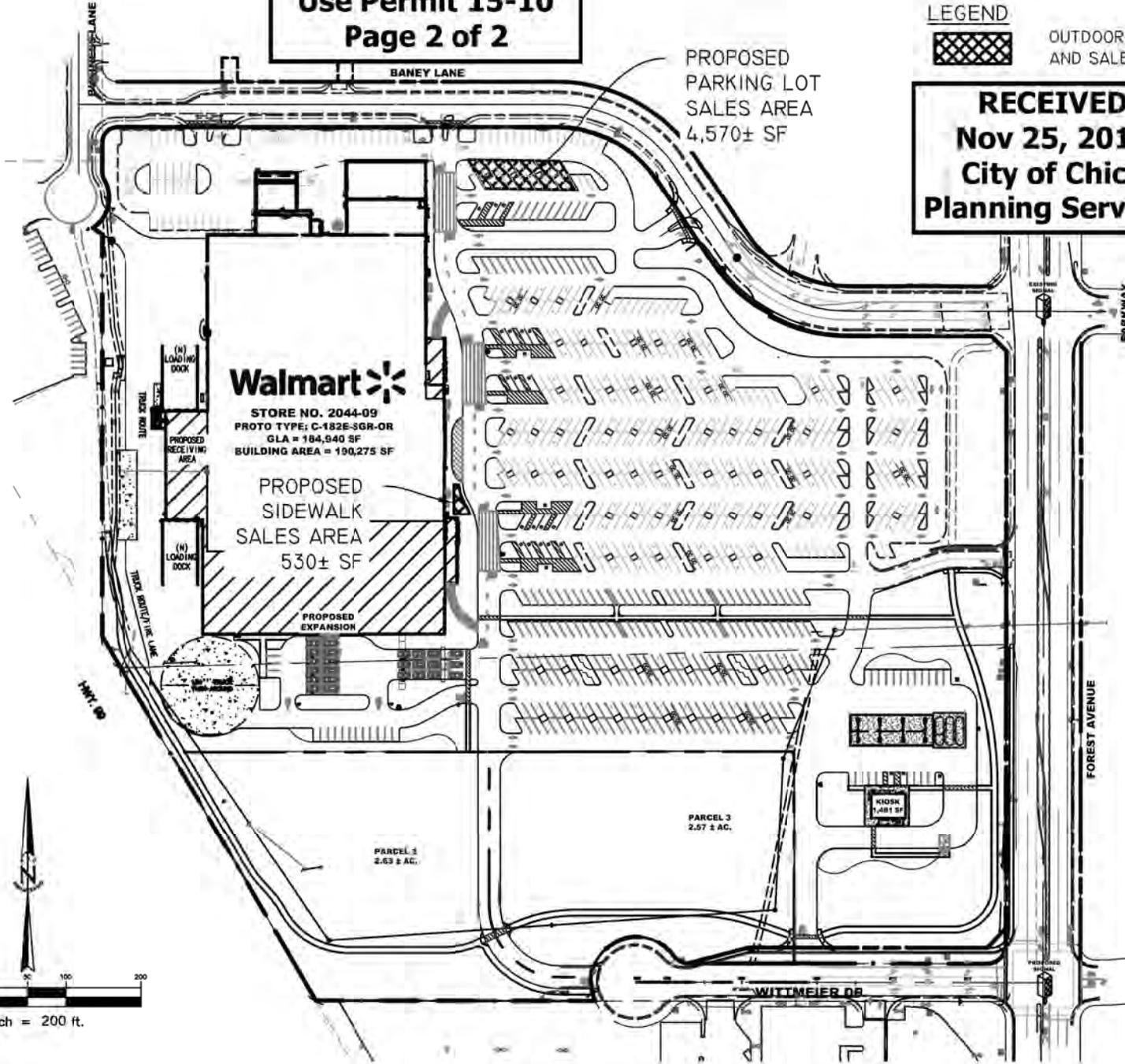
**Plat to Accompany
Use Permit 15-10
Page 2 of 2**

LEGEND



OUTDOOR DISPLAY
AND SALES AREA

**RECEIVED
Nov 25, 2015
City of Chico
Planning Services**



STORE NO. 2044-09
PROTO TYPE: C-182E-SGR-OR
GLA = 184,840 SF
BUILDING AREA = 190,275 SF

PROPOSED
SIDEWALK
SALES AREA
530± SF

PROPOSED
PARKING LOT
SALES AREA
4,570± SF

PROPOSED
EXPANSION

KIOSK
1,481 SF

PARCEL 1
2.63 ± AC.

PARCEL 3
2.57 ± AC.

1 inch = 200 ft.

Attachment C, Exhibit IV
or Display Exhibit.dwg Xrefs: P:\027-046\Planning\Conceptual\027046_XPRPN12.dwg
P:\027-046\Planning\Conceptual\027046_XTOP12.dwg
P:\027-046\Planning\Conceptual\027046_XPRPN-OFFSITE12.dwg
PLOT DATE: NOV 13, 2015 - 11:39:20 AM



RSC ENGINEERING
2250 Daughine Blvd, Suite 150
Chico, CA 95926
Ph: 916.786.2884 Fax: 916.788.4408

PROJECT NO:	027-046
DRAWN BY:	RSC Eng
CHECKED BY:	RSC Eng
DESIGNED BY:	RSC Eng

**OUTDOOR DISPLAY
AND SALES EXHIBIT
2044-09 CHICO EXPANSION**
2044 FOREST AVENUE
CHICO, CA 95928

SHEET TITLE
**OUTDOOR
SALES**

Exhibit
IV

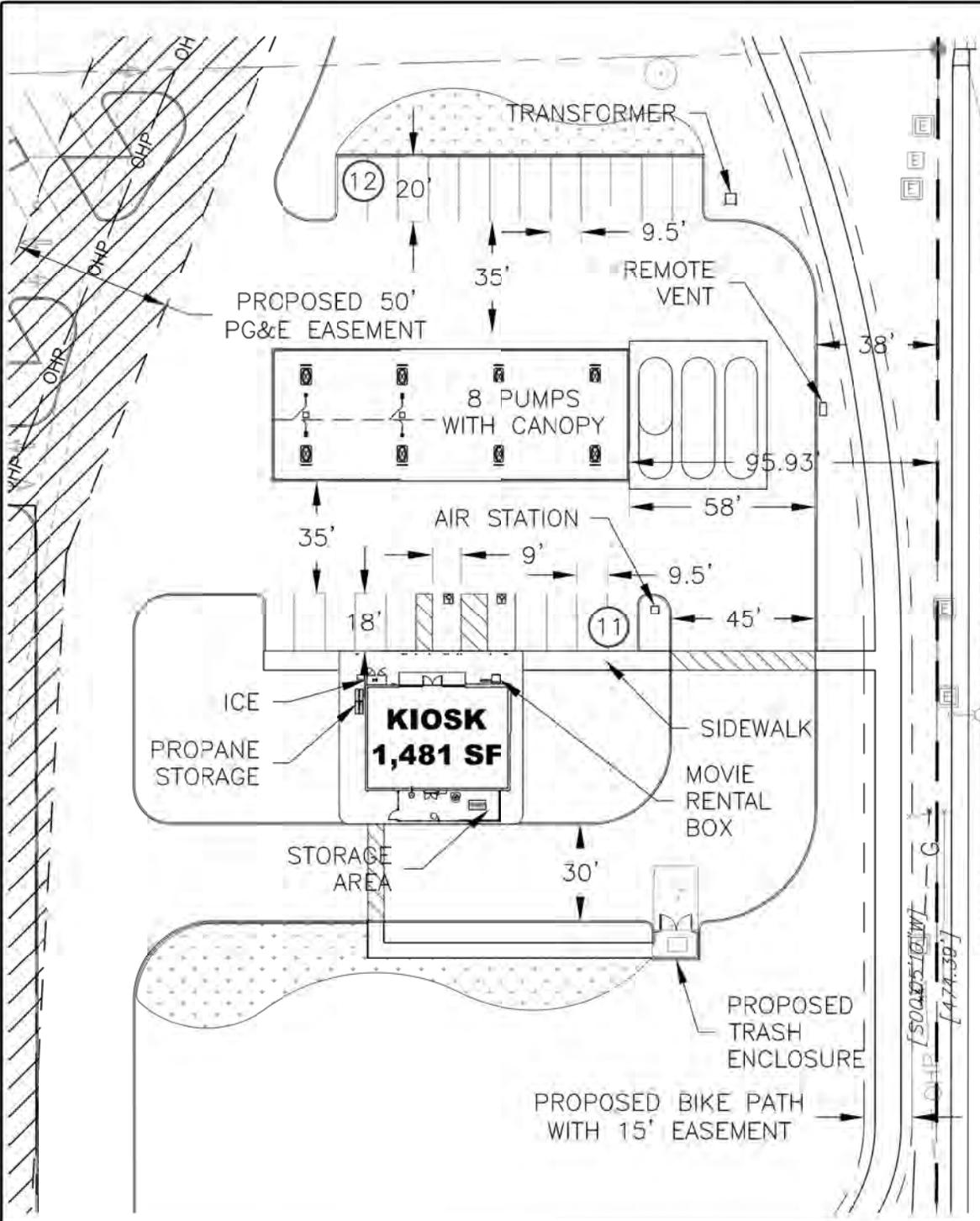
DATE: OCTOBER 14, 2015

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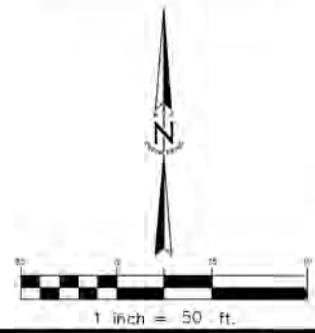
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Attachment C, Exhibit V

PLOT DATE: Sep 01, 2015 - 2:07:19 PM



VICINITY MAP

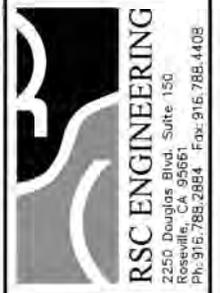


FOREST AVENUE

RECEIVED
 Sep 14, 2015
 City of Chico
 Planning Services

Plat to Accompany
Use Permit 15-12
Page 1 of 2

A.P. NO: 002-170-004
 APPLICANT: RSC ENGINEERING
 PARCEL 1 AREA: 21.88 AC
 SCALE: 1"=50'
 DATE: SEPTEMBER 1, 2015



PROJECT NO: 027-046
 DRAWN BY: RSC Eng
 CHECKED BY: RSC Eng
 DESIGNED BY: RSC Eng

FUEL STATION SITE PLAN
FOR STORE NO. 2044-09

2044 FOREST AVENUE
 CHICO, CA

SHEET TITLE
FUEL STATION
SITE PLAN

SHEET NO.
SP1
 OF 2

DATE: SEPTEMBER 1, 2015

PROJECT DATA:

LOCAL JURISDICTION: CITY OF CHICO, CA
 ZONING CLASSIFICATION: CR REGIONAL COMMERCIAL
 LAND USE CLASSIFICATION: RC REGIONAL COMMERCIAL
 BUILDING SETBACKS: 0' FRONT, REAR AND SIDE

SITE PARKING ANALYSIS

FUEL STATION KIOSK: 1,481 SF
 (4 BAYS/8 PUMPS)
 FUEL STATION PARKING
 STANDARD PARKING 21 SPACES
 ACCESSIBLE PARKING 2 SPACES (INC. 1 VAN)
 TOTAL 23 SPACES
 FUEL STATION PARKING RATIO: 15.53/1,000 SF

CITY OF CHICO REQUIREMENTS

FUEL STATION 4.0/1,000 6 SPACES
 TOTAL 6 SPACES

**TYPICAL PARKING STALL IS 9.5' X 20'

PROPOSED PARCEL 1

WALMART PARCEL: 21.88 AC

NOTE: PROPOSED FUEL STATION IS A PORTION OF PARCEL 1

PROPOSED PARCEL 1 SITE COVERAGE

BUILDING:	5.03 AC	22.7%
HARDSCAPE:	12.97 AC	58.9%
LANDSCAPE:	3.89 AC	18.4%
TOTAL:	21.88 AC	100%

LEGEND:

-  BOUNDARY LINE
-  RIGHT OF WAY LINE
-  PARKING COUNT
-  RELOCATED OVERHEAD POWER LINES
-  PROPOSED CURB



PROJECT NO: 027-046
 DRAWN BY: RSC Eng
 CHECKED BY: RSC Eng
 DESIGNED BY: RSC Eng

RECEIVED
Sep 14, 2015
City of Chico
Planning Services

Plat to Accompany
Use Permit 15-12
Page 1 of 2

A.P. NO: 002-170-004
 APPLICANT: RSC ENGINEERING
 PARCEL 1 AREA: 21.88 AC
 SCALE: N/A
 DATE: SEPTEMBER 1, 2015

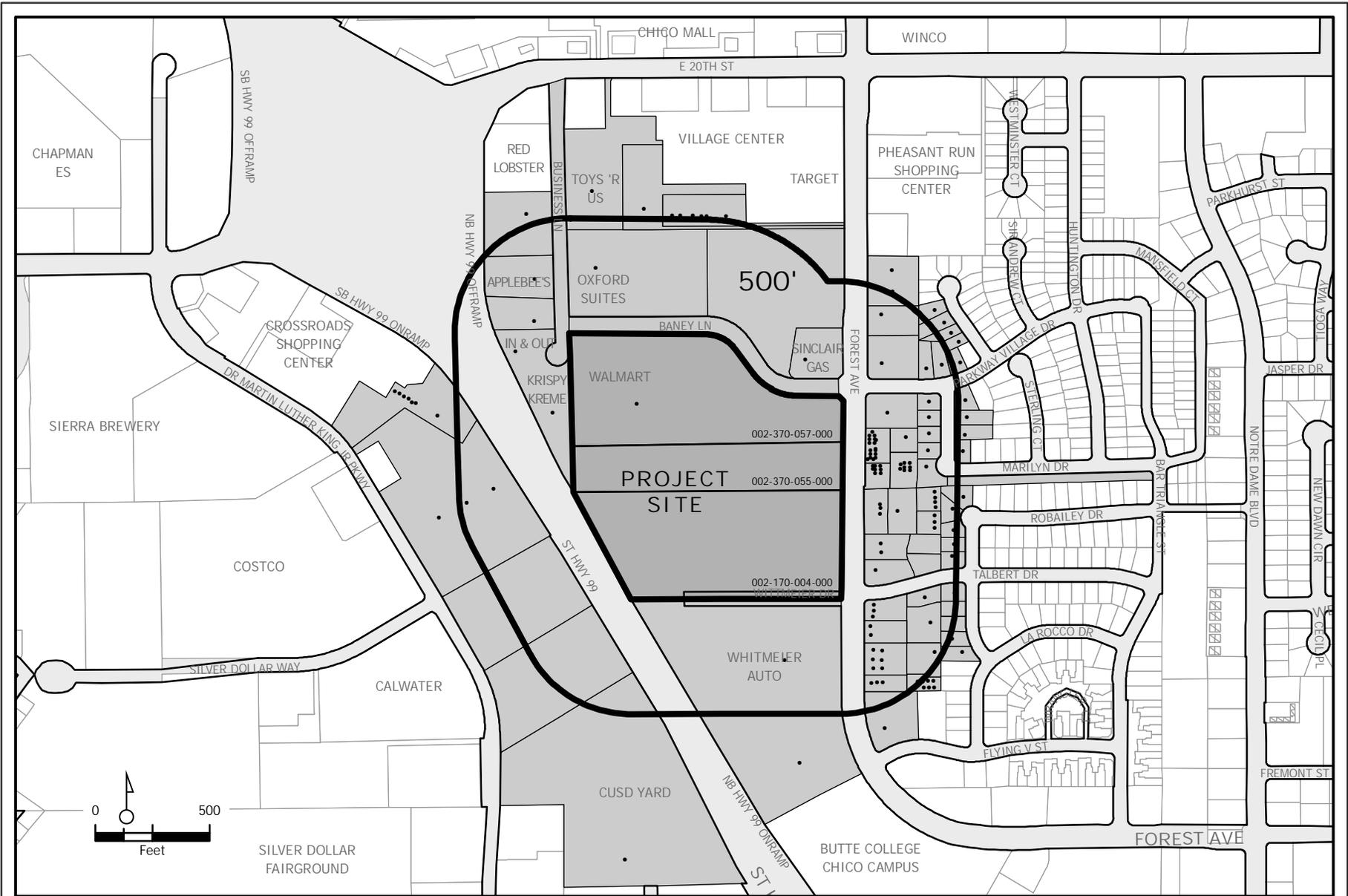
FUEL STATION SITE PLAN
FOR STORE NO. 2044-09
 2044 FOREST AVENUE
 CHICO, CA

SHEET TITLE
FUEL STATION
SITE PLAN

SHEET NO.
SP2
 OF 2

DATE: SEPTEMBER 1, 2015

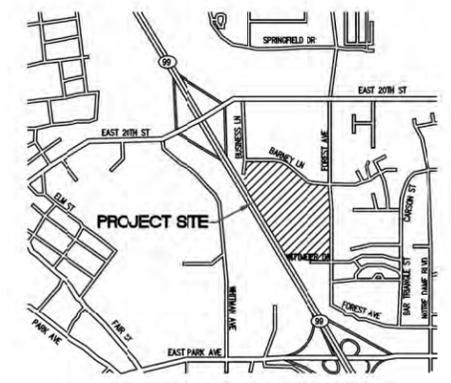
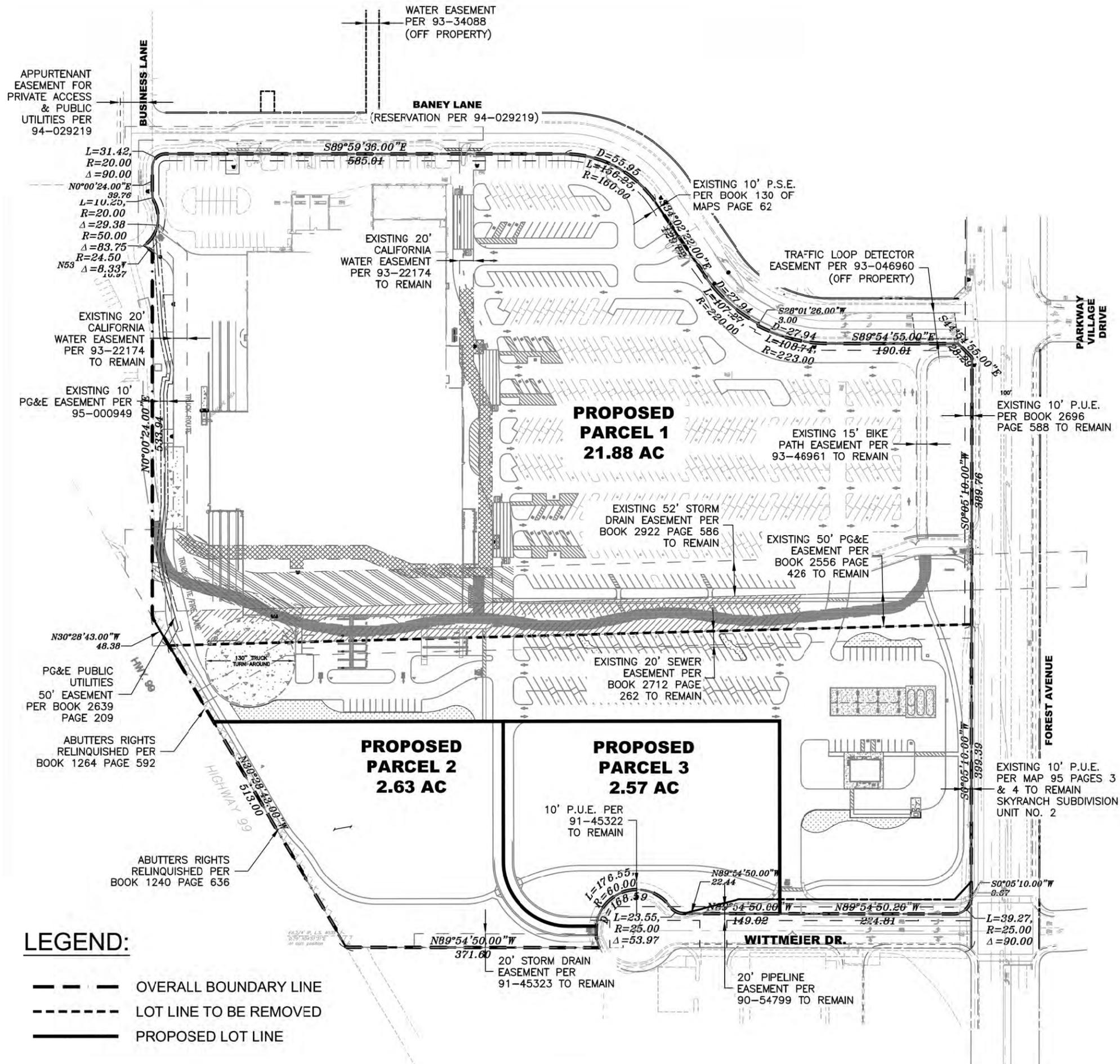
Attachment C, Exhibit V



Chico Wal-Mart Expansion Project
2044 Forest Avenue
APNs 002-170-004-000, 002-370-055-000, 002-370-057-000

- Notified Addresses
- Notified Parcels





VICINITY MAP

LEGEND:

- PORTION OF 50' PG&E EASEMENT TO BE ABANDONED
- PORTION OF 15' BIKE EASEMENT TO BE ABANDONED
- PORTION OF 52' STORM DRAIN EASEMENT TO BE ABANDONED
- PORTION OF 20' SEWER EASEMENT TO BE ABANDONED
- PORTION OF 20' WATER EASEMENT TO BE ABANDONED

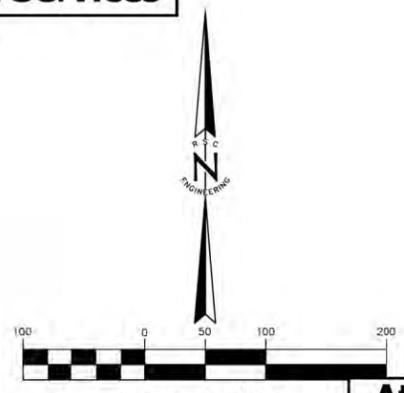
NOTES

1. BOOK 2987 PAGE 575 AGREEMENT PROVIDING FOR THE CONSTRUCTION OF UNDERGROUND STORM DRAIN FACILITIES WITHIN PUBLIC STORM DRAIN EASEMENT (NOT A PLOTTABLE EASEMENT).
2. INSTRUMENT NUMBER 91-17693 COVENANT TO PROVIDE SECONDARY ACCESS (NOT A PLOTTABLE EASEMENT).

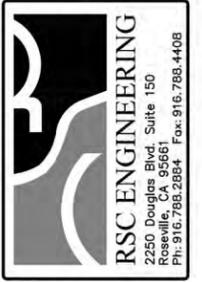
RECEIVED
Sep 21, 2015
City of Chico
Planning Services

LEGEND:

- OVERALL BOUNDARY LINE
- LOT LINE TO BE REMOVED
- PROPOSED LOT LINE



NO.	DATE	BY	DESCRIPTION

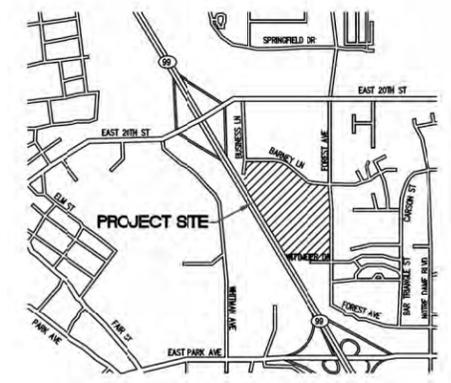
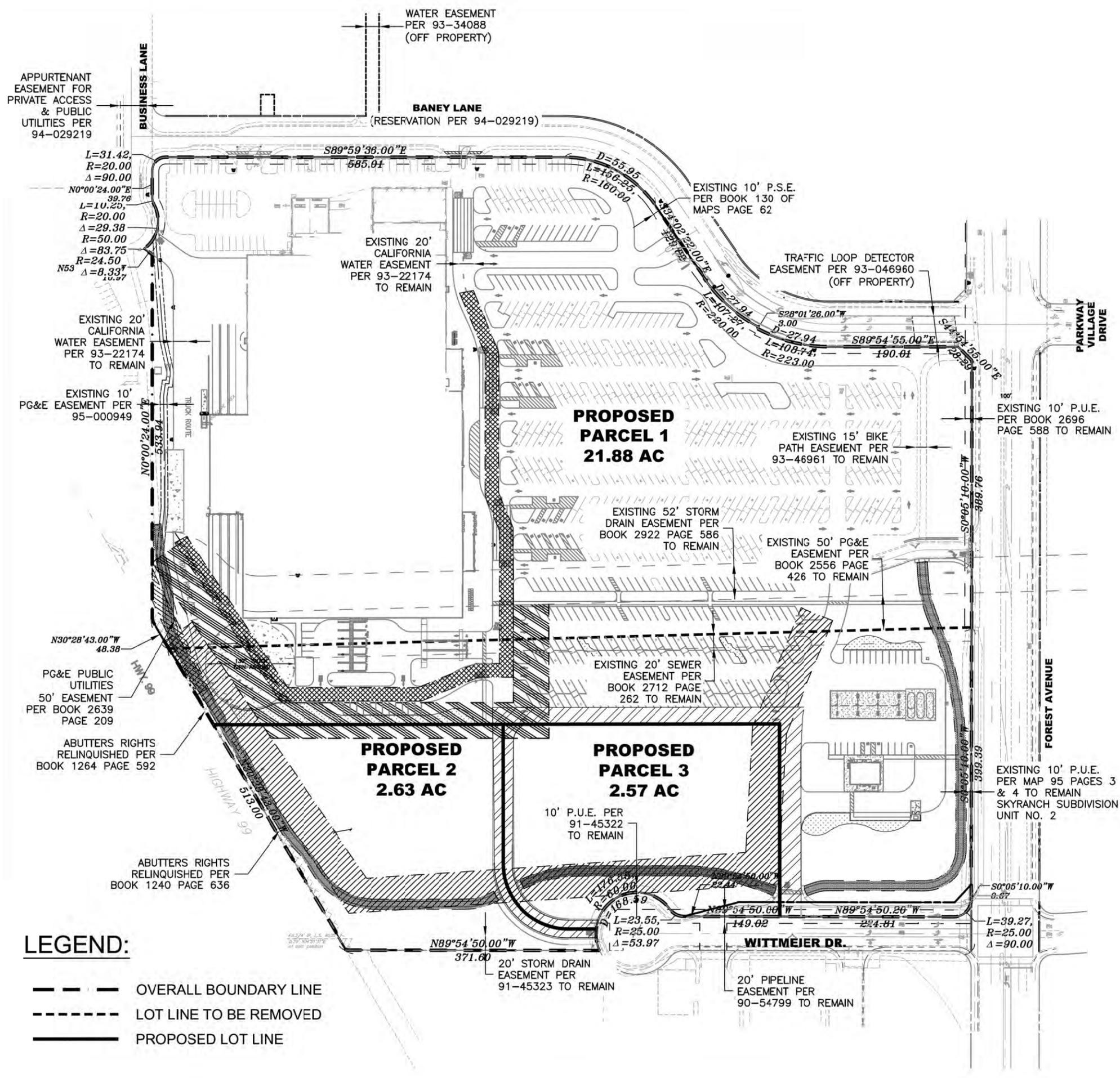


PROJECT NO:	027-046
DRAWN BY:	RSC Eng
CHECKED BY:	RSC Eng
DESIGNED BY:	RSC Eng

PM 15-02
VESTING TENTATIVE MAP
 2044 FOREST AVENUE
 CHICO, CA 95928

SHEET TITLE
TENTATIVE MAP

SHEET NO.
TM2



VICINITY MAP

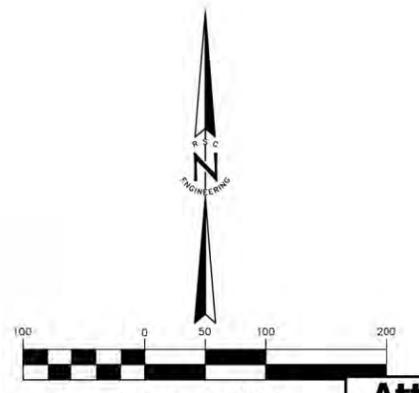
LEGEND:

- PROPOSED 50' PG&E EASEMENT
- PROPOSED 15' BIKE EASEMENT
- PROPOSED 52' STORM DRAIN EASEMENT
- PROPOSED 20' SEWER EASEMENT
- PROPOSED 20' WATER EASEMENT
- PROPOSED ACCESS EASEMENT (WIDTH VARIES)

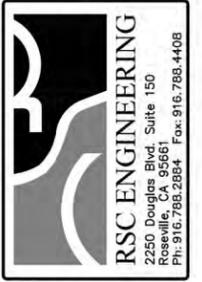
LEGEND:

- OVERALL BOUNDARY LINE
- LOT LINE TO BE REMOVED
- PROPOSED LOT LINE

RECEIVED
Sep 21, 2015
City of Chico
Planning Services



NO.	DATE	BY	DESCRIPTION



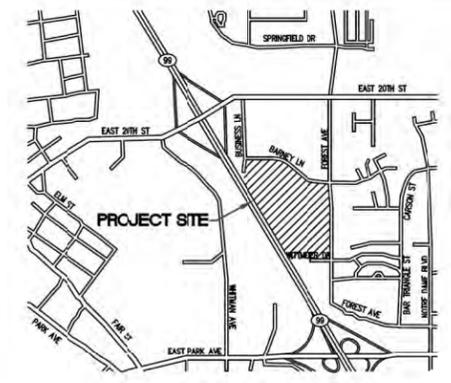
PROJECT NO:	027-046
DRAWN BY:	RSC Eng
CHECKED BY:	RSC Eng
DESIGNED BY:	RSC Eng

PM 15-02
VESTING TENTATIVE MAP
2044 FOREST AVENUE
CHICO, CA 95928

SHEET TITLE
TENTATIVE MAP

SHEET NO.
TM3

RECEIVED
Sep 21, 2015
City of Chico
Planning Services



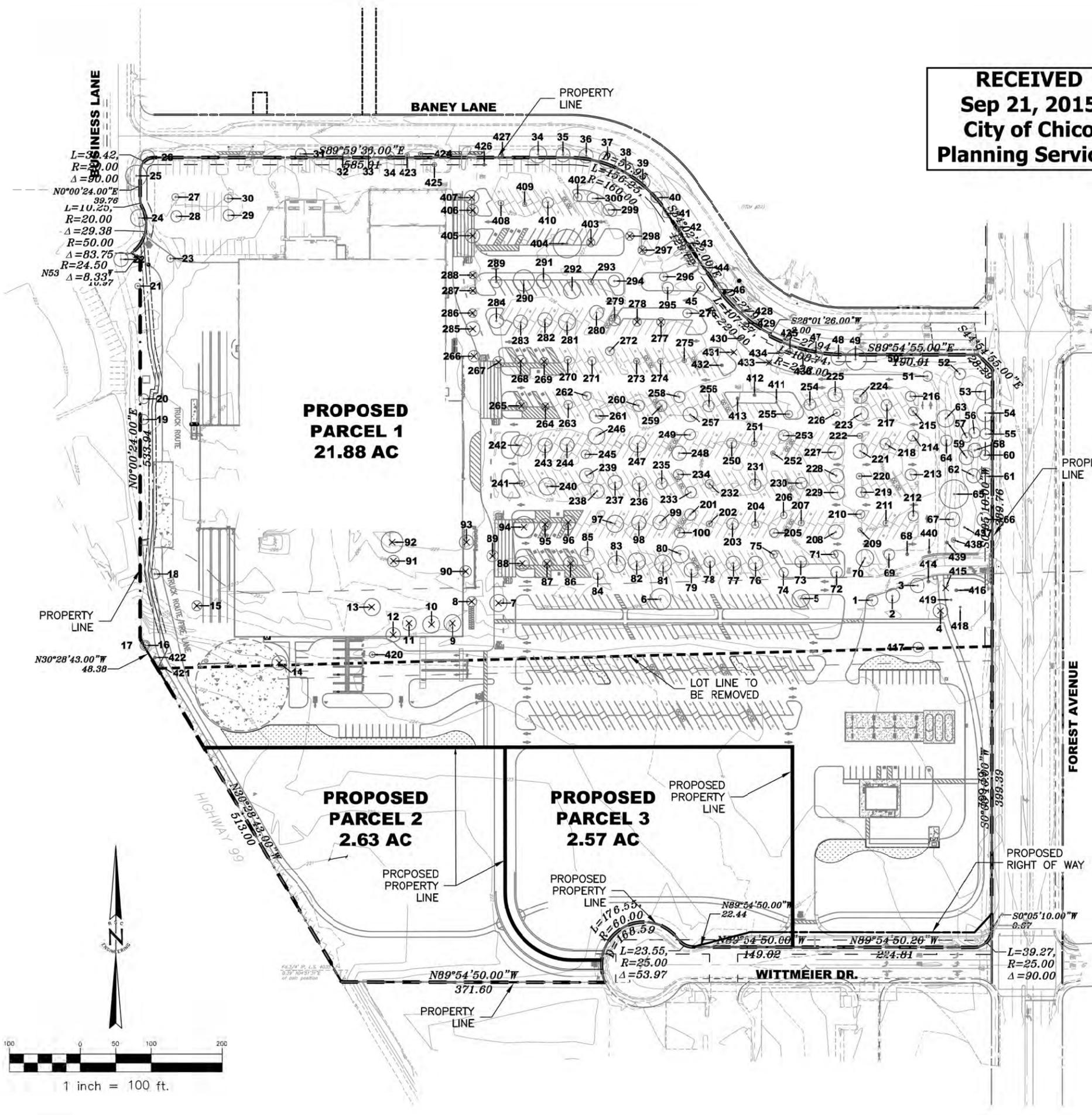
VICINITY MAP

LEGEND:

- LOT LINE TO BE REMOVED
- PROPOSED LOT LINE
- PROPOSED RIGHT OF WAY
- EXISTING TREE
- EXISTING TREE NUMBER
(REFER TO APPENDIX A - TREE INVENTORY BY
FOOTHILL ASSOCIATES, DATED AUG. 19, 2015)
- INDICATES EXISTING TREE
TO BE REMOVED*

* A TOTAL OF 42 TREES TO BE REMOVED ARE AS FOLLOWS:

Tree #	Species	# of Trunks	DBH (Inches)	Drip-line Radius (Feet)	Height (Feet)	Health	Structure
4	Chinese pistache	10	2,2,2,2,2,2,2,2,2,2	10	15	Good	Fair-Good
7	Honey locust	1	8	12	15	Good	Good
8	Honey locust	1	4	7	10	Good	Good
9	London planetree	1	9	12	20	Fair-Good	Good
10	London planetree	1	8	12	25	Fair-Good	Fair-Good
11	London planetree	1	6	10	15	Fair-Good	Good
12	London planetree	1	8	10	20	Fair	Fair-Good
13	Cork oak	1	13	12	20	Fair-Good	Fair-Good
14	Pecan	1	12	8	12	Good	Fair
15	Atlas cedar	1	8	7	10	Good	Fair-Good
86	Honey locust	1	9	10	15	Good	Fair-Good
87	Honey locust	1	5	6	10	Fair-Good	Fair-Good
88	Honey locust	1	8	10	12	Good	Fair-Good
89	Honey locust	1	7	8	12	Good	Fair-Good
90	Honey locust	1	9	8	12	Good	Good
91	Honey locust	1	5	8	12	Good	Good
92	Chinese pistache	1	11	15	25	Good	Good
93	Honey locust	1	10	10	15	Fair-Good	Fair-Good
94	London planetree	1	14	15	25	Good	Good
95	London planetree	1	6	8	12	Good	Good
96	London planetree	1	10	10	20	Good	Good
264	Honey locust	1	6	5	15	Good	Fair-Good
265	London planetree	1	7	8	15	Good	Fair-Good
266	Honey locust	1	7	8	20	Good	Fair-Good
267	London planetree	1	9	8	25	Good	Good
268	Honey locust	1	5	8	10	Good	Fair-Good
269	Honey locust	1	9	6	15	Good	Fair-Good
277	London planetree	1	6	6	12	Good	Good
278	London planetree	1	7	6	15	Fair-Good	Good
285	Honey locust	1	0	10	12	Good	Fair-Good
286	Honey locust	1	4	6	12	Good	Fair-Good
287	Honey locust	1	4	5	12	Good	Fair-Good
288	Honey locust	1	5	5	12	Good	Fair-Good
297	Cork oak	1	7	6	15	Good	Good
298	Cork oak	1	7	6	15	Good	Good
403	London planetree	1	6	6	15	Good	Good
405	Honey locust	1	6	10	12	Good	Fair-Good
406	Honey locust	1	5	8	8	Good	Fair
407	Honey locust	1	5	10	10	Good	Fair-Good
415	Crape myrtle	3	1 to 2	1	5	Good	Fair-Good
431	Crape myrtle	5	1 to 2	2	6	Good	Good
433	Crape myrtle	6	1 to 2	3	7	Good	Good



NO.	DATE	DESCRIPTION



RSC ENGINEERING
 2250 Douglas Blvd, Suite 150
 Roseville, CA 95661
 Ph: 916.788.2854 Fax: 916.788.4408

PROJECT NO: 027-046
 DRAWN BY: RSC Eng
 CHECKED BY: RSC Eng
 DESIGNED BY: RSC Eng

PM 15-02
VESTING TENTATIVE MAP
 2044 FOREST AVENUE
 CHICO, CA 95928

SHEET TITLE
TENTATIVE MAP

SHEET NO.
Attachment E

CITY OF CHICO SITE ANALYSIS

BUILDING ANALYSIS

EXISTING BUILDING:	125,889 SF
BUILDING EXPANSION:	64,386 SF
TOTAL BUILDING:	190,275 SF

BUILDING PARKING

STANDARD PARKING	594 SPACES
COMPACT PARKING	146 SPACES
ACCESSIBLE PARKING	24 SPACES (INC. 5 VAN)
TOTAL	764 SPACES

BUILDING PARKING RATIO: 4.02/1,000 SF

FUEL STATION ANALYSIS

FUEL STATION KIOSK:
(4 BAYS/8 PUMPS) 1,481 SF

FUEL STATION PARKING

STANDARD PARKING	9 SPACES
ACCESSIBLE PARKING	2 SPACES (INC. 1 VAN)
TOTAL	11 SPACES

FUEL STATION PARKING RATIO: 7.43/1,000 SF

CITY OF CHICO REQUIREMENTS

RETAIL GENERAL MERCHANDISE	4.0/1,000	761 SPACES
FUEL STATION	4.0/1,000	6 SPACES
TOTAL		767 SPACES

**PARKING COUNT AND RATIO DO NOT INCLUDE SPACES OCCUPIED BY CART CORRALS (36 SPACES) OR OUTDOOR DISPLAY AND SALES AREA (22 SPACES)

**TYPICAL PARKING STALL IS 9.5' X 20'

**40 BICYCLE SPACES TO BE PROVIDED (20 COVERED BIKE LOCKERS AND 20 UNCOVERED BIKE SPACES)

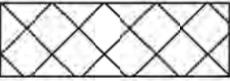
ACREAGE SUMMARY

WALMART PARCEL:	21.88 AC
PARCEL 2:	2.63 AC
PARCEL 3:	2.57 AC
TOTAL:	27.08 AC

SITE COVERAGE

BUILDING:	5.03 AC	18.6%
HARDSCAPE:	12.83 AC	47.4%
LANDSCAPE:	4.16 AC	15.3%
HYDROSEED:	5.06 AC	18.7%
TOTAL:	27.08 AC	100%

LEGEND

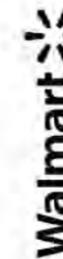
	BOUNDARY LINE
	RIGHT OF WAY LINE
	PARKING COUNT
	PROPOSED CART CORRALS
	RELOCATED OVERHEAD POWER LINES
	PROPOSED CURB
	EXISTING CURB
	42' TALL PARKING LOT LIGHTS
	PROPOSED EXPANSION
	HEAVY DUTY CONCRETE
	OUTDOOR DISPLAY AND SALES AREA
	BIOSWALE AREA

SITE DATA

LOCAL JURISDICTION:	CITY OF CHICO, CA
ZONING CLASSIFICATION:	CR REGIONAL COMMERCIAL
LAND USE CLASSIFICATION:	RC REGIONAL COMMERCIAL
BUILDING SETBACKS:	0' FRONT, REAR AND SIDE



VICINITY MAP



REV	DATE	DESCRIPTION

RSC ENGINEERING
2250 Douglas Blvd, Suite 150
Roseville, CA 95661
Ph: 916.786.2884 Fax: 916.786.4408

PROJECT NO:	027-046
DRAWN BY:	RSC Eng
CHECKED BY:	RSC Eng
DESIGNED BY:	RSC Eng

**PRELIMINARY SITE PLAN
WALMART STORE NO. 2044-09
EXPANSION
2044 FOREST AVENUE
CHICO, CA 95928**

SHEET TITLE
SITE PLAN

Attachment F

DATE: OCTOBER 8, 2015

CHICO, CA 027-046 (Preliminary) (Commercial) (10/08/15) (10/08/15)
 LAST MODIFIED: 10/08/15 11:01 AM
 PLOT DATE: 10/08/15 11:01 AM

SEE SHEET L1.1
MATCHLINE

(N)
LOADING
DOCK

HYDRANT TO
BE REMOVED

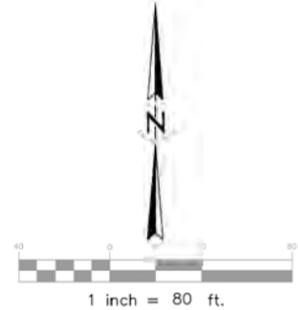
(N) HYDRANT

DECOMPOSED
GRANITE

FOUND 3/4" IRON
WITH PLUMB LINE
ACCEPTED AS N.W.
TBM #2 ELEV= 327
Lot = 354219.05
Long = 1214821.22
N. 2869008.0007
E. 6615265.9338

OUT LOTS UNDEVELOPED WITH
THIS PROJECT WILL RECEIVE
NON-IRRIGATED HYDROSEED.

1. All work shall be in accordance with the City of Chico, California, and State of California, and all applicable codes and regulations.
2. The contractor shall be responsible for obtaining all necessary permits and approvals from the City of Chico, California, and State of California.
3. The contractor shall be responsible for obtaining all necessary easements and rights-of-way from the City of Chico, California, and State of California.
4. The contractor shall be responsible for obtaining all necessary utility information from the City of Chico, California, and State of California.
5. The contractor shall be responsible for obtaining all necessary survey information from the City of Chico, California, and State of California.
6. The contractor shall be responsible for obtaining all necessary engineering information from the City of Chico, California, and State of California.
7. The contractor shall be responsible for obtaining all necessary landscape architecture information from the City of Chico, California, and State of California.
8. The contractor shall be responsible for obtaining all necessary construction information from the City of Chico, California, and State of California.
9. The contractor shall be responsible for obtaining all necessary maintenance information from the City of Chico, California, and State of California.
10. The contractor shall be responsible for obtaining all necessary safety information from the City of Chico, California, and State of California.



REV	NO	DESCRIPTION	DATE	BY	CITY APPROVAL

RSC ENGINEERING
2250 Douglas Blvd., Suite 130
Roseville, CA 95661
Ph: 916.785.2884 Fax: 916.788.4408

PROJECT NO: 027-046
DRAWN BY: BGP
CHECKED BY: JMA
DESIGNED BY: JMA

**PRELIMINARY SITE PLAN
WALMART STORE NO. 2044-09
EXPANSION**
2044 FOREST AVENUE
CHICO, CA 95928

SHEET TITLE
**LANDSCAPE
PLAN**

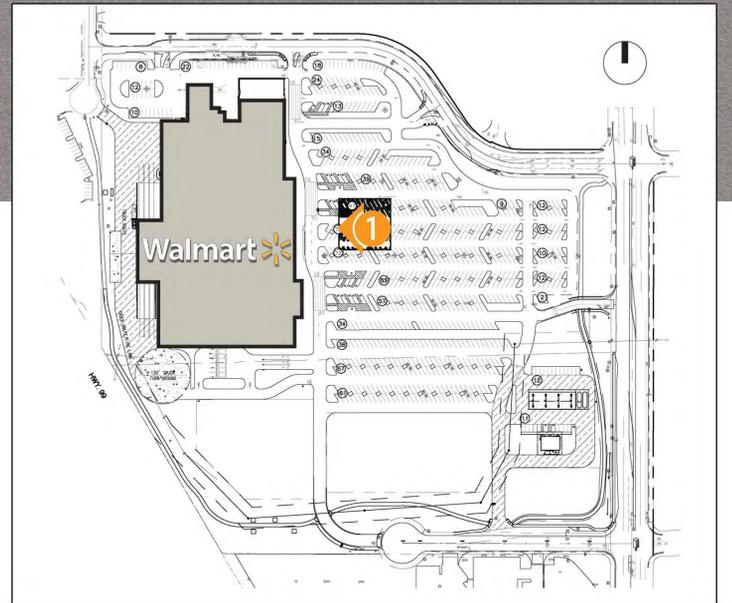
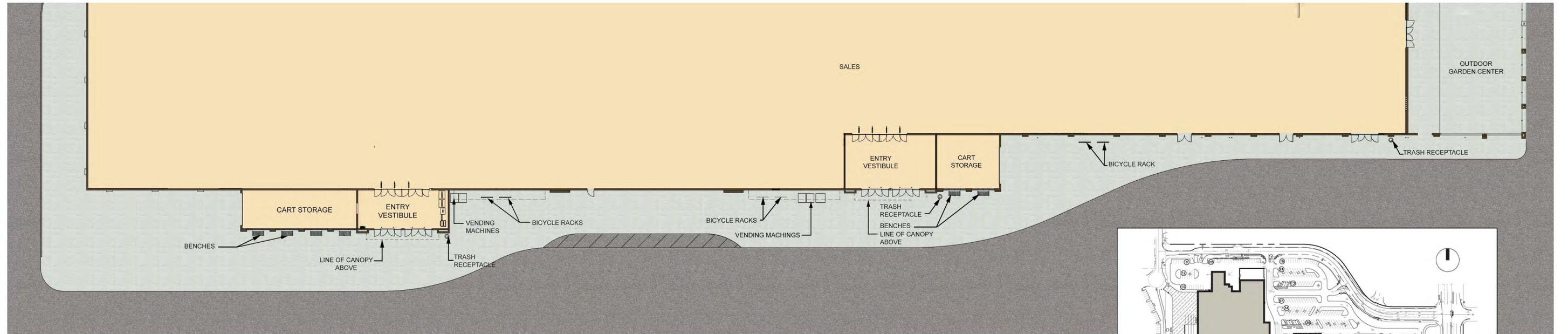
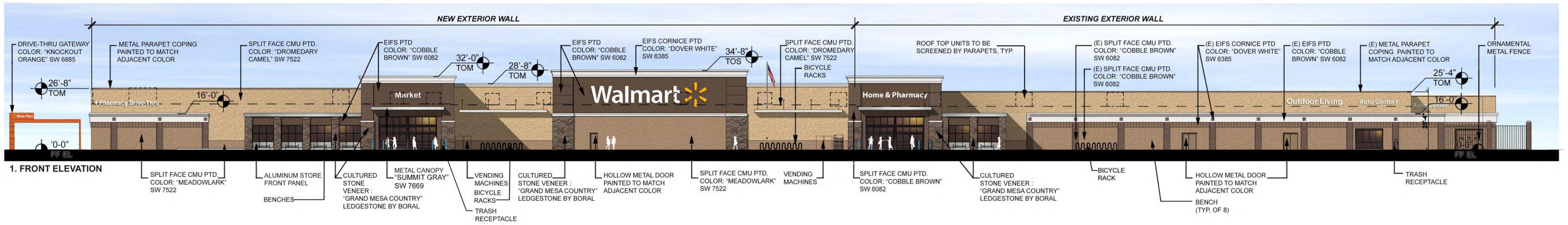
SHEET NO.
L1.3



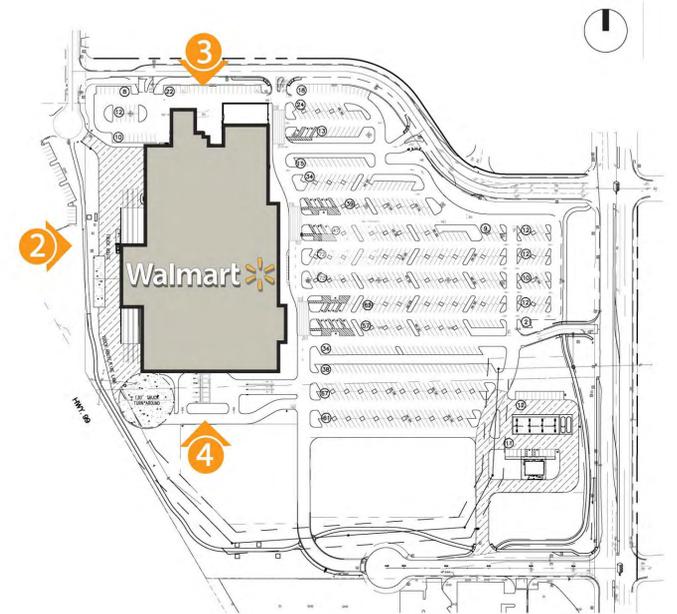
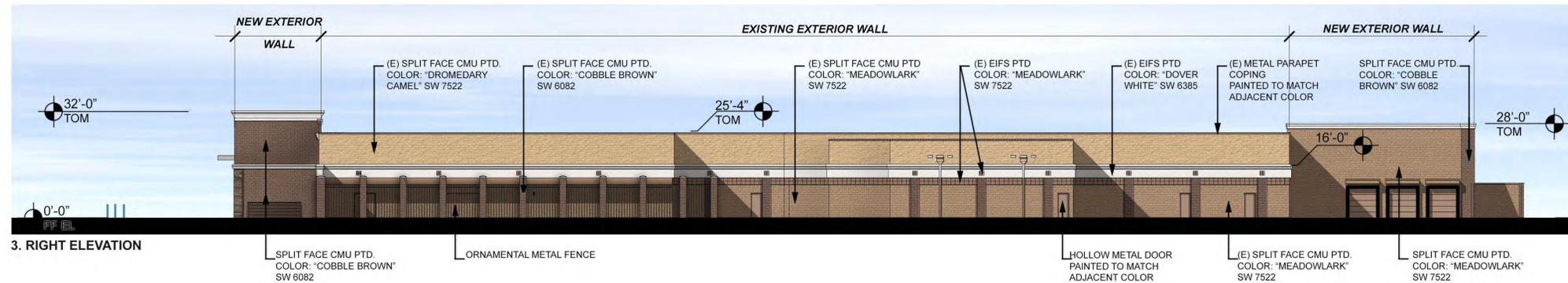
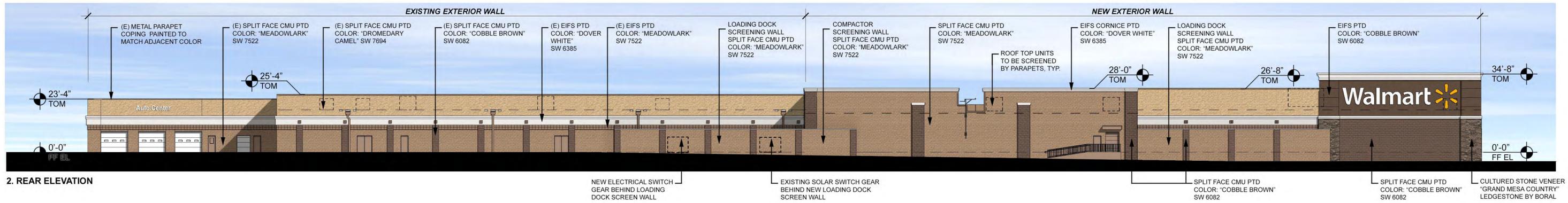
YAMASAKI
LANDSCAPE ARCHITECTURE
1223 HIGH STREET, AUBURN, CA 95603
(530) 883-0040
FAX (530) 883-0042

Attachment G

MATCHLINE
SEE SHEET L1.4

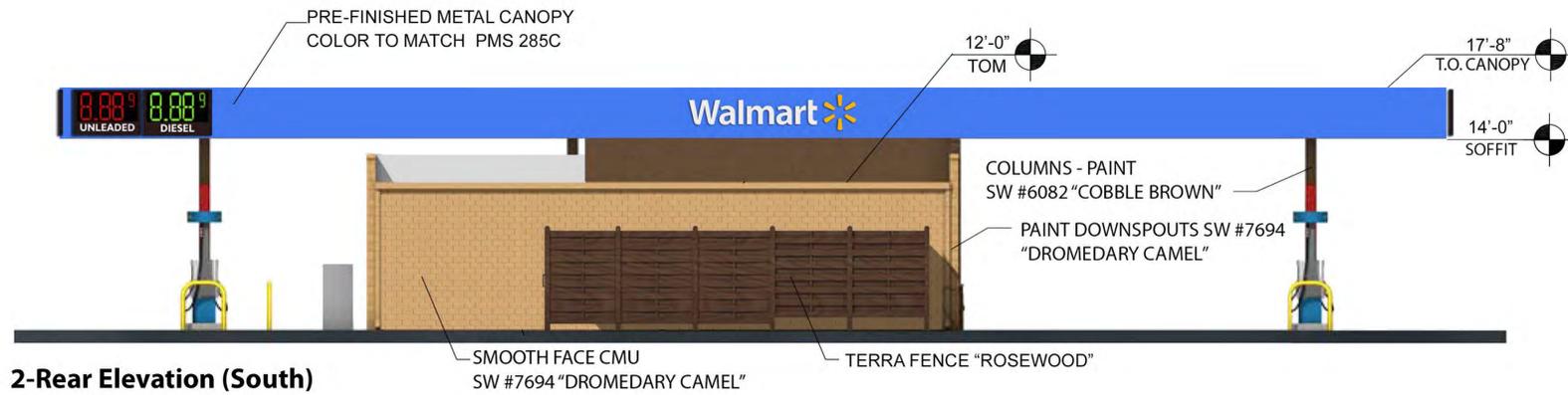


0 20 40 60 80 Job# 15.034

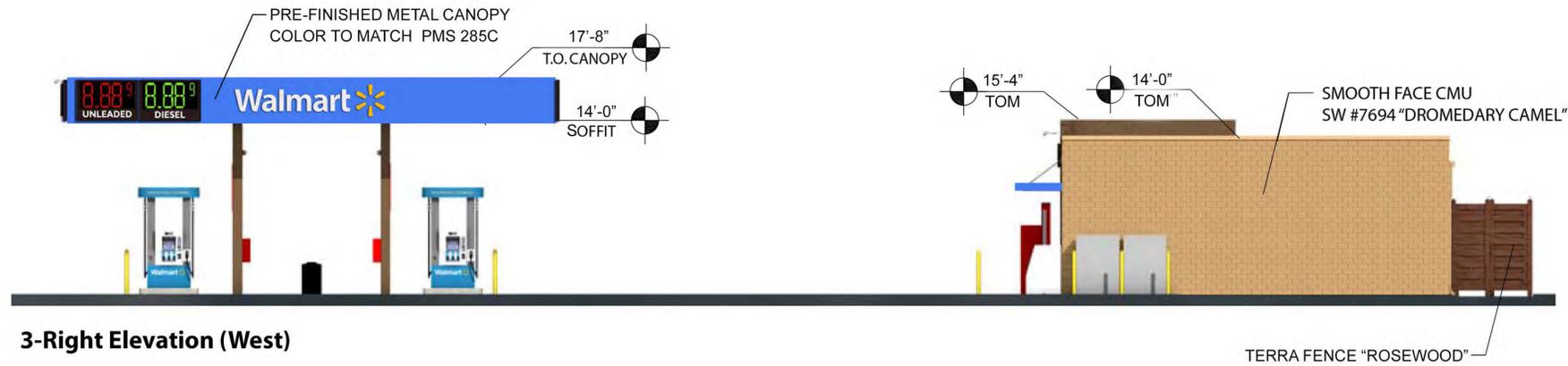




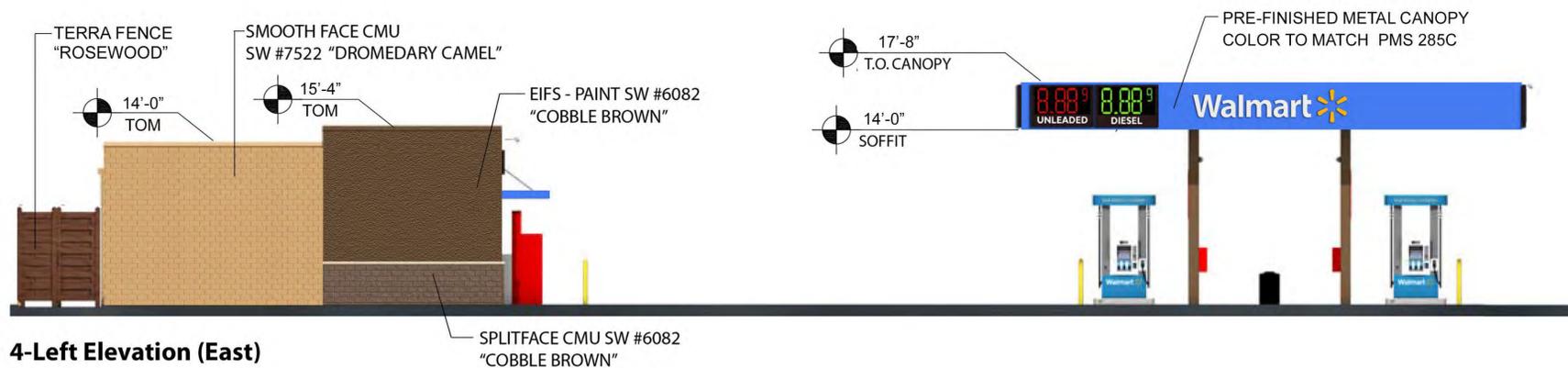
1-Front Elevation (North)



2-Rear Elevation (South)

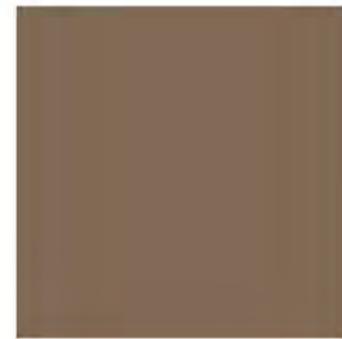


3-Right Elevation (West)



4-Left Elevation (East)





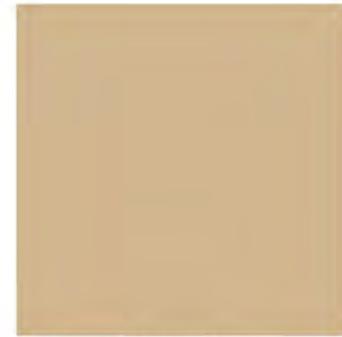
SHERWIN WILLIAMS
SW #6082 "COBBLE BROWN"



SHERWIN WILLIAMS
SW #6385 "DOVER WHITE"



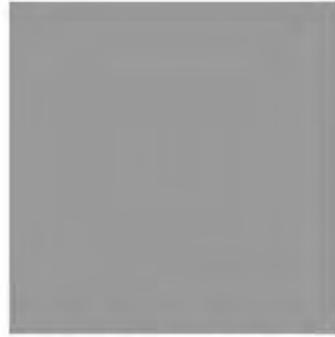
SHERWIN WILLIAMS
SW #6885 "KNOCKOUT ORANGE"



SHERWIN WILLIAMS
SW #7694 "DROMEDARY CAMEL"



SHERWIN WILLIAMS
SW #7522 "MEADOWLARK"



SW #7669 "SUMMIT GRAY"
SHERWIN WILLIAMS



GRAND MESA COUNTRY LEDGESTONE
BY BORAL



ALUMINUM
CLEAR ANODIZED



1-Front Elevation (Noth)



SHERWIN WILLIAMS
SW #6082 "COBBLE BROWN"



COLOR TO MATCH PMS 285C



SHERWIN WILLIAMS
SW #7694 "DROMEDARY CAMEL"



TERRA FENCE "ROSEWOOD"



ALUMINUM
CLEAR ANODIZED

GE
Lighting

Evolve™ LED Area Light

Scalable Area Light (EASA)



imagination at work

Attachment J

Product Features

The next evolution of the GE Evolve™ LED Area Light continues to deliver outstanding features, while adding greater flexibility, style and scalability. This latest design offers higher lumen outputs and provides even more photometric combinations in two ANSI color temperatures, providing the ability to meet even a wider range of area lighting needs.

Using reflective optic technology, GE offers superior vertical illuminance and uniformity, while minimizing glare as fixture lumens increase. This system delivers unusually low perceived glare when viewed from beneath the fixture near nadir. The Evolve™ LED Scalable Area Light provides reduced energy consumption, combined with a long rated life that virtually eliminates ongoing maintenance expenses, enabling significant operating cost benefits over the life of each fixture.

Applications

- Site, area, and general lighting applications utilizing advanced LED optical system providing high uniformity, excellent vertical light distribution, reduced offsite visibility, reduced on-site glare and effective security light levels.
- Design makes this product ideal for small to large retailers, commercial to medical properties, and big box retailers.

Housing

- Die-cast aluminum housing.
- Slim architectural design incorporates a heat sink light engine directly into the unit ensuring maximum heat transfer, long LED life, and a reduced Effective Projected Area (EPA).
- Meets 2G vibration standards per ANSI C136.31-2010. For 3G rating contact manufacturer.

LED & Optical Assembly

- Structured LED arrays for optimized area light photometric distribution.
- Evolve light engine with directional reflectors designed to optimize application efficiency and minimize glare.
- Utilizes high brightness LEDs, 70 CRI at 4000K and 5700K typical.
- LM-79 tests and reports are performed in accordance with IESNA standards.

Lumen Maintenance

- System rating is L85 at 50,000 hours. Contact manufacturer for Lxx rating (Lumen Depreciation) beyond 50,000 hours.

For Lumen Maintenance and TM-21 L70 Projections, see table published in OLP-3053.

Ratings

- /c/ listed, suitable for wet locations.
- IP65 rated optical enclosure per ANSI C136.25-2009.
- Temperature rated at -40° to 50°C (-40° to 35°C for 433W fixtures).
- Upward Light Output Ratio (ULOR) = 0.
- Compliant with the material restriction requirements of RoHS.

Mounting

Option A

- 10-inch (254mm) mounting arm for square pole prewired with 24-inch (610mm) leads.

Option B

- 10-inch (254mm) mounting arm for round pole prewired with 24-inch (610mm) leads.

Option C

- Slipfitter mounting for 2 3/8-inch (60mm) O.D. pipe prewired with 24-inch (610mm) leads.

Finish

- Corrosion resistant polyester powder painted, minimum 2.0 mil. thickness.
- Standard colors: Black & Dark Bronze.
- RAL & custom colors available.

Electrical

- 120-277 volt and 347-480 volt available.
- System power factor is >90% and THD <20%.*
- Class "A" sound rating.
- Photo electric sensors (PE) available for all voltages.
- GE dimmable PE socket is available for all fixtures, making the unit "adaptive controls ready."
- Integral surge protection:
 - For 120-277 VAC and 347-480 VAC per IEEE/ANSI C62.41.2-2002, 6kV/3kA Location Category C-Low (120 Events)

** System power factor and THD is tested and specified at 120V input and maximum load conditions.*

Warranty

- 5-year limited system warranty standard.

Ordering Number Logic

Evolve™ LED Scalable Area Light (EASA)



E A S A

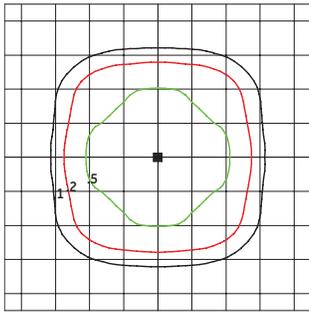
PROD. ID	VOLTAGE	OPTICAL CODE	DISTRIBUTION ORIENTATION	DRIVE CURRENT	LED COLOR TEMP	PE FUNCTION	MOUNTING ARM	COLOR	OPTIONS
E = Evolve A = Area S = Scalable A = Photometric Series A	0 = 120 - 277 1 = 120* 2 = 208* 3 = 240* 4 = 277* 5 = 480* D = 347* H = 347-480		F = Front L = Left R = Right N = Not Applicable	5 = 525MA (<433W only) 7 = 700MA (433W only)	40 = 4000K 57 = 5700K	1 = None 2 = PE Rec. 4 = PE Rec. with Shorting Cap 5 = PE Rec. with Control # 7 = Dimming PE Receptacle *† (UL only) 9 = Dimming PE Receptacle with Shorting Cap *† (UL only)	A = 10" Arm for Square Pole supplied with leads B = 10" Arm for Round Pole supplied with leads C = EXT Slip-fitter 2" Pipe (2.378 in. OD) supplied with leads	BLCK = Black DKBZ = Dark Bronze Contact manufacturer for other colors	D2 = Dimmable (0-10 Volt Input) F = Fusing T = Extra Surge Protection* XXX = Special Options
	*Specify single voltage if fuse option is selected.		<p>Light pattern thrown in direction specified in relation to Pole and Fixture.</p>			# PE control not available for 347-480V. Must be a discrete voltage (347V or 480V). † When ordering PE function socket 7 or 9, a dimming driver must also be ordered under the "OPTIONS" column * Order dimming control PE as a separate item			* Contact manufacturer for T option availability

	OPTICAL CODE	TYPE	TYPICAL INITIAL LUMENS		TYPICAL SYSTEM WATTAGE		DISTRIBUTION ORIENTATION AVAILABLE	BUG RATINGS*						IES FILE NUMBER	
			4000K	5700K	120-277V	347-480V		4000K			5700K			4000K	5700K
TYPE V	D5	Symmetric Medium	9,400	10,200	120	120	N	3	0	2	3	0	2	455198	455129
	E5	Symmetric Medium	12,400	13,400	162	162	N	4	0	2	4	0	2	455197	455128
	F5	Symmetric Medium	15,300	16,500	199	199	N	4	0	2	4	0	2	455196	455127
	G5	Symmetric Medium	18,100	19,600	236	236	N	4	0	2	4	0	2	455195	455126
	H5	Symmetric Medium	21,800	23,600	285	285	N	5	0	3	5	0	3	455194	455163
	J5	Symmetric Medium	30,300	31,800	433	433	N	5	0	3	5	0	3	456214	456215
	N5	Symmetric Short	10,200	11,000	120	120	N	3	0	1	3	0	1	455211	455142
	P5	Symmetric Short	13,300	14,400	162	162	N	3	0	2	3	0	2	455210	455141
	Q5	Symmetric Short	16,500	17,800	199	199	N	4	0	2	4	0	2	455209	455140
	R5	Symmetric Short	19,500	21,100	236	236	N	4	0	2	4	0	2	455208	455139
TYPE IV	S5	Symmetric Short	23,600	25,500	285	285	N	4	0	2	4	0	2	455207	455138
	T5	Symmetric Short	32,600	35,300	433	433	N	5	0	2	5	0	2	456216	456217
	A4	Asymmetric Forward	4,800	5,200	62	67	F, L, R	1	0	2	1	0	2	455206	455137
	B4	Asymmetric Forward	6,300	6,800	82	82	F, L, R	1	0	2	1	0	2	455205	455136
	C4	Asymmetric Forward	7,800	8,400	101	101	F, L, R	1	0	2	1	0	2	455204	455135
	D4	Asymmetric Forward	9,300	10,000	120	120	F, L, R	2	0	2	2	0	2	455203	455134
	E4	Asymmetric Forward	11,200	12,100	145	145	F, L, R	2	0	2	2	0	3	455202	455133
	F4	Asymmetric Forward	15,300	16,500	199	199	F, L, R	2	0	3	2	0	3	455201	455132
	G4	Asymmetric Forward	18,100	19,600	236	236	F, L, R	3	0	4	3	0	4	455200	455131
	H4	Asymmetric Forward	21,800	23,600	285	285	F, L, R	3	0	4	3	0	4	455232	455130
TYPE III	J4	Asymmetric Forward	30,300	32,800	433	433	F, L, R	3	0	5	3	0	5	456218	456219
	A3	Asymmetric Wide	5,100	5,500	62	67	F, L, R	1	0	1	1	0	1	455219	455150
	B3	Asymmetric Wide	6,800	7,300	82	82	F, L, R	1	0	1	1	0	1	455218	455149
	C3	Asymmetric Wide	8,300	9,000	101	101	F, L, R	2	0	2	2	0	2	455217	455148
	D3	Asymmetric Wide	9,900	10,700	120	120	F, L, R	2	0	2	2	0	2	455216	455147
	E3	Asymmetric Wide	11,900	12,900	145	145	F, L, R	2	0	2	2	0	2	455215	455146
	F3	Asymmetric Wide	16,500	17,800	199	199	F, L, R	2	0	2	2	0	2	455214	455145
	G3	Asymmetric Wide	19,500	21,100	236	236	F, L, R	2	0	2	3	0	3	455213	455144
	H3	Asymmetric Wide	23,600	25,500	285	285	F, L, R	3	0	3	3	0	3	455212	455143
	J3	Asymmetric Wide	32,600	35,300	433	433	F, L, R	3	0	4	3	0	4	456220	456221
TYPE II	A2	Asymmetric Narrow	5,100	5,500	62	67	F, L, R	1	0	1	1	0	1	455227	455158
	B2	Asymmetric Narrow	6,700	7,200	82	82	F, L, R	2	0	2	2	0	2	455226	455157
	C2	Asymmetric Narrow	8,200	8,900	101	101	F, L, R	2	0	2	2	0	2	455225	455156
	D2	Asymmetric Narrow	9,700	10,500	120	120	F, L, R	2	0	2	2	0	2	455224	455155
	E2	Asymmetric Narrow	11,700	12,700	145	145	F, L, R	2	0	2	2	0	2	455223	455154
	F2	Asymmetric Narrow	16,100	17,400	199	199	F, L, R	3	0	3	3	0	3	455222	455153
	G2	Asymmetric Narrow	19,100	20,600	236	236	F, L, R	3	0	3	3	0	3	455221	455152
	H2	Asymmetric Narrow	22,900	24,800	285	285	F, L, R	3	0	3	3	0	3	455220	455151
	J2	Asymmetric Narrow	32,400	35,100	433	433	F, L, R	3	0	4	3	0	4	456222	456223

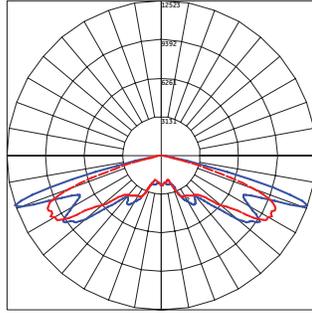
*Rating values for B and G are based on rated lumens and may vary due to flux tolerances.

Photometrics

EASA Type V - Symmetric Medium (J5)
31,800 Lumens, 5700K (GE456215.ies)

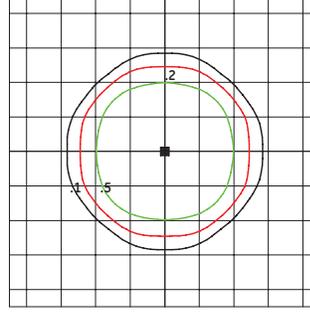


Grid Distance in Units of Mounting Height at 40° Initial Footcandle Values at Grade

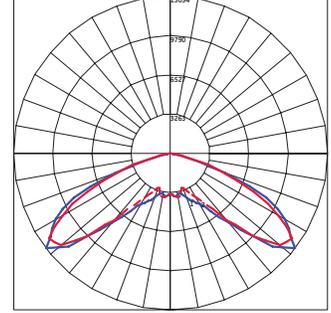


— Vertical plane through horizontal angle of maximum candlepower at 50°
- - Vertical plane through horizontal angle of 71°

EASA Type V - Symmetric Short (T5)
35,300 Lumens, 5700K (GE456217.ies)

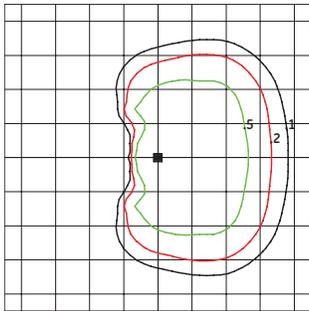


Grid Distance in Units of Mounting Height at 40° Initial Footcandle Values at Grade

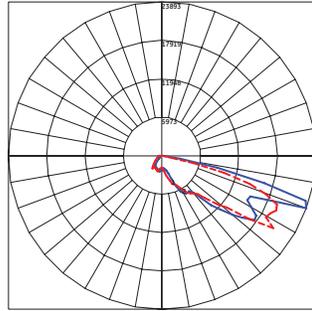


— Vertical plane through horizontal angle of maximum candlepower at 25°
- - Vertical plane through horizontal angle of 52°

EASA Type IV - Asymmetric Forward (J4)
32,800 Lumens, 5700K (GE456219.ies)

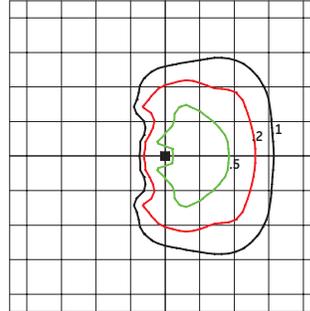


Grid Distance in Units of Mounting Height at 40° Initial Footcandle Values at Grade

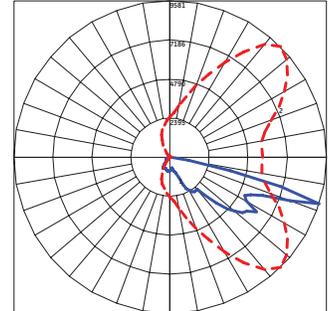


— Vertical plane through horizontal angle of maximum candlepower at 40°
- - Vertical plane through horizontal angle of 70°

EASA Type IV - Asymmetric Forward (E4)
12,100 Lumens, 5700K (GE455133.ies)

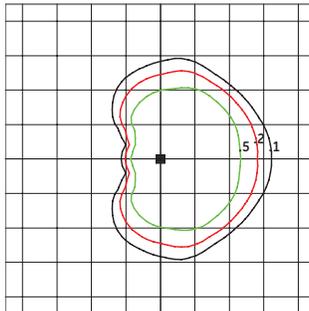


Grid Distance in Units of Mounting Height at 30° Initial Footcandle Values at Grade

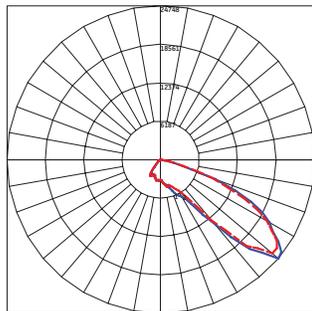


— Vertical plane through horizontal angle of maximum candlepower at 45°
- - Vertical plane through horizontal angle of 72°

EASA Type III - Asymmetric Wide (J3)
35,300 Lumens, 5700K (GE456221.ies)

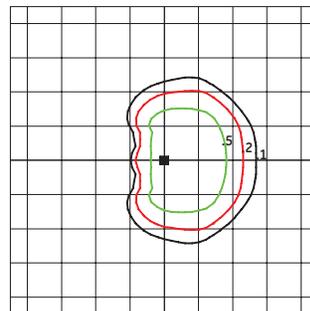


Grid Distance in Units of Mounting Height at 30° Initial Footcandle Values at Grade

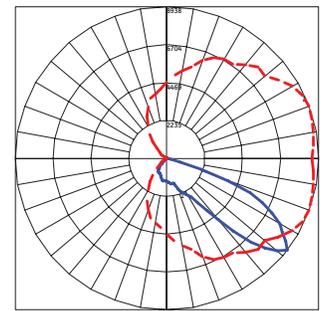


— Vertical plane through horizontal angle of maximum candlepower at 20°
- - Vertical plane through horizontal angle of 50°

EASA Type III - Asymmetric Wide (E3)
12,900 Lumens, 5700K (GE455146.ies)

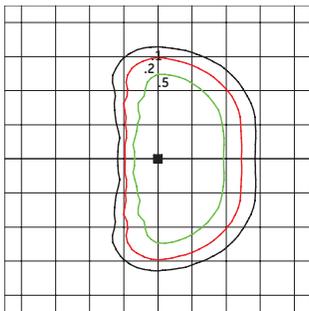


Grid Distance in Units of Mounting Height at 30° Initial Footcandle Values at Grade

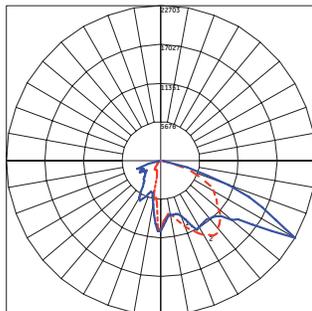


— Vertical plane through horizontal angle of maximum candlepower at 20°
- - Vertical plane through horizontal angle of 52°

EASA Type II - Asymmetric Narrow (J2)
35,100 Lumens, 5700K (GE456223.ies)

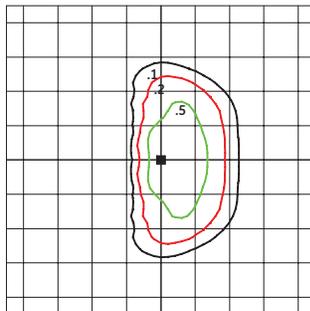


Grid Distance in Units of Mounting Height at 40° Initial Footcandle Values at Grade

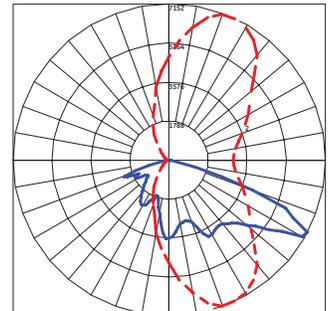


— Vertical plane through horizontal angle of maximum candlepower at 60°
- - Vertical plane through horizontal angle of 60°

EASA Type II - Asymmetric Narrow (E2)
12,700 Lumens, 5700K (GE455154.ies)



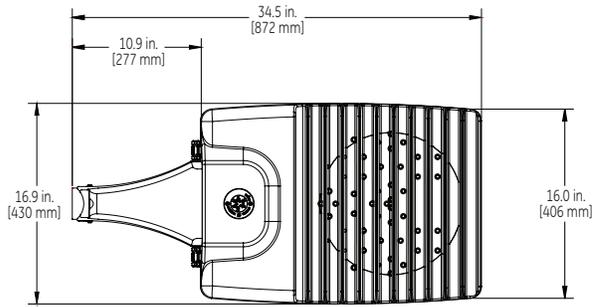
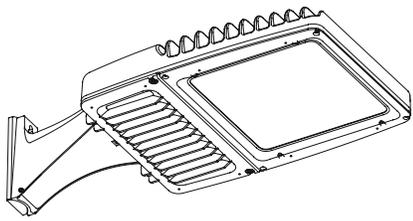
Grid Distance in Units of Mounting Height at 30° Initial Footcandle Values at Grade



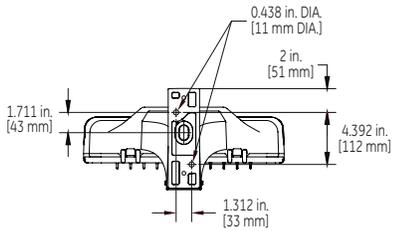
— Vertical plane through horizontal angle of maximum candlepower at 65°
- - Vertical plane through horizontal angle of 62°

Product Dimensions

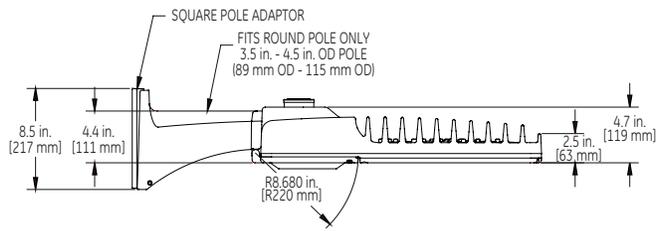
10" Arm For Square Pole Mount (Option A)



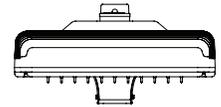
TOP VIEW



BACK VIEW



SIDE VIEW



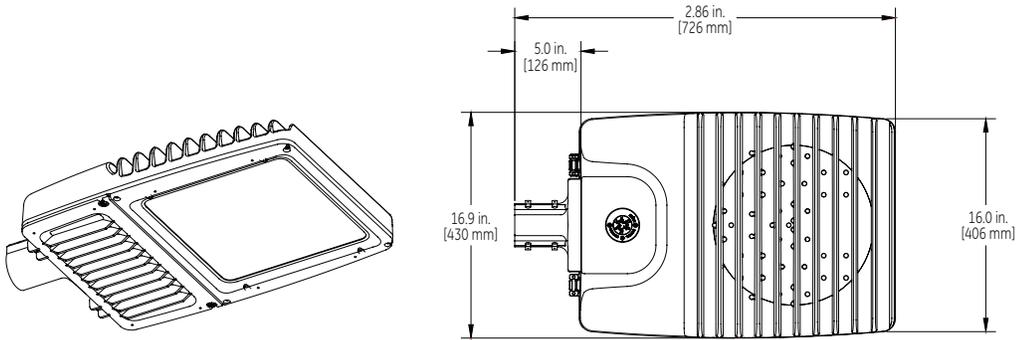
FRONT VIEW

DATA

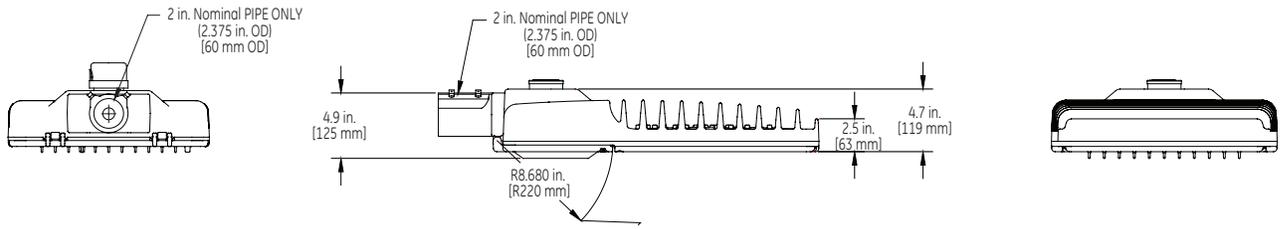
- Approximate net weight: 43-47 lbs (19.50 - 21.32 kgs)
Contact manufacturer for specific configuration weight.
- Effective Projected Area (EPA) with 10" Mounting Arm: 0.97 sq ft max (0.09 sq m)

Product Dimensions

Slipfitter Arm Mount (Option C)



TOP VIEW



BACK VIEW

SIDE VIEW

FRONT VIEW

DATA

- Approximate net weight: 41-45 lbs (18.60 - 20.41 kgs)
Contact manufacturer for specific configuration weight.
- Effective Projected Area (EPA) with Slipfitter: 0.47 sq ft max (0.04 sq m)

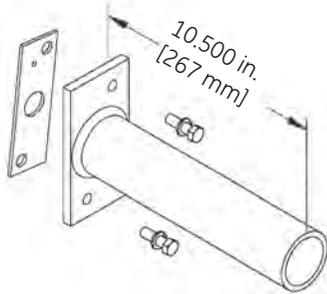
Mounting Information

Mounting Arms for Slipfitter

Order separately with Mounting Option C (External Slipfitter)

SQUARE POLE MOUNTING ARM

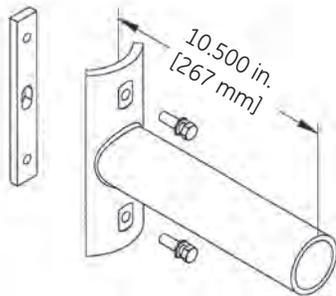
3.5 TO 4.5-inch (89 to 114mm) SQUARE
(WILL ALLOW 4 FIXTURES PER POLE @ 90 DEGREES.)



ORDER SEPARATELY FROM FIXTURE AS CATALOG NUMBER
SPA-EAMT10BLCK "Black"
SPA-EAMT10DKBZ "Dark Bronze"

ROUND POLE MOUNTING ARM

3.5 TO 4.5-inch (89 to 114mm) OD
(WILL ALLOW 4 FIXTURES PER POLE @ 90 DEGREES.)



ORDER SEPARATELY FROM FIXTURE AS CATALOG NUMBER
RPA-EAMT10BLCK "Black"
RPA-EAMT10DKBZ "Dark Bronze"

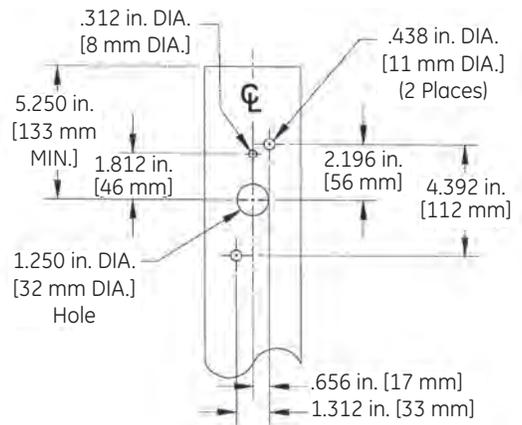
Wall Mounting Bracket Adapter Plate

ORDER SEPARATELY FROM FIXTURE AS CATALOG NUMBER
WMB-EAMT06

Other mounting patterns are available for retrofit installations.
Contact manufacturing for other available mounting patterns.

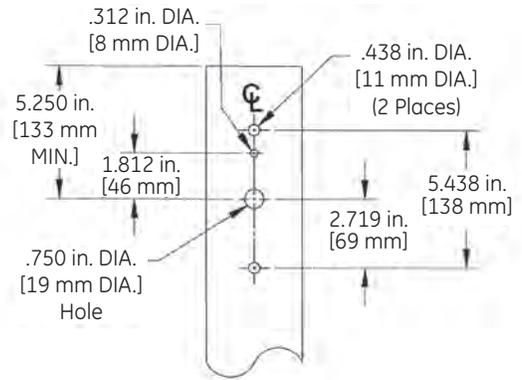
Drilling Templates for Slipfitter Arms & Arm Mount

SQUARE POLE MOUNTING



ROUND POLE MOUNTING

3.5 TO 4.5-inch (89 to 114mm) OD
round pole mounting arm



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GE
Lighting Solutions

Type
5W

EWSW0E4N57A1NBLCK
101 Watts

Evolve™ LED Area Light

Modular Fixture - Wall Pack & Egress (EWSW & EESW)



imagination at work

Product Features

The GE Evolve™ LED Area Light also offers a wall pack that delivers the same outstanding performance as the modular area light solution. Using the same light engines as the “single module” area light, the wall pack offers identical photometrics, which allows lighting designers to capitalize on the same features without compromise. In keeping with a sleek design strategy, this product offers a modern “wedge” look, balancing the need for photometric scalability with reliable workhorse performance. The wall pack provides 10 photometric combinations, available in two color temperatures, to meet a wide range of application needs. The same wedge platform also offers an egress option to compliment the family, and providing a complete and consistent look for all area lighting applications.

GE’s exclusive optical ring design effectively directs the light and produces superior vertical illuminance. Additionally, reduced energy consumption, combined with a long rated life that virtually eliminates ongoing maintenance expenses, enables the Evolve LED Wall and Egress to provide significant operating cost benefits over the life of each fixture.

Applications

- Wall mounted, single module site, area, and general lighting utilizing advanced LED optical system providing high uniformity, excellent vertical light distribution, reduced offsite visibility, reduced on-site glare and effective security light levels.

Housing

- Cast aluminum housing.
- Sleek architectural wedge design incorporates a modular heat sink light engine directly into the unit ensuring maximum heat transfer and long LED life.
- Meets 2G vibration standards per ANSI C136.32-2001. For 3G rating contact factory.

LED & Optical Assembly

- Structured LED arrays for optimized area light photometric distribution.
- Evolve modular light engine consisting of nested concentric directional reflectors designed to optimize application efficiency and minimize glare.
- Utilizes high brightness LEDs, 70 CRI at 4000K and 5700K typical, binned per ANSI C78.377-2008.
- LM-79 tests and reports are performed in accordance to IESNA standards.

Lumen Maintenance

- System rating is 50,000 hours at L85. Contact factory for L rating (Lumen Depreciation) beyond 50,000 hours.

Ratings

- UL/cUL listed, suitable for wet locations.
- IP 65 rated optical per ANSI C136.25-2009.
- Temperature Rated at -40° to 50°C.

Mounting

- Convenient Swing out wall mount to “J” box with inspection hole for IP 65.

Finish

- Corrosion resistant polyester powder painted, minimum 2.0 mil. thickness.
- Standard colors: Black & Dark Bronze.
- RAL & custom colors available.

Electrical

- Wall Pack: 120-277 volt and 347-480 volt available.
- Egress: 120-277 volt universal electronic driver.
- System power factor is >90% and THD <20%.
- Class “A” sound rating.
- Integral surge protection non-dimming:
 - For 120-277VAC per IEEE/ANSI C62.41.-1991, 4kV/2kA Location Category B2 (120 Events)
 - For 347-480VAC per IEEE/ANSI C62.41.-1991, 6kV/3kA Location Category B3 (120 Events)
- Integral surge protection GE dimming:
 - For 120-480VAC per IEEE/ANSI C62.41.2-2002, 6kV/3kA Location Category B (120 Events)
- EMI: FCC 47 47 CFR 15 Class A

Warranty

- 5-year limited system warranty standard.

Ordering Number Logic

Wall Pack (EWSW)



E W S W - - - - - **N** - - - - - **A** - **1** - **N** - - - - -

PROD. ID	VOLTAGE	OPTICAL CODE	DISTRIBUTION ORIENTATION	LED COLOR TEMP	LENS TYPE	PE FUNCTION	MOUNTING ARM	COLOR	OPTIONS
E = Evolve W = Wallpack S = Small W = Wedge	0 = 120 - 277 1 = 120* 2 = 208* 3 = 240* 4 = 277* 5 = 480* D = 347*		N = Not applicable	40 = 4000K 57 = 5700K	A = Acrylic	1 = None	N = Not Applicable	BLCK = Black DKBZ = Dark Bronze Contact factory for other colors	D = Dimmable (0-10 Volt Input)* F = Fusing XXX = Special Options *Contact factory for availability.



OPTICAL CODE	TYPE	TYPICAL INITIAL LUMENS		TYPICAL SYSTEM WATTAGE		DISTRIBUTION ORIENTATION AVAILABLE	BUG RATINGS						IES FILE NUMBER*		
		4000K	5700K	120-277V	347-480V		4000K			5700K			4000K	5700K	
TYPE IV	A4	Asymmetric Forward	3130	3400	50	53	N	1	2	1	1	2	1	454394	454395
	B4	Asymmetric Forward	4050	4400	63	66	N	1	2	2	1	2	2	454397	454398
	C4	Asymmetric Forward	4970	5400	76	80	N	1	2	2	1	2	2	454400	454401
	D4	Asymmetric Forward	5890	6400	89	94	N	1	2	2	1	3	2	454403	454404
	E4	Asymmetric Forward	6810	7400	101	106	N	1	3	2	2	3	2	454406	454407
TYPE III	A3	Asymmetric Wide	3130	3400	50	53	N	1	1	1	1	1	1	454421	454422
	B3	Asymmetric Wide	4050	4400	63	66	N	1	1	1	1	1	1	454424	454425
	C3	Asymmetric Wide	4970	5400	76	80	N	1	1	1	1	2	2	454427	454428
	D3	Asymmetric Wide	5890	6400	89	94	N	1	1	2	1	1	2	454430	454431
	E3	Asymmetric Wide	6810	7400	101	106	N	1	1	2	1	1	2	454433	454434

Egress (EESW)

E E S W - - - - - **N** - - - - - **A** - **1** - **N** - - - - -

PROD. ID	VOLTAGE	OPTICAL CODE	DISTRIBUTION ORIENTATION	LED COLOR TEMP	LENS TYPE	PE FUNCTION	MOUNTING ARM	COLOR	OPTIONS
E = Evolve E = Egress S = Small W = Wedge	0 = 120 - 277 1 = 120* 2 = 208* 3 = 240* 4 = 277*		N = Not applicable	40 = 4000K 57 = 5700K	A = Acrylic	1 = None	N = Not Applicable	BLCK = Black DKBZ = Dark Bronze Contact factory for other colors	F = Fusing XXX = Special Options

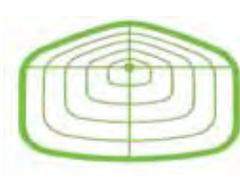


OPTICAL CODE	TYPE	TYPICAL INITIAL LUMENS		TYPICAL SYSTEM WATTAGE		DISTRIBUTION ORIENTATION AVAILABLE	BUG RATINGS						IES FILE NUMBER*		
		4000K	5700K	120-277V	347-480V		4000K			5700K			4000K	5700K	
WE	Egress - Asymmetric Wide	1290	1400	20	N/A	N	0	0	0	0	0	0	0	454576	454575

IESNA Classifications

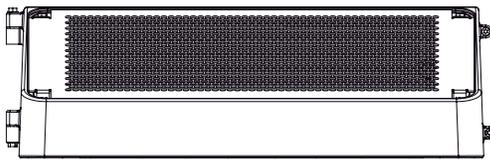
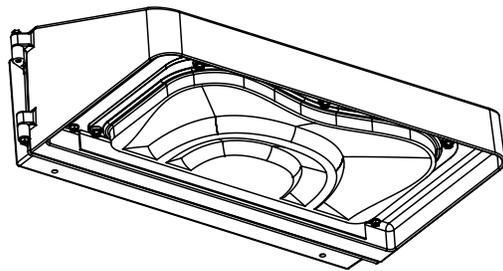


ASYMMETRIC FORWARD

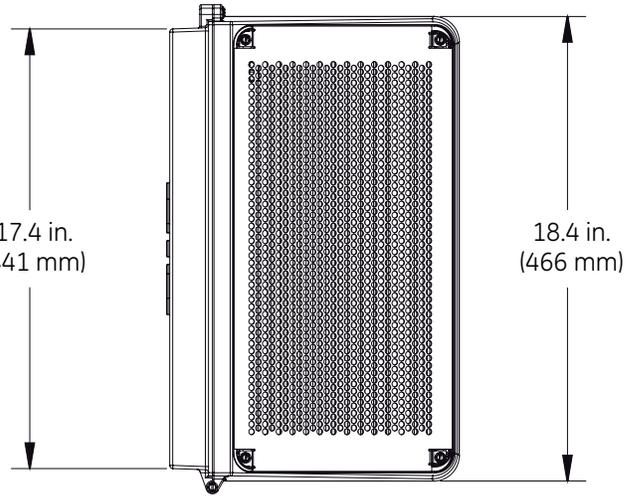


ASYMMETRIC WIDE

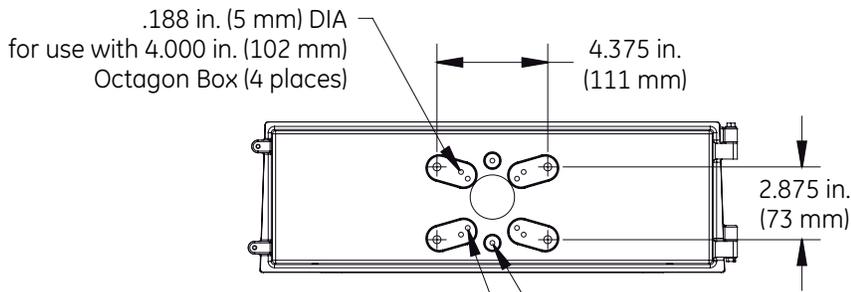
Product Dimensions



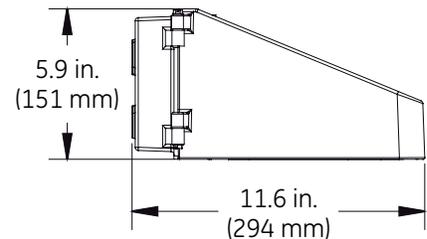
FRONT VIEW



TOP VIEW



BACK VIEW



SIDE VIEW

.188 in. (5 mm) DIA
for use with 4.000 in. (102 mm)
Octagon Box (4 places)

4.375 in.
(111 mm)

2.875 in.
(73 mm)

5.9 in.
(151 mm)

11.6 in.
(294 mm)

.188 in. (5 mm) DIA
for use with 3.250 in. (83 mm)
Octagon Box (4 places)

.188 in. (5 mm) DIA
for use with Gem-Box

DATA

- Approximate Net Weight for Wall Pack: 29 lbs (13 kgs)
- Approximate Net Weight for Egress: 24 lbs (11 kgs)



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1-888-69-43-533

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GE
Lighting Solutions

Type
11

EESW0WEN57A1NBLCK
20 Watts

Evolve™ LED Area Light

Modular Fixture - Wall Pack & Egress (EWSW & EESW)



imagination at work

Product Features

The GE Evolve™ LED Area Light also offers a wall pack that delivers the same outstanding performance as the modular area light solution. Using the same light engines as the “single module” area light, the wall pack offers identical photometrics, which allows lighting designers to capitalize on the same features without compromise. In keeping with a sleek design strategy, this product offers a modern “wedge” look, balancing the need for photometric scalability with reliable workhorse performance. The wall pack provides 10 photometric combinations, available in two color temperatures, to meet a wide range of application needs. The same wedge platform also offers an egress option to compliment the family, and providing a complete and consistent look for all area lighting applications.

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Applications

- Wall mounted, single module site, area, and general lighting utilizing advanced LED optical system providing high uniformity, excellent vertical light distribution, reduced offsite visibility, reduced on-site glare and effective security light levels.

Housing

- Cast aluminum housing.
- Sleek architectural wedge design incorporates a modular heat sink light engine directly into the unit ensuring maximum heat transfer and long LED life.
- Meets 2G vibration standards per ANSI C136.32-2001. For 3G rating contact factory.

LED & Optical Assembly

- Structured LED arrays for optimized area light photometric distribution.
- Evolve modular light engine consisting of nested concentric directional reflectors designed to optimize application efficiency and minimize glare.
- Utilizes high brightness LEDs, 70 CRI at 4000K and 5700K typical, binned per ANSI C78.377-2008.
- LM-79 tests and reports are performed in accordance to IESNA standards.

Lumen Maintenance

- System rating is 50,000 hours at L85. Contact factory for L rating (Lumen Depreciation) beyond 50,000 hours.

Ratings

- UL/cUL listed, suitable for wet locations.
- IP 65 rated optical per ANSI C136.25-2009.
- Temperature Rated at -40° to 50°C.

Mounting

- Convenient Swing out wall mount to “J” box with inspection hole for IP 65.

Finish

- Corrosion resistant polyester powder painted, minimum 2.0 mil. thickness.
- Standard colors: Black & Dark Bronze.
- RAL & custom colors available.

Electrical

- Wall Pack: 120-277 volt and 347-480 volt available.
- Egress: 120-277 volt universal electronic driver.
- System power factor is >90% and THD <20%.
- Class “A” sound rating.
- Integral surge protection non-dimming:
 - For 120-277VAC per IEEE/ANSI C62.41.-1991, 4kV/2kA Location Category B2 (120 Events)
 - For 347-480VAC per IEEE/ANSI C62.41.-1991, 6kV/3kA Location Category B3 (120 Events)
- Integral surge protection GE dimming:
 - For 120-480VAC per IEEE/ANSI C62.41.2-2002, 6kV/3kA Location Category B (120 Events)
- EMI: FCC 47 47 CFR 15 Class A

Warranty

- 5-year limited system warranty standard.

Ordering Number Logic

Wall Pack (EWSW)



E W S W - - - - - N - - - - - A - 1 - N - - - - -

PROD. ID	VOLTAGE	OPTICAL CODE	DISTRIBUTION ORIENTATION	LED COLOR TEMP	LENS TYPE	PE FUNCTION	MOUNTING ARM	COLOR	OPTIONS
E = Evolve W = Wallpack S = Small W = Wedge	0 = 120 - 277 1 = 120* 2 = 208* 3 = 240* 4 = 277* 5 = 480* D = 347*		N = Not applicable	40 = 4000K 57 = 5700K	A = Acrylic	1 = None	N = Not Applicable	BLCK = Black DKBZ = Dark Bronze Contact factory for other colors	D = Dimmable (0-10 Volt Input)* F = Fusing XXX = Special Options *Contact factory for availability.



OPTICAL CODE	TYPE	TYPICAL INITIAL LUMENS		TYPICAL SYSTEM WATTAGE		DISTRIBUTION ORIENTATION AVAILABLE	BUG RATINGS						IES FILE NUMBER*		
		4000K	5700K	120-277V	347-480V		4000K			5700K			4000K	5700K	
TYPE IV	A4	Asymmetric Forward	3130	3400	50	53	N	1	2	1	1	2	1	454394	454395
	B4	Asymmetric Forward	4050	4400	63	66	N	1	2	2	1	2	2	454397	454398
	C4	Asymmetric Forward	4970	5400	76	80	N	1	2	2	1	2	2	454400	454401
	D4	Asymmetric Forward	5890	6400	89	94	N	1	2	2	1	3	2	454403	454404
	E4	Asymmetric Forward	6810	7400	101	106	N	1	3	2	2	3	2	454406	454407
TYPE III	A3	Asymmetric Wide	3130	3400	50	53	N	1	1	1	1	1	1	454421	454422
	B3	Asymmetric Wide	4050	4400	63	66	N	1	1	1	1	1	1	454424	454425
	C3	Asymmetric Wide	4970	5400	76	80	N	1	1	1	1	2	2	454427	454428
	D3	Asymmetric Wide	5890	6400	89	94	N	1	1	2	1	1	2	454430	454431
	E3	Asymmetric Wide	6810	7400	101	106	N	1	1	2	1	1	2	454433	454434

Egress (EESW)

E E S W - - - - - N - - - - - A - 1 - N - - - - -

PROD. ID	VOLTAGE	OPTICAL CODE	DISTRIBUTION ORIENTATION	LED COLOR TEMP	LENS TYPE	PE FUNCTION	MOUNTING ARM	COLOR	OPTIONS
E = Evolve E = Egress S = Small W = Wedge	0 = 120 - 277 1 = 120* 2 = 208* 3 = 240* 4 = 277*		N = Not applicable	40 = 4000K 57 = 5700K	A = Acrylic	1 = None	N = Not Applicable	BLCK = Black DKBZ = Dark Bronze Contact factory for other colors	F = Fusing XXX = Special Options



OPTICAL CODE	TYPE	TYPICAL INITIAL LUMENS		TYPICAL SYSTEM WATTAGE		DISTRIBUTION ORIENTATION AVAILABLE	BUG RATINGS						IES FILE NUMBER*		
		4000K	5700K	120-277V	347-480V		4000K			5700K			4000K	5700K	
WE	Egress - Asymmetric Wide	1290	1400	20	N/A	N	0	0	0	0	0	0	0	454576	454575

IESNA Classifications

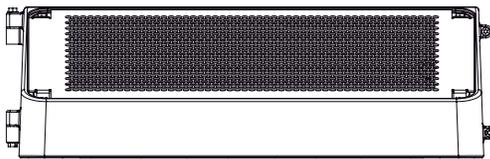
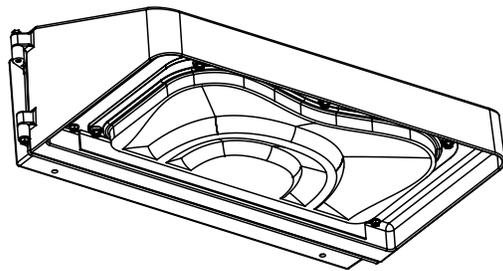


ASYMMETRIC FORWARD

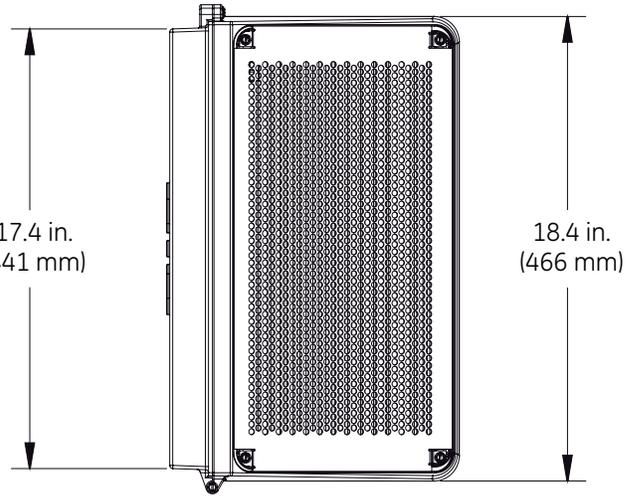


ASYMMETRIC WIDE

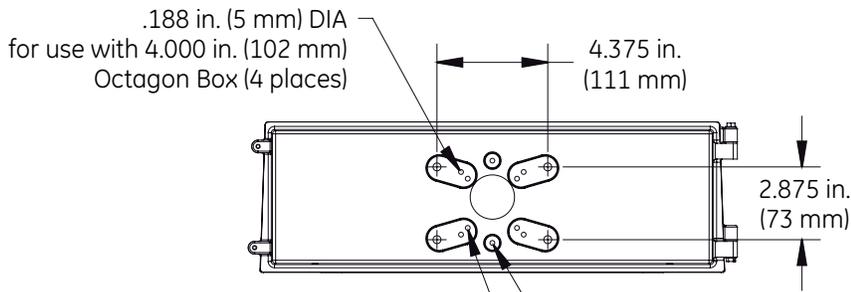
Product Dimensions



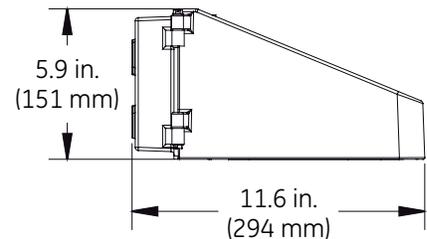
FRONT VIEW



TOP VIEW



BACK VIEW



SIDE VIEW

.188 in. (5 mm) DIA for use with 3.250 in. (83 mm) Octagon Box (4 places)

.188 in. (5 mm) DIA for use with Gem-Box

DATA

- Approximate Net Weight for Wall Pack: 29 lbs (13 kgs)
- Approximate Net Weight for Egress: 24 lbs (11 kgs)



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City of Chico Walmart Expansion Project Description November 2015

1.0 INTRODUCTION

The proposed project consists of the expansion and alteration of the existing Walmart store in the City of Chico, California. The project will include the addition of approximately 64,386 square feet of building area to the existing 125,889-square-foot store, for a total building area of 190,275 square feet. The expanded store will include additional food and general merchandise sales floor area; additional stockroom, office, and support areas; a new pharmacy/merchandise drive through and a new fueling station.

1.1 PROJECT LOCATION

The project site is located in the City of Chico, Butte County, California. Chico is located in the Sacramento Valley, approximately 85 miles north of the City of Sacramento. The Sacramento River runs approximately 10 miles to the west of Chico and forms the western boundary of Butte County. Lake Oroville is located approximately 20 miles to the southeast. State Route 99, one of the main north south highways in California, passes through the City. State Route 32, an east-west highway, also passes through Chico. Chico is the most populous city in Butte County with an urban area population of approximately 103,625 approximately 30.5 square miles in size.

The project site comprises 27.08 acres located at 2044 Forest Avenue. The site is bounded on the north by Baney Lane, on the east by Forest Avenue, on the south by Wittmeier Drive, and Business Lane and State Route 99 are west of the site. The State Route 99/East 20th Street interchange is located approximately one-quarter mile to the northwest.

1.2 EXISTING SETTING

The surrounding area is predominantly commercial in character. The Chico Mall, The Village Center, and other commercial establishments are located along East 20th Street. The Wittmeier Auto Center, the recently completed Butte Community College extension, and Lowe's are located on Forest Avenue, south of the project site. State Route 99, a four-lane freeway through Chico, occupies the area west of the project site. The Chico Crossroads Center and Costco are on the west side of State Route 99. East of Forest Avenue, are several office buildings, behind which is a residential area. Immediately north of the project site is a Sinclair gasoline station, vacant commercial land, and the Oxford Suites motel. The Krispy Kreme and the In-and-Out Burger restaurants are located on the western boundary of the project site, at the end of Business Lane.

The Chico General Plan designates the project site as Regional Commercial and the project site is zoned CR Regional Commercial. The project site contains two parcels. One of the parcels comprises 16.46 acres and currently contains a Walmart retail store and associated parking lot. The second parcel is 10.62 acres in size and is located adjacent to and south of the existing Walmart store. This parcel is currently undeveloped and relatively flat. Vegetation on the project site consists of grasses and weeds and contains small, isolated seasonal wetlands.

The existing Walmart store was previously approved and opened for business in 1994. Walmart is a worldwide retail store chain. The existing Walmart store on the project site is approximately 125,889 square feet in size and sells general consumer merchandise including clothing, electronics, hardware, small household appliances, home furnishings, sporting goods, and a limited amount of food and beverages. The existing store also houses a pharmacy, optical and automotive services, and a garden center. The parking lot is located in front of the store and contains 630 parking spaces. Landscaping has been installed in planter areas along the site perimeter on the western property line behind the store, along the frontages of Business Lane, Baney Lane and Forest Avenue, and throughout the parking lot in parking lot island and peninsula planters. A Class I bicycle path is located on the western and southern boundaries of the Walmart store and parking lot. This existing bicycle path is a small segment of a planned Class I bicycle/pedestrian path that will run on the east side of State Route 99 from Big Chico Creek to Skyway/Norte Dame Boulevard. Utility services include city sewer, California Water Service Company provides water, PG&E provides electricity and natural gas, AT&T provides telephone, and Comcast provides cable TV. The store is accessed from Baney Lane via three driveways, from Forest Avenue via one driveway, and from Business Lane via one driveway.

The vacant parcel to the south is not presently connected to any utilities. Wittmeier Drive is located along the southern boundary of the vacant parcel, but there is currently no driveway access serving the parcel.

1.3 REQUESTED PROJECT APPROVALS

CITY OF CHICO ENTITLEMENTS

Tentative Parcel Map

The Project will require a tentative parcel map to reconfigure the lot lines of the existing parcels (a 16.46-acre parcel and a 10.62-acre parcel) to create one 21.88-acre (Parcel 1) and two smaller parcels averaging 2.60 acres each (Parcels 2 and 3). The existing Walmart and the planned expansion will be located entirely on Parcel 1. The fueling station will also be developed on Parcel 1. Parcels 2 and 3 may be developed with other uses in the future.

In addition, as a part of the tentative parcel map, certain public utility and public access easements (sanitary sewer, storm drainage, water, utilities, and public bicycle path) are proposed for abandonment and relocation on the project site.

Use Permit

The Project site is located in the CR Regional Commercial zoning district, which allows retail use "by right." However, the Project will require a use permit for the fueling station and a use permit for the pharmacy/merchandise drive-through and permanent outdoor display and sales area.

Site Design and Architectural Review

The Project will require site design and architectural review by the Architectural Review Board for the proposed building elevations, site plan, lighting and landscaping plan.

Planned Development Permit

The Project will require a planned development permit to increase the maximum permitted signage on the Project site. As shown on the Project's conceptual sign program, wall signage is proposed along three sides of the Walmart building, on the drive thru canopies, and on the fuel station canopy and kiosk; and two ground-mounted signs (one existing and one new) are proposed on the Project site. The total proposed sign area exceeds the maximum allowed by the Municipal Code by approximately 1,000 square feet. The project requests an increase in the permitted signage to a maximum of 1,950 square feet, which provides a 100 square foot buffer for flexibility as the conceptual sign program is refined during Project review and processing.

This project requests a modification of the requirement in Zoning Code section 19.28.040(d)(1) that 25% of the gross site area be devoted to landscaped and usable open space areas; instead landscaping shall be substantially consistent with the project's proposed landscaping plans pursuant to Zoning Code section 19.28.040(d)(3).

OTHER AGENCY APPROVALS

Because proposed construction activities would disturb more than one acre, the project applicant must obtain a Construction Activity Storm Water Permit or water quality certification from the Regional Water Quality Control Board (RWQCB), Central Valley Region. If the wetlands previously delineated on the project site are determined to be jurisdictional waters of the United States, the project may be required to obtain a Section 404 permit from the U.S. Army Corps of Engineers before filling these wetland areas. The City of Chico will coordinate with Caltrans regarding the design and construction of any improvements to SR 99 associated with the approval of the Walmart Expansion project and projected future traffic in the area. Development and operation of the gas station may require a permit to operate from the Butte County Air Quality Management District.

1.4 PROJECT OBJECTIVES

The Project has the following objectives:

- Design a project consistent with the City of Chico Zoning Ordinance and develop the site consistent with general retail development allowed under the Regional Commercial zoning district.
- Develop a store that satisfies General Plan goals and policies for commercial and retail development and that is consistent with the site's Regional Commercial General Plan land use designation.
- Develop a store that enhances the economic vitality of the City of Chico and contributes to the community.

- Develop a large, state of the art retail store in close proximity to SR 99 that will accommodate the retail demands of the Chico community and surrounding areas and to expand/replace the existing small, outdated retail store.
- Develop an architectural design that softens the scale and mass of the building and provides access that minimizes potential automobile and pedestrian conflicts, and a pedestrian scale in and around the entry to the store.
- Design a site plan to minimize overall access and circulation conflicts, such as facilitation of the circulation between the store, service station and future development on the adjacent parcel.
- Provide landscaping to soften the design and create a pleasant, attractive appearance that unifies the site and complements the surrounding area.
- Design storage areas to blend in with the building itself and confine the areas to a defined space.
- Design seasonal sales areas to complement the building design and minimize pedestrian and automobile conflicts.
- Implement planned improvements and mitigation measures that minimize traffic impacts in the near term and the future.
- Phase the demolition of portions of the existing store and construction of the store to minimize economic impacts to employees and the community and potential hazards to the health and safety of employees and customers.¹
- Provide sufficient off-street parking to meet the City's standards.²
- Increase the site's acreage in order to accommodate the larger 190,275 square foot facility.
- Provide adequate infrastructure to support the project and develop a storm drainage system in compliance with standards set forth by the City of Chico.
- Provide a store that will provide significant benefits to the City and community in terms of employment opportunities, sales tax revenues, shopping opportunities and community programs.

¹ Only portions of the existing store will be demolished in order to provide for expansion, not the entire store.
² The City standard for a "warehouse retail store" is one parking space for each 200 square feet of gross floor area and one space for each company vehicle.

1.5 PROJECT DESCRIPTION

BUILDING EXPANSION AND MODIFICATIONS

The existing Walmart store is 125,889 square feet. The proposed expansion project will add an additional 64,386 square feet to the existing Walmart store, most of which will be added on the southern side of the store with some new construction also taking place on the west and east elevations. The total area of the Walmart store after the expansion would be 190,275 square feet. (See accompanying site plan.) The expansion will add approximately 29,883 square feet of food sales area, 19,082 square feet of food sales support area, and 10,467 square feet of general merchandise sales area. The remaining expansion area will be used for stockroom/receiving area, ancillary area, and the food tenant area³. Table 1 summarizes the existing and proposed floor plan.

**TABLE 1
EXISTING AND PROPOSED FLOOR PLAN**

	Existing	Proposed	Change
General Merchandise Sales Area	91,529 sq. ft.	101,996 sq. ft.	10,467 sq. ft.
Food Sales Area	6,314 sq. ft.	36,197 sq. ft.	29,883 sq. ft.
Food Sales Support Area	450 sq. ft.	19,532 sq. ft.	19,082 sq. ft.
Stockroom / Receiving Area	11,321 sq. ft.	14,300 sq. ft.	2,979 sq. ft.
Ancillary Area	9,600 sq. ft.	11,000 sq. ft.	1,400 sq. ft.
Tire and Lube Express	5,170 sq. ft.	5,170 sq. ft.	0 sq. ft.
Food Tenant Area	1,505 sq. ft.	2,080 sq. ft.	575 sq. ft.
Building Total	125,889 sq. ft.	190,275 sq. ft.	64,386 sq. ft.
Outdoor Garden Center	5,413 sq. ft.	5,413 sq. ft.	0 sq. ft.
Outdoor Display & Sales Area	5,100 sq. ft.	5,100 sq. ft.	0 sq. ft.
Total Area	136,402 sq. ft.	200,788 sq. ft.	64,386 sq. ft.

³ Note: All square footage figures presented in this project description are based on the best current information. Minor variances will likely occur in the final building design, resolution from design issues, architectural review conditions, and other considerations.

For purposes of providing a worst-case analysis, we request that the project EIR evaluate the expansion at 66,500 square feet, which is approximately 3% percent larger than 64,386 square feet. The total store square footage considered in the EIR would be 197,802 square feet.

The proposed addition to the existing building will be similar in design and massing to the existing building. (See accompanying proposed building elevations.) Additions to the west and east side provide articulation to the sides of the building and avoid long, straight walls and seek to minimize the “big box” look. The stated intent of the design is to provide a pleasant and distinctive setting that strengthens the site plan, enhances building elevations, softens parking areas by providing additional landscaping, buffers service areas and maintains continuity with the public right-of-way.

ADDITIONAL USES

The project may add the following uses within the Walmart store:

- Expanded grocery sales;
- Pharmacy/merchandise drive-through (described below);
- Fueling station;
- Other retail and service tenants; and

Pharmacy/Merchandise Drive-Through

The expanded store will provide a drive-through at the south side of the building. The drive-through provides three lanes, where check-in kiosks are located under a canopy. There is adequate space for three vehicles to be located under the canopy and one vehicle to be stacked outside the canopy in each of the three lanes. After checking in, customers proceed to one of 6 designated parking spaces located under a canopy, or one of the additional 4 spaces located in the drive-through area. Located immediately behind each space is a waiting space, not under canopy. Once they have their merchandise, customers exit the drive-through area via one of two “left turn only” drive-ways, then proceed down a drive aisle that connects to the main drive aisle along the front of the store.

PERMANENT OUTDOOR DISPLAY AND SALES AREA

The expanded store will provide permanent outdoor display and sales of merchandise in designated areas on the sidewalk and within the parking lot. Items on display and for sale may include home and garden supplies, seasonal merchandise, and propane tanks. The propane tanks will be securely stored outside the building. The location of the sidewalk display areas will not impede pedestrian traffic. The parking lot sales area will occupy 22 parking stalls within the parking lot in front of the garden center. The overall parking count minus the 22 stalls will be 775 stalls which still exceeds the City’s required parking of 767 stalls.

OPERATIONAL CHARACTERISTICS

Hours of Operation

The existing Walmart store operates 6am to midnight, seven days a week. Walmart anticipates that the expanded store would also be open 6am to midnight, seven days a week.

Site Circulation

The existing five driveways on the streets surrounding the project site include: Business Lane, three driveways on Baney Lane, and one on Forest Avenue. Two additional driveways on Wittmeier Drive would be constructed for the expansion. External and internal vehicle circulation for both customers and delivery trucks is described below.

Customer Access

Primary access to the site would continue to be provided via the two main driveways on Baney Lane. Walmart envisions that these two main driveways on the northern portion of the site will provide the majority of traffic circulation. Customers exiting the site via Baney Lane would be restricted to right turn movements only and would proceed to Forest Avenue. Concrete traffic islands are proposed to be installed to preclude left turns from the site for the purpose of minimizing the amount of traffic proceeding northbound on Business Lane. The existing traffic signal located at the intersection of Forest Avenue and Baney Lane is proposed to be modified as part of the proposed Walmart expansion and a new traffic signal is proposed at the intersection of Wittmeier Drive and Forest Avenue. One secondary access entrance to the project site exists along the central portion of the site and receives right-in/right-out traffic from Forest Avenue. Two new secondary access driveways would be constructed at Wittmeier Drive that would provide ingress/egress for the expanded Walmart store and for future development of parcels 2, 3 and the fuel station.

Internal Circulation

Internal circulation is directed throughout the interior of the project site by directional arrows on the pavement and peninsula and island planters at the terminus of the isles. Stop signs are proposed for critical drive aisle intersection locations.

Customer Drop-off/Merchandise Loading Area

A drop-off area for customers that have difficulty walking to the store from the parking lot is proposed at the front of the store. This area also serves as a package pick-up zone to allow customers who are purchasing large items to load their vehicles.

Delivery Truck Access

The primary truck route for deliveries to the store is from Business Lane. Trucks exiting SR 99 at the East 20th Street off-ramp turn south on Business Lane and proceed to Walmart access at the end of Business Lane behind the Walmart to the truck well bays to unload merchandise. The truck route/fire lane behind the existing store would remain, but it would

be extended southward into the area of proposed expansion. A truck turnaround approximately 130 feet in diameter would be designated at the end of the truck route extension, in the southwestern portion of the project site. Trucks would exit the site via Business Lane, right onto Baney Lane or Wittmeier Drive to return to State Route 99 via Forest Avenue to E. 20th Street or Norte Dame Boulevard or other stops in the area. For delivery of smaller merchandise, an at grade service door has been provided adjacent to the truck bay loading area. Smaller trucks access the site from Business Lane and park adjacent to these loading areas. Products will be carried into the store through the service delivery door.

Bicycle Access

The existing Class I bicycle path (and easement) would be moved further south, and would run along the western boundary of the newly created Parcel 1 and along the southern boundary of parcels 2 and 3 and along the southern and eastern boundaries of the fuel station. The existing Class II bicycle path east of the existing store, fronting Forest Avenue, would remain.

Loading Dock Operations

Truck deliveries would be made to six below-grade loading docks and one at-grade door. Walmart estimates that refrigerated trucks with 53' trailers would make five deliveries per week for meat and produce and five deliveries per week for freezer, dairy and deli items. Meat and produce would be delivered between 6:00 p.m. and 6:00 a.m., and freezer, dairy and deli items would be delivered between 9:00 a.m. and midnight. Walmart estimates that unrefrigerated trucks with 53' trailers would make nine deliveries per week for dry grocery items and ten deliveries per week for general merchandise items. General merchandise will be delivered between 4:00 p.m. and 10:00 p.m. Walmart estimates that unrefrigerated trucks with 53' trailers would deliver large seasonal items a total of six times per year. Walmart estimates that smaller vendor trucks, typically with a 32' trailer, will make deliveries ten to 12 times per week, depending on sales volumes.

Walmart proposes to build a sound wall along the edge of the loading dock to reduce noise from loading operations. The loading docks are all located at the west side of the site behind the store and noise from loading operations will be directed toward State Route 99, which is not considered to be sensitive to noise.

Parking, Landscaping, and Lighting

The existing 630-space parking lot would be expanded to the south of Parcel 1. The addition of 134 parking spaces would bring the total number of parking spaces on the project site to 764. The fuel station proposes an additional 11 parking spaces. The total parking on Parcel 1 is 775 stalls. The parking and service areas would be developed with paving and striping, landscaping, lighting, and signage so that the entire development is coherently integrated. These elements are intended to comply with the parking lot landscaping and lighting requirements set forth in the City of Chico Municipal Code which will require approval from the Architectural Review Board. Automatic lighting features would be provided to incorporate energy conservation technology.

Landscaping will be based on a landscaping plan. Existing site landscaping and automatic irrigation systems are intended to be preserved. Any vegetation damaged during construction is to be replaced with similar species. Planting of screening landscaping comprised of conifer trees and large shrubbery per Caltrans specifications would be continued further south, along the western site boundary adjacent to SR 99. Within the parking area, 15 inch box trees, shrubbery and groundcover of a variety of species is proposed in the parking lot island and peninsula planters. Automatic irrigation would be timed to provide optimal water conservation.

Lighting will be based on a lighting plan. Exterior lighting will be designed to create a nighttime environment that promotes safe movement of vehicular and pedestrian traffic through the parking area and into the store. Lighting is proposed to be energy efficient, LED fixtures. As required by City Ordinance, light fixtures located along property boundaries will include cut off shielding that directs lighting downward in order to prevent both glare impacts to drivers on State Route 99 and light trespass onto neighboring properties.

UTILITY AND ACCESS EASEMENTS

Utilities and access easements will be established in accordance with the tentative parcel map and a utilities plan. Certain utilities and other features on the project site would be installed within the relocated easements. The existing water easement surrounding the existing store would be abandoned. A new 20-foot water easement would be created that would surround the expanded store. The existing sanitary sewer easement located south of the existing store would be abandoned, and a new 20-foot sewer easement would traverse the south-central portion of Parcel 1. The existing storm drain south of the existing store would be abandoned, and a new storm drain would be installed that would be rerouted farther south around the expanded area of the store, then proceed east to Forest Avenue. The PG&E easement would be relocated to the southern boundary of Parcel 1 and then continue along the western and northern boundaries of Parcel 2.

The existing 15-foot easement for the bicycle path would be rerouted further south on Parcel 1 and would traverse the southern boundary of Parcels 2 and 3 north of Wittmeier Drive, exiting onto Forest Avenue.

Potential LID improvements for the new parking lot, new and remodeled service area and new roof area include storm water planters, Filtterra units or equivalent and other LID measures. The existing parking lot that will remain is not proposed to have new LID facilities or water quality adjustments to the existing drainage system. The actual size and location of LID measures may change based on calculations prepared during the improvement plan process.

APPLICABLE DESIGN GUIDELINES

This project addresses a number of site and building guidelines including:

DG 2.1.21 Safe and convenient bicycle and pedestrian connection is provided with the relocation of the off-street bike path around the project and a pedestrian walkway from Forest Avenue to the front entry.

DG 2.1.22 The separated sidewalk is 5 feet in width with an additional 2' decomposed granite separation from the parked cars and a 14' planter. The planter will allow for trees to provide shade over the pedestrian path.

DG 2.1.23 Crosswalks are striped across drive aisle to clearly delineate the pedestrian and bicycle crossings.

DG 2.1.24 Bike racks and lockers are located near the main entry.

DG 2.1.25 Parking areas are screened by landscaping the is adjacent to the street.

DG 2.1.27 The parking proposed just meets the minimum required by City Code.

DG 2.1.28 Trees are planted in the parking lot to meet City Code Standards.

DG 2.1.32 Bike racks and lockers are provided close to main entryways.

DG 2.1.33 Clearly marked crosswalks and a separated pedestrian walkway are provided on site. An offsite bike path traverses through the site as well.

DG 2.1.34 A separate sidewalk in the parking lot provides safe separated pedestrian access from motorists.

DG 2.1.36 The compactor, docks and storage areas are screened by walls from the public.

DG 1.8.11 A non-prototypical design is presented to provide a unique design for the City of Chico.

DG 1.1.34 Added benches for seating areas near store entrances for a more friendly human scale approach to the community.

DG 1.2.21 Enhanced the Architecture of the existing Building with the use of rich stone material.

DG 1.4.11 Use of tall pilasters and raised parapets to emphasize store entrances.

DG 2.2.25 Vertical variations and decorative cornices are integrated in the new construction of the expansion.

DG 2.2.26 Variation in parapet heights and treatment is used on all sides of the building for better integration of the existing building design with the new construction.

DG 2.2.31 Changes in Building materials and surface depths provide for a more visually interesting building.

DG 2.2.32 The use of a variety of Architectural elements and Building materials with an earth tone color scheme enhances the existing Building design and blends the new construction with the existing building.

ENERGY EFFICIENCY MEASURES/SUSTAINABILITY

The proposed project will incorporate sustainability features in building and site design with the goal of reaching a building efficiency rating that is greater than the Title 24 requirement. To achieve this reduction in energy consumption, the project may incorporate the following sustainability features or other features that provide equal or greater energy efficiency:

Lighting

1. Lighting occupancy sensors in non-sales areas, including restrooms, break rooms and offices.
2. Interior LED lighting technology for restrooms, freezer and cooler boxes, food prep areas and many refrigerated food cases. LED technology is a solid-state lighting technology which saves energy, reduces maintenance and provides superior optics, improved illumination and better lighting distribution while complying with IESNA recommended minimum lighting levels. LED technology contains no mercury or lead and produces light by passing electricity through a chemical compound which creates light without the need to heat filaments as in other light sources. LED lights are projected to last at least six years beyond conventional lighting, reducing maintenance cost. In refrigerated food cases, LED performs well in the cold and produces less heat than fluorescent bulbs—heat which must be compensated for by the refrigeration equipment.
3. Exterior LED lighting technology for exterior building signage and exterior building security lighting.
4. LED site lighting for the parking lot. LED fixtures provide noticeable reduction in both on and off site glare, improvements in trespass control (off-site spill light), light uniformity and vertical illuminance. In addition to these benefits, because the LED systems can use less light and aim it more efficiently, energy consumption is projected to be reduced by a minimum of 50% when compared to the metal halide conventional light fixtures.
5. All sales area lighting in the store, including the expansion area, will utilize T-8 Fluorescent lamps and electronic ballasts.

Energy Consumption

6. Centralized Energy Management System (EMS) to monitor and control the heating, air conditioning, refrigeration and lighting systems from corporate headquarters. The EMS enables Walmart to constantly monitor and control the expanded store's energy usage, analyze refrigeration temperatures, observe HVAC and lighting performance, and adjust system levels from a central location 24-hours per day, seven days per week. Energy usage for the entire store will be monitored and controlled in this manner.
7. High efficiency heating, ventilating and air-conditioning (HVAC) units.

8. Actively dehumidify facilities to increase efficiency of refrigeration system and reduce condensation on refrigerated glass doors.
9. White membrane roof for the building expansion to reduce building energy consumption and provide a lower heat island effect than a darker roofing color. Recognizing environmental concerns with the manufacture and disposal of PVC, Walmart has eliminated all PVC roofing and now uses a TPO-type membrane, which is more environmentally friendly.
10. Use non-ozone-depleting refrigerant in expansion facilities.
11. Reclaim waste heat from new onsite refrigeration equipment (expansion area) to supply approximately 70% of the hot water needed for the store.

Water Conservation

12. Install high-efficiency urinals that use 1/8 gallon (one pint) of water per flush, compared to the conventional one gallon per flush urinal.
13. Use sensor-activated 1/2 gallon per minute high-efficiency faucets in new restroom sinks.
14. Use high efficiency, lower water use toilets in all new restrooms.
15. Replace the traditional overhead irrigation system in the Garden Center with an automated irrigation controlled distribution system. This system uses time-controlled irrigation mats that are placed under the plants.
16. The landscape and irrigation design for new planting areas will follow the design guidelines outlined in California AB 1881, to provide a low water use landscape for the proposed site. The most up to date irrigation technologies available will be utilized to provide a water-efficient irrigation system in new planting areas. Drought-tolerant native plant species will be used in new planting areas to aid in achieving efficient water use goals.

Materials

17. Replace 15-20 percent of existing cement with fly ash, or a 25-30 percent replacement with slag, in concrete mixes, to offset greenhouse gases emitted in the cement manufacturing process.
18. Use Non-Reinforced Thermoplastic Panel (NRP) in lieu of Fiber Reinforced Plastic (FRP) sheets in areas where plastic sheeting is appropriate, including food preparation areas, utility and janitorial areas, and associate break rooms.
19. Use a plant based oil extracted from a renewable resource as a concrete form release agent (a product sprayed on concrete forms to allow ease of removal after the concrete has set) in the expansion area.

20. Use better performing standard paint products with lower VOC content limits to reduce the volatile organic compounds (VOCs) of exterior and interior field paint coatings.
21. Use 55 gallon drums and 275 gallon totes for paint to reduce the number of one gallon and five gallon buckets needed.
22. Incorporate a Construction and Demolition (C&D) program to capture and recycle as much of the metal, wood, floor and ceiling tile, concrete, asphalt and other materials generated as part of Walmart's demolition and construction process as possible.
23. Construction of the expansion area will use steel containing approximately 85-90 percent recycled structural steel.
24. All of the plastic baseboards and much of the plastic shelving included in the expansion area will be composed of recycled plastic.
25. The project will use a film on all freezer doors that combats condensation and requires no additional energy consumption.

GENERAL PLAN CONSISTENCY ANALYSIS
Chico Walmart Expansion Project, 2044 Forest Avenue
(PM 15-02, AR 15-16, UP 15-10 and UP 15-12)

The 2030 Chico General Plan provides the following guidance for using the document:

When making decisions, goals and policies should be examined comprehensively, not individually. It is not the intent of the General Plan to predetermine decisions, but rather to help guide the decision-making process.

Hence the project need not be consistent with every General Plan policy to warrant a finding of consistency with the overall document, and there is no individual policy in the General Plan that, by itself, prohibits approval of a given project. The following analysis is intended to highlight all applicable policies that apply to the project, and assess project consistency with the General Plan document as a whole.

LAND USE AND OPPORTUNITY SITE DESIGNATIONS

The project site is designated “Regional Commercial” on the General Plan Land Use Diagram. The General Plan describes the Regional Commercial land use designation, in part, as follows:

This designation provides sites for larger retail and service businesses that serve residents from the City and the region.

The project site is also located within Regional Center Opportunity Site #10 (E. 20th Street). The E. 20th Street Regional Opportunity Site is defined in the General Plan as follows:

This Regional Center contains the Chico Mall and a number of national chain retail stores. It also contains an approximately 8-acre vacant property on the west side of Forest Avenue and a large vacant lot between the Kohl's and the Chico Mall on Springfield. The retail centers in this area are all single story with large surface parking areas, providing the potential to intensify the commercial uses. Its convenient location near an exit from State Route 99 increases its revitalization potential as a thriving regional commercial center.

GENERAL PLAN GOALS, POLICIES AND ACTIONS

Applicable General Plan goals, policies and actions include:

Policy LU-1.3 (Growth Plan) - Maintain balanced growth by encouraging infill development where City services are in place and allowing expansion into Special Planning Areas.

Goal LU-4: Promote compatible infill development.

Policy LU-4.2 (Infill Compatibility) - Support infill development, redevelopment, and rehabilitation projects that are compatible with surrounding properties and neighborhoods.

Policy LU-4.4 (Positive Contributions) – Encourage infill development that provides missing neighborhood elements, such as neighborhood retail, enhanced architectural quality, and circulation improvements for pedestrians, bicycles and vehicles, or that otherwise contributes positively to existing neighborhoods.

Goal LU-5: Support development and redevelopment of the designated Opportunity Sites.

Policy LU-5.1 (Opportunity Sites) - Facilitate increased density and intensity of development and revitalization in the following Opportunity Sites [including the East 20th Street Regional Center Opportunity Site].

Policy CIRC-1.2 (Project-level Circulation Improvements) – Require new development to finance and construct internal and adjacent roadway circulation improvements as necessary to mitigate project impacts, including roadway, transit, pedestrian, and bicycle facilities.

Policy CIRC-1.3 (Citywide Circulation Improvements) – Collect the fair share cost of circulation improvements necessary to address cumulative transportation impacts, including those to state highways, local roadways, and transit, pedestrian and bicycle facilities, through the City's development impact fee program.

Policy CIRC-1.4 (Level of Service Standards) – Until a Multimodal Level of Service (MMLoS) methodology is adopted by the City, maintain LOS D or better for roadways and intersections at the peak PM period, except as specified below:

- LOS E is acceptable for City streets and intersections under the following circumstances:
 - Downtown streets within the boundaries identified in Figure DT-1.
 - Arterials served by scheduled transit.
 - Arterials not served by scheduled transit, if bicycle and pedestrian facilities are provided within or adjacent to the roadway.
- Utilize Caltrans LOS standards for Caltrans' facilities.
- There are no LOS standards for private roads.

Exceptions to the LOS standards above may be considered by the City Council where reducing the level of service would result in a clear public benefit. Such circumstances include, but are not limited to, the following:

- If improvements necessary to achieve the LOS standard results in impacts to a unique historical resource, a highly sensitive environmental area, requires infeasible right-of-way acquisition, or some other unusual physical constraint exists.
- If the intersection is located within a corridor that utilizes coordinated signal timing, in which case, the operation of the corridor as a whole should be considered.

Action CIRC-2.2.3 (Traffic-Calming Measures) – Install appropriate traffic calming devices, such as bulbing and reduced street widths, to discourage speeding and “cut-through” traffic on existing local streets.

Action CIRC-3.1.1 (Add Bicycle Facilities) – Incorporate bicycle facilities identified in the Chico Urban Area Bicycle Plan into public road construction projects and private development projects.

Policy CIRC-3.6 (Bicycle Parking) – Provide safe and secure bicycle parking and support facilities.

Policy CIRC-5.3 (Transit Connectivity in Projects) – Ensure that new development supports public transit.

Policy CIRC-8.2 (Parking Improvements) – Ensure that new parking facilities and renovations are designed to be safe, efficient, and pedestrian-friendly.

Policy CIRC-9.1 (Reduce Peak-Hour Trips) – Strive to reduce single occupant vehicle trips through the use of travel demand management strategies.

Action CIRC-9.1.2 (Existing Employer Trip Reduction Programs) – Encourage employers to provide transit subsidies, bicycle facilities, alternative work schedules, ridesharing, telecommuting and work-at-home programs, and preferential parking for carpools/vanpools.

Action CIRC-9.1.3 (New Employer Trip Reduction Programs) – As a condition of project approval, require new non-residential projects that will employ more than 100 people to submit a Travel Demand Management Plan that identifies strategies, such as those listed in Action CIRC-9.1.2, to reduce single occupancy vehicle trips.

Policy CIRC-9.3 (Emphasize Trip Reduction) – Emphasize automotive trip reduction in the design, review, and approval of public and private development.

Policy SUS-5.2 (Energy Efficient Design) – Support the inclusion of energy efficient design and renewable energy technologies in public and private projects.

Policy CD-3.2 (Bicycles and Pedestrians) – Maintain and enhance the pedestrian- and bicycle-friendly environment of Chico.

Goal CD-5: Support infill and redevelopment compatible with the surrounding neighborhood.

Policy CD-5.3 (Context Sensitive Design) – For infill development, incorporate context sensitive design elements that maintain compatibility and raise the quality of the area’s architectural character.

Goal ED-1: Maintain and implement an Economic Development Strategy to enhance Chico’s long-term prosperity.

Policy ED-1.3 (Regulatory Environment) – Ensure that regulations and permitting processes for the conduct of commerce and land development do not unreasonably inhibit local business activity.

Policy OS-1.2 (Regulatory Compliance) – Protect special-status plant and animal species, including their habitats, in compliance with all applicable state, federal and other laws and regulations.

Policy OS-1.3 (Light Pollution) – Reduce excessive nighttime light and glare.

Action OS-6.1.1 (Urban Forest Maintenance) – Maintain and expand the urban forest by: Requiring street and parking lot tree planting in new development. Working with commercial parking lot owners to improve the shade canopy.

PROJECT CONSISTENCY ANALYSIS

The project would result in compatible infill development/redevelopment within a Regional Commercial Opportunity Site, consistent with goals LU-4, LU-5, CD-5, and policies LU-1.3, LU-4.2 and LU-5.1, in that the proposed layout would not cause significant conflicts with existing or anticipated Regional Commercial uses in the area. The proposed parcels are of sufficient size to accommodate development in compliance with the City's regulations and all necessary utilities and services are available to serve the parcels, lending further support for a finding of infill compatibility for the project.

As explained in the project EIR, approving the project may result in an unacceptable Level of Service (LOS) for a southbound segment of SR-99 between SR-32 and East 20th Street under the 2035 cumulative plus project modeling scenario, during the Saturday P.M. peak hour. Because an acceptable level of service may not be maintained for this facility, there is a potential for inconsistency with Policy CIRC-1.4.

However, the proposed project design, as modified by mitigation measures identified in the EIR, would achieve consistency with the remaining policies in the Circulation Element. The proposal is consistent with policies CIRC-1.2, CIRC-2.2.3 and CIRC-3.6, in that it would construct necessary internal and adjacent roadway circulation improvements, use traffic calming measures to reduce cut-through traffic, and include safe and secure bicycle parking facilities. Mitigation measures and existing City processes would ensure project consistency with CIRC-1.3, CIRC-3.1.1, CIRC 5.3, CIRC-9.1, CIRC-9.1.2, CIRC-9.1.3 and CIRC-9.3, by requiring payment of fair share costs for citywide street facility improvements (MM TRANS-3b), incorporating a planned bikeway connection through the site (MM TRANS-7), supporting public transit (MM AIR-2h), and reducing vehicle trips through the use of travel demand management strategies (MM AIR-2f), respectively.

Consistent with policy OS-1.2, mitigation measures ensure that project-related impacts to the onsite wetland are avoided, or mitigated by requiring the applicant to obtain all necessary state and federal permits for future development that may affect special status species or their habitat.

The energy efficiency measures outlined in the applicant's project description for the store expansion are consistent with policy SUS-5.2, which calls for supporting the inclusion of energy efficient design in private projects.

The site design and architectural aspects of the project are generally consistent with policies CD-3.2, CD-5.3, and LU-4.4, in that the existing public bike path would be replaced in a manner that would facilitate future connectivity to other public bikeways, the store expansion would include higher quality materials (such as multiple storefront windows and decorative stone columns), and a better-articulated façade and roofline relative to the existing building.

Approval of the project is consistent with General Plan goals and policies to enhance Chico's long-term prosperity (ED-1), and ensure that regulations and permitting processes for the conduct of commerce do not unreasonably inhibit local business activity (ED-1.3).

Conditions are recommended to ensure project consistency with policy OS-1.3, regarding excessive nighttime lighting, and OS-6.1.1, regarding the provision of adequate amounts of parking lot shading. Based on all the above and subject to conditions as noted, staff believes the project is consistent with the City's General Plan.

DESIGN GUIDELINES CONSISTENCY ANALYSIS
Chico Walmart Expansion Project, 2044 Forest Avenue
(PM 15-02, AR 15-16, UP 15-10 and UP 15-12)

DG 1.1.11 – Incorporate recognizable design elements that relate to the immediate neighborhood or community, including uninterrupted vistas of the natural environment.

DG 1.1.34 - Utilize elements that reinforce a sense of place, referencing architectural or cultural ties to the surrounding neighborhood and then to the greater Chico community.

DG 1.2.12 - Respond to the context and potential linkages to surrounding areas.

DG 1.2.13 - Create a scale and character of development that does not overwhelm the surrounding neighborhood.

DG 1.2.21 - Relate the design of building facades and roofs to the immediate neighborhood or greater Chico community through the use of architectural or cultural motifs, historical references, or references to the natural environment.

DG 1.2.22 - Utilize rooflines and exposed (pitched) roofs to add character and style to a building, reinforcing its sense of place.

DG 1.2.32 - Use building materials that reinforce a sense of permanence, history or place.

DG 1.5.13 - Integrate a finished height of parking lot light fixtures that is below the expected canopy of shade trees. Pole-mounted lights in excess of 18 feet in height is discouraged.

DG 1.5.16 - Use pedestrian scale lighting along pedestrian walkways to avoid glare.

DG 1.8.11 - Request optional corporate design solutions for review rather than struggling to approve the first design proposal that is less than acceptable.

DG 1.8.12 - Explore accents to specific building elements (such as color or logo motifs) as acceptable design compromises that maintain corporate identity while providing a design that is also compatible with the community.

DG 2.1.11 - Strengthen neighborhood identity by incorporating design elements that reflect the surrounding neighborhood or environment.

DG 2.1.22 - Provide highly identifiable and amply wide (such as a minimum of six-feet) sidewalks for safe and convenient customer access to primary building entrances and through parking lot areas.

DG 2.1.23 - Provide safe pedestrian and bicycle crossings across parking lot driveways. To delineate pathways, visible changes in texture and or color are recommended.

DG 2.1.24 - Incorporate design features that include pedestrian, bicycle, and public transportation amenities.

DG 2.1.25 - Screen parking areas from street views and avoid elevating parking area grade above street grade.

DG 2.1.26 - Minimize the visual impact of vehicles by locating parking areas to the rear or sides of developments rather than along street frontages.

DG 2.1.27 - Minimize views of parking areas to allow the architectural significance of the buildings and landscaping to take precedence. Parking that exceeds the minimum required by City code is discouraged.

DG 2.1.28 - Provide shade trees per City code standards.

DG 2.1.31 - Provide covered bicycle parking with compatible architectural design in all commercial developments.

DG 2.1.32 - Locate bicycle parking close to main entrances.

DG 2.1.33 - Clearly designate bicycle and pedestrian routes through parking lots by signage, special surface treatment, overhead features, shade, or landscaping.

DG 2.1.34 - Safely separate bicycle and pedestrian routes from motorist routes in parking lots, or otherwise clearly delineate by elevation changes, special surfacing, overhead features, treatments, or signage.

DG 2.1.36 - Maximize screening and buffering of trash enclosures, storage areas, expansive paving, service yards, and utility equipment from public view. Screening involves blocking views of the object with a structure, while buffering involves softening the visual impact of the unsightly object with landscaping or other aesthetic technique.

DG 2.2.11 - Use appropriate massing, fenestration, articulation, materials, and buffering to provide a pedestrian-level scale.

DG 2.2.12 - Create harmonious spatial relationships with buildings. Avoid repeating buildings with the same form and massing which can create an uninteresting and monotonous atmosphere. For projects containing a variety of building sizes, locate lower buildings closer to the street and taller buildings to the rear or interior of the site.

DG 2.2.13 - Minimize trademark building design elements that create "sameness". Incorporate design elements that create unique identity and are complementary to surrounding architectural styles.

DG 2.2.22 - Incorporate varied building depth and shadow in order to avoid long, unarticulated elevations.

DG 2.2.23 - Create a sense of focus so people may easily find the entrance and incorporate as a dominant design element to create a sense of place. Roof overhangs, awnings, and wall recesses are examples of features that help define a sense of entry for a building.

DG 2.2.24 - Incorporate roof design as an integral component of the architecture to enhance the overall aesthetics. When roof elements are exposed, treat as integral to all elevations.

DG 2.2.25 - Avoid continuous flat roofs with monotonous cornices or parapets.

DG 2.2.26 - Avoid mansard roofs or parapets which appear as “stuck on” or are not integrated onto the roof with equal design treatment on all sides. Treat parapet walls as an integral part of the building design on all elevations.

DG 2.2.28 - Minimize wall mounted utility equipment from view, including electrical panels, gas meters, conduit, plumbing, and downspouts, and either integrate within the building structure or paint to match the facade.

DG 2.2.31 - Include variations in the depth of surfaces or changes in surface materials to add visual interest to walls.

DG 2.2.32 - Choose building colors and accent materials from a rich palette that enhances the streetscape, rather than simply blends with surrounding architecture, in order to avoid bland colors and frivolous ornamentation.

DG 2.2.33 - Carry the architectural scheme (form, materials, color and detailing) of each building throughout all elevations to achieve design continuity.

PROJECT CONSISTENCY ANALYSIS

The project is generally consistent with Design Guidelines that call for commercial buildings to: incorporate varied building depths, materials and rooflines (DGs 2.2.22, 2.2.25, 2.2.31), and create a sense of focus toward main entrances through distinguished tower elements, awnings and storefront windows (DG 2.2.23). Additional DGs encourage integral parapet roof designs that avoid a "stuck on" appearance (DGs 2.2.24, 2.2.26). To ensure consistency with all six of these DGs, a condition is recommended to require tower elements, including cornice, to extend back onto the main roof a minimum of six feet to ensure that they appear sufficiently integrated with the structure.

The store expansion is appropriately scaled for this Regional Commercial shopping area comprising the Chico Mall and other large format retailers (DG 1.2.13). Bicycle and pedestrian paths are well marked and in many cases lined with landscaping, which enhances safety (DGs 2.1.23, 2.1.33, 2.1.34). Design Guideline consistency is further enhanced by screening HVAC units with roof parapets, parking lot and drive-through areas with landscaping, and screened wall-mounted utilities, as called-for by DGs 2.1.25, 2.1.36 and 2.2.28.

Store elevations reflect superior design solutions over the initial project submittal, and eschew large, bright-blue trademark elements as can be seen in other communities (DGs 1.8.11, 1.8.12, 2.2.13). The proposed fuel station convenience store and canopy, along with existing monument sign would provide a reasonable amount of corporate branding near the street frontage. The side elevations of the convenience store lack much architectural interest, however, side views of the convenience store would diminish over time as landscaping matures. The ample landscaping proposed around the fuel station would set it well apart from most existing fuel stations in the City.

The project relates to the greater area and conveys permanence through the use of stone and split-faced CMU, and colors are chosen from a rich palette with white accents that are well placed to complement future signage (DGs 1.2.21, 1.2.32, 2.2.32). The proposed painted and raised wall patterns would promote a pedestrian scale for the main building by breaking up the elevations into single-story sized elements, reducing the overall perceived height (DG 2.2.11).

As an expansion project, the site design is somewhat limited based on existing conditions. The parking lot is located in front of the building, separated from Forest Avenue by a wide landscape buffer, and lower buildings would be located closer to the street (DGs 2.2.12). However, the proposal makes use of opportunities to improve site access with driveway improvements on Baney Lane and Wittmeier Drive.

Conditions are recommended to ensure project consistency with DG 1.5.13 and 1.5.16, regarding excessive nighttime lighting, and DG 2.1.28, regarding the provision of adequate amounts of parking lot shading.

Based on all the above and subject to conditions as noted, staff believes the project is consistent with the City's adopted Design Guidelines.

Chico, CA Code of Ordinances

19.76.070 Drive-in and drive-through facilities.

Any retail trade or service use providing drive-in or drive-through facilities shall be designed and operated to effectively mitigate problems of air pollution, congestion, excessive pavement, litter, noise, and appearance in the following manner:

- A. Pedestrian walkways should not intersect the drive-through drive aisles, but where they do, they shall have clear visibility, and be emphasized by enhanced paving or markings.
- B. Drive-through aisles shall have a minimum 15-foot interior radius at curves and a minimum 12-foot width. Each drive-through entrance and exit shall be at least 100 feet from an intersection of public rights-of-way, measured at the closest intersecting curbs, and at least 25 feet from the curb cut on the adjacent property. Also, each entrance to an aisle and the direction of flow shall be clearly designated by signs and/or pavement markings or raised curbs outside of the public right-of-way.
- C. Each drive-through aisle shall provide sufficient stacking area at a minimum of 20 feet per vehicle in advance of the service window or automated teller machine (ATM), to accommodate a minimum of four vehicles for pharmacies, banks, and financial services or six vehicles for all other drive-through uses.. In lieu of this standard, an interior traffic study which models vehicular queuing may be prepared for City staff review. The stacking area shall not interfere with other on-site circulation and parking facilities.
- D. The provision of drive-through service facilities shall not justify a reduction in the number of required off-street parking spaces.
- E. All service areas, trash storage areas, and ground-mounted and roof-mounted mechanical equipment shall be screened from ground-level view from adjacent properties or public rights-of-way.
- F. Menu boards shall not exceed 24 square feet in area, with a maximum height of 6 feet, and shall face away from public rights-of-way. Outdoor speakers shall be located at least 50 feet from any residentially zoned parcel. Noise levels measured at the property line of a drive-in or drive-through facility shall not increase the existing ambient noise levels in the surrounding area.
- G. Each drive-through aisle shall be include a combination of landscaping, low walls, and/or berms to prevent headlight glare and to reduce visibility of vehicles from impacting adjacent streets and parking lots.
- H. An 8-foot-high solid decorative wall shall be constructed on each property line that is adjoining a residentially zoned parcel. The design of the wall and the proposed construction materials shall be subject to architectural review.

(Ord. 2185, Ord. 2440 §51)

19.76.120 Outdoor retail sales and activities.

Outdoor sales and equipment rental establishments, where the business is not conducted entirely within a structure or enclosed area, shall comply with the following standards:

A. Temporary Outdoor Retail Sales and Activities. The temporary outdoor display of merchandise shall comply with the following standards:

1. Temporary Uses. Temporary outdoor sales are subject to Chapter 19.22 (Temporary Uses).

2. Duration of Sales. The sales shall be of a temporary nature, lasting not longer than a total of six months.

3. Maximum Sales Area. The area devoted to temporary outdoor sales shall not exceed the following:

a. CC and CN and ML districts: 2 1/2 percent of the gross floor area of the structure occupied by the business; and

b. CS and CR districts: 5 percent of the gross floor area of the structure occupied by the business.

B. Permanent Outdoor Retail Sales and Activities. The permanent outdoor display of merchandise shall comply with the following standards:

1. Location of Sales Area. The shall occupy a fixed and approved location that does not disrupt the normal function and safety of the site or its circulation and does not encroach upon required parking spaces, driveways, pedestrian walkways, or required landscaped areas. The outdoor sales shall be located entirely on private property and outside of any required setback. A minimum setback of 10 feet from any public right-of-way is required.

2. Adjacent to a Public Right-of-Way. Outdoor sales areas adjacent to public rights-of-way shall be screened with decorative solid walls, fences, or landscaped berms, a minimum of 36 inches high and merchandise displays shall not exceed a height of three feet above finished grade.

3. Outdoor Vending Machines. Outdoor vending machines, as defined in Chapter 19.04 (Definitions), are subject to the performance standards below.

a. Accessory Use. Outdoor vending machines shall be an accessory use to an approved primary use and may not be located on an unimproved lot.

b. Location.

(1) Building Frontage. Outdoor vending machines are permitted along the building frontage that includes the primary business entrance. Where the primary business entrance is located at the building corner, outdoor vending machines will be permitted on only one side of the building.

(2) Clear Path of Travel. Outdoor vending machines shall not obstruct pedestrian pathways, driving aisles, parking spaces, or any areas necessary for proper pedestrian or vehicular circulation or loading activities. A clear path of travel at least four feet wide must be provided around outdoor vending machines.

(3) Public Rights-of-Way. Outdoor vending machines shall not be installed in, or within four feet of, the public right-of-way, or located in such a manner as to encourage or require customers to stand or park in the right-of-way in order to use the machine.

(4) Architectural Review. The placement of outdoor vending machines shall be considered as part of the architectural review process for new development. Outdoor vending machines installed subsequent to an approved project shall be considered a modification to an approved project and subject to section 19.18.070 (Conformance to Plans).

c. Area. The area occupied by outdoor vending machines may not exceed 10 percent of the width of the building frontage along which they are located, nor shall the combined area of all vending machines at any one site exceed 32 square feet.

d. Signage and Window Coverage. No more than 25 percent of a window area may be covered with signage or outdoor vending machines combined. Sign copy on outdoor vending machines shall be limited to the exterior panels of the machine and shall only advertise the product or service provided by the machine.

e. Design and maintenance.

(1) Utility Connections. Any required exposed conduits, pipes, or utility connections shall be secured to the building and painted, or otherwise screened, to match the building exterior, and shall not bridge a span greater than 18 inches.

(2) Maintenance. All outdoor vending machines shall be maintained in a clean, working and attractive condition. If the outdoor vending machine is removed, the area shall be cleaned and restored, including the removal of any conduits or other connection hardware.

C. General Requirements. The following requirements shall apply to all (temporary and permanent) outdoor retail sales and activities:

1. Signs. There shall be no signs visible from the public street in addition to those allowed by Chapter 19.74 (Signs).

2. Height of Displayed Materials. The outdoor display of merchandise shall not exceed a height of 7 feet above finished grade for a single display item. Stacked displays may not exceed a height of 6 feet above finished grade.

3. Relationship to Main Use. Outdoor sales shall be directly related to a business establishment on the parcel. The use of the property shall comply with the standards for the zoning district.

4. Review and Approval Required. Any use proposing permanent outdoor merchandise display or other outdoor business activities shall be subject to review and approval by the Zoning Administrator, in compliance with Chapter 19.24 (Use Permits).

5. Outdoor Storage Areas. Outdoor storage areas are subject to the regulations of Section 19.60.060 (H)(3) (Outdoor Storage and Work Yards). Temporary outdoor storage areas are not subject to screening requirements.

D. Exceptions. The provisions of this section do not apply to the following:

1. Sales or distribution of newspapers or periodicals in compliance with the provisions of the Municipal Code.

2. Sales from the public right-of-way in compliance with the provisions of the Municipal Code.

3. Temporary sales not within a structure or enclosed area, in compliance with Chapter 19.22 (Temporary Uses).

4. Sales of seasonal agricultural food products and flowers on private, non-residential property, not to exceed 6 months annually.

(Ord. 2185, Ord. 2427 §53, Ord. 2440 §54)

Chico, CA Code of Ordinances

19.76.090 Gas stations.

Where allowed by Division IV (Zoning Districts, Allowable Land Uses, and Zone-Specific Standards), gas stations require use permit approval in compliance with Chapter 19.24, and shall be constructed and operated in the following manner:

A. New Gas Stations. New gas stations shall comply with the following standards, in addition to the standards contained in Subsection B (Modification or Expansion of an Existing Gas Station), below:

1. A maximum of two gas stations shall be allowed at each intersection.
2. The minimum site area shall be 15,000 square feet.
3. The minimum frontage shall be 100 feet on each street.
4. Pump islands shall be located a minimum of 15 feet from any property line to the nearest edge of the pump island; however, a canopy or roof structure over a pump island may encroach up to 10 feet within this distance. When the property line is a street right-of-way line, at least 3 feet in width along the line shall be landscaped. Additionally, the cashier location shall provide direct visual access to the pump islands and the vehicles parked adjacent to the islands.
5. There shall be no more than two vehicular access points to or from adjacent public rights-of-way.
6. There shall be a minimum distance of 30 feet between curb cuts along a street frontage.
7. Landscaping shall be provided and permanently maintained in compliance with the following regulations, as well as those outlined in Chapter 19.68 (Landscaping Standards):
 - a. Landscaping shall cover a minimum of 10 percent of the gas station site area, exclusive of required setbacks.
 - b. A minimum 5-foot-wide, inside dimension, and 6-inch-high curbed landscaped planter area shall be provided along the front property lines, except for openings to facilitate vehicular circulation to adjacent properties, and along side and rear property lines adjoining residentially zoned properties. Where adjoining a periphery wall, trees planted not more than 16 feet apart shall be included in the planter areas which shall be a minimum of 6 feet wide, inside dimension.
 - c. An on-site planter area of not less than 200 square feet shall be provided at the corner of two intersecting streets. Landscaping shall not exceed a height of 30 inches at this location.
 - d. Additional landscaping may be required by the Zoning Administrator or if applicable the Board, to screen the gas station from adjacent properties.
8. All exterior light sources, including perimeter, and flood, shall be energy-efficient, stationary, and shielded to ensure that all light is directed away from adjacent properties and public rights-of-way. All canopy lights shall be fully recessed. Lighting shall not be of a high intensity so as to cause a traffic hazard, be used as an advertising element, or adversely affect adjacent properties, in compliance with Section 19.60.050 (Exterior lighting).

9. Openings of service bays shall be designed to minimize the visual intrusion onto adjacent properties.

10. A gas station which adjoins property in a residential zoning district shall provide a 6-foot-high decorative masonry wall along the common property line, compatible with on-site development and adjacent properties, subject to architectural review.

B. Modification or Expansion of an Existing Gas Station. Any modification or expansion of an existing gas station shall comply with all of the following standards:

1. All activities and operations shall be conducted entirely within an enclosed structure, except as follows:

- a. The dispensing of petroleum products, water, and air from pump islands;
- b. The provision of emergency service of a minor nature; and
- c. The sale of items via vending machines which is subject to Section 19.76.120.

2. No vehicle may be parked on sidewalks, parkways, driveways, or alleys;

3. No vehicle may be parked on the premises for the purpose of vehicular sales;

4. All on-site signs shall be in compliance with Chapter 19.74 (Signs);

5. No used or discarded vehicle parts or equipment, or disabled, junked, or wrecked vehicles shall be located in any open area outside of the main structure;

6. Noise from bells, loudspeakers, or tools shall be in compliance with Section 19.60.080 (Noise) and shall not be audible from residentially zoned parcels between the hours of 7:00 p.m. and 7:00 a.m. on weekdays and Saturdays, and before 10: 00 a.m. and after 7:00 p.m. on Sundays and nationally recognized holidays;

7. Gas stations may receive used motor oil for subsequent recycling and removal, subject to approval by the City Fire Department; and

8. Where an existing gas station adjoins property in a residential zoning district, a 6-foot-high decorative masonry wall may be required along the common property line as a condition of use permit approval for any on-site improvement, expansion or modification, if such a condition is necessary for land use compatibility.

(Ord. 2185; Ord. 2223, Ord. 2440 §52)

Mike Sawley

From: Debbie Presson
Sent: Wednesday, August 17, 2016 2:42 PM
To: All City Council; Mark Orme; Chris Constantin; Mark Wolfe; Mike Sawley
Subject: FW: Chico [#3901]

For your information.

From: Wufoo [mailto:no-reply@wufoo.com]
Sent: Wednesday, August 17, 2016 11:31 AM
To: Debbie Presson
Subject: Chico [#3901]

Name * becky short

Email * b.short@enloe.org

Phone Number (530) 896-0961

Department: City Clerk

Comments

i would like this to go to city council-i am hoping a no vote on walmart happens-you do not tax food walmart employees are pretty much minimal wage & medical for insurance. food 4 less/costco have liveable wages and benefits. they are already having a tough time getting teachers & doctors to our area-spouses work & no good paying jobs for them to come to & no airport to fly out for vacation-you have to go to redding,sacramento or bay area to fly out - with what rents are going for who can afford a thousand a month rent on minimal wage??i refuse to step foot in walmart several of us from work take roadtrips to roseville & shop whole foods,sprouts and other stores that have thing like sophies vegan seafood & other vegetarian foods that stores up here do not have whole foods has had all of their products the last 2 years. do not forget chico was one of the first communities to claim bankruptcy with a high cost of living & low wage jobs & its going down that road again thank you

Attachment P

Mike Sawley

From: Wufoo <no-reply@wufoo.com>
Sent: Friday, September 09, 2016 7:52 PM
To: Zoning
Subject: Chico [#3969]

Name *	Patricia McHenry
Email *	mchenry.dolores@gmail.com
Phone Number	(530) 345-5537
Department:	Planning Services Department
Comments	<p>I HOPE SUPER WAL-MART COMES TO CHICO. IT'S ABOUT TIME. I WOULD LOVE TO VOTE IN THE NOV.ELECTION.</p> <p>FOR THE UP COMING SEATS FOR THE CITY OF CHICO. BUT LIVI G IN DURHAM, I CAN'T. THE CITY OF CHICO IS NOT BUSINESS FAMILY. WILLOWS OR ORVILLE ISN'T THAT FAR. ALL CHICO DOES IS CATER TO COLLEGE KIDS.</p> <p>ANY AND ALL MOM AND POP STORES LEFT IN THE FIFTIES. THEY WERE CALLED SUPER MARKETS.</p> <p>I DO HOPE SUPER WAL-MART COMES TO CHICO. IT'S LONG OVER DUE.</p>

Mike Sawley

From: Wufoo <no-reply@wufoo.com>
Sent: Friday, September 09, 2016 7:52 PM
To: Zoning
Subject: Chico [#3969]

Name *	Patricia McHenry
Email *	mchenry.dolores@gmail.com
Phone Number	(530) 345-5537
Department:	Planning Services Department
Comments	<p>I HOPE SUPER WAL-MART COMES TO CHICO. IT'S ABOUT TIME. I WOULD LOVE TO VOTE IN THE NOV.ELECTION.</p> <p>FOR THE UP COMING SEATS FOR THE CITY OF CHICO. BUT LIVI G IN DURHAM, I CAN'T. THE CITY OF CHICO IS NOT BUSINESS FAMILY. WILLOWS OR ORVILLE ISN'T THAT FAR. ALL CHICO DOES IS CATER TO COLLEGE KIDS.</p> <p>ANY AND ALL MOM AND POP STORES LEFT IN THE FIFTIES. THEY WERE CALLED SUPER MARKETS.</p> <p>I DO HOPE SUPER WAL-MART COMES TO CHICO. IT'S LONG OVER DUE.</p>

Attachment P

Mike Sawley

From: Charles & Sally Withuhn <cswithuhn@yahoo.com>
Sent: Wednesday, September 14, 2016 9:19 AM
To: Mike Sawley
Subject: Wal Mart

Hi Mike,

We are concerned about the lack of compliance and lack of City Code enforcement in respect to the parking lot shade requirement that is lacking in the current Wal Mart parking lot. Now Wal Mart wants City cooperation with their new plans and they have not cooperated with the City or the City code or the citizen's need for shade in their previous plans.

You know what Chico would be like without trees. We feel this is a pivotal issue.
Charles Withuhn, BEC/Chico Tree Advocates
530-518-1417

Attachment P

Mike Sawley

From: G S <shuttershultz@gmail.com>
Sent: Wednesday, September 14, 2016 3:29 PM
To: Mike Sawley
Subject: Re: Chico Walmart Expansion Project - Notice of Availability of a Final Environmental Impact Report
Attachments: FEIR distribution memo.pdf

Mike:

The very fact that they are getting the signage ready indicates they feel they have once again won. They will never give up. You cannot change the environment and expect a positive outcome, unless it is to save lives. This entire project is so unnecessary. I am so disappointed at all of the building going on in Chico. Putting the houses so close to each other is not even healthy. Yes, I know it brings money, so never mind the environment, crime, pollution, traffic, etc. How sad, so much for the small town feeling and beauty it was so noted for.

Gail

On Fri, Sep 9, 2016 at 12:05 PM, Mike Sawley <mike.sawley@chicoca.gov> wrote:

Dear Interested Parties, attached please find a Notice of Availability for the Final Environmental Impact Report (Final EIR) prepared for the Chico Walmart Expansion Project. The Final and Draft EIR and supporting appendices are available at [this link](#).

Hard copies of the document will be forward to each Planning Commission member and City Council member. Members: please retain your Draft and Final EIR documents as they will only be incorporated by reference in future staff reports.

The project is currently scheduled to go before the Chico Planning Commission on October 6, 2016, at which time the Commission will consider certification of the EIR as well as the requested project entitlements. Please note that the Planned Development Permit for this project (PDP 15-01), which only addresses project signage, has been withdrawn. The applicant is refining the sign package, which will have to undergo separate approval at a future date.

Feel free to contact me at my direct line below, or via email if you have any questions or comments.

Thank You,

Mike Sawley, AICP

Attachment P

Associate Planner

City of Chico Community Development Dept.

P.O. Box 3420, Chico, CA 95927

(530) 879-6812



<http://www.ci.chico.ca.us/>

<http://chico.facilitiesmap.com/>

Mike Sawley

From: G S <shuttershultz@gmail.com>
Sent: Wednesday, September 14, 2016 3:44 PM
To: Mike Sawley
Subject: Fwd: Podcast 034 | Utah | The Minimalists

Perhaps this web site says it all perfectly - enjoy

----- Forwarded message -----

From: **The Minimalists** <noreply+feedproxy@google.com>

Date: Tue, Sep 13, 2016 at 5:17 AM

Subject: Podcast 034 | Utah | The Minimalists

To: shuttershultz@gmail.com

Podcast 034 | Utah | The Minimalists

Podcast 034 | Utah

Posted: 13 Sep 2016 03:00 AM PDT

By [Joshua Fields Millburn & Ryan Nicodemus](#) · Follow: [Twitter](#), [Facebook](#), [Instagram](#)



In this episode of *The Minimalists Podcast*, Joshua Fields Millburn & Ryan Nicodemus answer questions in front of a sold-out crowd in Salt Lake City, Utah, including:

- What is your relationship with everything that remained after your purge of most of your physical belongings?
- How does minimalism apply to life beyond physical belongings?
- Do you feel pressure to be role models of minimalism, or is it just part of who you are?

Attachment P

- How do we help our kids adopt minimalist practices when they're so entrenched in a compulsory consumption society?
- How do we explain to loved ones that we appreciate items they've given to us after we've donated the items when they're no longer useful to us?
- What are the best methods for storing digital files?
- Will you share more personal insights into your lives as minimalists in future content?

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<https://soundcloud.com/theminimalists/p034>

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Mentioned in This Episode

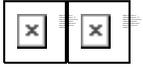
- Book: *[Essential](#)*
- Book: *[Everything That Remains](#)*
- Book: *[Minimalism: Live a Meaningful Life](#)*
- Essay: [Minimalism for Families with Children](#)
- Essay: [Packing Party](#)
- Essay: [Photo-Scanning Party](#)
- Essay: [The Things We Are Prepared to Walk Away From](#)
- Meetup: [Minimalist.org](#)
- Music: [Parlor Hawk](#)
- Own: [Minimalism Documentary](#)
- Recommendation: [Bluehost](#)
- Recommendation: [Doxie Scanners](#)
- Soundtrack: [Minimalism Documentary](#)
- Watch: [Minimalism Documentary](#)

Our sincere thanks to all the folks at [Megaplex Theatres at The Gateway](#) in Salt Lake City for graciously hosting us and for helping us capture this podcast episode. This episode was produced by [Shawn Harding](#). Our theme music was written and performed by [Peter Doran](#). Our podcasts are completely free, so if you found value in this episode, and you'd like to help us produce more, please consider [donating](#) a dollar to The Minimalists. Your donations help keep this podcast 100% advertisement-free (because advertisements suck).

If you'd like to comment on the podcast, you can leave a review on [iTunes](#). Not only do we read every review, but your positive, creative reviews also help our simple-living message reach more ears.

If you have a question for The Minimalists you'd like us to answer on our podcast, please leave a message on our voicemail at [406-219-7839](tel:406-219-7839). We'll also answer some questions from [Facebook](#), [Instagram](#), and [Twitter](#) on our podcast: [#AskTheMinimalists](#).

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Attachment P

Mike Sawley

From: Lanette Henderson <lanetteddy60@hotmail.com>
Sent: Saturday, September 17, 2016 12:21 PM
To: Mike Sawley
Subject: Re: Chico Walmart Expansion Project - Notice of Availability of a Final Environmental Impact Report

Thank you for the information.

Please try not to be too picky about some of these environmental items. People seem to work things out when things get crowded...they pick different times to shop or use the area. Have a bit of faith in the people. The New WalMart can be a big help, too.

When all the new restaurants went in along the freeway, it was crowded at first, but you can see that they have slacked off a bit. Just certain times of the year are they really busy. Give WalMart a chance...it could go good or bad for them. Then, they will make adjustments accordingly.

I think we need more work on keeping the people from littering all over Chico...especially on the bike paths...horrible litter! Bus stops filthy. Roads are cracked in long crooked lines, trees are dead in many neighborhoods and the branches hang out in the streets...come winter our environment will be dangerous. Areas by the creeks (dry ones esp.) are messy with debris. The Esplanade side roads are bumpy, lumpy, unslightly and dangerous for bikes, walking and even for cars. The billboards are an environmental distraction for drivers of vehicles and should be taken down. They get in the way of wires and trees. We have enough clutter in the air with all the signal lights, let alone the billboards.

Just a few observations.

Lanette Henderson

From: Mike Sawley <mike.sawley@Chicoca.gov>
Sent: Monday, September 12, 2016 11:28 AM
To: Mike Sawley
Subject: FW: Chico Walmart Expansion Project - Notice of Availability of a Final Environmental Impact Report

Dear Interested Parties, attached please find a Notice of Availability for the Final Environmental Impact Report (Final EIR) prepared for the Chico Walmart Expansion Project. The Final and Draft EIR and supporting appendices are available at [this link](#).

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Feel free to contact me at my direct line below, or via email if you have any questions or comments.

Thank You,

Mike Sawley, AICP
Associate Planner

Attachment P

Stina Cooley

From: Judith Keen <jkeen2193@sbcglobal.net>
Sent: Tuesday, September 20, 2016 4:03 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, Please approve the expansion of Wal-Mart we need the jobs and the savings. Judith Keen

Stina Cooley

From: Randi Rovetto <bjdjr1939@yahoo.com>
Sent: Tuesday, September 20, 2016 4:11 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,
please let the expansion of the Chico Wal-Mart be approved. we need that project to pass for the people of Chico.

Stina Cooley

From: Laurie Gardner <ljgcountry@hotmail.com>
Sent: Tuesday, September 20, 2016 4:14 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,
Let them build! We need them.
Laurie Gardner

Sent from my Sprint Samsung Galaxy S® 6.

Stina Cooley

From: Wanda Crowe <mamacrowe71@gmail.com>
Sent: Tuesday, September 20, 2016 4:17 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Please vote yes on expanding Walmart. They are building a super Walmart in Oroville. If you don't approve the Walmart here in chico I know several people who will be shopping in Oroville.

Stina Cooley

From: lamplighter14159@gmail.com
Sent: Tuesday, September 20, 2016 4:18 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,
We need a superstore in Chico

Sent from my iPhone

Stina Cooley

From: Abby Winn <moonlitpetals@gmail.com>
Sent: Tuesday, September 20, 2016 4:21 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Please help provide this town with a truly one stop shop. Having a full grocery in Walmart when bring so many people there, would be so helpful to everyone, but especially people like me who are single parents with children who can only stand one store, or college kids who just want to go to one place to get everything they need, so they can hurry up and go back home to study.

I was born and raised in this town and I have seen it expand more then I ever thought it would...I have also moved around the country and come back here. I am amazed that we haven't gotten with the times and expanded our walmart.

Please lets have a different outcome this time and Get a better Walmart!!

Abby Winn

Stina Cooley

From: Karen Mosier <rmkm75@att.net>
Sent: Tuesday, September 20, 2016 4:24 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, Im in full support of the Walmart project. It's disappointing to think that much smaller populated cities (12,000)residents have such a wonderful store like the one we are trying to get when we have over 85,000 residents with a greater need.

Thank you

Sent from my iPhone

Stina Cooley

From: revmichaeldracul <revmichaeldracul@gmail.com>
Sent: Tuesday, September 20, 2016 4:25 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, I support Walmart and thier Expansion Plans, This is America and Capitalism is how it operates, If Businesses can't lower their prices to a reasonable rate then it's time for them to fold and quit being Greedy and raising thier prices to ungodly rates.I have personally witnessed Local Businesses Purchase at Retail and Sell Retail Doubled! There are People that are poor and can't afford rediculouly inflated prices so they do without. It's Walmarts Land and Property they should be able to do with thier own Business as they wish! America is losing its Freedom's, There needs to be a change on this or We will see our Country go down the tube and not be able to recover just because of Political and Business Greed!

Stina Cooley

From: William Vandygriff <izansdaddy@gmail.com>
Sent: Tuesday, September 20, 2016 4:44 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I ask that you approve the Wal-Mart expansion plan. This will bring more jobs to Chico, which we all know is of great necessity. Thank you for your time.

Stina Cooley

From: Linda McCormick <mccpaperpeddler@aol.com>
Sent: Tuesday, September 20, 2016 4:59 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Unfortunately my husband and I are out of town until October 17th and we will be unable to attend the planning commission meeting. WE live in paradise but we are weekly visitors to Wal-Mart, winco and costco. THE other two stores have had expansions approved and we would like to recommend that Walmart is also afforded that chance. Thank you. Rick and Linda McCormick

Stina Cooley

From: Dennis Weinreich <bestbirds2013@aol.com>
Sent: Tuesday, September 20, 2016 5:18 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

My wife and I are long time Chico residents..We have lived in Chico, since the 1940's.. I just wanted to say We are in favor of the new Walmart expansion here in Chico...

Sincerely,

Dennis & Connie Weinreich

Stina Cooley

From: Jim Lentz <steepleseven@gmail.com>
Sent: Tuesday, September 20, 2016 5:31 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, If you vote down the Walmart Project again, I'll take All my business shopping to Oroville. James Lentz

Stina Cooley

From: Kristine Candelaria <knmcandel@yahoo.com>
Sent: Tuesday, September 20, 2016 6:45 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, I completely approve the expansion of our existing Walmart store. I think it will be a great asset to our fast growing community. Kristine

Sent from my iPad

Stina Cooley

From: Bruce Snyder <snydersellshomes@gmail.com>
Sent: Tuesday, September 20, 2016 6:57 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Sorry we will be in Utah at the time of the meeting. But I still support the upgrade for Wall Mart. Thanks, Pat Snyder

Stina Cooley

From: Marco Tulio Jimenez <jimenez.marco43@yahoo.com>
Sent: Tuesday, September 20, 2016 7:59 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Sent from my iPhone

Stina Cooley

From: andiken1 <andiken1@aol.com>
Sent: Tuesday, September 20, 2016 8:03 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Please vote to allow Walmart to expand. My husband now shops at the Willows Walmart because the Chico store doesn't have what he wants for his lunches and while he is there, he picks up everything else we need. Chico is losing money from those tax dollars. I don't understand why you feel the need to stop this. Walmart supplies jobs and supports the community. They make many generous donations for fundraisers. Give them a break!

Sent from my Verizon, Samsung Galaxy smartphone

Stina Cooley

From: Carol Dennis <caroldennis5444@gmail.com>
Sent: Tuesday, September 20, 2016 11:02 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

We would very much like to see the new Wal Mart in Chico. The great discounts are always needed, we are retired and money doesn't go very far.

We live in Paradise and don't mind driving to Chico as there just isn't a store here that would match up to it.

Thank you

Jerry and Carol Dennis
1488 Bille Rd
Paradise, CA 95969

530-872-3423

Stina Cooley

From: MICHAEL T. HAMRICK <mh37198@gmail.com>
Sent: Wednesday, September 21, 2016 12:39 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,
I think the expansion of Wal-Mart would be great for our local economy.

Stina Cooley

From: Shirley Stoops <slstoops51@gmail.com>
Sent: Wednesday, September 21, 2016 12:41 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I very much want to see a Walmart Super Center in Chico. I love all the food products that you can find there. I like the one stop shopping experience. Don't take the lube bay away either. I love getting my oil changed there. I can get my eye exams and glasses and my meds.

If you don't approve it this time you are very prejudice and bias.

Sincerely,

Shirley Stoops

Stina Cooley

From: Shirley Stoops <slstoops51@gmail.com>
Sent: Wednesday, September 21, 2016 2:00 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I very much want to see a Walmart Super Center in Chico. I love all the food products that you can find there. I like the one stop shopping experience. Don't take the lube bay away either. I love getting my oil changed there. I can get my eye exams and glasses and my meds.

If you don't approve it this time you are very prejudice and bias.

Sincerely,

Shirley Stoops

Stina Cooley

From: Marilyn Herdeg <my9angels@outlook.com>
Sent: Wednesday, September 21, 2016 3:34 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

The people that are not in favor of this expansion has the right to tell us where to shop? They want us to buy expensive items at other stores where Wal-Mart sells items for less expensive. Most of this city is on a limited income and can't afford to buy at Raleys vs Wal-Mart. I once lived in Redding and they had a small area also, they expanded and the only time it was crowded was at Christmas just like other stores were very crowded. The other stores have such higher prices that people like me can't afford to go and buy a pair of jeans for \$50.00 but the same brand we can buy for \$20.00 at Wal-Mart. This isn't fair for me and for all the other people in this city to make us go without an item because we just can't afford it. Just like the Chico Mall, because they didn't expand or create a better food court, you don't have much business at the Mall, only at Christmas.

I was so devastated that you didn't vote for this expansion years ago, but no one knows how hard it is for disability people like myself to have to get in and out of a car to all the stores to buy your things. Where Wal-Mart I need to get out of my car once.

This is such a great idea for us and why you should approve this expansion. Just hiring and have more associates on the floor to help the customers. There are so many people that need jobs and this will help the city of Chico, just like Oroville, Redding, Anderson.

Have you check the people that are against this expansion if they shop at Wal-Mart? These people worry about the small business around. It didn't seem to impact the other cities that I have mentioned, but if they were in Wal-Mart and needed a hammer, you would buy one. If you were at another hardware store and needed a hammer, you wouldn't go all the way across town and get one, you would buy it at the store you were in. It won't impact other stores.

Stina Cooley

From: James McDaniel <bonnyjim@aol.com>
Sent: Wednesday, September 21, 2016 8:55 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, I hope you do read these pleas to approve the Wal-Mart expansion. We don't have a Wal-Mart in Paradise and depend upon the Chico one for most of our purchases. However, we do use the Yuba City and Marysville stores, at times, to be able to stock up on things we cannot get in the Chico store. This takes sales tax money out of our county whereas we could be spending it here if we had an expanded Wal-Mart. Believe me, there are things that Wal-Mart stores carry that no one else does.

Thank you, Bonny McDaniel

James McDaniel
bonnyjim@aol.com

Stina Cooley

From: ann knight <toniannie39@yahoo.com>
Sent: Wednesday, September 21, 2016 9:44 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,
Please consider letting Walmart to expand. Many seniors citizens need this store.
Thank You

Stina Cooley

From: Jeff Robinson <robinson@theskybeam.com>
Sent: Wednesday, September 21, 2016 8:45 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission:

Granting Walmart permission to expand is the right thing to do. The other "chain" stores in town were allowed to expand ie: Costco and Winco. Walmart fills a need for many shoppers and their existence does not detract from all the other shopping destinations.

I would like to see the planning commission and the city council treat Walmart fairly like they have the other businesses who want to improve.

Jeff and Cheryl Robinson



Virus-free. www.avast.com

Stina Cooley

From: Me <bandme1393@comcast.net>
Sent: Wednesday, September 21, 2016 11:28 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I am writing to let you know how excited I am to hear another chance is given to Walmart to expand. My husband and I have lived in this area for almost 40 years now. We appreciate ALL the vendors in the area. Although we cannot always afford to shop at the downtown stores we do appreciate them. We don't believe expanding the Chico Walmart will effect the local vendors. We that shop in the area do our best (I think) to spread the "wealth". Not everyone is of the same income and we think Walmart is a great value. We also think about there being more jobs generated. SO needed in this area.

Please think (when you vote for or against) about the community AS A WHOLE. Our needs are many and varied. PLEASE VOTE YES ON EXPANDING OUR WALMART. Thank you.

Sincerely, Bob & Kathy Michael

Sent from my iPhone

Stina Cooley

From: shirley <shirley_2_1@yahoo.com>
Sent: Wednesday, September 21, 2016 12:28 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

I support the new Walmart.

SHIRLEY CARLQUIST
485 1/2 E 8th Ave
Chico, Ca 95926

Stina Cooley

From: Armando Lopez <armandolopez4352@sbcglobal.net>
Sent: Wednesday, September 21, 2016 3:04 PM
To: Stina Cooley
Cc: Armando Lopez
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, vote yes to remodel Walmart.

Stina Cooley

From: Dale Burrow <dburro@pacbell.net>
Sent: Wednesday, September 21, 2016 10:28 PM
To: Stina Cooley; us7-9fcd541bd-17e7011079@conversation01.mailchimpapp.com
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Forest Ave is commercial from Springfield Dr to Skyway accommodating hundreds of businesses, offices and thousands of customers daily. What possible effect can Walmart expanding by a few thousand square feet have on the environment? My bet would be none, but it sure riles politicians and busy-bodies who want to control other peoples' business. If Walmart succeeds in taking some business from other stores it will be because their customers make that decision because they are more satisfied for whatever reason. We don't actually need government interfering in our choices.

Let Walmart make their own business decisions regarding expansion and confine your activities to something useful.

Respectfully, Dale Burrow, Chico, CA

Stina Cooley

From: Jessica Arevalo <Rebeljessie09@yahoo.com>
Sent: Friday, September 23, 2016 4:10 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Sent from my iPhone

Stina Cooley

From: L August Rockwell <augrockwell@hotmail.com>
Sent: Saturday, September 24, 2016 9:00 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Planning Commission, I've been coming to Walmart for probably 25 years I'm old and it helps if I can buy everything at Walmart instead of running around to 4 or 5 different stores I definitely support Walmart their prices are wonderful they keep people working and the prices down we need more people that don't charge you an arm and a leg for something that it isn't worth it just so they can make a living that's why I like Walmart is because they care about the people I definitely support Walmart I just can't come there I have a sick husband

Stina Cooley

From: Dennis Weinreich <bestbirds2013@aol.com>
Sent: Saturday, September 24, 2016 11:08 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

My wife and myself approve for the expansion of Walmart, here in Chico, California

Sincerely,

Dennis & Connie Weinreich

Stina Cooley

From: Fredric Hawkins <tedhawk@att.net>
Sent: Saturday, September 24, 2016 11:23 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Support Walmart project
Sent from my iPhone

Stina Cooley

From: Carol Kaiser <kaiserhausen@yahoo.com>
Sent: Saturday, September 24, 2016 11:36 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Please allow Chico to benefit from a newly remodeled and updated Walmart store!! These new stores have more organic choices, better gluten free sections and updated delis for healthy lunch choices. I drive all over the United States and these new stores are welcomed by communities just like ours. I also feel strongly that we are big enough that the nearby Winco and Costco will still flourish with their loyal customers. I always like choice in my life and feel a little competition is healthy and can bring out the best in retail. Thank you for listening. I have lived in Chico for 25 years now and love this town for its charm and character but also its willingness to stay modern and up to date.

Sent from my iPhone

Stina Cooley

From: gardnerpatricia322 <gardnerpatricia322@gmail.com>
Sent: Saturday, September 24, 2016 11:42 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I sincerely hope that you will green light the project for the Super Walmart. I believe it will be a great addition for Chico and provide more employment that is desperately needed and pump more money into our local economy. Thanks for considering my thoughts on the subject.

Patty Gardner

Sent from my Sprint Samsung Galaxy Note5.

Stina Cooley

From: Philip A Roberts <philiparoberts@sbcglobal.net>
Sent: Saturday, September 24, 2016 11:46 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I believe it is time to let Walmart proceed with their project for a Super Walmart. There time has come and they will bring jobs and more sales taxes for the city coffer. Having a major retailer to shop at that has a full range of products as Walmart does makes it easier for families both from an economic and convenience stand point. Perhaps there will be some retailers negatively affected. However, those businesses must adjust to the times and they can still be productive and profitable if they make the effort to do so.

Philip Roberts
213 Denali Dr.
Chico, CA 95973

Stina Cooley

From: Michelle Jackson <rancho4335@icloud.com>
Sent: Saturday, September 24, 2016 12:25 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I live in Chico and I would really appreciate it if the planning commission would approve the WalMart Superstore proposal for expansion .

Thank you.

Michelle Jackson Kemp
4335 Rancho Road

Sent from my iPhone

Stina Cooley

From: Tuula Hall <TuulaHall@hotmail.com>
Sent: Saturday, September 24, 2016 12:29 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I support to expand the Walmart store.

tuula hall

Stina Cooley

From: kimberli titus <kktitus1224@gmail.com>
Sent: Saturday, September 24, 2016 1:35 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I truly believe that allowing Walmart to die its full expansion would be a big positive for the city of Chico. Not only providing more jobs, but somewhere to shop that is a one stop shop and low prices so people can afford great and healthy food. Also I know they do get a lot of fresh produce from local farms and also meat from local farmers.

Sent from my iPhone

Stina Cooley

From: Dorothy Charlton <charltondorothy@gmail.com>
Sent: Saturday, September 24, 2016 1:43 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission

I want the Chico Walmart to expand and remodel!!! We need this in our town.

Dorothy Charlton

Stina Cooley

From: marilyn porter <skippyporter@yahoo.com>
Sent: Saturday, September 24, 2016 2:12 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, I am a supporter of the planned Walmart expansion . I am on Social Security and would benefit on the approval of this expansion. I drive to either Yuba City or Anderson to shop in their super center because even with the gas I still save shopping in their store. I would love to keep my money in Chico and with this approval I would be able to do so..please consider approving this expansion. Thank you Marilyn Porter

Stina Cooley

From: Augie DePhillips <augiedep@yahoo.com>
Sent: Saturday, September 24, 2016 7:05 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

My wife and I are very much in support of having a supper Walmart in Chico. It making shopping much easier. We are senior citizens and a Supper Walmart saves us gas and the convenience of not having to go all over town. I strongly ask you to support this effort.

Sincerely
August and Sharon DePhillips
279 Rio Lindo Ave #20
95926

Stina Cooley

From: jj <scubastieb@yahoo.com>
Sent: Sunday, September 25, 2016 8:21 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Over the last many years we have seen, wal greens, McDonald's, cvs stores go up all over Chico yet wal mart along brings in more tax revenue to Chico and butte county than all those stores alone. Plus wal mart employees and donates more locally than all the others. City officials have ran Chico into the ground and should take lesson on how to build a. It's from, red bluff, Roseville, rocklin granite bay. Pretty bad when the bums have same rights as business owners and other tax paying residents.

Sent from my iPhone

Stina Cooley

From: RUTH HUTCHISON <randbhutchison@sbcglobal.net>
Sent: Sunday, September 25, 2016 2:25 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I would very much like to see a super Walmart built in Chico!!! With so many students and the rate of welfare, we could definitely benefit from having one.

Thank you.

Stina Cooley

From: donna chamberlain <donnadonna_c@yahoo.com>
Sent: Monday, September 26, 2016 1:15 PM
To: Stina Cooley
Subject: "Suppotr Walmar Project"

A lot of us in Chico would like to see a larger and new Walmart with a full grocery Dept. Please pass the new expansion of Chico Walmart. Thank You Donna Chamberlain

Stina Cooley

From: Jhschico <jhschico@aol.com>
Sent: Monday, September 26, 2016 3:07 PM
To: Stina Cooley
Subject: Support Walmart Project

My wife and I strongly support the Walmart expansion
John and Kay Innocenti

John's Handyman Service
236A West East Ave #350
Chico, CA 95926
530-345-7074

Stina Cooley

From: Jeri Cloyd <cloydnuts@yahoo.com>
Sent: Monday, September 26, 2016 3:44 PM
To: Stina Cooley
Subject: "support Walmart Project"

Very much in favor of the expansion program for Walmart, hopefully into a full Super Walmart. I feel it would have a positive effect on our community and give middle to lower income families the opportunity to shop as they can afford. I vote yes definitely on this project.

As for me and my house I will serve the Lord!
Jeri Cloyd

Stina Cooley

From: sutrorock-wm@yahoo.com
Sent: Monday, September 26, 2016 4:58 PM
To: Stina Cooley
Subject: Support Walmart Project

Please support the Walmart expansion project.

-- Steve and Kathy Schmidt

Stina Cooley

From: jerrapatrik <jerrapatrik@yahoo.com>
Sent: Monday, September 26, 2016 5:38 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

As an associate of the Chico walmart I know how much we need to have more groceries I hear it from customers all day long so please make our dreams of expansion come true

Sent from my Boost Mobile Phone.

Stina Cooley

From: stephen jones <stephenjones1220@gmail.com>
Sent: Monday, September 26, 2016 6:30 PM
To: Stina Cooley
Subject: Support Walmart project

Stina Cooley

From: Tana Jade <kcortez24@gmail.com>
Sent: Monday, September 26, 2016 7:01 PM
To: Stina Cooley
Subject: SUPPORT WALMART PROJECT

Stina Cooley

From: Diane Flaherty <dianefl@sbcglobal.net>
Sent: Monday, September 26, 2016 9:54 PM
To: Stina Cooley
Subject: Support Walmart Project

I feel Chico needs to have a "real Walmart"! One with a complete food market. I've spent winters in Yuma, AZ where there are 3 Super Walmarts and they're all successful.

It would make one stop shopping.

Go WalMart!!!! If Oroville can have one, why can't we?

Diane Flaherty

Sent from my iPhone

Stina Cooley

From: osdawesa@aol.com
Sent: Tuesday, September 27, 2016 7:13 AM
To: Stina Cooley
Subject: Support Walmart Project

We are in support of allowing Walmart to expand.
We miss the convenience of the one-stop shopping experience that Fred Meyer's provided and would appreciate having that available again.
It would be good for the *majority* of the citizens of Chico and for the city itself, gaining the extra revenue it would bring.

The Fannin Family

Stina Cooley

From: Teresa Moser <iluvtching@gmail.com>
Sent: Tuesday, September 27, 2016 9:02 AM
To: Stina Cooley
Subject: Support Walmart Project

Dear Ms. Cooley,

This is in response to the Planning Commission's upcoming October 6 meeting to approve or refuse the Chico Walmart superstore expansion.

Please register my email as in support of the planned expansion. My husband and I drive over to the Willows Walmart to take advantage of the grocery shopping in their superstore. The quality of the meat is superior, making it worth the trip.

However, it's time consuming to make a trip over there, and we have to plan ahead to keep perishables cold and allow a whole afternoon. We would much rather shop in a Chico Walmart superstore for convenience and saving time.

And, our grocery money would be spent in our own community. Like us, many people on reduced or limited incomes benefit by shopping at Walmart, making it possible to get their needs met affordably. Therefore, we would appreciate it if the Planning Commission would approve the expansion of the Chico Walmart into a superstore.

Thank you.

Sincerely,
Teresa Moser
150 Terrace Drive
Chico, CA 95926

Stina Cooley

From: Schmidt, Diane <DSchmidt@csuchico.edu>
Sent: Tuesday, September 27, 2016 9:28 AM
To: Stina Cooley
Subject: Support Walmart Project

Dear Ms. Cooley,

I support the Walmart expansion. I am not able to come to the meeting but have spoken before supporting the previous proposal for expansion. I am a person with disabilities and find that my access to goods is greatly expanded when visiting Super Walmarts in other communities. At this time, our Walmart is over-subscribed by customers so that it is crowded and often times they are out of goods that I can only find at Walmart because the shelf space is so limited. When visiting other communities I find that I can go to one store for most of my necessities. In Chico, I would have to go to multiple stores just to get the things I need. This creates a burden for me due to my disabilities. As a result, I often have to buy things on-line. I prefer to support my community by buying in Chico, but faced with the choice of getting what I need and not getting what I need either because Walmart sold out, or other stores failing to stock these products, I have to choose buying outside of Chico. An expanded Walmart serves the community broadly by providing real consumer choice, but serves people with disabilities directly by offering one-stop shopping and product variety important for managing those disabilities. Thank you. Diane

*Dr. Diane E. Schmidt
Professor, Policy Analyst, Ranking Faculty in Public Administration
California State University, Chico
Department of Political Science
Chico, CA 95929-0455
(530) 898-6176
(530) 898-6910, fax*

"Winning is getting up one more time than you are knocked down"

Stina Cooley

From: ridgewalker@sbcglobal.net
Sent: Tuesday, September 27, 2016 9:29 AM
To: Stina Cooley
Subject: Support Walmart Project

Stina Cooley

From: Dorinda <dbelcher67@gmail.com>
Sent: Tuesday, September 27, 2016 11:52 AM
To: Stina Cooley
Subject: Support Walmart Project

Sent from my iPhone

Stina Cooley

From: Genny Davis <mikegenny@sbcglobal.net>
Sent: Tuesday, September 27, 2016 1:04 PM
To: Stina Cooley
Subject: Support Walmart Project

Sent from my iPhone

Stina Cooley

From: William Kurdziel <kurdziel@mail.csuchico.edu>
Sent: Tuesday, September 27, 2016 1:09 PM
To: Stina Cooley
Subject: Support Walmart Project

Dear Ms. Cooley,

We are frequent shoppers at Walmart and admire the store's neatness, prices, and courteous employees. We miss not being able to buy groceries there in a store like the Super Walmarts we've visited in other cities. Those stores are beautiful, offer low prices for groceries, and provide good wages to many more employees.

PLEASE VOTE YES ON THIS COMMON SENSE PROJECT.

Kay Kurdziel
1752 Oro-Chico Hwy
Durham, CA 95938

Stina Cooley

From: Cindra Lacey <cindral@sbcglobal.net>
Sent: Tuesday, September 27, 2016 2:59 PM
To: Stina Cooley
Subject: support walmart project

Please include me and myself for supporting the expansion of the Walmart store in Chico, ca. It will not take away from any business in the area. It will only increase more ease in shopping in the original store. Increase tax benefits for Chico' coffers.

Stina Cooley

From: Peggy Ferris <peggy.ferris@yahoo.com>
Sent: Tuesday, September 27, 2016 4:44 PM
To: Stina Cooley
Subject: Support Walmart Project

Stina Cooley

From: Mike Dehnke <mcdehnke@att.net>
Sent: Tuesday, September 27, 2016 4:49 PM
To: Stina Cooley
Subject: Support Walmart project

Sent from my iPad

Stina Cooley

From: zookey lee <zookeylee@gmail.com>
Sent: Tuesday, September 27, 2016 5:09 PM
To: Stina Cooley
Subject: Please expand walmart

Chico needs this more Jobs for the low income. Please do this it would benefit this town. Thank You Jaime Barry

Stina Cooley

From: Judy Martinez <calmexwife@gmail.com>
Sent: Tuesday, September 27, 2016 5:21 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,
I support Walmart project.
Judy Martinez

Stina Cooley

From: Laurie Gardner <ljgcountry@hotmail.com>
Sent: Tuesday, September 27, 2016 5:24 PM
To: Stina Cooley
Subject: Support Walmart Project - October 6

Dear Planning Commission,

I am voicing my opinion. I think Chico can greatly benefit from the Walmart expansion! We need it here. The jobs it would create would be wonderful!

Thank you for your
Laurie Gardner

Stina Cooley

From: beatrice pardue <beeeyeepee@gmail.com>
Sent: Tuesday, September 27, 2016 5:24 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

We need Walmart to add on a grocery store. It was fantastic when Fred Meyers was in Chi o, I did ALL my shopping there. Now, once a year when on vacation in oregon, I do my shopping there

Please allow Walmart to expand. Thank you.

Stina Cooley

From: Mike Painter <md.painter@outlook.com>
Sent: Tuesday, September 27, 2016 5:28 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

As a senior citizen on a fixed income, I find that larger Walmart would be a valuable asset to me and others like me.

Why pay two to three times the price at another store?

Most people shop at supermarkets and other big box stores, they are no more "local" than Walmart.

Stina Cooley

From: gale golden <ggolden54@yahoo.com>
Sent: Tuesday, September 27, 2016 5:25 PM
To: Stina Cooley
Subject: In Support of the Wal-Mart Expansion Project

To Whom this Concerns:

I am in favor of the Wal-Mart expansion for Chico, CA. Thank you for your reconsideration Chico City Council.

Stina Cooley

From: Dillon_Clark1980 <Dillon_Clark1980@yahoo.com>
Sent: Tuesday, September 27, 2016 5:35 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I, Dillon Clark, am writing you to vote in favor of the Super-Wal-Mart petition.....for your consideration.

Thank you!

Sent from my Verizon, Samsung Galaxy smartphone

Stina Cooley

From: 2014djp.dp <2014djp.dp@gmail.com>
Sent: Tuesday, September 27, 2016 5:36 PM
To: Stina Cooley
Subject: support walmart project - oct 6th

dear planning commission,
I support the expansion...Sent from my Boost Mobile Phone.

Stina Cooley

From: Ralph Dacy <ralph.dacy@gmail.com>
Sent: Tuesday, September 27, 2016 5:41 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Please support Walmart's plan to remodel and expand our Chico store.

Regards,

Ralph & Victoria Dacy
6556 Coahsset Rd, Chico

--
Sent from Gmail Mobile

Stina Cooley

From: Lovelace, Kathy <LovelaceKa@butte.edu>
Sent: Tuesday, September 27, 2016 5:42 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I believe that the Walmart Project should be allowed and that it would be a great thing for the community. I think that the additional jobs and revenue to the community would be a supper benefit. I am in support of the Wal Mart Project one hundred percent. Thank You, Kathy Lovelace Butte County resident

Stina Cooley

From: Moria Favors <xraytechstudent@yahoo.com>
Sent: Tuesday, September 27, 2016 5:43 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

I believe the growth of the store will benefit it's customers greatly. It will provide more jobs and give the public more choices for their shopping needs.

Thank you,
Moria H Favors

Sent from my Verizon 4G LTE Droid

Stina Cooley

From: alicia herrbach <alinatsie@live.com>
Sent: Tuesday, September 27, 2016 5:44 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I'm not quite sure why you would not approve the expansion for walmart. Every place around is expanding which is providing more jobs. I just love how 1 person can reject an idea but doesn't struggle financially like most of the population. We need the expansion!!@

Sent via the Samsung Galaxy S7, an AT&T 4G LTE smartphone

Stina Cooley

From: joe Stricker <joestricker8@gmail.com>
Sent: Tuesday, September 27, 2016 5:57 PM
To: Stina Cooley
Subject: Deny Chico Walmart project

I am writing to voice my opinion against the Chico Walmart expansion project.

The added competition in the area of grocery stores will be a detriment to an already crowded market. I believe that there will be no added tax revenue related to the food sales.

Please vote to day the
Chico Walmart expansion and remodel.

Thanks,
Joe

Stina Cooley

From: Vyvette Mossman <auntievette@gmail.com>
Sent: Tuesday, September 27, 2016 5:59 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I am hoping that you will let Walmart expand this time without making them jump through all of your hoops to be denied in the end, again!

Vyvette Mossman

Stina Cooley

From: diane Sills <dsills@att.net>
Sent: Tuesday, September 27, 2016 6:01 PM
To: Stina Cooley
Subject: Support Walmart Project

Please approve the Walmart Expansion project. I thoroughly support this much needed expansion of Walmart in Chico.
I shop there often.

Thank you,
Diane Sills

Stina Cooley

From: Fredric Hawkins <tedhawk@att.net>
Sent: Tuesday, September 27, 2016 6:03 PM
To: Stina Cooley
Subject: Support Walmart Project - October 6

Dear Planning Commission,

Support Walmart project
Sent from my iPhone

Stina Cooley

From: Alan Schmidt <agschmidt.esq@gmail.com>
Sent: Tuesday, September 27, 2016 6:06 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Sent from my iPhone

Stina Cooley

From: Fred Lybolt <fredalicia3@yahoo.com>
Sent: Tuesday, September 27, 2016 6:11 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I live in Paradise and a very small self appointed group of radical's have denied 50,000 people living on the ridge a Walmart for shopping. We voted over 65 percent for Walmart but the politicians ignored the masses.

The tail wagging the dog again.

I now have to drive 20 miles to Chico to shop and get medicine.

Example again of poor government not representing the people.

Support Walmart and the majority of the people.

Lets get the dog wagging the tail and not a bunch of radicals.

Fred & Alicia Lybolt
13481 Adrian Dr
Magalia.
530 873 1517

Stina Cooley

From: Donna Stinchcomb <donnastinchcomb@hotmail.com>
Sent: Tuesday, September 27, 2016 6:51 PM
To: Stina Cooley
Subject: SUPPORT WALMART PROJECT

My husband Ron and I both support the Walmart project.

Thanks!
Donna

Stina Cooley

From: greenguy5383 <greenguy5383@yahoo.com>
Sent: Tuesday, September 27, 2016 6:54 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I would like to see Walmart have the remodel it is asking for. That is where I like shopping and I hate to go to more than one store to get what I need when I go shopping in Chico. I drive all the way from Magalia CA to go shopping and I have problems with making more stops than I need to.

Stina Cooley

From: Dominic Devlin <dominicdevlin@icloud.com>
Sent: Tuesday, September 27, 2016 7:01 PM
To: Stina Cooley
Subject: Support Walmart Project - October 6

Dear Planning Commission, the Walmart is to small for a city this size no one can find Clothing are other stuff we needed and at Christmas there no where to park your car

Sent from my iPhone

Stina Cooley

From: revmichaeldracul <revmichaeldracul@gmail.com>
Sent: Tuesday, September 27, 2016 7:07 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, The Chico Walmart needs to be left alone if they wish to remodel it is their own business! If local Stores can't match their price then they need to quite being so damn greedy and realize we live in a Capitalistic Society lower thier prices and forget about it!

Stina Cooley

From: Christine Yeo <yeoch14@sbcglobal.net>
Sent: Tuesday, September 27, 2016 7:10 PM
To: Stina Cooley
Subject: Support Walmart Project

It is time that Chico grow up. I have lived in Chico for the last 14 years, during that time I've seen the population grow from 55K to approximately 85K yet the City Council refuses to acknowledge/accept that the city is no longer a little mom and pop bedroom community. The downtown is fine for an occasional shopping day, but it's time the Chico City Council realizes that change is inevitable. If the council does not start allowing change, the city will die. I, for one, am tired of the cronyism that sways the council's votes. The voices of only a few are heard by the city council and its time that you realize that just because we weren't born and raised here doesn't mean you should ignore us.

Stina Cooley

From: Katrina Brymer <kbrymer@mail.csuchico.edu>
Sent: Tuesday, September 27, 2016 7:16 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, I support the expansion of the Walmart store in Chico. This will bring more jobs to our community as well as give us more items to shop for. The drive through pharmacy is a welcome edition especially for many elderly in our community. I vote yes for this expansion.

Sincerely yours,
Katrina Brymer

Sent from my iPhone

Stina Cooley

From: James McDaniel <bonnyjim@aol.com>
Sent: Tuesday, September 27, 2016 7:17 PM
To: Stina Cooley
Subject: Need the Expanded Wal-Mart Store, Please!

Dear Planning Commission, Again, I urge you to approve the expansion of this store. All other cities have a larger Wal-Mart. Why do we have to go to Marysville or Yuba City to get items we can't find in Paradise or Chico? Believe me, we have looked (items for my small binder...none in Chico but both plastic sheets and paper in the Wal-Mart Super Centers, and, of course, no Blue Bunny Ice Cream in Chico).

James McDaniel
bonnyjim@aol.com

Stina Cooley

From: Jim Piluso <jgolfn@pacbell.net>
Sent: Tuesday, September 27, 2016 7:22 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Sent from my iPhone

Stina Cooley

From: Carol Nielsen <cal4moi78@yahoo.com>
Sent: Tuesday, September 27, 2016 7:21 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Somebody sure dropped the ball on not taking advantage of Wal Mart being built out on North Esplanade at the ratty looking golf course. I live in Nord and to go major shopping, I have a long way to go to shop. That was when it was first discussed!! Now, hundreds of homes and apartment buildings have been built and hundreds in the process. North Chico is exploding in new homes, What is up with you people who think you are so smart and ahead of everyone else in this community??? Is anyone awake down there??? Is anyone paying attention???

There is more to Chico than your end of town!!! Get it???

Of course there is no reason to stop expansion on the Wal Mart that now exists. Chico is growing like crazy and Wal Mart has a lot to offer. Please pay attention!

Carol Nielsen 13223 Taylor St. Chico, Ca. 95973 I would love to hear from you.

Stina Cooley

From: Jolene Keillor <jrskeillor@yahoo.com>
Sent: Tuesday, September 27, 2016 7:26 PM
To: Stina Cooley
Subject: Support Walmart Project - October 6

Dear Planning Commission,
please approve chico walmart expansion. We live in Paradise and i shop here almost daily. It's what i can afford to support my family. If they don't get the approval for expansion then we will be forced to shop at the walmart supper center in Oroville.

Sincerely,
Jolene Keillor

Sent from my iPhone

Stina Cooley

From: Londa Coy <londamcoy@gmail.com>
Sent: Tuesday, September 27, 2016 7:43 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I would like to encourage you to approve Walmarts expansion plan.

I feel like they have been singled out in not being allowed to expand in the past.

I love Costco, and glad they are here and we're allowed to build a new store and gas station, so why not Walmart?

Thank you for listening.

Londa Coy

Stina Cooley

From: Richard Caldwell <rdc4now@gmail.com>
Sent: Tuesday, September 27, 2016 7:54 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission, Let Walmart expand to save Chico low income families food expense!

Sent from my iPad

Stina Cooley

From: Keng Lee <hmongintv@yahoo.com>
Sent: Tuesday, September 27, 2016 8:05 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

As a mother of 4 small children, it is extremely time-consuming and a hassle to do grocery shopping and household errands. An expansion of Walmart would be extremely helpful to mothers like me. The only store that made me feel welcomed with kids is Walmart. Walmart is a family and kids-friendly zone. I support Walmart expansion, and I hope that the next time I do go out for errands, I can get it all done in one place. Pharmacy, grocery, clothing, and gosh so much more Walmart has to offer.

Ong Thao

Stina Cooley

From: Nicole Lawhun <pointsettanoel@yahoo.com>
Sent: Tuesday, September 27, 2016 8:41 PM
To: Stina Cooley
Subject: Support Walmart Project - October 6

Dear Planning Commission, I as a Mom and daughter wife and a Community leader need affordable options and Chico wallmart is that solution !

Sent from my iPhone

Stina Cooley

From: tom kitchen <kitchentom83@yahoo.com>
Sent: Tuesday, September 27, 2016 8:44 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

As a concerned Chico citizen, i'm asking you to support the WALMART extension .
Dear Planning Commission,

Stina Cooley

From: Alice Ensley <aliceensley@yahoo.com>
Sent: Tuesday, September 27, 2016 9:23 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,
Please support Chico Wal-Mart's plan to remodel.
Thank you for your consideration.
Gratefully,
Alice Ensley

Sent from my iPhone

Stina Cooley

From: Cathy Caldwell <cec467@gmail.com>
Sent: Tuesday, September 27, 2016 9:40 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

It is time that you support the expansion of Walmart!

Sent from my iPad

Stina Cooley

From: catlady417 Dykes <catlady417@hotmail.com>
Sent: Tuesday, September 27, 2016 9:54 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,
Please approve the proposed Walmart expansion. I believe it would help Chico by providing jobs and additional shopping opportunities.

Thank you for your consideration

Virginia Dykes

Sent from my iPad

Stina Cooley

From: Tisha <tleija2014@gmail.com>
Sent: Tuesday, September 27, 2016 10:22 PM
To: Stina Cooley
Subject: Support Walmart Project - October 6

Dear Planning Commission,

In so much support of the new Walmart expansion! More jobs and more options for a growing community is very needed

Stina Cooley

From: Vicki H Lott <vickihlott@icloud.com>
Sent: Tuesday, September 27, 2016 10:30 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Hi

Please be open to a bigger Walmart that will be such a boon to the shoppers, the tax base and the better stock of merchandise for all.

My husband and I loved the two big Walmarts that offered so much more in every way, including a big produce offering and so much more.

This was in Palm Desert & the city of Indio in southern calif We also had the pleasure of shopping at one of the bigger ones in Rapid City, South Dakota The current store here in Chico is staffed with excellent employees, but is cramped and very limited on what they can offer, size wise.

We are pleased to shop at Walmart due to location and good prices that are below other stores in the area Please be progressive and open minded to a new beginning of a store that is friendly and gives great service in a bigger, well lighted, beautiful store with twice as much space See the returns in the tax money that will aid the City of Chico in so many ways Vicki & Chancy Lott

Sent from my iPad

Stina Cooley

From: Sarah A <imacrazzylady@gmail.com>
Sent: Tuesday, September 27, 2016 10:31 PM
To: Stina Cooley
Subject: Support Walmart Project - October 6

Dear Planning Commission, please i beg of you to let the walmart expansion go. If you neglect to do so i think ppl will start finding out why costco can keep building freely in town without any issues... someone say pay offs? Or is it you just love unions over family run companies. Either way let walmart build. Ive been to several super walmarts and they r great. Fresh affordable healthier food than we get now. I want my child to grow up healthy in chico eithout having to pay top dollar prices at trader joes n such. Not to mention the jobs it will provide.

Stina Cooley

From: jen730@aol.com
Sent: Tuesday, September 27, 2016 10:42 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Please support Walmart's plan to remodel and expand the store. The people of Chico deserve to have a better community for the next generation. This expansion will only bring growth economically for this city. Please don't pass on this fantastic opportunity to help bring something great to our home.

Thank you.

Sent from my MetroPCS 4G LTE Android device

Stina Cooley

From: Earl Aagaard <emjaagaard@gmail.com>
Sent: Tuesday, September 27, 2016 10:43 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I'm writing to urge you (I'm pleading with you) to be fair to Walmart and support their plans to remodel and expand their current store.

Their lot is plenty big enough, it's in an appropriate location, the traffic and other side effects have been dealt with in a responsible manner, and what remains are opponents who argue in bad faith.

Please take a stand for the even-handed application of the laws, regulations, etc., where WHO is asking for support is less important than the FACTS of the case.

Support Walmart's plan to remodel and expand a store that I frequent two or three times/week.

Thanks very much,

Earl Aagaard

Stina Cooley

From: francesmcoulbourn@sbcglobal.net
Sent: Tuesday, September 27, 2016 11:16 PM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I live across from Walmart on Parkway Village Dr. I know that in the past the expansion of Walmart has been a big negative on your agenda. I am asking that you please rethink the expansion for the following reasons. Chico is a beautiful city with people in many socioeconomic conditions. There are different niches that support and shop in different areas of our city. Realizing that the downtown corridor of shops is or has been a priority for the council, many of our citizens cannot afford the prices of the specialty shops. Walmart, as you may notice always has a full parking lot. It is what most of your citizens can afford. An expansion would not hurt but help the economy, more citizens would have employment and perhaps the stores that are complaining about the expansion would offer competitive prices. That would help the citizens in the area. Please consider the expansion in a positive light. Thank you. Frances Coulbourn.

Stina Cooley

From: wanda story <dogs997@yahoo.com>
Sent: Tuesday, September 27, 2016 11:26 PM
To: Stina Cooley
Subject: Support Walmart Project - October 6

Dear Planning Commission, Please pass the EIR for Wal-Marts and allow them to expand. Thank you for your time. Wanda J Story

Stina Cooley

From: Sarah Plants <plantsfamily@sbcglobal.net>
Sent: Wednesday, September 28, 2016 12:30 AM
To: Stina Cooley
Subject: Support Walmart Project - October 6

Dear Planning Commission,

Please allow the walmart expansion. It will provide more stable jobs for the community. It will also provide affordable goods for those who want to shop there.

Stina Cooley

From: Frank and Sue Smith <suefrank4656@sbcglobal.net>
Sent: Wednesday, September 28, 2016 1:39 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

I believe an expanded Walmart will draw more shoppers into Chico and will not put anyone out of business. More shoppers means more sales tax collected and more money stays in Chico. The last Planning Commission on reviewing Walmart's first expansion plans objected to it because of so called "increased traffic congestion", yet approved Butte Collage's plans to open a Chico campus just right up the street from Walmart, and increased the traffic congestion even more. I supported the first application and I support this latest application for expansion.

Frank R. smith
Chico, CA

Stina Cooley

From: Jamie L. Tibbles <iluvpugs33@gmail.com>
Sent: Wednesday, September 28, 2016 1:54 AM
To: Stina Cooley
Subject: Support Walmart Project - October 6

Dear Planning Commission,
Please support walmarts plan of expanding the Chico store.

Thanks,
Jamie Tibbles

Sent from my iPhone

Stina Cooley

From: Angela Sousa <angiesousa@icloud.com>
Sent: Wednesday, September 28, 2016 6:50 AM
To: Stina Cooley
Subject: Support Walmart Project - Oct 6th

Dear Planning Commission,

Let them BUILD its ugly over their
Sent from my iPhone